

US Route 202 (Concord Pike)
Rocky Run Parkway to Whitby Drive
Pedestrian Access Route Assessment



URBAN
ENGINEERS

FINAL REPORT

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NEW CASTLE COUNTY, DELAWARE

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EXECUTIVE SUMMARY

Urban Engineers, Inc. prepared this study to evaluate the condition of Pedestrian Access Routes (PARs) along the US Route 202 corridor in New Castle County, Delaware, and develop recommendations for PAR-related improvements. Existing and recommended pedestrian facilities were assessed based on DelDOT's *Pedestrian Accessibility Standards for Facilities in the Public Right of Way (2018 PAS)*, dated February 2018, along with other relevant state standards and regulations. The **Evaluation of Existing Conditions** section includes an analysis of crash data; data collection to determine compliancy of existing sidewalks, curb ramps, and bus stops; inventories for transit facilities, traffic signals, and lighting; and an overall assessment of pedestrian network connectivity. The **Recommended Improvements** section includes facility recommendations to achieve a PAS-compliant pedestrian network and improve pedestrian safety and connectivity. Urban coordinated with DelDOT and DTC over the course of the study to gather data, review preliminary findings, and refine recommendations. Meeting minutes and background information provided by DelDOT and DTC are included as appendices to this report.

Evaluation of Existing Conditions

US 202, also known as Concord Pike, is a north-south principal arterial extending north from the City of Wilmington into Pennsylvania. The study area is located along the portion of US 202 in Talleyville and includes both directions of US 202 between the southern intersection of Rocky Run Parkway and Whitby Drive, Silverside Road from US 202 to Brookfield Lane, Mt. Lebanon Road from US 202 to the westernmost entrance to Woodley Park, and Garden of Eden Road from US 202 to the trailhead at Brandywine Park.

Adjacent Projects

Through Contract T201701401, DelDOT will be addressing commitments to compliance issues on past pavement rehabilitation projects across the state. This contract includes the redesign of 27 curb ramps at seven intersections in the study area, along with new push buttons where needed at the redesigned ramp locations. DelDOT's Development Coordination Section identified four developer projects in the study area including Concord Plaza on Silverside Road, Columbia Place on Garden of Eden Road, reconstruction of the Brandywine Branch YMCA on Mount Lebanon Road, and 3701 Concord Pike. Potential impacts and benefits from these projects were considered in the evaluation of existing facilities and proposed PAR improvements. Recommendations from this study will complement adjacent projects.

Roadway Characteristics, Land Use, and Crash Analysis

The 1.07-mile section of US 202 within the study area is a six-lane divided highway with eight signalized intersections and numerous un-signalized driveways. The posted speed limit is 40 MPH between Whitby Drive and Righter Parkway and 50 MPH north of Righter Parkway. The median for US 202 varies in width and composition along the corridor, from a narrow concrete median barrier to a wide median area containing commercial businesses. Silverside Road is a four-lane divided highway with a posted speed limit of 40 MPH, Garden of Eden Road is a two-lane roadway with a posted speed of 25 MPH, and Mount Lebanon Road is a two-lane roadway with a posted speed of 35 MPH.

Land uses along the US 202 corridor are primarily commercial with pockets of institutional, office, and civic uses. The northern portion of the study area contains several large-scale land uses that generate significant walking trips including the Brandywine Commons development, the Widener University Law School campus, the Delaware Corporate Center offices, and the Concord Square retail complex. Pedestrian activity generators in the central bifurcated section of US 202 include retail and fast food businesses, while the southern portion of the study area contains another large-scale shopping center – Concord Gallery – and smaller businesses on the east side of the road. Silverside Road, Mount Lebanon Road, and Garden of Eden Road contain a mix of commercial, residential, and institutional uses.

Crash summaries were obtained from DelDOT’s Crash Analysis Reporting System (CARS) for all crashes occurring in the study area between December 2012 and December 2017. A total of 572 crashes were reported during this 5-year period, resulting in 1 fatality and 72 personal injuries. To meet the needs of DelDOT’s safety audit and PAR programs, a 10-year period was used to evaluate bicycle/pedestrian crashes. Between December 2007 and December 2017, there were seven bicycle/pedestrian crashes that resulted in one fatality and four injuries. Three of the pedestrian crashes occurred on Silverside Road during dark conditions.

Sidewalk and Curb Ramp Inventory

The sidewalk network within the study area is largely continuous, although there are several notable gaps including a section along northbound US 202 near Rocky Run Boulevard and portions of Garden of Eden Road. Sidewalk segments were field-measured in January 2018 to determine compliancy related to width, cross-slope, running slope, and condition. Based on the 2018 PAS, the sidewalk inventory was characterized as follows:

| Category | Length | % Total |
|-------------------------------|---------------|-------------|
| Missing Sidewalk | 2,300 | 12% |
| Compliant Sidewalk | 10,700 | 54% |
| Non-Compliant Sidewalk | 4,700 | 24% |
| Sidewalk in Development | 1,900 | 10% |
| Total Sidewalk Network | 19,600 | 100% |

Curb ramps are present at all signalized crossing locations and most driveway crossings in the study area. Existing curb ramps were field-measured and analyzed for compliancy based on the 2018 PAS. Of the study area’s 151 curb ramps, 68 were found to be compliant while 52 were found to be non-compliant. The remaining 31 ramps were not evaluated as they will be reconstructed as part of the T201701401 Contract or the YMCA expansion.

Signalized Intersection, Transit, and Roadway Lighting Inventories

There are ten signalized intersections in the study area. Existing pedestrian facilities including signal heads, push buttons, and clearance timings were field-inventoried in January 2018 and analyzed for deficiencies based on the 2018 PAS and the Delaware Manual of Uniform Traffic Control Devices (DE MUTCD), Section 4E.08. All of the intersections exhibited multiple deficiencies, most of which relate to push buttons, pedestrian clearance intervals, and tripping hazards. Deficiencies at the intersections of

Silverside Road with Rose Avenue and Brookfield Lane are currently being addressed in conjunction with the Concord Plaza development.

The Delaware Transit Corporation (DTC) operates DART Routes 2 and 35 along US 202 with eighteen bus stops located in the study area. Each stop was field measured to determine compliancy related to width, length, running slope, and condition. Three of these stops provide a waiting pad, shelter, and bench while six stops feature a waiting pad or wide sidewalk that can serve as the waiting pad. The remaining stops do not have waiting areas or any other amenities.

Most of US 202 features roadway lighting, which is provided through a combination of DelDOT light standards and utility pole-mounted luminaires. Roadway lighting is not provided along Silverside Road, Garden of Eden Road, or Mount Lebanon Road. A photometric model of existing conditions identified deficient lighting conditions at most of the signalized intersections along US 202 along with several lighting gaps.

Recommended Improvements

The proposed PAR network plan integrates improvements to sidewalks, curb ramps, crosswalks, signalized intersections, bus stops, and roadway lighting systems to address deficiencies identified through the existing conditions evaluation. Recommendations were developed in accordance with state standards and regulations, including the 2018 PAS and the DE MUTCD.

Sidewalk and Curb Ramp Improvements

The proposed PAR improvements will establish a network of compliant sidewalks and curb ramps along US 202, Silverside Road, Mount Lebanon Road, and Garden of Eden Road. Sidewalk, curb ramp, and crosswalk-related improvements are quantified in the table below:

| Improvement Type (units) | Sidewalk (feet) | Curb Ramps (each) | Crosswalks (each) | Bus Stops (each) |
|-------------------------------------|----------------------------|------------------------------|------------------------------|-----------------------------|
| Proposed | 1,500 | 36 | 19 | 0 |
| Existing to be Reconstructed | 4,500 | 47 | 0 | 11 |
| Existing to Remain | 10,300 | 56 | 34 | 7 |
| Planned Project | 1,900 | 48 | 9 | 0 |
| Sidewalk Passing Area | 11 | N/A | N/A | N/A |
| Spot Repair | 20 | N/A | N/A | N/A |

The proposed PAR network includes 36 new curb ramps and approximately 1,500 feet of new sidewalk. The majority of new sidewalk is located in two areas: 600 feet along northbound US 202 near Rocky Run Parkway and 400 feet along the south side of Garden of Eden Road between US 202 and the planned Columbia Place development. Conceptual designs were developed at these two locations to evaluate alternatives and identify feasibility considerations related to right-of-way, drainage, and topography.

The proposed PAR network also includes reconstructing 48 curb ramps and approximately 3,300 feet of sidewalk to meet 2018 PAS standards. In addition, sidewalk passing areas are recommended for existing sidewalk segments that are compliant but less than 5 feet wide. The sidewalk plan includes a category for “spot repairs” to address isolated deficiencies located in stretches of sidewalk that are otherwise

compliant. Examples include vertical elevation differences, horizontal gaps, and obstructions. At commercial driveways where curb ramps are being added or reconstructed, the use of high-visibility crosswalks should be considered based on criteria established by DelDOT's Traffic Section.

[Pedestrian Improvements at Signalized Intersections](#)

Recommendations related to signal equipment and/or operations were organized into three main categories – pedestrian improvements at the US 202/Brandywine Boulevard intersection, new crosswalks at other signalized intersections, and maintenance items. The US 202/Brandywine Boulevard intersection is challenging for pedestrians due to frequent driveways, multiple curb barriers, sidewalk obstructions, and a lack of pedestrian crossing opportunities into the median area. A conceptual design was developed to improve pedestrian circulation and accessibility by expanding sidewalks, reducing curb radii, and providing protected crossings to access the median area.

New marked pedestrian crossings are recommended for missing legs at the US 202/Mt. Lebanon Road and US 202/Whitby Drive intersections to improve pedestrian access to activity generators. Results from a signal timing analysis shows that these new crossings can be accommodated within existing concurrent vehicle phases. Additional crosswalk locations that were included for consideration will need further evaluation during the design phase. A list of “quick fix” maintenance-level items related to push buttons, pedestrian signal timing, and crosswalk striping was developed over the course of this study. These items were addressed in the spring of 2018 under the direction of DelDOT's Traffic Section.

[Transit Recommendations](#)

Transit recommendations include reconstructing non-compliant sidewalk at three existing bus stops, adding compliant pads at five locations where none exist today, and relocating the southbound bus stop near Brandywine Boulevard closer to the intersection. Recommendations also include adding amenities at several bus stop locations and evaluating bus stop relocations at the signalized intersections of US 202 with Righter Parkway and Silverside Road. Coordination with DTC will be necessary early in the design phase to confirm the exact location and extent of these improvements.

[Roadway Lighting Recommendations](#)

Lighting recommendations include adding or modifying lighting at the signalized intersections of US 202 with Rocky Run Parkway, Brandywine Boulevard, Mount Lebanon Road, and Whitby Drive and addressing lighting gaps at three locations: southbound US 202 just north of Whitby Drive, southbound US 202 at Wawa/Fedex Driveway, and northbound US 202 to the south of Righter Parkway. For lighting along Silverside Road, the recommended next step is to evaluate the feasibility and cost of developing a continuous lighting system between US 202 and Brookfield Avenue. This would include performing a lighting warrant evaluation at the signalized intersections with Rose Avenue and Brookfield Lane. For the existing and proposed mid-block crossings on Garden of Eden Road, it is recommended that a study be conducted to evaluate the need for lighting at these locations.

INTRODUCTION

The purpose of this study was to evaluate the condition of Pedestrian Access Routes (PARs) and provide recommendations for PAR-related improvements along the US Route 202 (Concord Pike) corridor in New Castle County, Delaware. Existing and recommended pedestrian facilities were assessed based on DelDOT’s *Pedestrian Accessibility Standards for Facilities in the Public Right of Way (2018 PAS)*, dated February 2018, along with other relevant state standards and regulations. The existing conditions evaluation included an analysis of crash data; data collection to determine compliancy of existing sidewalks, curb ramps, and bus stops; inventories for transit facilities, traffic signals, and lighting; and an overall assessment of pedestrian network connectivity. The study includes recommendations to achieve a PAS-compliant pedestrian network and improve pedestrian safety and connectivity.

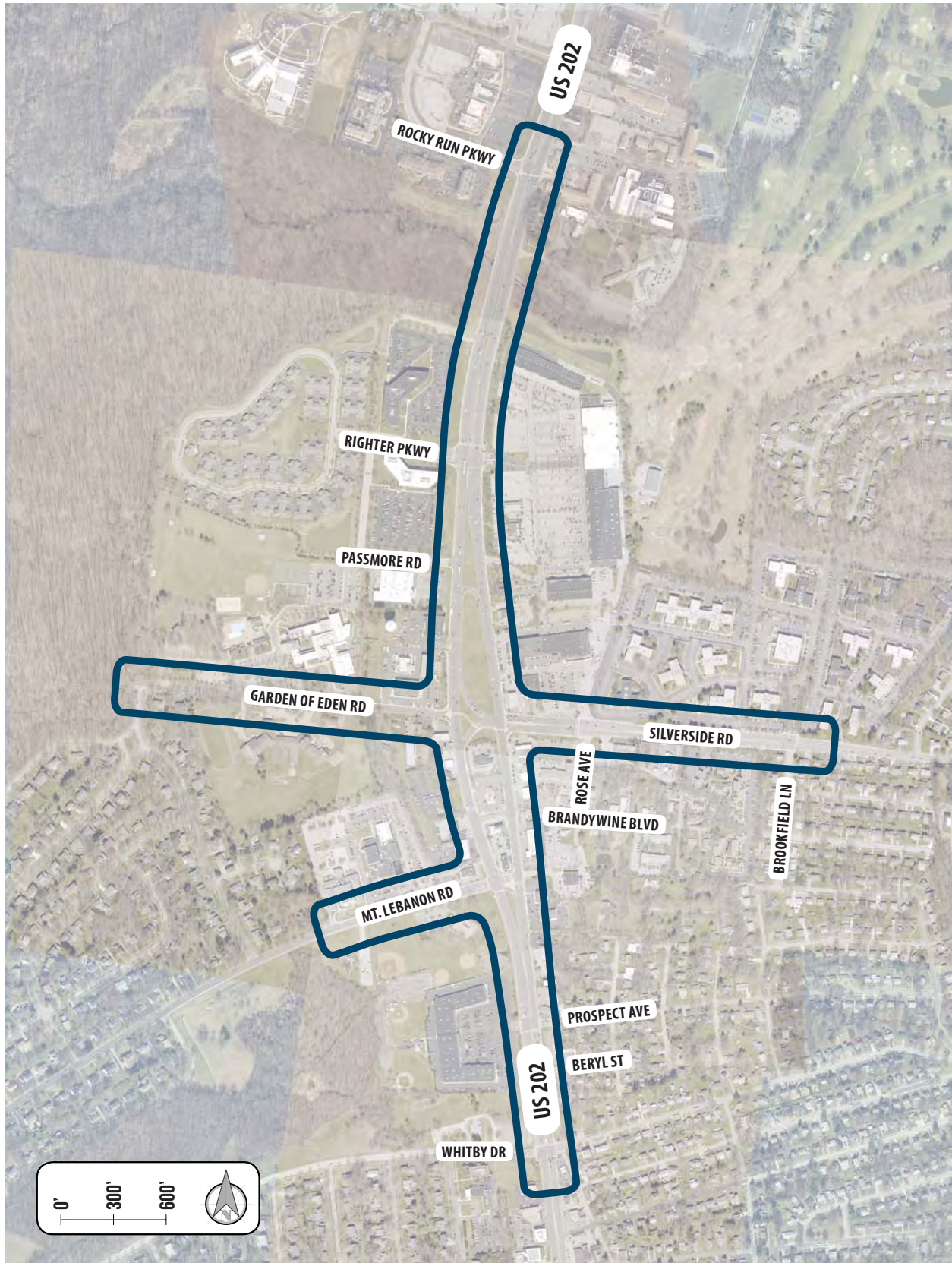
Study Area

Figure 1 shows the study area location, which is located along US 202 to the north of the City of Wilmington in Talleyville. The project limits include both directions of US 202 between the southern intersection of Rocky Run Parkway and Whitby Drive, Silverside Road from US 202 to Brookfield Lane, Mt. Lebanon Road from US 202 to the westernmost entrance to Woodley Park, and Garden of Eden Road from US 202 to the trailhead at Brandywine Park. This section of US 202 is 1.07 miles in length.



US 202 looking north from Prospect Ave

Figure 1 – Study Area Map



ADJACENT PROJECTS

DelDOT's Transportation Solutions and Development Coordination Sections were contacted to determine active DelDOT and developer projects in the study area. Relevant projects are shown in **Figure 2** and discussed in the following section. Available development plans obtained from DelDOT are included in **Appendix B**.

Potential impacts and benefits from adjacent projects were factored into the planning process when evaluating existing facilities and developing proposed PAR improvements. Recommendations from this study will complement these adjacent projects.

DelDOT Projects

Transportation Solutions confirmed one DelDOT-led roadway project within the study area:

- [Statewide Pavement & Rehabilitation Backlog Curb Ramp Design \(T201701401\)](#)
DelDOT has commitments to address compliance issues on past pavement rehabilitation projects across the state. As part of this effort, AECOM is under agreement with DelDOT (Contract T201701401) to redesign 27 new curb ramps at seven intersections in the study area. The project will provide new push buttons where needed at the redesigned ramp locations. Curb ramps that are being designed through Contract T201701401 were not inventoried as part of this study.

Developer Projects

Development Coordination confirmed the following developer projects that are currently underway within the study area (available plans are included in **Appendix B**):

- [Concord Plaza – Silverside Road](#)
This mixed-use redevelopment project will be located on three adjacent parcels on the north side of Silverside Road to the east of the Talleyville Shopping Center. The project will contain approximately 6,200 total square feet in four 5-story buildings and two 4-story buildings, with 341 apartments, office space, retail space, and other amenities including a fitness center, lounge, technology center, private meeting rooms, and swimming pool. The approved construction plans show a 5-foot wide sidewalk across the entire property frontage, new accessible curb ramps and high-visibility crosswalks across both of the site driveways, and a reconstructed curb ramp on the east side of the shopping center driveway. The plans show new lighting poles along the southern side of the internal parking lot, but do not provide any roadway lighting directly adjacent to Silverside Road. This project was under construction at the time of this study.



Concord Plaza under construction

In conjunction with the Concord Plaza development, the intersections along Silverside Road adjacent to the project at Rose Avenue and Brookfield Lane will be upgraded. Proposed signal plans were obtained from DeIDOT Development Coordination and are included in **Appendix B**. At both intersections, the plans call for reconstructing the curb ramps at each of the corners. These intersections were under construction at the time of this study.

- [Columbia Place – Garden of Eden Road](#)

This proposed residential development would replace the former Pilot School on the south side of Garden Of Eden Road with a mix of 149 residential units including single family homes, condominiums, carriage homes, and a clubhouse. The existing site has two driveways and is currently vacant. The approved construction plans show a 5-foot wide sidewalk across the entire property frontage with new curb ramps and a high-visibility crosswalk across one consolidated driveway. The plans also show two midblock crossings of Garden of Eden Road, including an existing crossing on the west end of the site and a proposed crossing on the east end of the site. The project will reconstruct the existing curb ramp on the south side of the existing mid-block crossing and provide new curb ramps at the proposed mid-block crossing. Construction on this project had not yet started at the time of this study.

- [Brandywine Branch YMCA](#)

Construction for a building expansion of the YMCA on the north side of Mount Lebanon Road began in the spring of 2017. The 16,000 square foot 2nd floor addition will house a youth wellness center, new and renovated classrooms for preschool, community education rooms, a larger fitness center, and a second fitness studio. The project will include new accessible curb ramps and crosswalks across both driveways, but will not reconstruct the existing sidewalk along the north side of Mount Lebanon Road. The project will also add an accessible curb ramp on the south side of the road to accommodate a new marked crosswalk across Mount Lebanon Road at the western, exit-only entrance. This project was under construction at the time of this study.

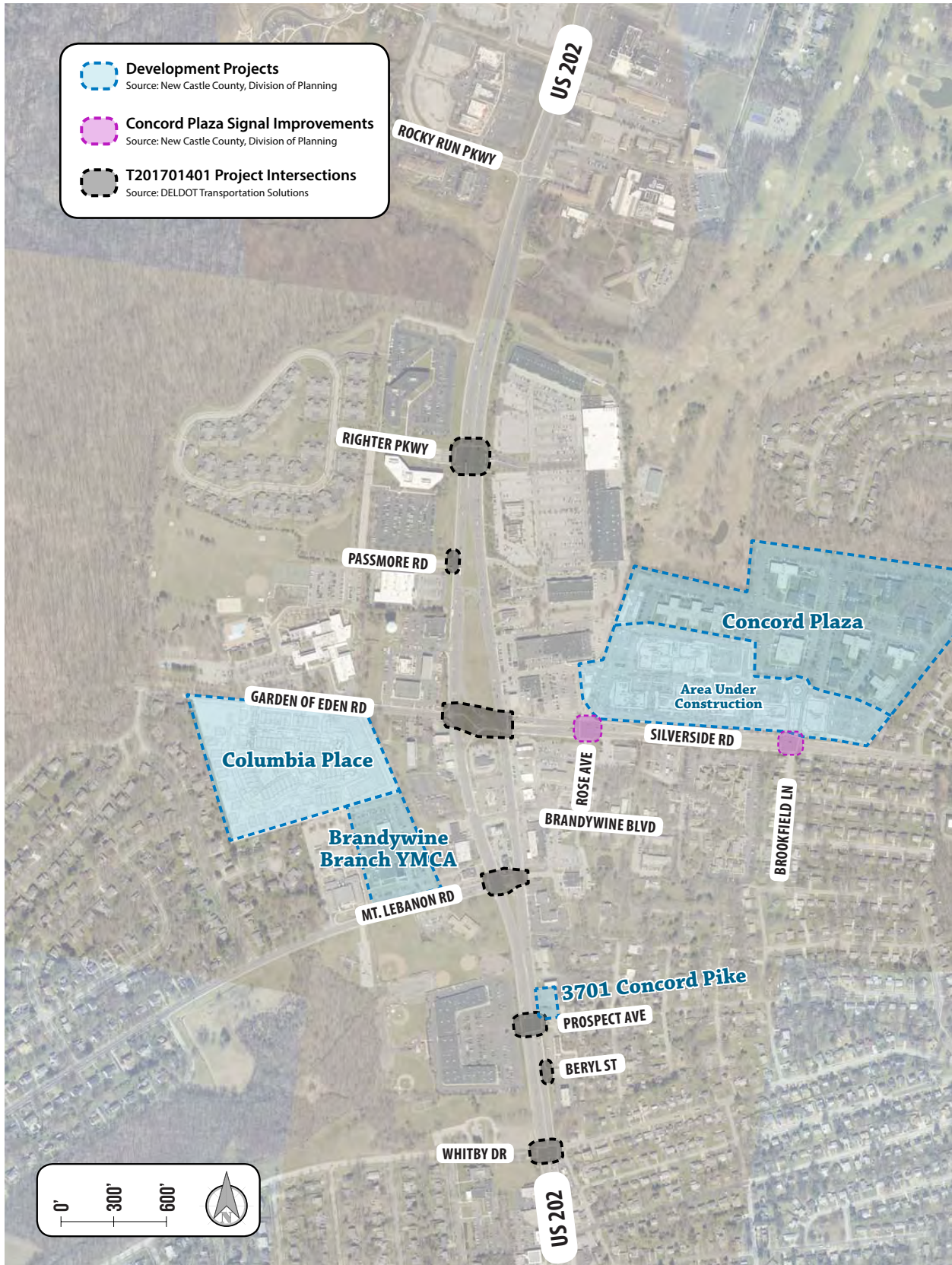


YMCA under construction

- [3701 Concord Pike](#)

Redevelopment plans for this property show an expanded parking lot and reconfiguration along the property frontage. The proposed site plans include reconstructing both curb ramps along US 202 and replacing sidewalk and curb ramps along Prospect Avenue. The project was under design review at the time of this study.

Figure 2 – Adjacent Projects



EVALUATION OF EXISTING CONDITIONS

US 202 (also known as Concord Pike) is a six-lane divided highway that is classified as a principal arterial. The 1.07-mile section of US 202 within the study area includes eight signalized intersections with numerous un-signalized driveways and entrances. The posted speed limit is 40 MPH between Whitby Drive and Righter Parkway and 50 MPH north of Righter Parkway. Per DelDOT's 2016 Traffic Summary, the annual average daily traffic (AADT) along this section of US 202 is 46,502 vehicles per day.

The median for US 202 varies in width and composition along the corridor. Proceeding north from Whitby Drive, US 202 has a concrete median that begins to widen as it approaches Mount Lebanon Road. Between Mount Lebanon Road and Silverside Road, the northbound and southbound lanes are bifurcated and split by a median area containing commercial businesses. North of Silverside Road, the wide grass median area tapers back to a standard concrete median at Righter Parkway.

Silverside Road from US 202 to Brookfield Lane is a four-lane divided highway with a concrete median and a posted speed limit of 40 MPH. Silverside Road has signalized intersections at US 202, Rose Ave, and Brookfield Lane. Garden of Eden Road from the Brandywine Creek State Park to US 202 is a two-lane roadway with a posted speed of 25 MPH. There is a designated school zone in front of the Albert Einstein School with a posted speed of 20 MPH. Mount Lebanon Road between US 202 and the westernmost Woodley Park entrance is a two-lane roadway with a posted speed of 35 MPH.

The sidewalk network within the study area is largely continuous, although there are several missing segments including a section along northbound US 202 near Rocky Run Boulevard and portions of Garden of Eden Road. Most of the existing sidewalk is located directly adjacent to the curb without a grass buffer. Drainage inlets with curb and gutter are present along all of US 202, Silverside Road, and Mount Lebanon Road and portions of Garden of Eden Road. The uncurbed portion of Garden of Eden Road features a drainage swale on the north side of the road.

There are no on-street bicycle facilities in the study area. Per the New Castle County Bicycle Map, US 202 is identified as a high traffic roadway that is challenging for cyclists, and therefore is not designated as a bicycle route. Shoulder widths along US 202 varies from 14 feet at the widest points to sections with no shoulder due to turning lanes at signalized intersections and major commercial driveways. This creates an inconsistent/unpredictable environment for cycling in the corridor. Silverside Road, which is listed as a Connector Bicycle Route, has marked bicycle lanes east of Brookfield Lane but no bicycle lanes or bicycle-compatible shoulders between US 202 and Brookfield Lane. The western end of Garden of Eden Road connects to a trail system that runs through Brandywine Creek State Park; however, there is currently no accessible route between the trail and US 202.

The Delaware Transit Corporation (DTC) operates DART Bus Routes 2 & 35 along US 202 and Silverside Road with 18 total bus stops in the study area. Both routes connect the study area with the Wilmington AMTRAK Station to the south and the Brandywine Town Center to the north. Route 2 operates along US 202, while Route 35 operates along the southern portion of US 202 and Silverside Road. Much of the pedestrian activity along US 202 and Silverside Road is generated by transit riders accessing local job and shopping opportunities.

Land Use

Existing land uses are shown in **Figure 3**. US 202 is primarily a commercial corridor with institutional and civic uses interspersed among both small and large-scale retail and office uses. Residential and recreational parcels are located in the neighborhoods adjacent to US 202, with a few single-family residences located along portions of Silverside Road and Garden of Eden Road.

The northern portion of the study area contains several large-scale land uses that generate significant walking trips. At Rocky Run Parkway, the Brandywine Commons development on the west side of US 202 contains two hotels and several chain restaurants, while the Widener University Law School campus is located on the east side of the road. To the south at Righter Parkway are the Delaware Corporate Center offices along SB US 202 and the Concord Square retail complex along NB US 202.



Widener University Law School

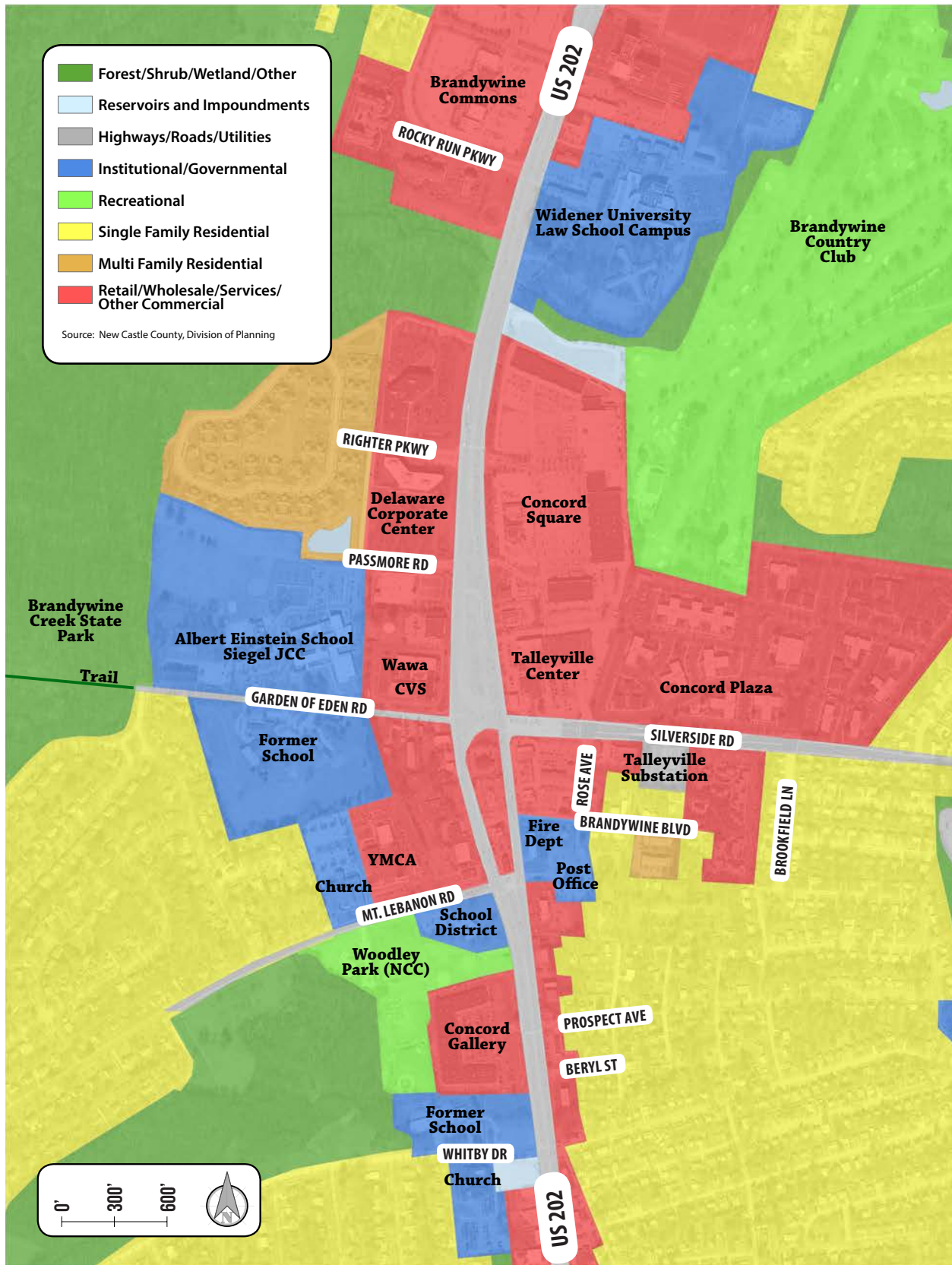
The central bifurcated section of US 202 contains a dense mix of land uses consisting primarily of retail and fast food businesses. Pedestrian activity generators in this area include the Wawa/CVS at the northwest corner of Garden of Eden Road – the TD Bank, Arby's, drycleaner, and Taco Bell in the median area – and the Memorial Hall-Talleyville Fire Company and Post Office on the east side of US 202 at Mt. Lebanon Road. The southern portion of the study area between Mount Lebanon Road and Whitby Drive contains another large-scale shopping center – Concord Gallery – on the west side of US 202 and smaller businesses on the east side of the road.

Silverside Road, Mount Lebanon Road, and Garden of Eden Road contain a mix of commercial, residential, and institutional uses that generate local walking and biking trips. The north side of Silverside Road contains Talleyville Center and the Concord Plaza redevelopment site, while the south side contains a mix of commercial and residential uses. The Albert Einstein School and Siegel Jewish Community Center are located on the north side of Garden of Eden Road, while the south side contains the now-demolished Pilot School along with several residential and commercial uses. The Brandywine Branch of the YMCA, the Brandywine Valley Baptist Church, and the County's Woodley Park form a cluster of civic uses along Mt. Lebanon Road. Planned development projects for Silverside Road and Garden of Eden Road are expected to generate additional pedestrian activity.



Walking/Biking Activity

Figure 3 – Existing Land Use Map



Crash Data

Crash summaries were obtained from DeIDOT’s Crash Analysis Reporting System (CARS) for all crashes occurring in the study area over a 5-year period (December 2012 to December 2017). These summaries are included in **Appendix C**. A total of 572 crashes were reported during this 5-year period, resulting in 1 fatality and 72 personal injuries. To meet the needs of DeIDOT’s safety audit and PAR programs, a 10-year period was used to evaluate bicycle/pedestrian crashes (December 2007 to December 2017). Over this period, there were four pedestrian crashes and three bicycle crashes that resulted in one fatality and four injuries. These crashes are summarized in **Table 1** and shown in **Figure 4**.

Vehicular crash groupings for the 5-year period are shown in **Figure 4**. Over 75% of the crashes occurred along US 202, with the highest groupings located at or near the Silverside Road and Mt. Lebanon Road intersections. Noticeable groupings were also present at the signalized intersections with Rocky Run Parkway, Righter Parkway, Prospect Avenue, and Whitby Drive. Smaller groupings were present at the signalized intersections on Silverside Road.

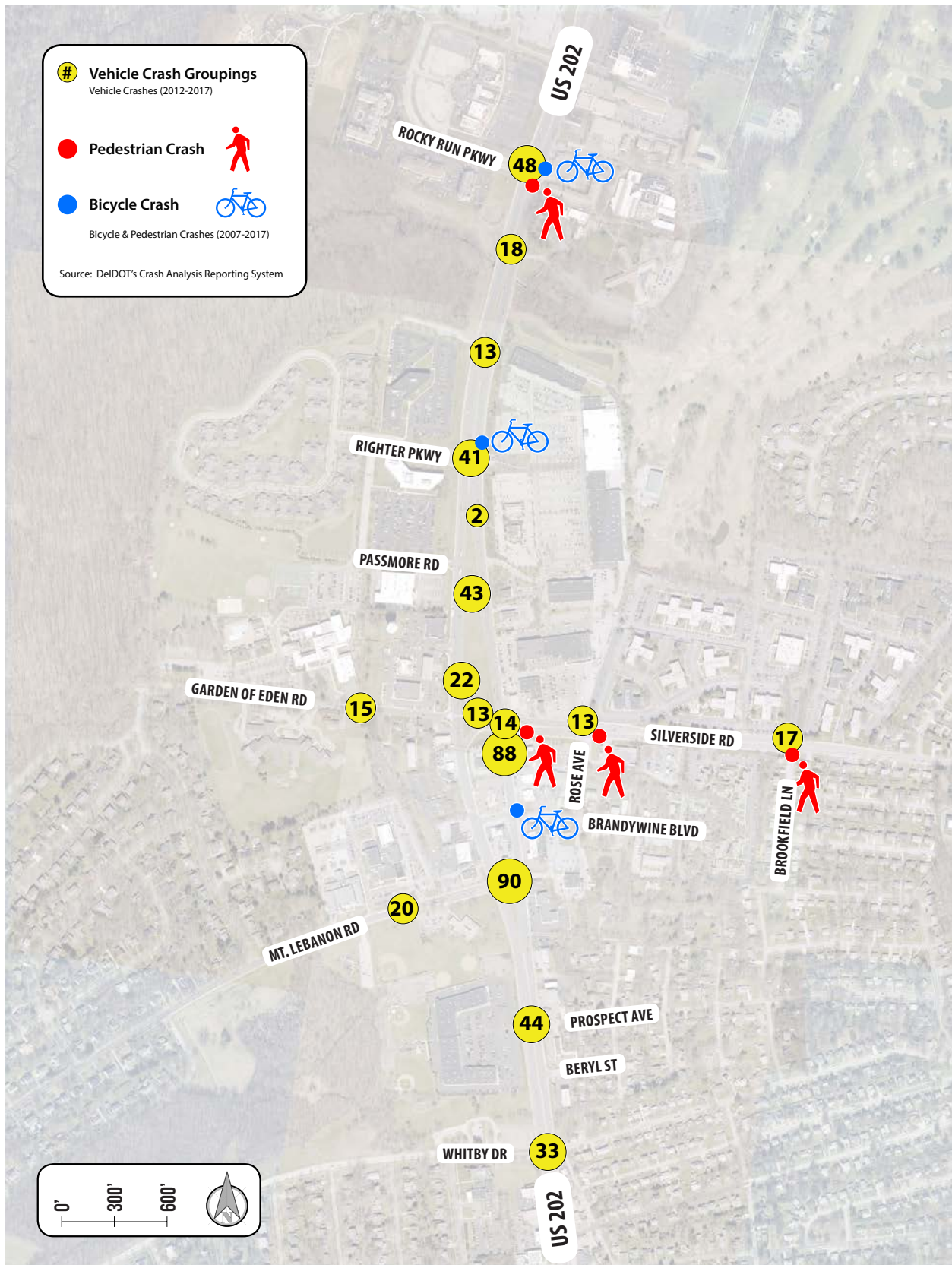
Environmental factors, manner of impact, and contributing circumstances are summarized in **Appendix C**. The vast majority of total crashes occurred in dry (81%) and daylight (76%) conditions, with alcohol being a factor in only 2% of crashes. The majority of incidents were rear-end crashes (53%) followed by angle crashes (26%) and sideswipe crashes (12%). The most prevalent contributing circumstances were “driver inattention” (22%) and “following too close” (22%). Failure to yield right-of-way was a factor in 10% of the crashes.

Table 1 – Summary of Bicycle & Pedestrian Crashes

| # | Date | Primary Street | Closest Side Street | Lighting | Severity | Contributing Factors |
|---|------------|----------------|---------------------|--------------------|-------------------------|---|
| 1 | 3/1/2008 | Silverside Rd | Rose Avenue | Dark - No Lighting | Injury - Incapacitating | Pedestrian hit crossing on unmarked leg of crosswalk. |
| 2 | 1/10/2010 | US 202 | Righter Pkwy | Dark - Lighting | Injury - Incapacitating | Bicyclist hit crossing against traffic signal. |
| 3 | 12/20/2010 | Silverside Rd | 100' east of US 202 | Dark - No Lighting | Injury | Pedestrian hit crossing away from crosswalk after exiting bus. |
| 4 | 4/5/2012 | US 202 | Rocky Run Pkwy | Daylight | No injury | Bicyclist riding on sidewalk and approaching intersection flipped over handlebars while braking. |
| 5 | 8/27/2014 | US 202 | Rocky Run Pkwy | Daylight | Fatality | Pedestrian hit crossing Route 202. No other details provided. |
| 6 | 8/21/2016 | US 202 | Brandywine Blvd | Daylight | Injury | Bicyclist hit by vehicle turning right on red. Bicyclist under the influence and riding the wrong way. |
| 7 | 11/24/2016 | Silverside Rd | Brookfield Ln | Dark - No Lighting | No injury | Pedestrian hit by left turning vehicle while crossing in crosswalk. Dark conditions were cited as a factor. |

Source: DeIDOT Crash Summaries for a 10-year period between 12/19/2007 and 12/19/2017

Figure 4 – Crash Map



Sidewalk and Curb Ramp Inventory

The sidewalk network within the study area is largely continuous, although there are several missing segments including a section along northbound US 202 near Rocky Run Boulevard and portions of Garden of Eden Road. Most of the existing sidewalk is located directly adjacent to the curb without a grass buffer. Sidewalk segments were measured in the field to determine compliancy related to width, cross-slope, running slope, and condition. Based on the 2018 PAS, the sidewalk inventory was categorized as follows:

- **Missing Sidewalk:** Gaps in the sidewalk network
- **Compliant Sidewalk:** Width greater than 60" and cross slope not exceeding 2%
- **Compliant Sidewalk (Requires Passing Areas):** Width between 48" and 60" and cross slope not exceeding 2%. Passing areas must be provided at intervals not to exceed 200 feet.
- **Non-Compliant Sidewalk:** Width less than 48" or cross slope exceeding 2%
- **Sidewalk in Development:** New sidewalk proposed through adjacent development projects

Figure 5 shows results from the sidewalk inventory for the entire study area, which are summarized in the table below. Larger-scale maps, along with the field measurements and compliancy analysis results, are included in **Appendix D**. Recognizing the accuracy limitations of smart levels and effect that environmental factors such as temperature can have on field measurements, the non-compliant sidewalk segments were separated into two categories – (a) segments meeting minimum width requirements but having cross slopes between 2-3% and (b) any segment with a cross slope exceeding 3%. Segments with cross slopes between 2-3% should be re-evaluated during the design phase.

| Category | Length | % Total |
|---|---------------|-------------|
| Missing Sidewalk | 2,300 | 12% |
| Compliant Sidewalk | 10,700 | 54% |
| Compliant Sidewalk (width 60" or greater) | 6,800 | 35% |
| Compliant Sidewalk (width between 48" and 60") | 3,900 | 20% |
| Non-Compliant Sidewalk | 4,700 | 24% |
| Non-Compliant Sidewalk (cross slope between 2-3%) | 2,800 | 13% |
| Non-Compliant Sidewalk (cross slope exceeds 3%) | 1,900 | 11% |
| Sidewalk in Development | 1,900 | 10% |
| Total Sidewalk Network | 19,600 | 100% |

Curb ramps are present at all signalized crossing locations and most driveway crossings in the study area. Existing curb ramps were field-measured and analyzed for compliance based on the 2018 PAS. Field measurements and compliancy analysis results are provided in **Appendix D**.

Figure 6 shows results from the curb ramp inventory (larger-scale detail maps are included in **Appendix E**). Of the study area's 151 curb ramps, 68 were found to be compliant while 52 were found to be non-compliant. The remaining 31 ramps were not evaluated as they will be reconstructed as part of the T201701401 Contract and/or the YMCA expansion.

Figure 5 – Sidewalk Inventory

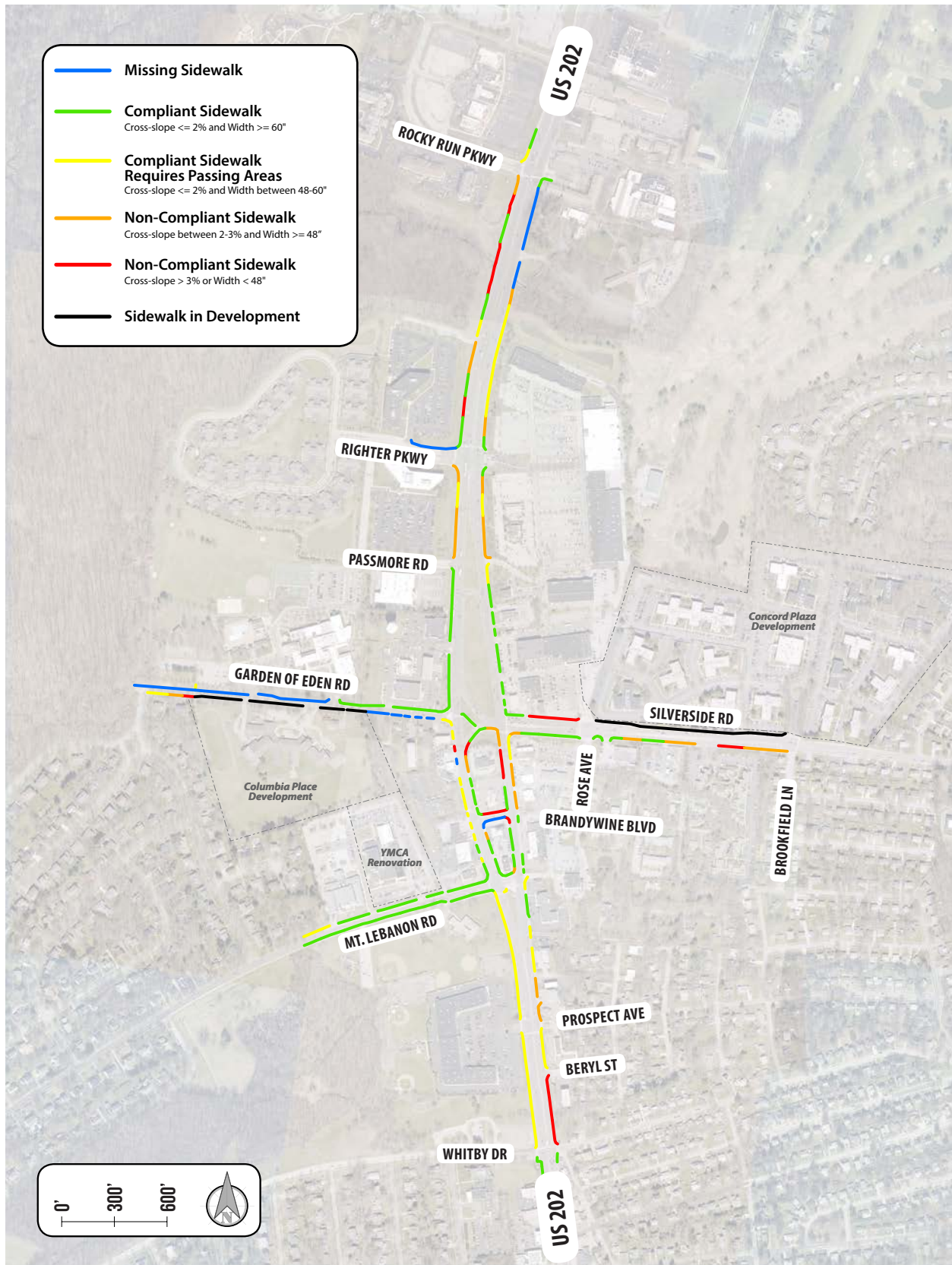
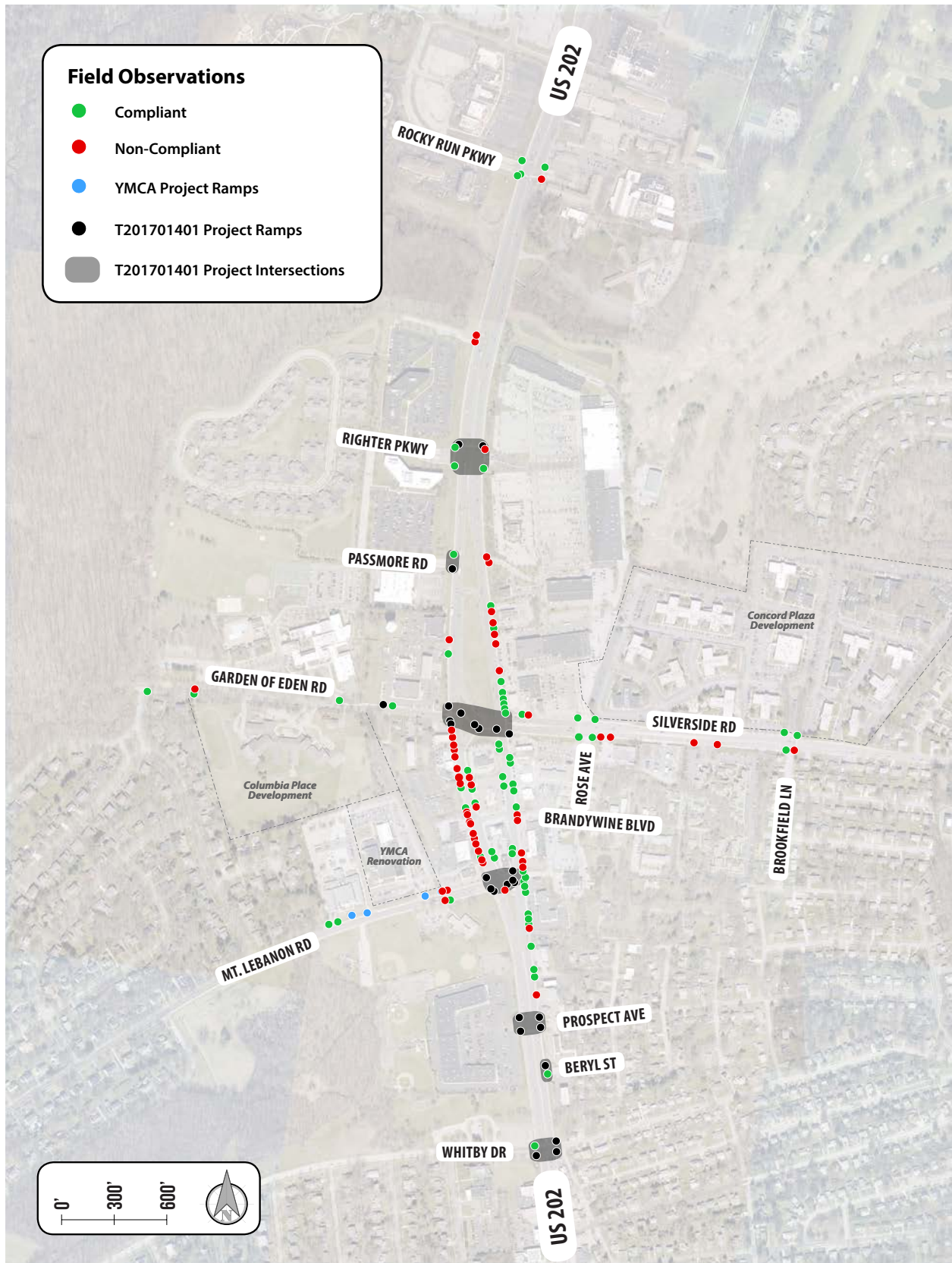


Figure 6 – Curb Ramp Inventory



Additional deficiencies related to PAS compliance along pedestrian access routes were recorded in conjunction with the sidewalk and curb ramp inventory. These include curb barriers (i.e. curbed corners that are missing a ramp), vertical elevation differences exceeding the ¼” threshold, horizontal gaps exceeding the ½” threshold, and obstructions such as light poles that leave less than the required 34” pinch point clearance.

Existing conditions related to sidewalks, curb ramps, and other deficiencies at specific areas within the study area are described below. An exception is curb ramps located at signalized intersections, which are discussed under the section on signalized intersections.

East Side of US 202

At the north end of the study area, there is approximately 600 feet of missing sidewalk along northbound US 202 between Rocky Run Parkway and the Rocky Run Creek. This stretch is located on property owned by the Widener University Law School and passes in front of their southernmost entrance, which has a curbed median with no curb ramps. The adjacent property is higher than the roadway grade in this area and is retained by a 4.5 foot high barrier that slopes back to roadway grade in both directions.



Missing sidewalk along NB US 202

South of Rocky Run Creek, the sidewalk in front of the Concord Square shopping plaza is a minimum 48” wide and generally in good condition. There are two non-compliant ramps at the southern entrance to the plaza. Between Passmore Road and Silverside Road, the sidewalk widens to 78” with an increasing number of commercial driveways. Several vertical elevation differences and non-compliant curb ramps were observed in this stretch. Between Silverside Road and Mt. Lebanon Road, the sidewalk width varies with numerous driveways interrupting the PAR and multiple non-compliant ramps. The parking stalls in front of the Talleyville Town Shops lack bumper blocks, which can lead to parked cars encroaching into the PAR, and the Talleyville Fire Company has a 175-foot wide driveway opening with no defined pedestrian corridor.

South of Mt. Lebanon Road, the recently redeveloped shopping center containing Seasons Pizza has new 60” wide sidewalks and marked crosswalks at the driveways. Between the shopping center and Beryl Street, the sidewalks are 48” wide and in good condition, although several curb ramps were identified as non-compliant. The two curb ramps along US 202 on the 3701 Concord Pike property will be reconstructed as part of redevelopment of that property. The sidewalk between Beryl Street and Whitby Drive is in very poor condition due to narrow widths, settling, and a 3” vertical gap.

West Side of US 202

The sidewalk network along the west side of US 202 is continuous but varies in condition. At the north end of the study area, there are sections with cross slopes exceeding 3% and the width narrows to 42” at the crossing of Rocky Run Creek. Between the creek and Passmore Road, the sidewalk condition

varies with several segments exceeding a 3% cross slope, and the curb ramps at the northern driveway to the office complex are non-compliant. Utility covers located within the PAR have created non-compliant horizontal gaps at four locations along this stretch, and there are vertical elevation differences at three additional locations south of Righter Parkway. Between Passmore Road and Garden of Eden Road, the sidewalks are 60" or wider and in good condition.

The segment between Garden of Eden Road and Mount Lebanon Road is particularly challenging for pedestrians due to frequent commercial driveways, sidewalks with no buffer between the curb or parking lot, and numerous non-compliant ramps. This section includes seventeen non-compliant curb ramps, two vertical elevation differences, and two sidewalk obstructions.



Horizontal gap at utility cover

Between Mt. Lebanon Road and Whitby Drive, there is a 1,500 foot stretch of 48" wide sidewalk with grass buffer. Aside from the Concord Gallery shopping center entrance, no passing zones are provided within this stretch. Similar to the segment near Righter Parkway, utility covers located within the PAR have created non-compliant horizontal gaps and/or vertical elevation differences at three locations in this area.

Median of US 202

Between Mt. Lebanon Road and Silverside Road, US 202 is bifurcated with commercial land uses located between the northbound and southbound lanes. In general, the pedestrian environment in the median is challenging due to frequent driveways, multiple curb barriers, sidewalk obstructions, and auto-oriented land uses. The sidewalk cross-slope exceeds 3% in several areas, including a segment on the northern end that is over 11%. There is a very narrow 27" wide segment along the north side of Brandywine Boulevard along with multiple curb barriers on both sides of the road. Additional issues related to pedestrian crossings in this area are discussed in the section on signalized intersections.



North side of Brandywine Blvd

Garden of Eden Road

Despite the presence of pedestrian activity generators including the Albert Einstein School and the Brandywine Creek State Park trailhead, most of Garden of Eden Road does not have sidewalks. On the north side, the 60" wide sidewalk leading from US 202 terminates at the school's eastern driveway. This is followed by an approximately 1000 foot stretch with no sidewalk between the driveway and the park entrance. The roadside area through this stretch contains a drainage swale along most of its length and sections that slope upward away from the road.

On the south side of the road near the park entrance, there is a 250 foot-long segment of 48" wide sidewalk with a grass buffer. In two separate locations, the sidewalk has heaved due to tree roots, resulting in vertical elevation differences of an inch or greater. The sidewalk ends at a marked mid-block crosswalk leading onto the Albert Einstein School property. The north ramp for this crosswalk was found to be non-compliant, while the southern ramp is compliant but obscured by vegetation.

There are no sidewalks on the south side of Garden of Eden Road between the midblock crossing and US 202. Within this stretch, nearly 1,000 feet of sidewalk is planned as part of the Columbia Place development, which also includes a new mid-block crosswalk at the school's eastern driveway. To the east of the Columbia Place development, approximately 400 feet of sidewalk is missing across four adjacent property frontages. Two of these properties are residential with a grass lawn extending to the curbline, while two are commercial with parking lots extending to the curbline. Sidewalk resumes on the Shell Station at the corner of US 202 and Garden of Eden Road.

Silverside Road

Sidewalks are continuous along the south side of Silverside Road within the study area. The segment between US 202 and Rose Avenue is 96" wide and in good condition. The next segment between Rose Avenue and Brookfield Lane is also 96" wide (or greater), but has sections exceeding the minimum cross-slope due to settlement and multiple locations with vertical elevation differences. There are four non-compliant ramps within this segment.

On the north side of Silverside Road, the 280 foot-long stretch of sidewalk between US 202 and Rose Avenue is in poor condition with a width less than 48" and a cross slope exceeding 3%. The sidewalk and curb ramps east of Rose Avenue were not inventoried as they will be reconstructed as part of the Concord Plaza redevelopment.



Elevation difference near inlet

Mount Lebanon Road

Sidewalk along the north side of Mount Lebanon Road is 60" wide and in good condition. Near the study area limit, tree roots have resulted in a 5" vertical elevation difference. Expansion plans for the YMCA property include reconstructing four curb ramps along their frontage. Both curb ramps at the Boston Market entrance were found to be non-compliant.

Sidewalk along the south side of Mount Lebanon Road is also 60" wide and in good condition; however, there are three locations where the vertical elevation difference exceeds 1" due to the sidewalk settling adjacent to drainage inlet boxes. One curb ramp at the school district driveway was found to be non-compliant.



Elevation difference due to tree roots

Signalized Intersections

The ten signalized intersections in the study area are shown in **Figure 7** and described below. Existing pedestrian features including signal heads, push buttons, and clearance timings were field-inventoried at each of these intersections. Detailed field measurements and compliancy analysis results for push buttons are provided in **Appendix F**.

[US 202 & Rocky Run Parkway \(N479\)](#)

This four-leg intersection controls traffic between US 202, Rocky Run Parkway, and the entrance to the Widener University Law School. Observed deficiencies include:

- Two pedestrian pushbuttons on poles on a single pole in the SE corner
- One of the pushbuttons in the island on the SW corner is missing a directional arrow
- The curb ramp on the SE corner has a landing area cross-slope exceeding 3%
- Crosswalk striping does not fully extend to the curb ramp on the SE corner

[US 202 & Righter Parkway \(N481\)](#)

This four-leg intersection controls traffic between US 202, Righter Parkway, and the entrance to the Concord Square shopping center. Two of the curb ramps at this intersection are scheduled for replacement as part of the T201701401 Contract. Observed deficiencies/issues include:

- Two pedestrian pushbuttons on a single pole in the SE and SW corners
- Pedestrian clearance times for crossing the northern leg of 202 should be re-evaluated
- No landing area for pedestrian pushbuttons on NW corner
- Crosswalk markings for the southern leg are faded
- The curb ramp on the NE corner has a vertical elevation difference between the DWS pad and the curb that is greater than $\frac{1}{4}$ "
- Vehicle detection issues at the Concord Square exit driveway due to a concrete paver area adjacent to the stop bar

[US 202 Northbound U-turn \(N503\)](#)

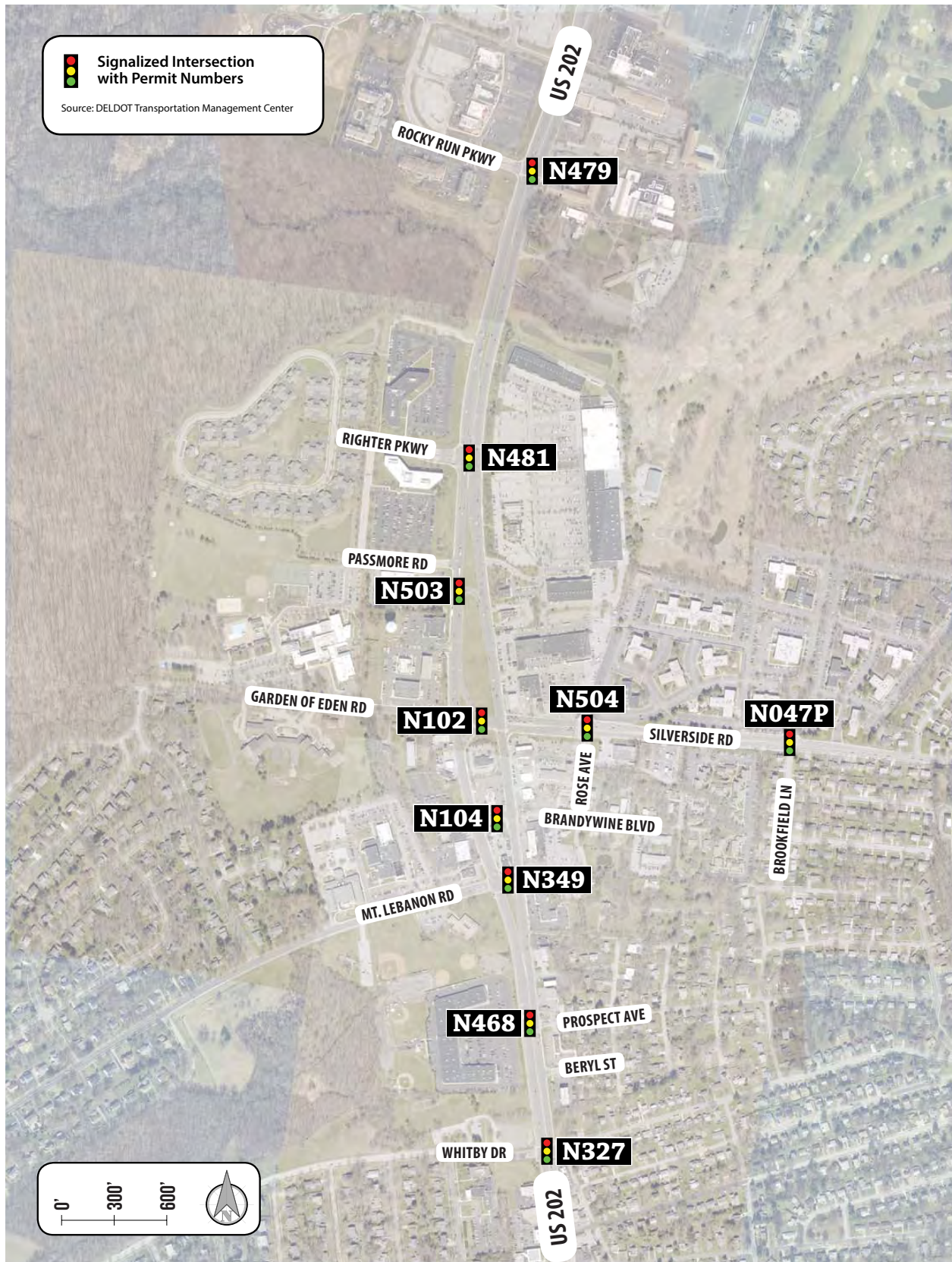
The purpose of this traffic signal is to accommodate a U-turn movement from northbound US 202 onto southbound US 202. The U-turn consists of one lane and is only signalized in the southbound direction. This intersection is currently not part of a pedestrian access route and thus does not have crosswalks, curb ramps, or pedestrian signal heads.

[US 202 & Garden of Eden Road/Silverside Road \(N102\)](#)

This signalized intersection consists of two distinct signal installations that control traffic for Garden of Eden Road, Silverside Road, and both directions of US 202. The northbound and southbound direction of US 202 are divided by a wide median and there is no through movement from Garden of Eden Road to Silverside Road. Eight of the nine curb ramps at this intersection are being redesigned as part of the T201701401 Contract. The remaining ramp was found to be compliant. Observed deficiencies include:

- Two pedestrian pushbuttons on a single pole on the median island at Silverside Road. There is also no landing area at this location.
- No pedestrian pushbutton sign on pole in larger island that actuates crossing 202 SB
- Potential tripping hazard on interior push button (8" foundation exposure).

Figure 7 – Signalized Intersections



US 202 & Brandywine Boulevard (N104)

This signalized intersection consists of two distinct signal installations that control traffic along Brandywine Boulevard and both directions of US 202. In general, pedestrian facilities are either non-existent or deficient at this intersection. Crosswalks and curb ramps are not provided to cross into the median area from either side of US 202, and there are numerous non-compliant features within the median area including ramps and sidewalks. The only signalized pedestrian crossing is the eastern leg of the intersection with northbound US 202. This crossing has pedestrian signal heads and pushbuttons but does not have an audible tone or pedestrian recall for the side street crossing.

US 202 & Mount Lebanon Road (N349)

This signalized intersection consists of two distinct signal installations that control traffic along Mount Lebanon Road and both directions of US 202. Seven of the eleven ramps at this intersection are being redesigned as part of the T201701401 Contract. The remaining four ramps were found to be compliant, although the landing area for the ramp in the grass median area is obscured by overgrown vegetation. Observed deficiencies include:

- No pedestrian crossing is provided across the north leg in both directions of US 202
- No pedestrian signal heads on the east leg of the northbound US 202 intersection
- Most of the pushbuttons are missing directional arrows
- Multiple pushbuttons are not perpendicular to the crosswalk they control
- No audible tone or LED indication for pushbutton actuation
- Pushbutton on NW corner is not adjacent to a landing area
- Two pushbuttons on a single pole in the median island
- Different styles of pushbuttons throughout intersection



Missing signal heads at Mt. Lebanon intersection

US 202 & Prospect Ave (N468)

This four-leg intersection controls traffic between US 202, Prospect Avenue, and the Concord Gallery shopping plaza. All four of the curb ramps at this intersection are being redesigned as part of the T201701401 Contract. Observed deficiencies include:

- No pedestrian crossing is provided across the north leg of the intersection
- Two pedestrian pushbuttons are located on a single pole on the SE and SW corners. The reach dimension for push buttons on the SE corner exceeds 10”.
- Pedestrian clearance times for crossing US 202 and the shopping center driveway should be re-evaluated
- Pedestrian signal heads on the SW corner are misaligned (twisted) and not visible from the corresponding crosswalks
- There are potential tripping hazards on the NW and SW corners (5” & 7” foundation exposure)



Misaligned pedestrian signal heads at Prospect Ave

US 202 & Whitby Drive (N327)

This four-leg intersection controls traffic between US 202, Whitby Drive to the west, and Florence Avenue to the east. Three of the four curb ramps are being redesigned as part of the T201701401 Contract. Observed deficiencies include:

- No pedestrian crossing is provided across the south leg of the intersection
- Two pedestrian pushbuttons are located on a single pole on the NE and NW corners. The reach dimension for push buttons on the NE corner exceeds 10”.
- No arrow indications on pushbuttons on NE and NW corners to indicate which crossing the pushbuttons actuate
- The pole and pushbutton on the SE corner is 12’ from face of curb
- There is a potential tripping hazard on the NW corner (5” foundation exposure)

Silverside Road & Rose Avenue (N504)

This four-leg intersection controls traffic between Silverside Road, Rose Avenue, and the entrance to the Talleyville Shopping Center. The intersection currently has pedestrian crossings on three of the four legs, with the east leg crossing not provided. During the time of this study, the intersection was undergoing improvements as part of the adjacent Concord Plaza development on the north side of Silverside Road. Signal plans obtained from DeIDOT show that new pedestrian signal heads, crosswalks, curb ramps, and push buttons will be installed at all four legs of the intersection.

Silverside Road & Brookfield Lane (N047P)

This four-leg intersection controls traffic between Silverside Road, Brookfield Lane, and the entrance to the Concord Plaza development. The intersection currently has pedestrian crossings on three of the four legs, with a west leg crossing not provided. During the time of this study, the intersection was undergoing improvements as part of the adjacent Concord Plaza development. Signal plans obtained from DeIDOT show that new pedestrian signal heads, crosswalks, curb ramps, and push buttons will be installed on the three legs with existing crossings, while the west leg will remain without a crossing.

Summary of Deficiencies

Deficiencies at each signalized intersection related to signal heads, push buttons, and clearance times are summarized in **Table 2**. Unless noted in the descriptions below, intersections had the following pedestrian features per Delaware Manual of Uniform Traffic Control Devices (DE MUTCD) Section 4E.08:

- Pedestrian signal heads and curb ramps at marked crosswalks
- Pedestrian pushbuttons/poles located between 1.5’-10’ from the edge of curb
- Pedestrian pushbuttons/poles located parallel to the crosswalk they control
- Pedestrian pushbuttons located 42” above ground level
- Pedestrian pushbuttons with arrow indicators and audible tones
- Landing areas located adjacent to the pushbuttons
- Pedestrian recall for the side street crossings
- Complete network of sidewalks and curb ramps through the intersection

Additional corridor-wide observations based on DE MUTCD Section 4E.08 are summarized below and included in **Table 2**:

- Some intersections included two pedestrian buttons on the same pole, without separation. Per the DE MUTCD, this is undesirable unless a physical condition makes it impractical.
- Some pushbuttons had arrow indicators. When two pushbuttons are located on the same pole, arrows shall indicate which pushbutton operates which crosswalk.
- Some pushbuttons have an audible tone and red LED to indicate the pushbutton was activated
- Some pedestrian countdowns extended through the change (yellow) interval, while others terminated at the end of the green interval. Both are acceptable per the DE MUTCD.
- None of the signalized intersections with crosswalks on three of the four legs had “No Pedestrian Crossing Signs (R9-3)” for the missing leg.

Table 2 – Summary of Deficiencies at Signalized Intersections

| Signalized Intersections | Pedestrian Clearance Times | Pedestrian Pushbuttons | Pedestrian Signal Heads | Pedestrian Recall for Side Street | Arrow Indications on Pushbuttons | Audible Tone on Pushbuttons | Foundation Tripping Hazard |
|---------------------------------|---------------------------------|------------------------|-------------------------|-----------------------------------|----------------------------------|-----------------------------|----------------------------|
| US 202 @ Rocky Run Pkwy | Adequate | 2 per pole | Present | Yes | Yes | Yes | No |
| US 202 @ Righter Pkwy | Re-evaluate | 2 per pole | Present | Yes | Yes | Yes | No |
| US 202 @ U-Turn Signal | No pedestrian facilities | | | | | | |
| US 202 @ Silverside Road | Adequate | 2 per pole | Present | Yes | Yes | Yes | Yes |
| US 202 @ Brandywine Blvd | Adequate | 1 per pole | Present | No | Yes | No | No |
| US 202 @ Mount Lebanon Rd | Adequate | Re-evaluate | Missing (SE/NE) | Yes | No | No | No |
| US 202 @ Prospect Ave | Re-evaluate | 2 per pole | Misaligned (SW) | Yes | Yes | Yes | Yes |
| US 202 @ Whitby Dr | Adequate | 2 per pole | Present | Yes | No | Yes | Yes |
| * Silverside Rd @ Rose Ave | Re-evaluate | 2 per pole | Present | No | No | No | No |
| * Silverside Rd @ Brookfield Ln | Re-evaluate | 2 per pole | Present | Yes | No | No | No |

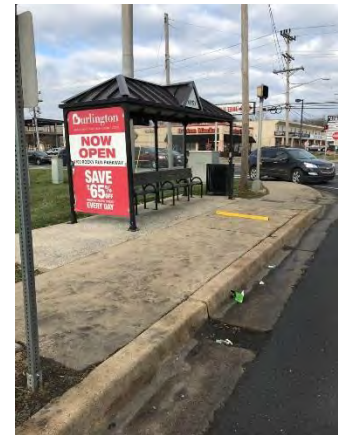
* Under construction during field observation in January 2018

Transit Inventory

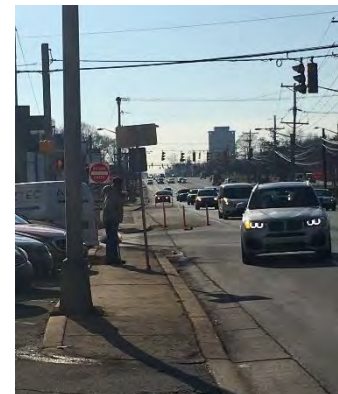
The Delaware Transit Corporation (DTC) operates DART Routes 2 and 35 within the study area. Both routes provide service between the Brandywine Town Center Park & Ride (north of the study area) and Amtrak Station in downtown Wilmington (south of the study area). Route 2 operates in both directions of US 202 through the study area. Route 35 operates in both directions of US 202 south of Silverside Road, and then diverts from US 202 onto Silverside Road and Shipley Road to access the Brandywine Town Center.

Figure 8 shows location, ridership, and compliancy for the eighteen bus stops present within the study area, while **Table 2** summarizes facilities and ridership at each stop. Bus stops with the highest ridership are located at the north end of the corridor at Rocky Run Blvd and in the central bifurcated portion of US 202 between Silverside Road and Mt. Lebanon Road. DTC provided data on ridership patterns within the study area, including ridership load profiles from December 2016. In the AM period, trip activity is primarily generated by riders boarding in Wilmington accessing retail jobs within the corridor, while in the PM period, trips are primarily generated by retail workers heading back into Wilmington, both in the late afternoon and early evening. This activity is supplemented by shoppers who have flexibility to access corridor businesses later in the AM period and return during the late lunch or early afternoon periods.

Each bus stop was measured in the field for width, length, running slope, and condition and analyzed for compliancy based on the 2018 PAS. Field measurements and compliancy analysis results are provided in **Appendix G**. Three of the stops provide a waiting pad, shelter, and bench. In addition, six stops feature a waiting pad or wide (8'+) sidewalk that can serve as the waiting pad. The remaining stops do not have waiting areas or any other amenities. The presence and condition of sidewalks, curb ramps, and lighting in the vicinity of bus stops are discussed in those respective sections.



Bus stop with shelter at Mt. Lebanon Road



Bus stop without amenities along NB 202



Bus stop with waiting pad At Concord Square

Figure 8 – Transit Facilities and Ridership

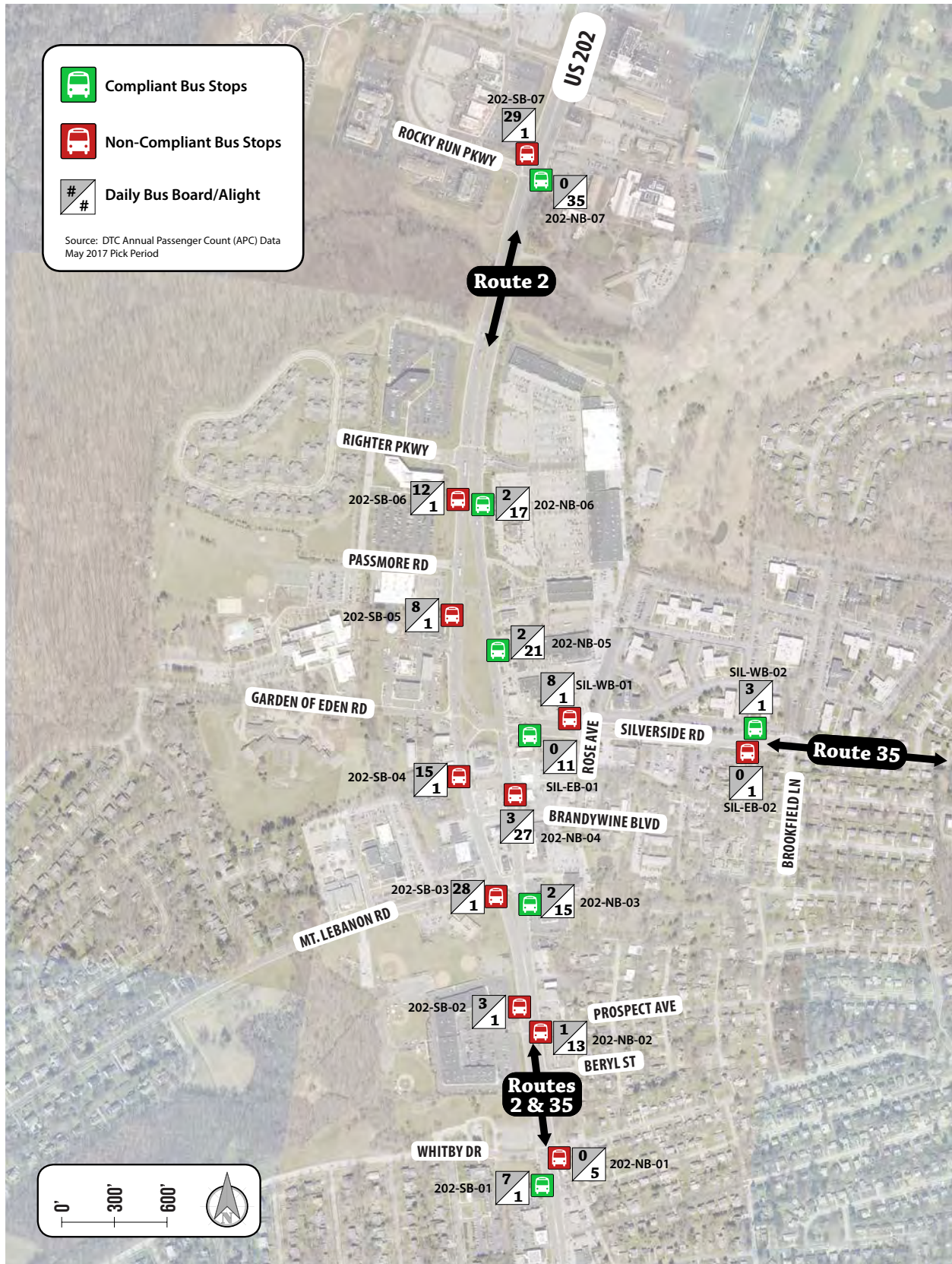


Table 3 – Transit Facilities and Ridership Inventory

| Location | Bus Stop IDs | Routes | Type | Amenities | Lighting | On | Off |
|-------------------------------|--------------|--------|----------|-----------|----------|----|-----|
| US 202 @ Florence Ave | 202-NB-01 | 2 & 35 | Nearside | None | Yes | 0 | 5 |
| US 202 @ Prospect Ave | 202-NB-02 | 2 & 35 | Nearside | None | Yes | 1 | 13 |
| US 202 @ Seasons Pizza | 202-NB-03 | 2 & 35 | Nearside | Pad | Yes | 2 | 15 |
| US 202 @ Brandywine Blvd | 202-NB-04 | 2 & 35 | Farside | None | Yes | 3 | 27 |
| US 202 @ Silverside Rd | 202-NB-05 | 2 | Midblock | Pad | Yes* | 2 | 21 |
| US 202 @ Concord Sq | 202-NB-06 | 2 | Nearside | Pad | No | 2 | 17 |
| US 202 @ Rocky Run Pkwy | 202-NB-07 | 2 | Nearside | Wide SW | Yes | 0 | 35 |
| US 202 @ Rocky Run Pkwy | 202-SB-07 | 2 | Nearside | None | Yes | 29 | 1 |
| US 202 @ Righter Pkwy | 202-SB-06 | 2 | Farside | Pad | Yes | 12 | 1 |
| US 202 @ Garden of Eden Rd | 202-SB-05 | 2 | Midblock | None | Yes | 8 | 1 |
| US 202 @ Silverside Rd | 202-SB-04 | 2 & 35 | Midblock | None | Yes | 15 | 1 |
| US 202 @ Mt. Lebanon Rd | 202-SB-03 | 2 & 35 | Farside | Shelter | Yes | 28 | 1 |
| US 202 @ Prospect Ave | 202-SB-02 | 2 & 35 | Nearside | None | Yes | 3 | 1 |
| US 202 @ Whitby Rd | 202-SB-01 | 2 & 35 | Farside | Shelter | Yes | 7 | 1 |
| Silverside Rd @ US 202 | SIL-EB-01 | 35 | Farside | Wide SW | No | 0 | 11 |
| Silverside Rd @ the Commons | SIL-EB-02 | 35 | Nearside | Wide SW | No | 0 | 1 |
| Silverside Rd @ Concord Plaza | SIL-WB-02 | 35 | Farside | Shelter | No | 3 | 1 |
| Silverside Rd @ Rose Ave | SIL-WB-01 | 35 | Farside | None | No | 8 | 1 |

** Adequate lighting at stop but not between stop and nearest crosswalk*

Roadway Lighting Inventory

Most of US 202 within the project area features roadway lighting. North of Garden of Eden Road and Silverside Road, DeIDOT light standards are present on both sides of US 202 with luminaires mounted at 40 feet above ground. Between Righter Parkway and Mount Lebanon Road, the median area features DeIDOT light standards on the western side and utility pole-mounted luminaires on the eastern side. South of Garden of Eden Road and Silverside Road, utility company-owned luminaires are installed on utility poles on both sides of US 202.

Roadway lighting is not provided along the portions of Garden of Eden Road and Mount Lebanon Road located within the study area. Roadway lighting is also not provided on Silverside Road, including the signalized intersections with Rose Avenue and Brookfield Lane. Crash reports for a 10-year period indicated that three pedestrian crashes occurred along Silverside Road during dark conditions, with one crash at each of the signalized intersections and one at the EB bus stop near US 202.

A photometric model of existing conditions was developed based on the roadway lighting inventory described above. Ambient lighting from parking lots, buildings, and other point sources off the roadway were not included in the model. Results from the model are shown in [Figure 9](#), included in [Appendix H](#), and described below.

[Lighting at Signalized Intersections](#)

Using the photometric model, lighting conditions were analyzed at each of the signalized intersections. [Table 4](#) presents results from the analysis compared with DeIDOT-recommended light levels for each facility. Based on these results, the following intersections were identified as having lighting conditions that significantly deviate from DeIDOT guidelines:

- US 202 @ Rocky Run Parkway
- US 202 @ Righter Parkway
- US 202 @ Brandywine Boulevard – East
- US 202 @ Mt Lebanon Road
- US 202 @ Whitby Drive
- Silverside Road @ Rose Avenue (no lighting present)
- Silverside Road @ Brookfield Lane (no lighting present)

[Lighting Gaps between Continuously Lit Areas](#)

Based on DeIDOT lighting guidance, the following lighting gaps along US 202 were identified:

- SB US 202 just north of Whitby Drive
- SB US 202 near Wawa/Fedex driveway
- NB US 202 near bus stop to the south of Righter Parkway
- Silverside Road within project limits
- Mid-block pedestrian crossing on Garden of Eden Road

[Lighting at Bus Stops](#)

Lighting conditions at each of the bus stops are described in [Table 5](#).

Table 4 – Intersection Lighting: Existing Conditions

| Signalized Intersections | DeIDOT Guidelines | | Existing | |
|--------------------------------|--------------------------------|--------------------------|--------------------------------|--------------------------|
| | Average Maintained Illuminance | Avg:Min Uniformity Ratio | Average Maintained Illuminance | Avg:Min Uniformity Ratio |
| US 202 @ Rocky Run Pkwy | 1.2 | 3:1 | 0.6 | 2:1 |
| US 202 @ Righter Pkwy | 1.2 | 3:1 | 0.7 | 4:1 |
| US 202 @ Garden of Eden Rd | 1.2 | 3:1 | 1.2 | 5:1 |
| US 202 @ Silverside Rd | 1.2 | 3:1 | 1.9 | 3:1 |
| US 202 @ Brandywine Blvd East | 1.2 | 3:1 | 0.4 | 2:1 |
| US 202 @ Brandywine Blvd West | 1.2 | 3:1 | 1.9 | 4:1 |
| US 202 @ Mt Lebanon Rd | 1.2 | 3:1 | 1.2 | 13:1 |
| US 202 @ Prospect Ave | 1.2 | 3:1 | 1.2 | 6:1 |
| US 202 @ Whitby Dr | 1.2 | 3:1 | 1.7 | 9:1 |
| Silverside Rd @ Rose Ave | 1.0 | 4:1 | 0.0 | NA |
| Silverside Rd @ Brookefield Ln | 1.0 | 4:1 | 0.0 | NA |

Notes:

DeIDOT-recommended light levels for all US 202 intersections, including the crosswalks, are as follows:

- *Average maintained illuminance of 1.2 foot-candles or greater*
- *Average-to-Minimum illuminance uniformity ratio of 3:1 or less*

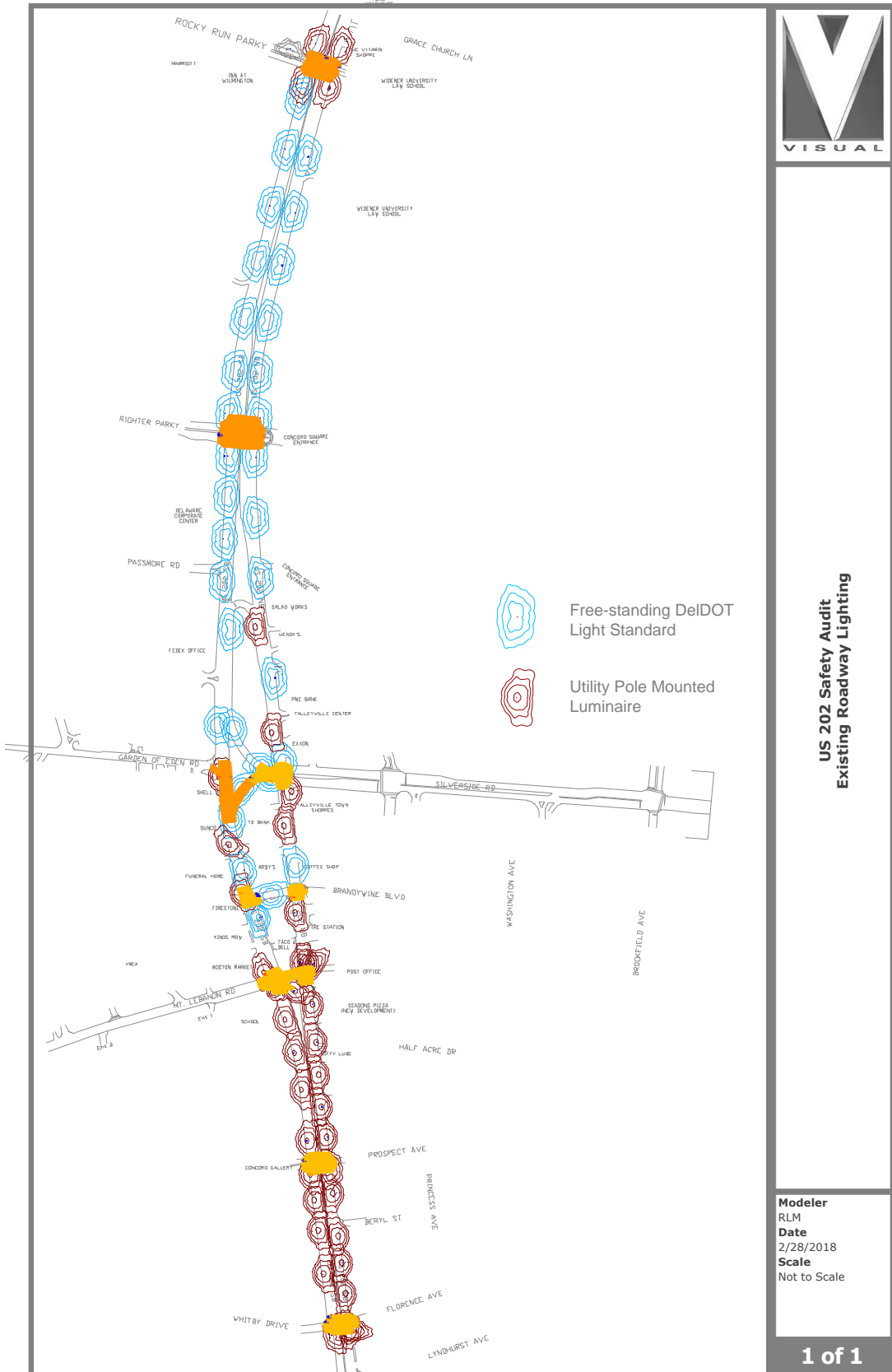
DeIDOT-recommended light levels for Silverside Rd intersections, including the crosswalks, are as follows:

- *Average maintained illuminance of 1.0 foot-candles or greater*
- *Average-to-Minimum illuminance uniformity ratio of 4:1 or less*

Table 5 – Bus Stop Lighting: Existing Conditions

| Bus Stop IDs | Location | Existing Lighting Conditions |
|--------------|---|--|
| 202-SB-07 | Just north of Rocky Run Pkwy | Intersection lighting present. |
| 202-SB-06 | South of Righter Pkwy | Located in continuously illuminated area. |
| 202-SB-05 | South of Passmore Rd | Light on east side of SB US 202. No nearby crosswalks. |
| 202-SB-04 | South of Garden of Eden Rd | Located directly under utility pole-mounted light. |
| 202-SB-03 | Just south of Mt. Lebanon Rd | Located at the intersection |
| 202-SB-02 | Just north of Concord Gallery Shopping Center | Located directly under utility pole-mounted light. |
| 202-SB-01 | South of Whitby Dr | Located directly under utility pole-mounted light. Lighting present between bus stop and intersection. |
| 202-NB-01 | Just south of Whitby Dr | Intersection lighting present. |
| 202-NB-02 | Just south of Prospect Ave | Intersection lighting present. |
| 202-NB-03 | South of Mt. Lebanon Rd | Directly under utility pole-mounted light. |
| 202-NB-04 | Just north of Brandywine Blvd | Intersection lighting present. |
| 202-NB-05 | North of Silverside Rd | Sidewalk not continuously lit to intersection. |
| 202-NB-06 | 270' south of Righter Pkwy | Not illuminated. |
| 202-NB-07 | Just south of Rocky Run Pkwy | Intersection lighting present. |
| SIL-EB-01 | 150' east of US 202 Northbound | Not illuminated. No continuous lighting to nearest crosswalk at US 202 NB. |
| SIL-EB-02 | 250' west of Brookefield Ln | Not illuminated. No continuous lighting to nearest crosswalk at Brookefield Ln. |
| SIL-WB-02 | 250' west of Brookefield Ln | Not illuminated (other than bus shelter). No continuous lighting to nearest crosswalk at Brookefield Ln. |
| SIL-WB-01 | 100' west of Rose Ave intersection | Not illuminated. No continuous lighting to nearest crosswalk at Shopping Center intersection. |

Figure 9 – Photometric Analysis



Signage and Pavement Markings

Signing and pavement markings within the study area are generally in good condition. The southern crosswalk at the US 202/Righter Parkway intersection was observed to be faded during initial site visits, but subsequently was restriped in June 2018 through coordination with DelDOT's Traffic Section.

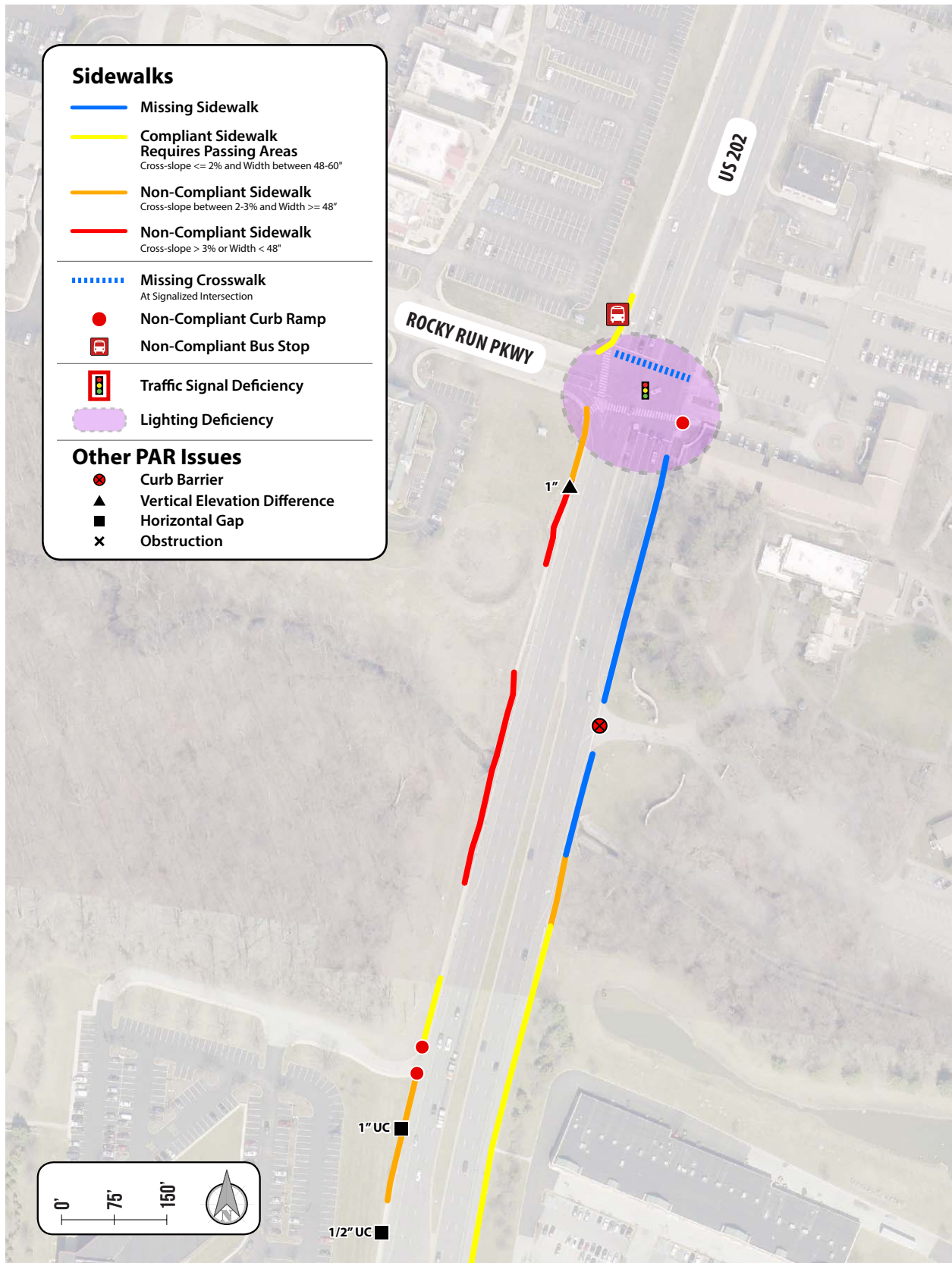
At signalized intersections, crosswalk markings are missing at the locations listed below. No Pedestrian Crossing (R9-3) signs are not provided at any of the locations where crosswalks are missing on the fourth leg of the intersection:

- Northern leg of US 202 & Rocky Run Road
- Northern legs of US 202 (Northbound & Southbound) & Mt. Lebanon Road
- Northern leg of US 202 & Prospect Avenue
- Southern leg of US 202 & Whitby Drive
- Eastern leg of Silverside Road and Rose Ave
- Western leg of Silverside Road and Brookfield Lane

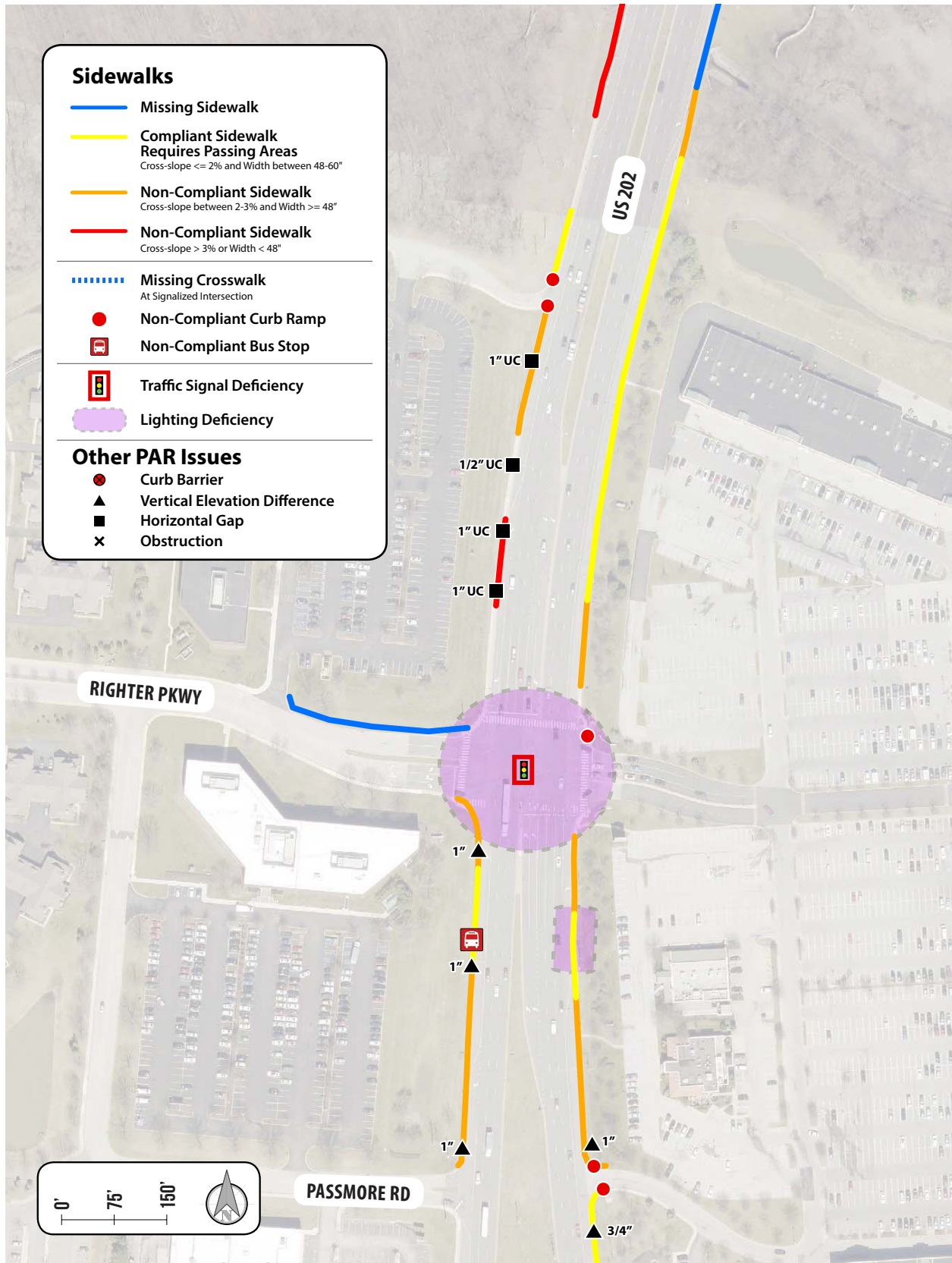
Deficiency Analysis

Deficiency maps were developed to summarize results from the existing conditions analysis, including missing areas of sidewalks; non-compliant sidewalks, curb ramps, and bus stops; and deficiencies related to lighting and signalized intersections (see **Figure 10, Sheets 1-7**). The deficiency maps were used as the starting point for developing the recommendations outlined in the following section. These maps do not include deficiencies that will be addressed by adjacent developer projects or Contract T201701401.

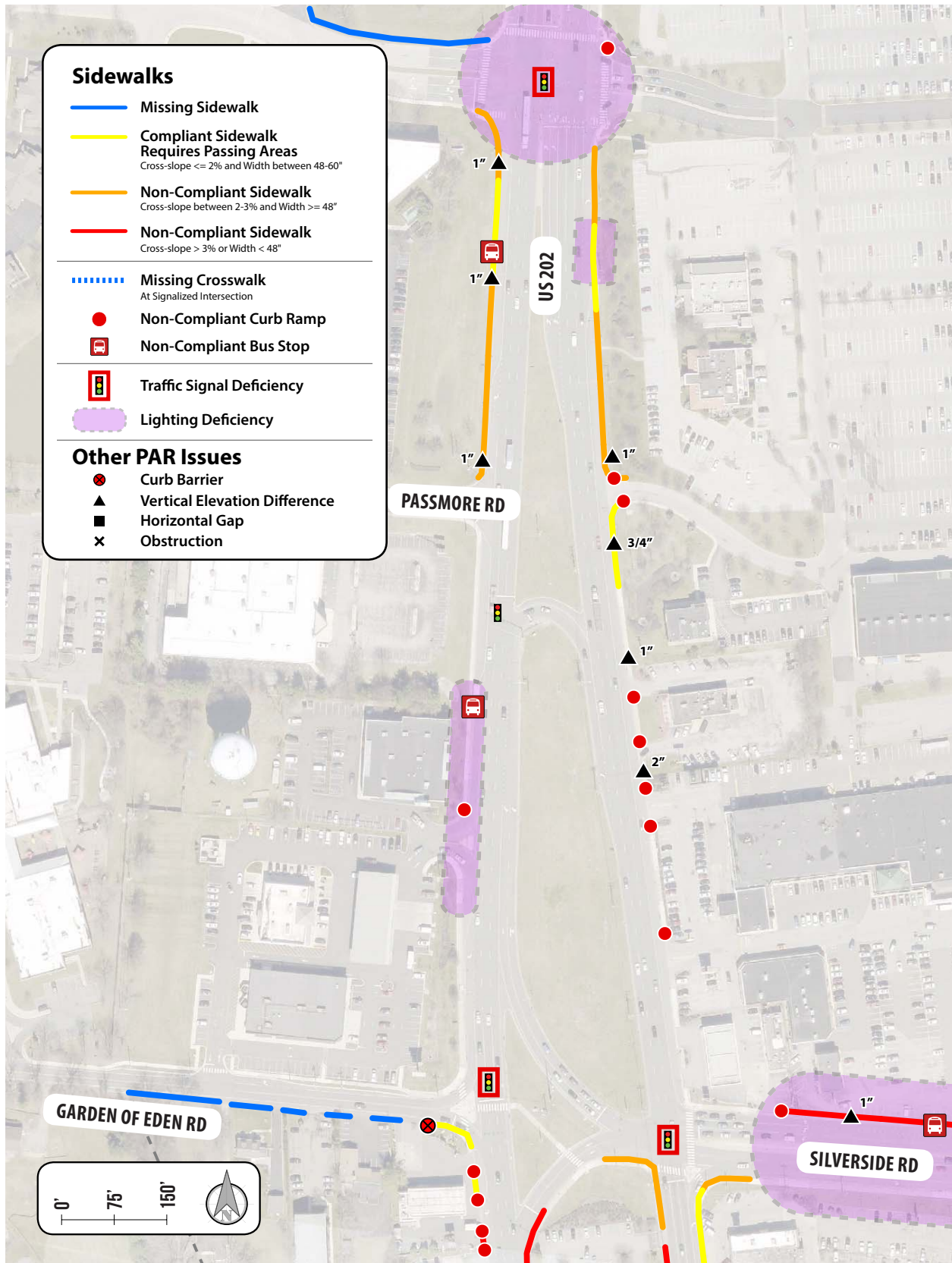
Figure 10 – Summary of Deficiencies - Sheet 1 of 7



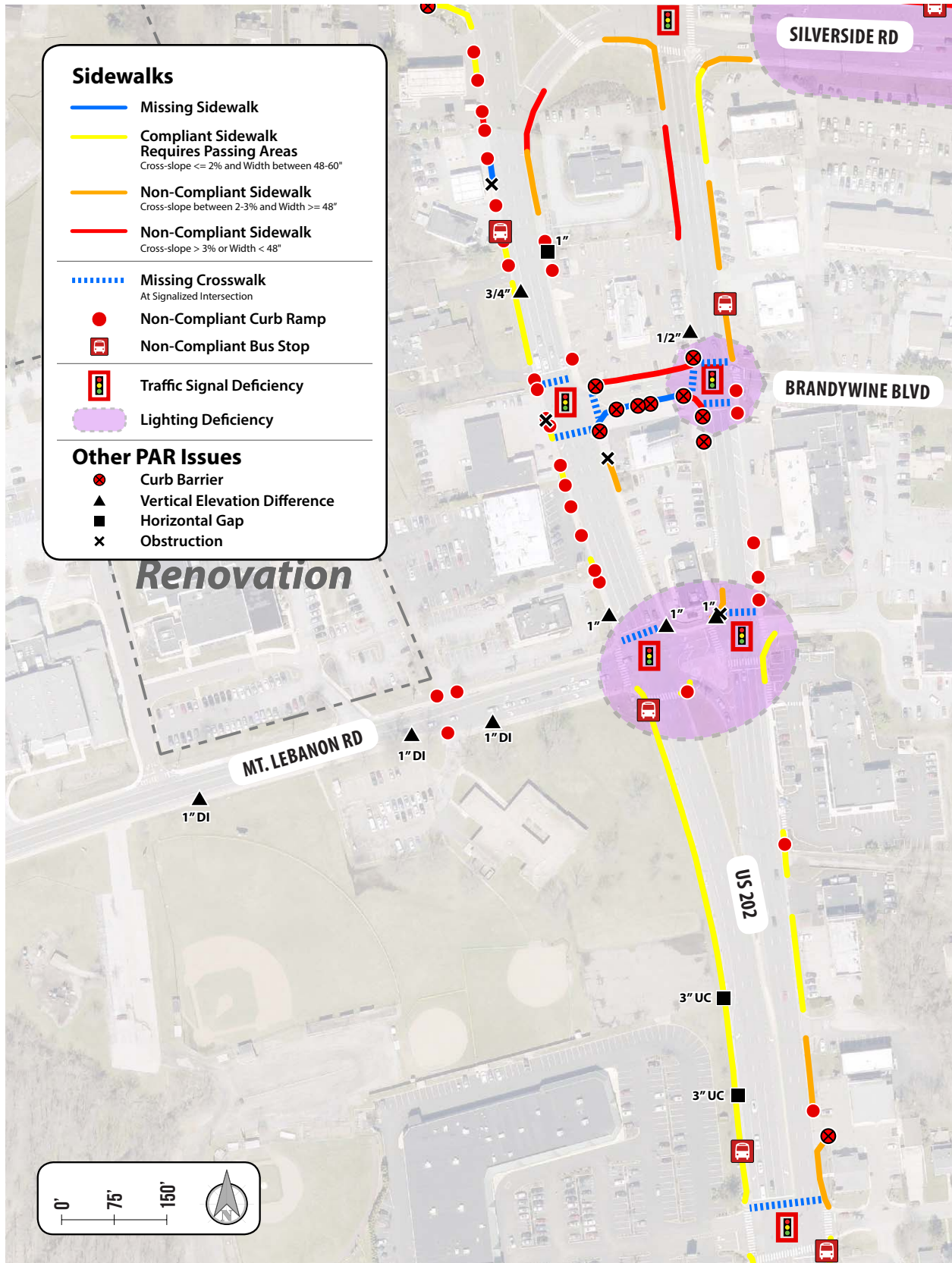
Summary of Deficiencies - Sheet 2 of 7



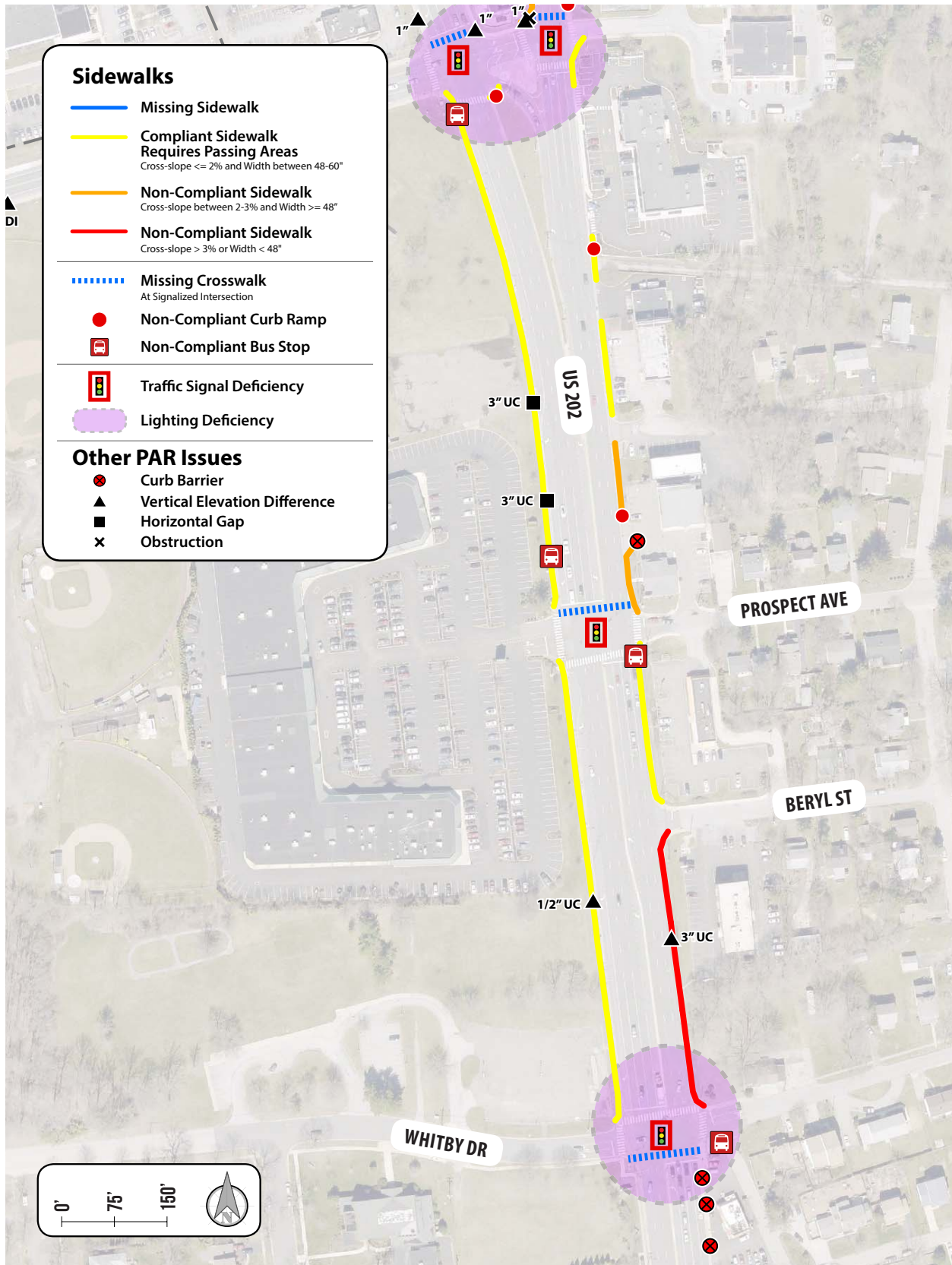
Summary of Deficiencies - Sheet 3 of 7



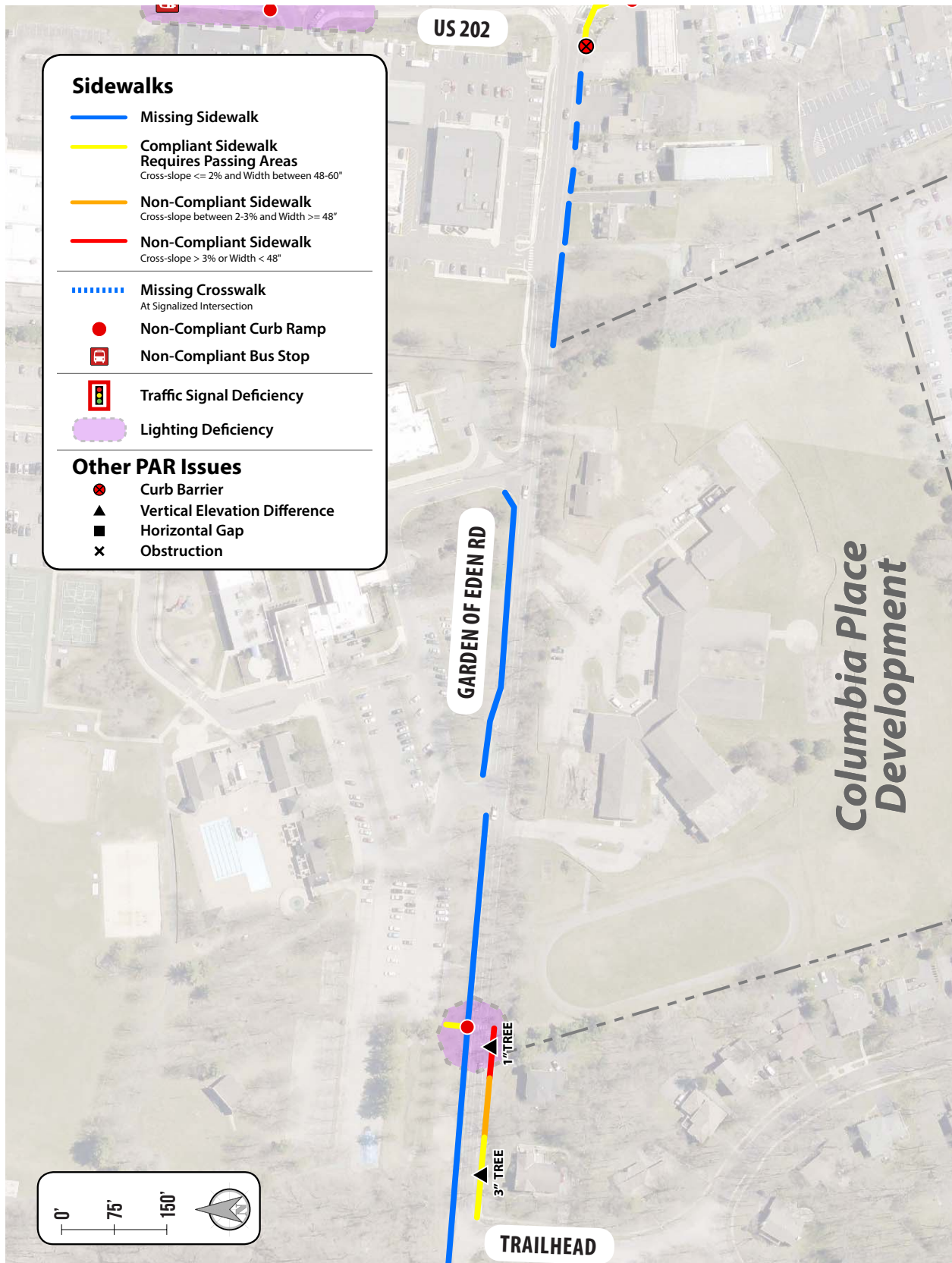
Summary of Deficiencies - Sheet 4 of 7



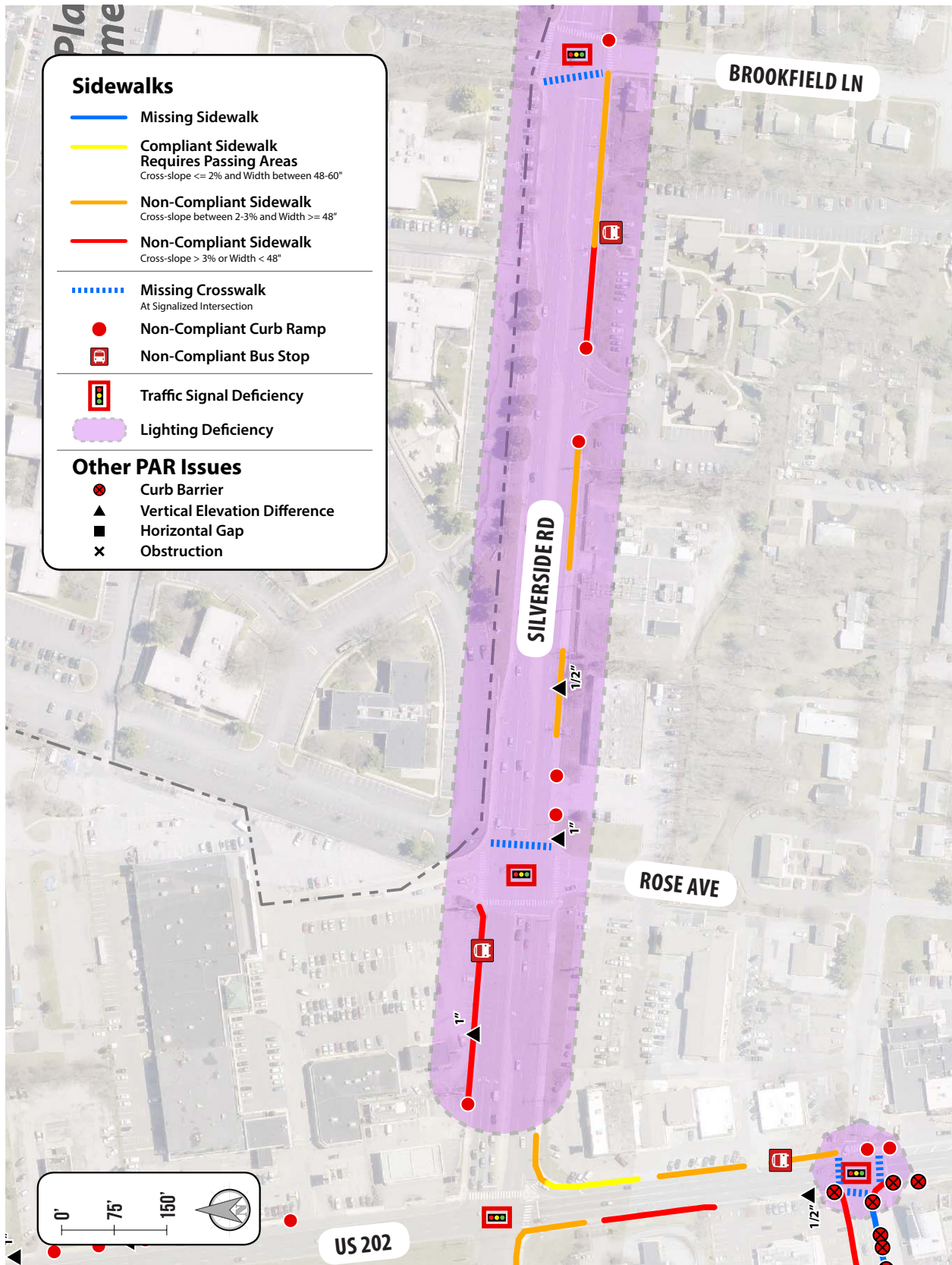
Summary of Deficiencies - Sheet 5 of 7



Summary of Deficiencies - Sheet 6 of 7



Summary of Deficiencies - Sheet 7 of 7



RECOMMENDED IMPROVEMENTS

Recommendations were developed to address deficiencies outlined in the previous section and achieve a compliant pedestrian network. The recommended improvements were developed in accordance with state standards and regulations, including DelDOT’s *Pedestrian Accessibility Standards for Facilities in the Public Right of Way (2018 PAS)*, dated February 2018.

Figure 11 (Sheets 1-7) shows the proposed PAR network for the study area, which includes improvements to sidewalks, curb ramps, crosswalks, signalized intersections, bus stops, and roadway lighting systems. Recommendations and associated design challenges are outlined in this section by improvement type.

Sidewalk and Curb Ramp Improvements

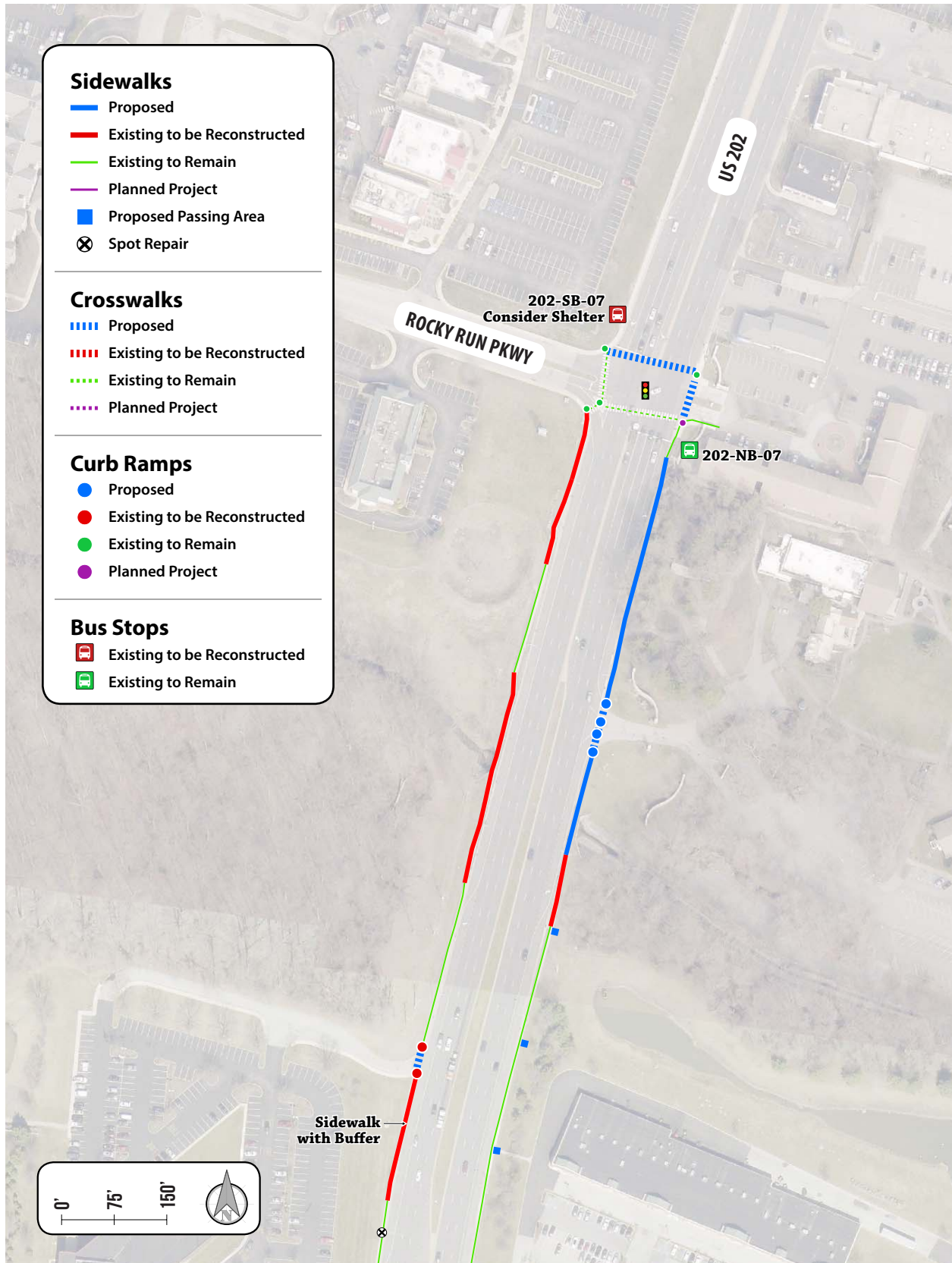
The proposed PAR recommendations will establish a network of PAS-compliant sidewalks and curb ramps along US 202, Silverside Road, Mount Lebanon Road, and Garden of Eden Road. Sidewalk, curb ramp, and crosswalk-related recommendations were categorized as follows:

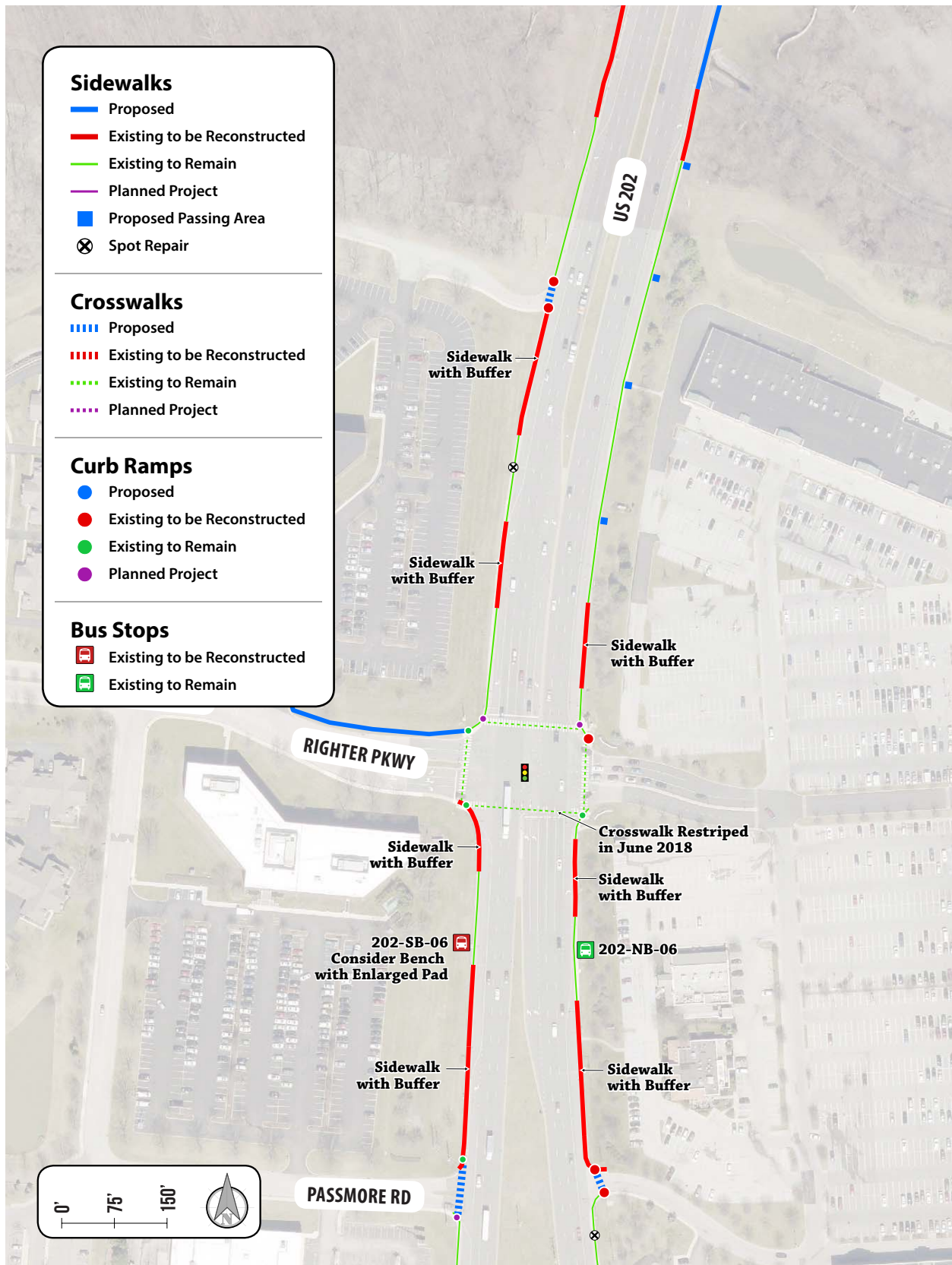
- **Proposed:** Construct compliant facility where none exists today
- **Existing to be Reconstructed:** Replace existing non-compliant facility with compliant facility
- **Existing to Remain:** Compliant facility to remain in place with no improvements needed
- **Planned Project:** Compliant facility to be implemented by planned development project or roadway project (i.e. Contract T201701401)
- **Sidewalk Passing Area:** Preliminary locations for new 5’x5’ pads to accommodate existing sidewalk segments that are less than 60” wide and over 200 feet in length
- **Spot Repair:** Localized sidewalk repairs to address vertical/horizontal gaps or obstructions

The recommended improvements are quantified in the table below and described in more detail on the following pages.

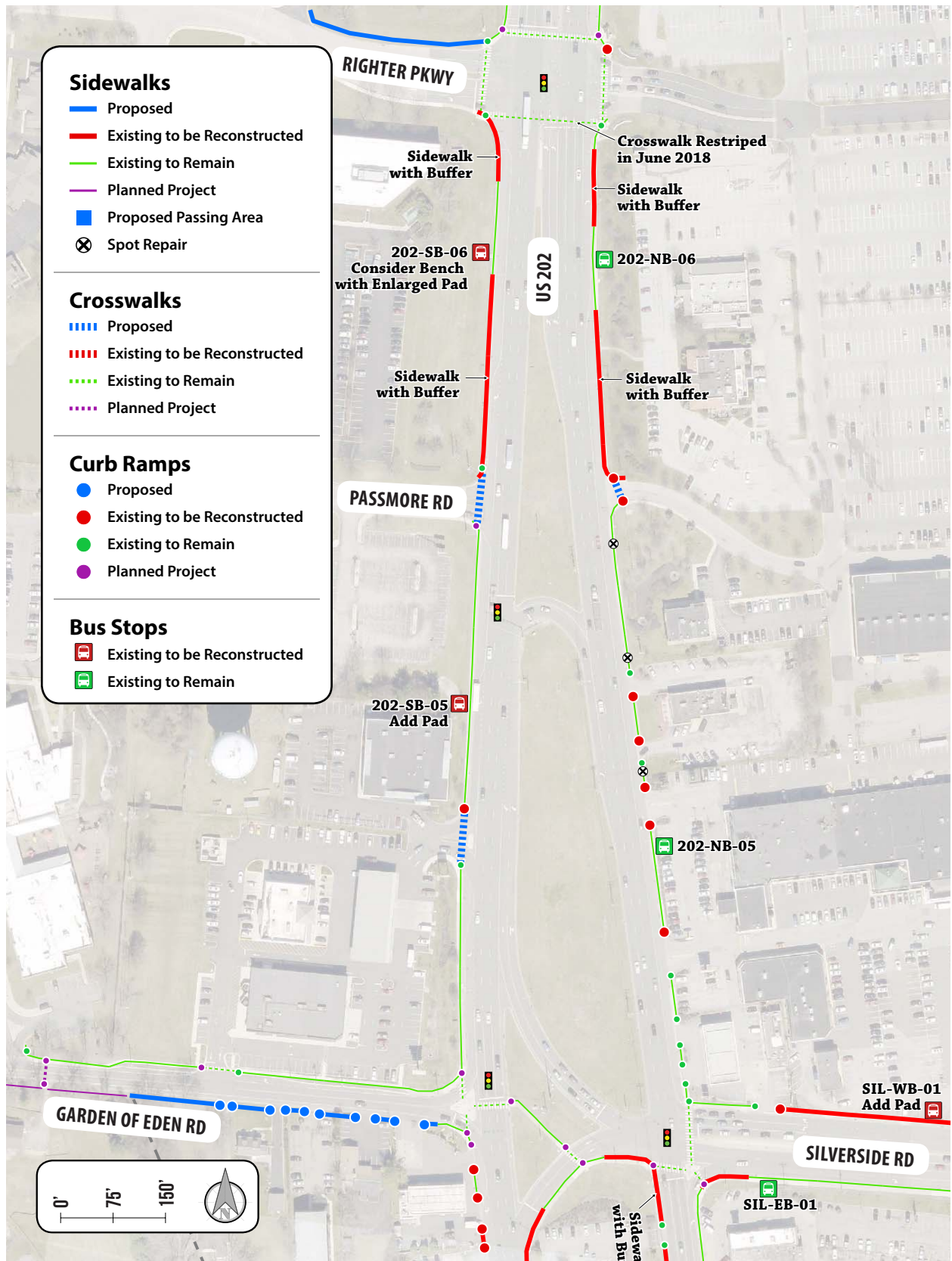
| Improvement Type (units) | Sidewalk (feet) | Curb Ramps (each) | Crosswalks (each) | Bus Stops (each) |
|------------------------------|-----------------|-------------------|-------------------|------------------|
| Proposed | 1,500 | 36 | 19 | 0 |
| Existing to be Reconstructed | 4,500 | 47 | 0 | 11 |
| Existing to Remain | 10,300 | 56 | 34 | 7 |
| Planned Project | 1,900 | 48 | 9 | 0 |
| Sidewalk Passing Area | 11 | NA | NA | NA |
| Spot Repair | 20 | NA | NA | NA |

Figure 11 – Proposed PAR Network - Sheet 1 of 7

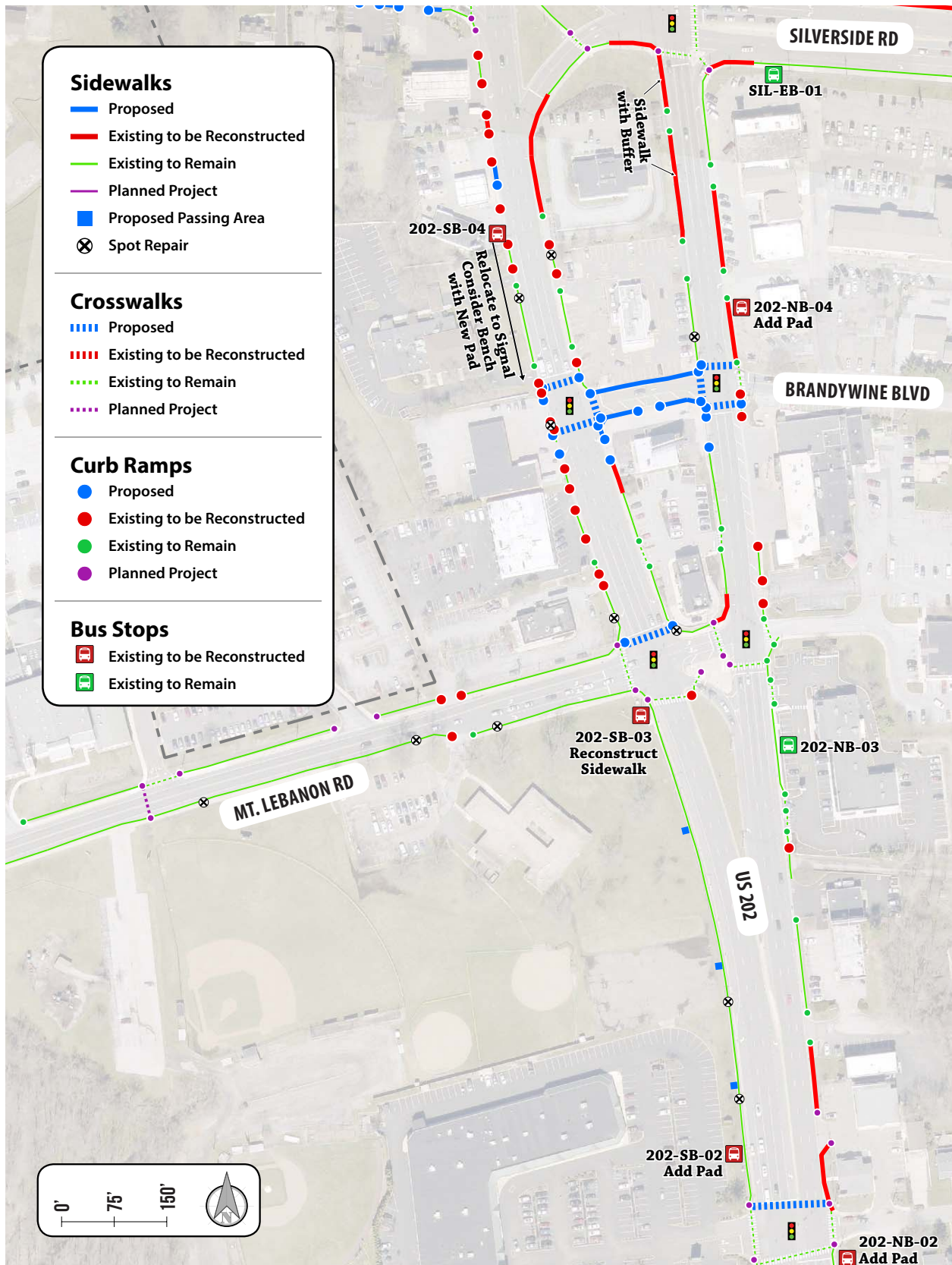


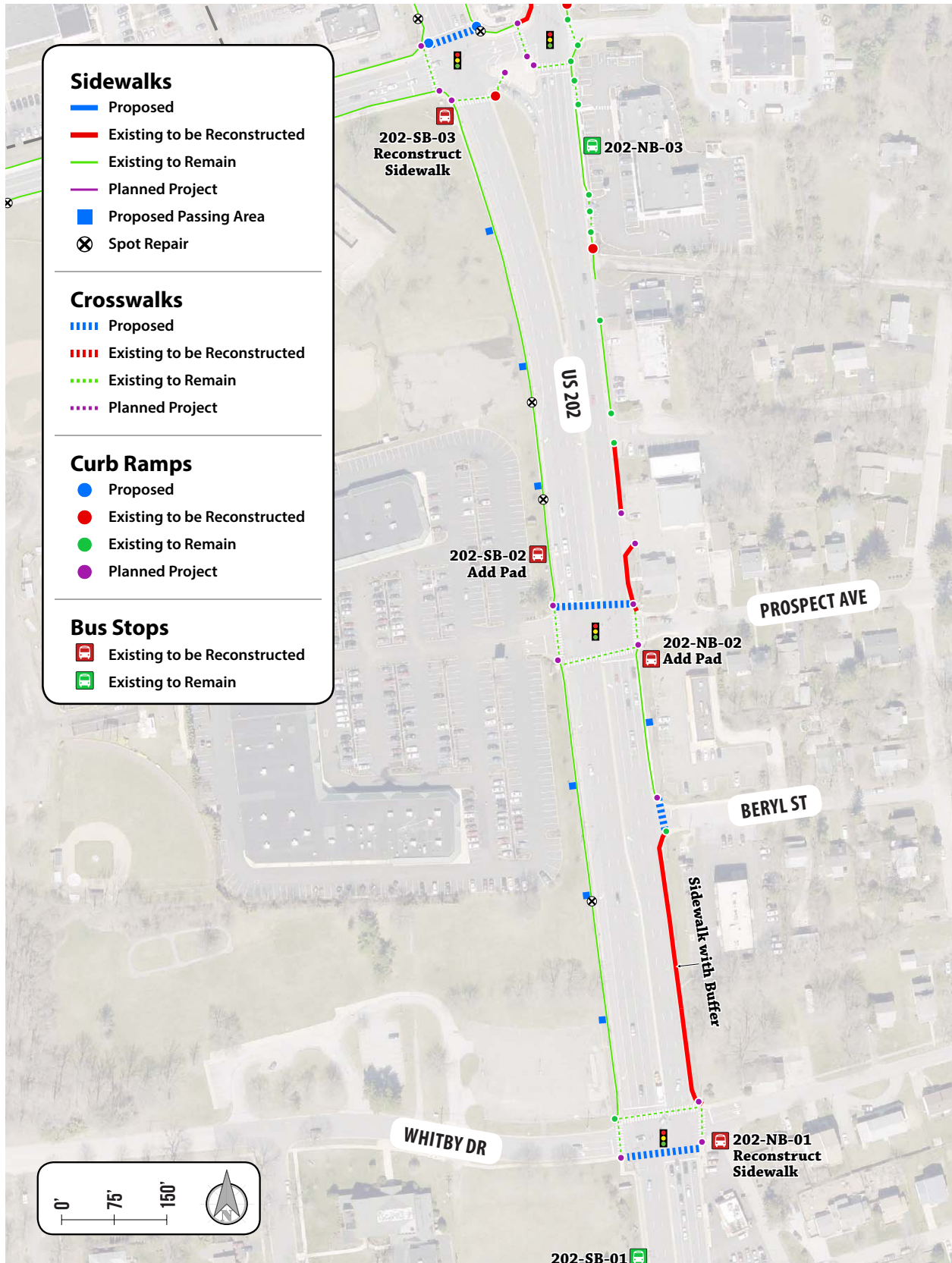


Proposed PAR Network - Sheet 3 of 7

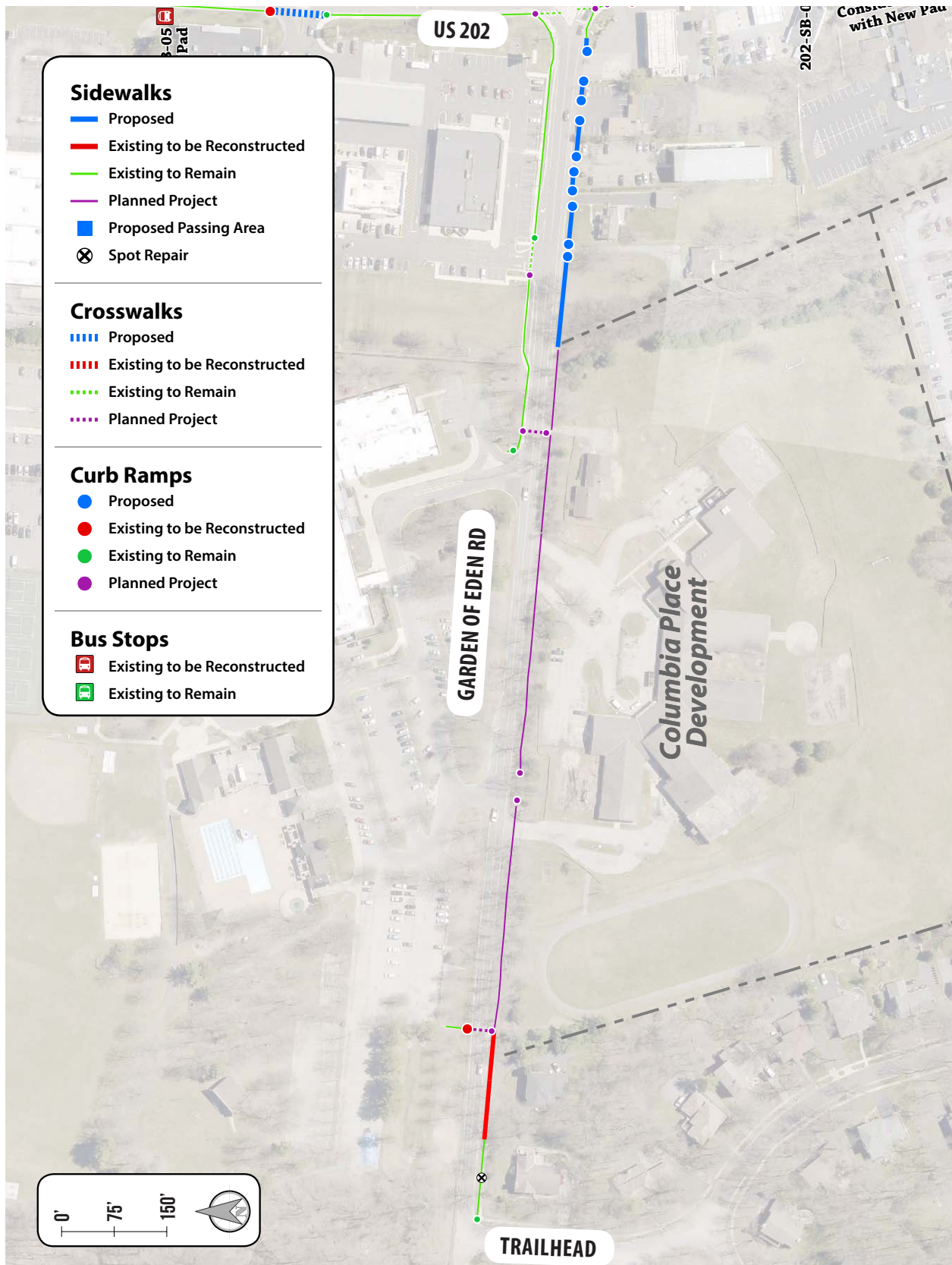


Proposed PAR Network - Sheet 4 of 7

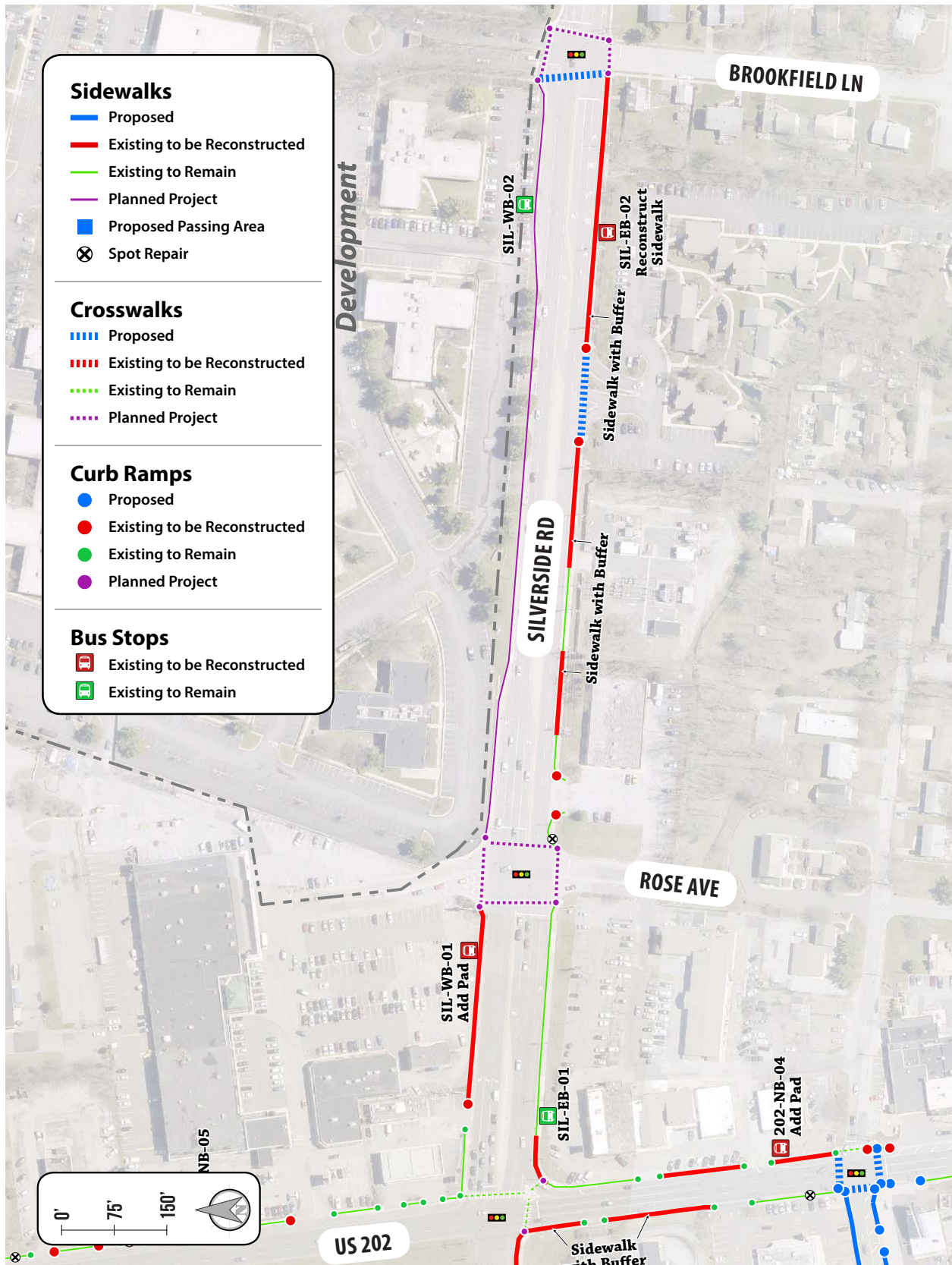




Proposed PAR Network - Sheet 6 of 7



Proposed PAR Network - Sheet 7 of 7



Proposed Facilities

The proposed PAR network includes 36 new curb ramps, which is in addition to the 48 curb ramps that will be added through planned development projects and DeIDOT's on-going curb ramp design project (Contract T201701401). Specific curb ramp orientations and types will be determined during the design phase. The network also includes approximately 1,500 feet of new sidewalk. Per the 2018 PAS, new sidewalk should have a minimum 5-foot width and, in areas where sufficient right-of-way is available or can readily be obtained, should include a minimum 3-foot buffer from the back of curb. The majority of new sidewalk is concentrated in two areas:

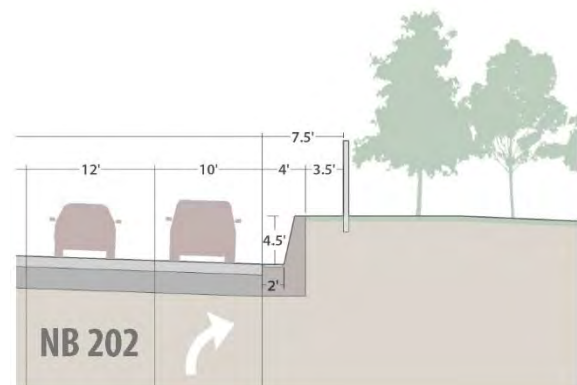
Segment #1 – Northbound US 202 near Rocky Run Parkway

Approximately 600 feet of new sidewalk is recommended along northbound US 202 to address a PAR gap between Rocky Run Parkway and the Rocky Run Creek. **Figure 12** shows a concept plan for the proposed sidewalk alignment.

The land adjacent to the proposed sidewalk is higher than the roadway grade and retained by a 4.5 foot high barrier. **Figure 13** shows potential options to address the grade difference that were evaluated during this study. These include: (1) constructing new sidewalk adjacent to the existing barrier and adding a railing; (2) constructing new sidewalk at roadway grade by removing the barrier and re-grading the slope; and (3) constructing new sidewalk at roadway grade by replacing the barrier with a new retaining wall. For each of these options, the recommended section includes a buffer area between the adjacent right turn lane and sidewalk. While detailed right-of-way information was not available for this study, measurements scaled from a historical land development plan for parcel 0602900009 (included in **Appendix B**) show that up to 30 feet of right-of-way may be available adjacent to the parcels owned by Widener University. These options should be evaluated further during engineering design based on updated right-of-way and topographic survey information.



Missing sidewalk at Widener University



Existing section at barrier location

At the north end of the segment near Rocky Run Parkway, a short curb wall will likely be needed to avoid impacts to the Widener University sign. Feasibility considerations at the south end of the segment include an existing light pole located 57" from the curb and a drainage area containing a culvert endwall. Potential options to avoid the light pole include installing sidewalk adjacent to the curb or placing the sidewalk behind the pole. Re-grading will be necessary adjacent to the drainage area to provide enough space for a 5-foot wide sidewalk. It is also recommended that the median island at the southern driveway to Widener University be enlarged to provide enough space for a pedestrian refuge with curb ramps.

Figure 12 – Concept Plan for Widener Frontage Sidewalk

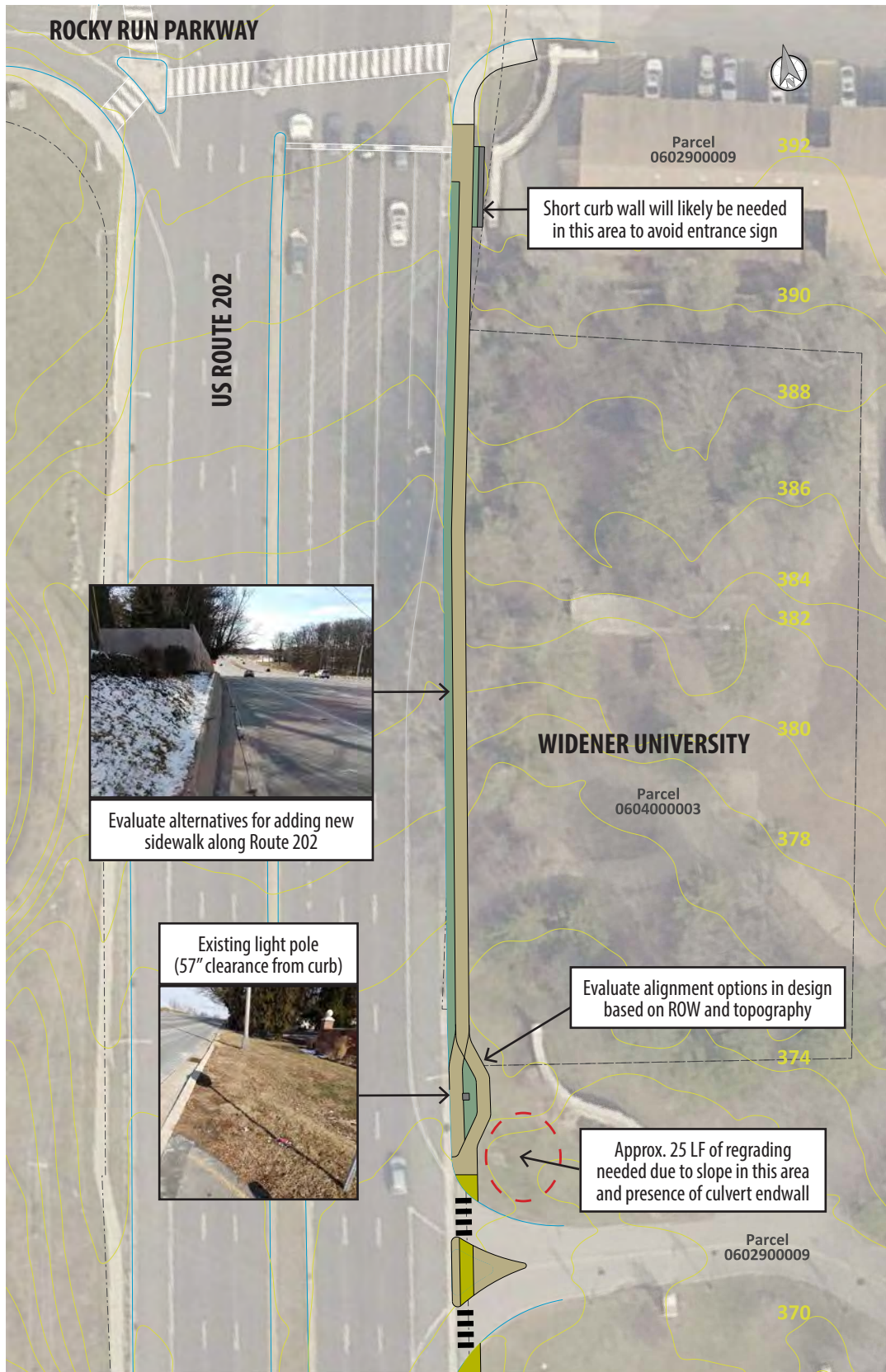
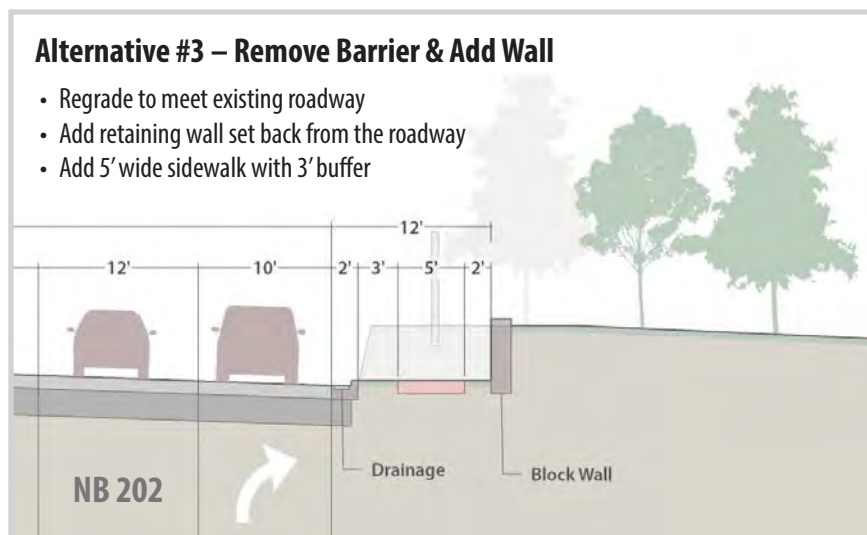
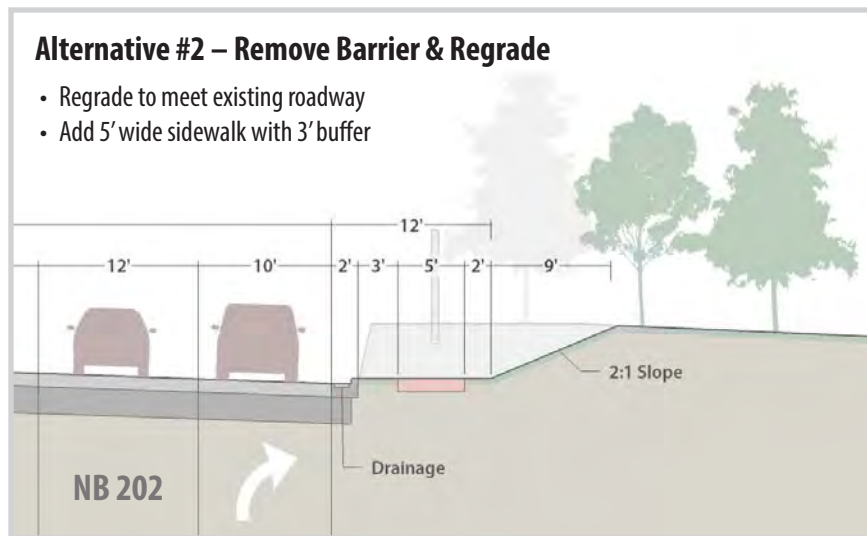
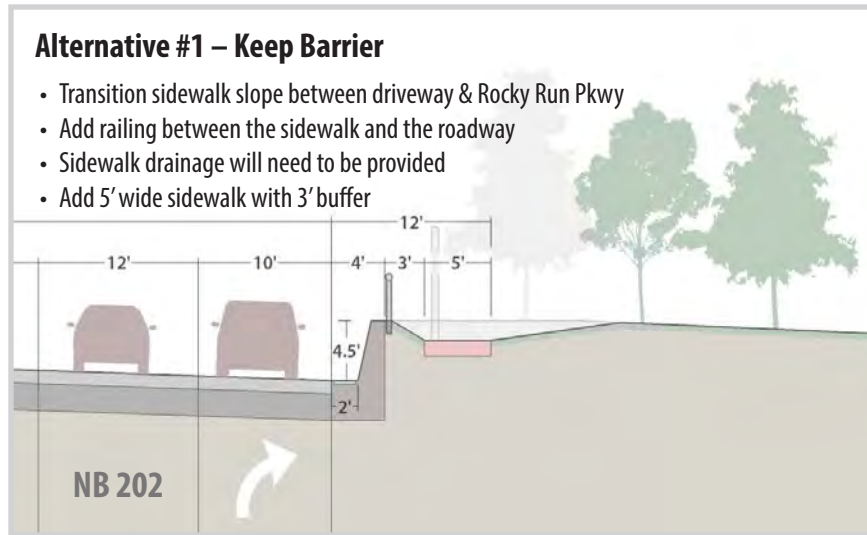


Figure 13 – Alternative Sections for Widener Frontage Sidewalk



Segment #2 – Garden of Eden Road

Approximately 400 feet of new sidewalk is recommended along the south side of Garden of Eden Road between US 202 and the planned Columbia Place development. The western end of this segment would connect with future sidewalk shown in the Columbia Place development plans (attached in [Appendix B](#)), thereby creating a continuous PAR between US 202 and the trailhead on the west end of Garden of Eden Road.

Figure 14 shows a concept plan for the proposed sidewalk segment, which crosses four private properties. Two of these properties contain residential uses with lawn frontages, while the other two are commercial uses with paved parking lots extending to the curblines. A defined pedestrian space should be provided across each of the properties, which may require driveway modifications on the commercial properties as shown in the concept plan. Grading and drainage layouts will determine whether curb ramps are needed at each of the driveway crossings.



Missing sidewalk on Garden of Eden Rd

The extent of available right-of-way will be an important consideration for this segment. While detailed right-of-way information was not available for this study, a historical land development plan for parcel No. 605100010 (included in [Appendix B](#)) shows that the first 15 feet on it is "Dedicated to Public Use," which would provide enough space for a 5-foot wide sidewalk with buffer area. Historical development plans were not available for the other parcels.

Existing Facilities to be Reconstructed

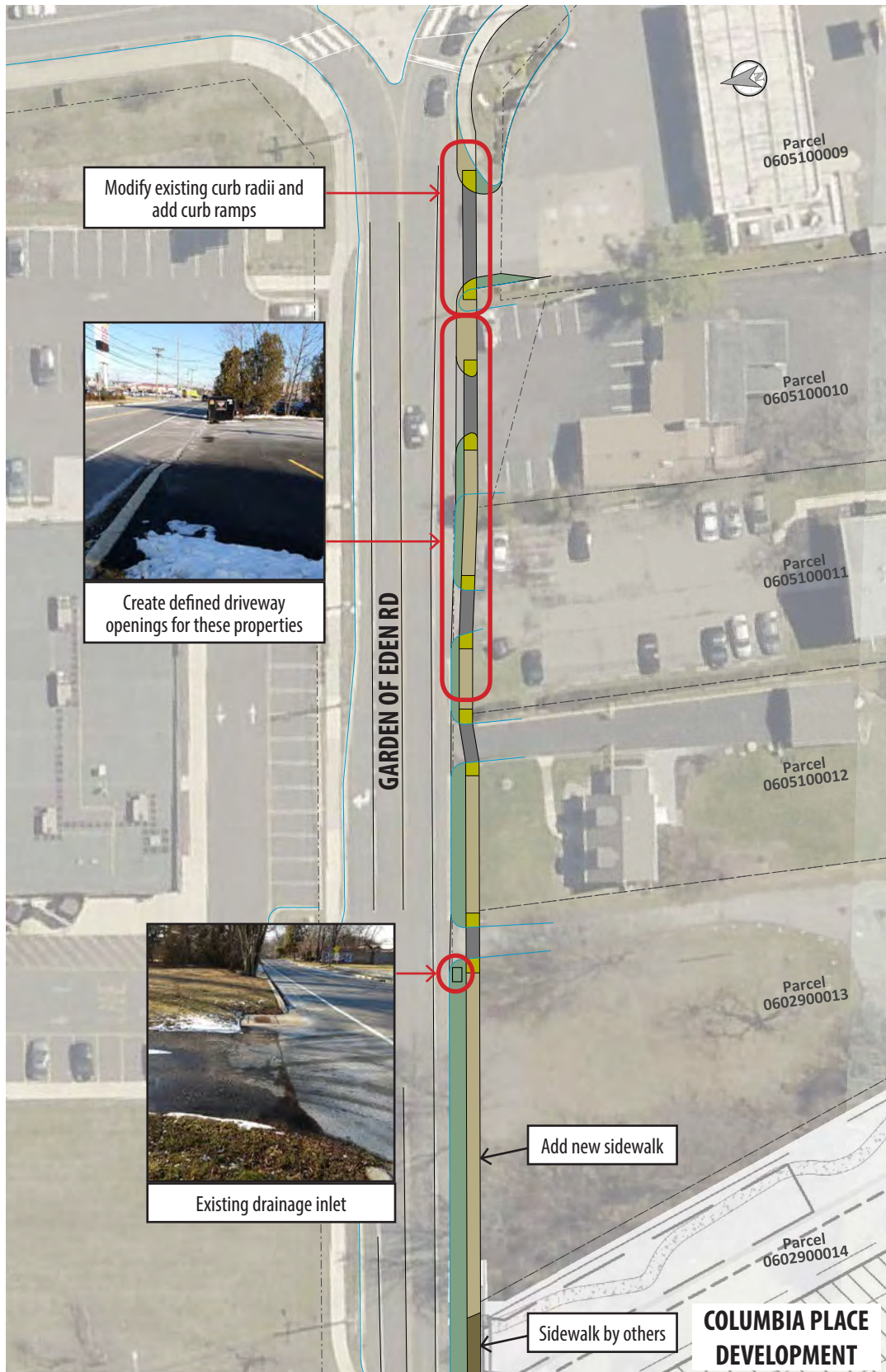
The proposed PAR network includes reconstructing 47 curb ramps and approximately 4,500 feet of sidewalk to meet 2018 PAS standards. The sidewalk to be reconstructed either exceeds a 2% cross-slope or is less than 48" wide. Recognizing the accuracy limitations of smart levels and effect that environmental factors such as temperature can have on field measurements, sidewalk segments that were measured having cross slopes between 2-3% should be re-evaluated during the design phase. Per the 2018 PAS, reconstructed sidewalk should have a minimum 5-foot width and, in areas where sufficient right-of-way is available or can readily be obtained, should include a minimum 3-foot buffer from the back of curb. Recommended buffer areas are noted on [Figure 11](#).

Sidewalk Passing Areas

Per the 2018 PAS, passing areas must be provided along new sidewalks having a width less than 5 feet. Passing areas are to be at least 5 foot wide by 5 foot long and must be provided at intervals not to exceed 200 feet. Driveways can be used as passing areas provided they meet dimensional requirements. Based on existing sidewalk widths and driveway locations, it was determined that three areas along US 202 will require the addition of passing areas (shown [Figure 11](#)):

- Southbound US 202 between Whitby Drive and Mt. Lebanon Road (6 passing areas)
- Northbound US 202 just north of Righter Parkway (5 passing areas)
- Northbound US 202 between Beryl St and Prospect Ave (1 passing area)

Figure 14 – Concept Plan for Garden of Eden Road Sidewalk



Spot Repairs

The sidewalk plan includes a category for “spot repairs” to address isolated deficiencies located in stretches of sidewalk that are otherwise compliant. Examples include vertical elevation differences, horizontal gaps, and obstructions. Identifying these areas helps to minimize the amount of sidewalk that need to be reconstructed. Potential repair treatments for the 20 spot locations shown on **Figure 11** will be developed at the design stage and may include beveling discontinuous surfaces, widening sidewalk near obstructions, or replacing short segments of sidewalk.

Crosswalks at Driveways

Marked crosswalks are an important feature in defining the pedestrian access route and increasing the visibility of pedestrians to motorists turning in and out of businesses. New sidewalks, curb ramps, and crosswalks that were added with the recent Taco Bell and Seasons Pizza developments provide a model for pedestrian design in this corridor.



New crosswalk installation at Seasons Pizza

At commercial driveways where curb ramps are being added or reconstructed, the use of high-visibility crosswalks should be considered based on criteria established by DeIDOT’s Traffic Section.

Figure 11 shows recommended locations for new crosswalk markings at major driveways and side street crossings. Other commercial driveways within the corridor should be evaluated for marked crosswalks based on the estimated volume and speed of vehicles using these driveways.

Pedestrian Improvements at Signalized Intersections

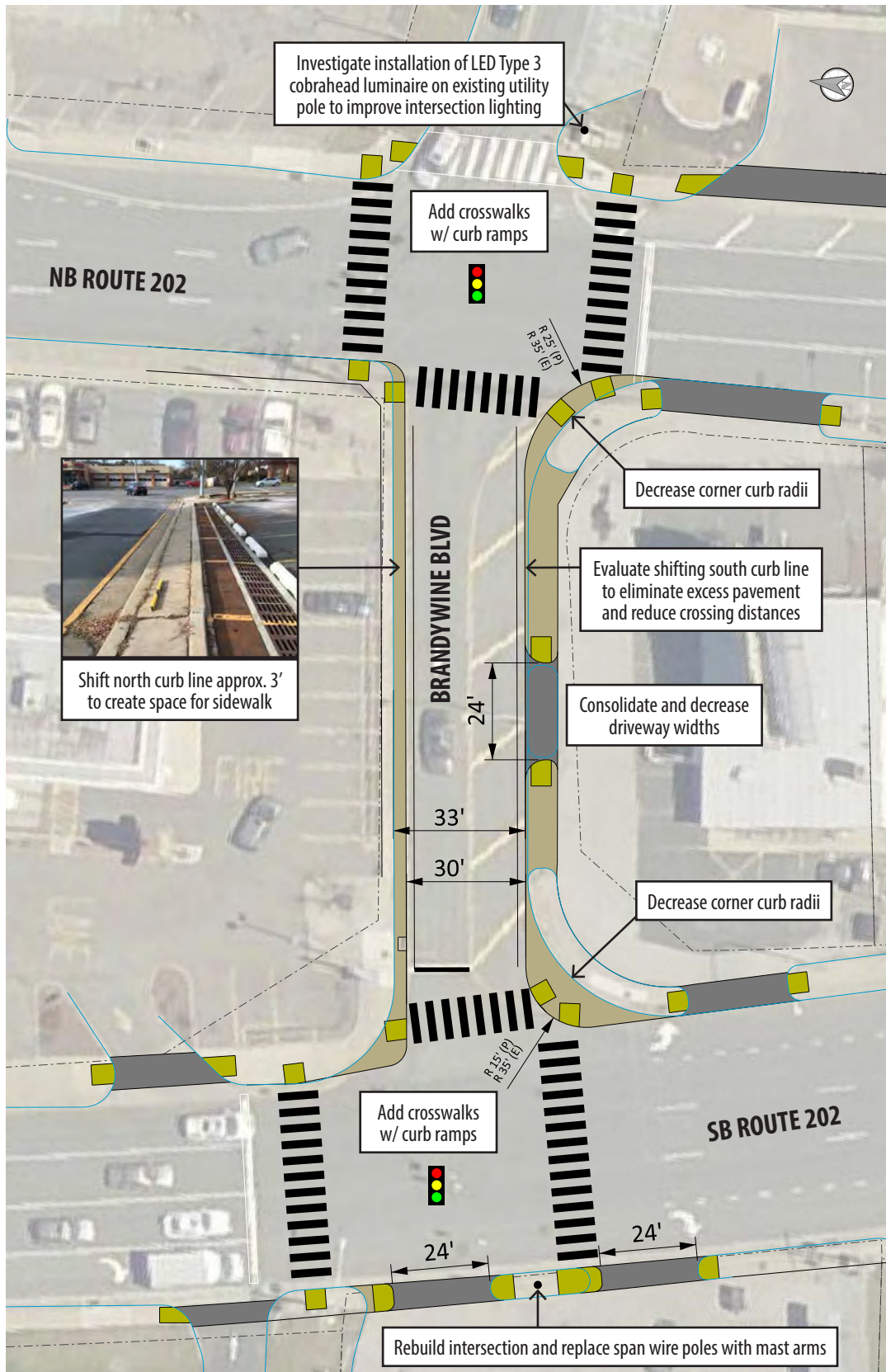
The existing conditions evaluation identified several types of deficiencies at signalized intersections. Recommendations related to signal equipment or operations – including push buttons, pedestrian clearance times, pedestrian recall, and missing pedestrian crossings – are provided in this section. Recommendations related to PAR elements at signalized intersections – including curb ramps and sidewalks – are addressed in previous sections (except where specifically noted).

The recommendations in this section are organized into three main categories – pedestrian improvements at the US 202/Brandywine Boulevard intersection, new crosswalks at other signalized intersections, and maintenance items. A signal timing analysis was performed to evaluate locations where new crosswalks are proposed. This analysis compared the required pedestrian crossing time to the available concurrent vehicle time at each location to gauge potential impacts to signal operations. Results from the analysis are included in **Appendix F** and noted where relevant.

US 202/Brandywine Boulevard Intersection

The signalized intersections of Brandywine Boulevard with northbound and southbound US 202 are challenging for pedestrians due to frequent driveways, multiple curb barriers, sidewalk obstructions, and a lack of pedestrian crossing opportunities into the median area. **Figure 15** shows a concept plan for improving pedestrian circulation and accessibility in this area. The concept plan was reviewed with DeIDOT’s Traffic Section and modified based on feedback provided.

Figure 15 – Concept Plan for Brandywine Boulevard Intersection



On the north side of Brandywine Boulevard, a 5-foot wide sidewalk can be provided by shifting the curb line approximately 3 feet to the south. On the south side of Brandywine Boulevard, consolidation and narrowing of driveways is recommended to reduce pedestrian crossing distances and exposure to turning vehicles. Shifting the curb line north to eliminate some or all of the striped buffer area would further reduce crossing distances and should be evaluated during the design phase.

New marked crosswalks are recommended across both directions of US 202 and Brandywine Boulevard to allow pedestrian access to businesses in the median. Results from the signal timing analysis show that four of the six new crossings can be accommodated within the concurrent vehicle phases, while the two new crossings of southbound US 202 are within one second of the concurrent phase. At two of the proposed crossing locations – the southern crossing of SB US 202 and the eastern crossing of Brandywine Boulevard – vehicles would be making left turns at the same time pedestrians are crossing. A Lead Pedestrian Interval (LPI) should be considered at these locations, along with R10-15 signs to reinforce that turning vehicles must yield to pedestrians.



PAR obstruction along southbound US 202

To comprehensively address signal-related deficiencies at this intersection, the recommendation from DelDOT's Traffic Section was to rebuild the traffic signal and replace the existing spanwire poles with mastarms. This would eliminate a PAR obstruction along southbound US 202 and better locate the vehicular signal heads per current standards. At both intersections with US 202, curb radii reductions are recommended to reduce vehicle turning speeds and shorten pedestrian crossing distances. Turning templates were analyzed for the conceptual design and results show that an aerial fire truck can make all necessary turns. Output from the Autoturn runs are included in [Appendix F](#).

Crosswalks at Signalized Intersections

New marked pedestrian crossings are recommended for the northern leg of the southbound US 202/Mt. Lebanon Road intersection and the southern leg of the US 202/Whitby Drive intersection (shown on [Figure 11](#)). These crosswalks will improve pedestrian access to activity generators including businesses and bus stops. Results from the signal timing analysis described earlier shows that these two crosswalks can be accommodated within existing concurrent vehicle phases and thus would not have a significant effect on traffic operations.

Recommended crosswalks at the missing legs of US 202/Prospect Ave, US 202/Rocky Run, and Silverside Road/Brookfield Lane will need further analysis in the design phase to determine potential traffic impacts. New pedestrian crossings at the U-turn north of Silverside Avenue, the northern legs of the US 202/Silverside Road intersection, and the northern leg of northbound US 202/Mt. Lebanon Road are not recommended at this time, but could be considered in the design phase based on further analysis.

Maintenance Recommendations

A list of "quick fix" maintenance-level items related to push buttons, pedestrian signal timing, and crosswalk striping was developed over the course of this study (included in [Appendix F](#)). These items were addressed in the spring of 2018 under the direction of DelDOT's Traffic Section.

Transit Recommendations

Transit recommendations include reconstructing non-compliant sidewalk in front of three bus stops and adding compliant pads at five locations where none exist today. In addition, DTC’s draft *Bus Stop Facilities Matrix* (included in **Appendix G**) was used to evaluate the need for amenities such as benches or shelters at existing stops. Proposed bus stop improvements are listed in **Table 6** and shown on **Figure 11**. Coordination with DTC will be necessary early in the design phase to confirm the exact location and extent of these improvements.



Riders waiting for bus at US 202 / SB Rocky Run Parkway stop

Bus stop relocations/consolidations were also considered as part of this study. It is recommended that bus stop 202-SB-04 be relocated from its current midblock location – which is non-compliant and constrained in width – to a new location adjacent to the intersection. This will provide more space for transit riders and, in combination with new crosswalks at Brandywine Boulevard, encourage riders to access the median area at the intersection rather than crossing midblock. Relocating bus stops at Righter Parkway and Silverside Road closer to the signalized intersections may also help to encourage use of the crosswalks. Any potential bus stop relocations and/or consolidations should be coordinated with DTC early in the design phase.

Table 6 – Transit Recommendations

| Location | Bus Stop IDs | Existing Amenities | On | Off | Recommendations |
|-------------------------------|--------------|--------------------|----|-----|-----------------------------------|
| US 202 @ Florence Ave | 202-NB-01 | None | 0 | 5 | Reconstruct SW |
| US 202 @ Prospect Ave | 202-NB-02 | None | 1 | 13 | Add Pad |
| US 202 @ Seasons Pizza | 202-NB-03 | Pad | 2 | 15 | - |
| US 202 @ Brandywine Blvd | 202-NB-04 | None | 3 | 27 | Add Pad |
| US 202 @ Silverside Rd | 202-NB-05 | Pad | 2 | 21 | - |
| US 202 @ Concord Sq | 202-NB-06 | Pad | 2 | 17 | - |
| US 202 @ Rocky Run Pkwy | 202-NB-07 | Wide SW | 0 | 35 | - |
| US 202 @ Rocky Run Pkwy | 202-SB-07 | None | 29 | 1 | Consider Shelter w/ New Pad |
| US 202 @ Righter Pkwy | 202-SB-06 | Pad | 12 | 1 | Consider Bench w/ Enlarged Pad |
| US 202 @ Garden of Eden Rd | 202-SB-05 | None | 8 | 1 | Add Pad |
| US 202 @ Silverside Rd | 202-SB-04 | None | 15 | 1 | Consider Bench w/ New Pad |
| US 202 @ Mt. Lebanon Rd | 202-SB-03 | Shelter | 28 | 1 | Reconstruct SW |
| US 202 @ Prospect Ave | 202-SB-02 | None | 3 | 1 | Add Pad |
| US 202 @ Whitby Rd | 202-SB-01 | Shelter | 7 | 1 | - |
| Silverside Rd @ US 202 | SIL-EB-01 | Wide SW | 0 | 11 | - |
| Silverside Rd @ the Commons | SIL-EB-02 | Wide SW | 0 | 1 | Reconstruct SW |
| Silverside Rd @ Concord Plaza | SIL-WB-02 | Shelter | 3 | 1 | - |
| Silverside Rd @ Rose Ave | SIL-WB-01 | None | 8 | 1 | Add Pad |

Roadway Lighting Recommendations

Lighting recommendations were developed in accordance with DeIDOT Lighting Guidelines and reviewed with DeIDOT's Traffic Section. A photometric model of the proposed conditions was created based on the lighting improvements outlined below. Output from the model is included in **Appendix G**.

Lighting at Signalized Intersections

Recommendations to improve lighting levels at signalized intersections along US 202 include:

- **US 202 @ Rocky Run Pkwy:** Investigate installation of new LED Type 3 cobrahead luminaire with at least 20,000 initial lumens in EB approach median, set back 10' from stop bar.
- **US 202 @ Brandywine Blvd – East:** Investigate installation of new LED Type 3 cobrahead luminaire with at least 10,000 initial lumens on existing utility pole on southeast corner of intersection.
- **US 202 @ Mt Lebanon Rd:** Investigate changing the orientation of the utility pole-mounted 250 Watt HPS Type 3 cobrahead luminaire on northwest corner of US 202 southbound and Mt Lebanon Road intersection.
- **US 202 @ Whitby Drive:** Investigate installation of new LED Type 3 cobrahead luminaire with at least 10,000 initial lumens on existing utility pole on southwest corner of intersection.

Lighting Gaps between Continuously Lit Areas

Recommendations to address existing lighting gaps along US 202 include:


- **SB US 202 just north of Whitby Drive:** Investigate installation of utility pole-mounted luminaire on existing pole
- **SB US 202 at Wawa/Fedex Driveway:** Investigate installation of one state-owned cobrahead luminaire near Wawa entrance sign or utility pole-mounted luminaire on a new pole
- **NB US 202 at Bus Stop 202-NB-06 to the south of Righter Parkway:** Investigate installation of utility pole-mounted luminaire on existing utility pole directly at bus stop

Silverside Road

Several factors highlight the need for better lighting on Silverside Road including three pedestrian crashes during dark conditions, the presence of bus stops and other activity generators, and the mixed-use Concord Plaza development, which will likely generate additional pedestrian activity in this area. The recommended next step is to evaluate the feasibility and cost of developing a continuous lighting system along Silverside Road between US 202 and Brookfield Avenue. This would include performing a lighting warrant evaluation at the signalized intersections with Rose Avenue and Brookfield Lane.

Mid-Block Crossings on Garden of Eden Road

Given that DeIDOT currently does not have warrants for lighting at mid-block crossing locations, a study is recommended to evaluate the need for lighting at the existing and proposed mid-block crossings on Garden of Eden Road. Utility poles are present on the north side of Garden of Eden Road, however there are none in close vicinity of the existing crosswalk. Options to add lighting at this location, if necessary, include (a) installing luminaires on two consecutive utility-owned poles near the crosswalk, or (b) installing utility-owned pedestrian-scale light poles on both approaches to the crosswalk. LED luminaires with 5,000-10,000 initial lumen output could provide over 1 foot-candle of illumination at the crosswalk.



US Route 202 (Concord Pike)
Rocky Run Parkway to Whitby Drive
Pedestrian Access Route Assessment

APPENDICES

Appendix A – Meeting Minutes

Appendix B – Plans for Adjacent Projects

Appendix C – Crash Summaries

Appendix D – Sidewalk Inventory & Analysis

Appendix E – Curb Ramp Inventory & Analysis

Appendix F – Traffic Signal Inventory & Analysis

Appendix G – Transit Inventory & Analysis

Appendix H – Lighting Inventory & Analysis

JUNE 2018

CONTRACT NO. T201801101, AGREEMENT 1793 – TASK 3
NEW CASTLE COUNTY, DELAWARE

APPENDIX A

MEETING MINUTES



MEMORANDUM OF MEETING

Project: DeIDOT PAR Agreement 1793, Task Order #3:
US Route 202 Pedestrian Access Route Assessment

Subject: Project Kickoff Meeting

Location: Urban Engineers Office
2 Penns Way, Suite 309
New Castle, DE 19720

Date/Time: 2:00 PM, December 5, 2017

Attended by:

| | | |
|------------------|------------------------|--------------------|
| Tom Nickel | DeIDOT PAR Coordinator | 302-760-2401 |
| John Pietrobono | Urban Engineers | 302-689-0260 x1501 |
| Walter Mudrowsky | Urban Engineers | 302-689-0260 x1235 |
| Erika Rush | Urban Engineers | 215-922-8081 x1654 |
| John Federico | Urban Engineers | 215-922-8081 x1358 |

A kickoff meeting for Task Order #3 – US Route 202 Pedestrian Access Route Assessment was held on Tuesday, December 5, 2017 at Urban Engineers’ office in New Castle, DE. The purpose of the meeting was to review the scope, schedule, coordination, and client expectations for this assignment. Specific discussion items are described below based on the agenda (attached with minutes):

Client Expectations

- DeIDOT’s expectations for this project are to (a) assess the ADA-compliance of pedestrian facilities within the corridor; (b) perform a holistic assessment of pedestrian circulation; and then (c) develop recommendations to address identified needs.
- The study’s final report will be used to develop the scope of the design project for this corridor, and can also be leveraged by the Traffic Management Unit for the purposes of data collection and performance measures.
- DeIDOT and Urban agreed on targeting the Spring of 2018 for completing this assignment.

DeIDOT Coordination

- The scope of work includes coordination with various DeIDOT units to obtain crash data, traffic data, and bus ridership information. Tom asked Urban to develop a shotgun email that can be sent to each unit within DeIDOT, and will send Urban a sample email and list of contacts.
- Urban should coordinate with Vince Davis to develop a planning-level understanding of potential stormwater management considerations for the study area
- Urban will contact Development Coordination to obtain information about current and proposed development projects within the study area. Tom made note of two projects:
 - A redevelopment project on the north side of Silverside Rd across from Brookefield Ave.
 - A 55+ housing development on the south side of Garden of Eden Road

Data Collection Considerations

- The DeIDOT Gateway data from 2012 provides a “snapshot” assessment of ADA facilities at that time. While there have been two updates to the Gateway data since 2012, the system is not being updated on a regular basis and planning efforts such as this have not been used to update the data. Information related to ADA compliance is collected based on inspections when projects are constructed. Therefore, Urban will not need to return GIS data to DeIDOT in a format that is compatible with Gateway.
- DeIDOT has commitments to past pavement rehabilitation projects within the study area and is under agreement with AECOM (Contract T201701401) to design some of the curb ramps at intersecting public roads. Urban will not need to assess ADA compliance for these ramps, as they will be made compliant through a separate construction project. Tom shared preliminary plans with Urban showing the preliminary curb ramps designs, and will send Urban a KMZ file locating the ramps.
- Tom is aware of an ADA complaint from a constituent who had difficulties walking between the Taco Bell in the Route 202 median and the nearby TD Bank. The study should address this complaint.
- A citizen asked why a section of sidewalk is missing along NB Route 202 in front of the Widener University Law School. Urban will address this area of missing sidewalk in the study.

Action Items

As a result of the meeting discussion, the following actions will be taken:

| Item | Action | Party |
|------|---|--------|
| 1. | Send Urban a sample request email and list of contacts | DeIDOT |
| 2. | Coordinate with DeIDOT Units as needed for project data | Urban |
| 3. | Send KMZ file to Urban showing AECOM ramp locations | DeIDOT |

It is believed that the enclosed represents an accurate description of the major events that transpired at this meeting. Your notification of any errors or omissions is essential, as the foregoing is intended to be part of the record, and is the basis upon which we will proceed.

Respectfully submitted,

URBAN ENGINEERS, INC.



John Federico, PE, PP, AICP

cc: Attendees
Project File

Att: Agenda

Task Order #3
Pedestrian Access Route and Safety Audit Project
US Route 202

Project Kickoff Meeting

Tuesday, December 5, 2017, 2:00 PM
Urban Engineers Office
2 Penns Way, Suite 309
New Castle, DE 19720

AGENDA

- Client Expectations

- Project Overview:
 - Scope of Work
 - Data Collection
 - Curb Ramp Designs by Others
 - Project Schedule
 - Format of Deliverables
 - GIS Data for Gateway

- DelDOT Coordination Meetings:
 - Transportation Solutions (Current & Proposed DelDOT Projects)
 - Development Coordination (Current & Proposed Developer Projects)
 - Traffic/Traffic Safety (Counts, Crash Data, Lighting)

- Additional Items / Next Steps



MEMORANDUM OF MEETING

Project: DeIDOT PAR Agreement 1793, Task Order #3:
US Route 202 Pedestrian Access Route Assessment

Subject: Progress Meeting #1

Location: Urban Engineers Office
2 Penns Way, Suite 309
New Castle, DE 19720

Date/Time: 1:00 PM, March 6, 2018

Attended by:

| | | |
|------------------|------------------------|--------------------|
| Tom Nickel | DeIDOT PAR Coordinator | 302-760-2401 |
| John Pietrobono | Urban Engineers | 302-689-0260 x1501 |
| Walter Mudrowsky | Urban Engineers | 302-689-0260 x1235 |
| John Federico | Urban Engineers | 215-922-8081 x1358 |

A progress meeting for Task Order #3 – US Route 202 Pedestrian Access Route Assessment was held on Tuesday, March 6, 2018 at Urban Engineers’ office in New Castle, DE. The purpose of the meeting was to review progress on this task order and determine the appropriate next steps. John Federico presented Urban’s findings from the existing conditions inventory and analysis tasks by paging through a summary progress report with the group. After reviewing the report, John presented a set of initial recommendations for consideration and discussion.

Specific discussion items are described below based on the agenda (attached with minutes):

Review of Task A (Existing Conditions) and Task B (Analysis)

- **Transit Inventory** – Tom asked if the running slope was recorded at bus stops. John F. replied that the running slope at bus stops was recorded under the sidewalk inventory, and will add these values to the transit inventory sheet.
- **Transit Inventory** – Tom recommended that Urban coordinate with DTC to see if they have any ridership thresholds for adding shelters and other amenities at bus stops.
- **Lighting Inventory** – Tom recommended that Urban coordinate with DeIDOT’s Traffic Section to determine their policy on lighting midblock crosswalks.

- **Traffic Signal Inventory** – Tom noted that the “reach” column on the push button inventory sheets was not filled out and asked if this measurement was recorded. John F. said he will check with staff to see if this information was recorded, and if not, Urban will obtain.
- **Curb Ramp Inventory** – In general, DWS are necessary at signalized, stop, and yield controlled driveways where curb ramps are present, and may also be appropriate at other high volume and/or high-speed driveways. Tom noted that DeIDOTs Traffic Section considers high volume driveways as having a minimum ADT of 400 vehicles/day. Tom said it would be useful to employ this approach for driveway crossings where the need for a DWS is unclear. Where volumes are not available, the ITE trip generation rates for adjacent land uses can be used as a proxy. Following this discussion, Tom provided guidance as to whether DWS are applicable at a number of driveways that were noted as “candidates” in the curb ramp tables. Urban will employ the ITE-based approach for any relevant driveways that were not reviewed.
- **Sidewalk Inventory** – Tom was in agreement with the methodology and criteria used to characterize the sidewalk segments, but asked that Urban show sidewalk segments having a cross-slope between 2-3% as a separate category.

Discussion of Task C (Initial PAR Recommendations)

- **Bus Stops** – Tom noted that DTC is currently developing a bus stop policy to provide guidance on bus stop siting. Urban should review the transit inventory with Bill Thatcher at DTC to determine how this policy may influence bus stop recommendations within the corridor.
- **Sidewalk Recommendations** – John F. asked about DeIDOT’s preference regarding buffer areas for sidewalks. Tom said providing a buffer area is desirable if it can be accommodated within the existing right-of-way. Tom suggested contacting Marc Cote with DeIDOT’s Development Coordination to see if development plans or other right-of-way information is available for the sites where buffers are recommended. Tom offered to assist with this effort.
- **Detail Area #1: Widener University Frontage** – Urban presented three options for adding sidewalk along the Widener University frontage. Tom said that each of the alternatives appeared viable, though he noted that Alternative #1 (which locates the sidewalk behind the existing barrier) would likely be undesirable from a maintenance perspective unless it was owned and maintained by Widener. Urban will check if the property has been designated as historic.
- **Detail Area #2: Garden of Eden Road Sidewalk** – Tom affirmed that an important goal of this project is to establish a continuous and accessible pathway between Concord Pike and the State Park on at least one side of Garden of Eden Road. Most of the north side of the road lacks sidewalk and is challenging to retrofit due to slope and drainage issues. Therefore, it was agreed to continue evaluating the feasibility of sidewalk along the south side, including the availability of right-of-way and impacts to frontages.
- **Detail Area #3: Concord Pike/Brandywine Blvd Intersection** – Tom expressed support for the concept plan with the modifications outlined below. The modified concept plan should be reviewed with DeIDOT’s Traffic Section for their feedback.

- At the Brandywine West intersection, investigate the feasibility of replacing the span wire poles with mast arms to remove the existing sidewalk obstruction
 - Run the turning template for a hook and ladder truck through the intersection to check the curb radii and adjust as needed
 - Consider bumping out the south side curb line to reduce crossing distances and provide more space for curb ramps, subject to the turning templates noted above
- **Lighting Recommendations** – Tom said the initial lighting recommendations should be reviewed with DeIDOT’s Traffic Section prior to conducting a photometric analysis of the proposed conditions.

Action Items

As a result of the meeting discussion, the following actions will be taken:

| Item | Action | Party |
|------|--|-------|
| 1. | Add running slope data to the transit inventory sheets | Urban |
| 2. | Confirm if “reach” dimensions were recorded for push buttons | Urban |
| 3. | Modify sidewalk inventory to include a category for cross-slopes between 2-3% | Urban |
| 4. | Modify concept plan for Concord Pike/Brandywine intersection based on DeIDOT feedback | Urban |
| 5. | Check if the Widener property is designated as historic | Urban |
| 6. | Check with Marc Cote on the availability of development plans showing ROW for parcels where new sidewalk is recommended | Urban |
| 7. | Coordinate with DTC to: <ul style="list-style-type: none"> ● determine thresholds for bus amenities ● determine current policy related to bus stop siting ● review transit-related recommendations | Urban |
| 8. | Coordinate with DeIDOT’s Traffic Section to: <ul style="list-style-type: none"> ● determine lighting policy at midblock crosswalks ● review initial lighting recommendations ● review concept plan for Concord Pike/Brandywine intersection | Urban |

It is believed that the enclosed represents an accurate description of the major events that transpired at this meeting. Your notification of any errors or omissions is essential, as the foregoing is intended to be part of the record, and is the basis upon which we will proceed.

Respectfully submitted,

URBAN ENGINEERS, INC.

John Federico, PE, PP, AICP

Attachment: Agenda

APPENDIX B

PLANS FOR ADJACENT PROJECTS

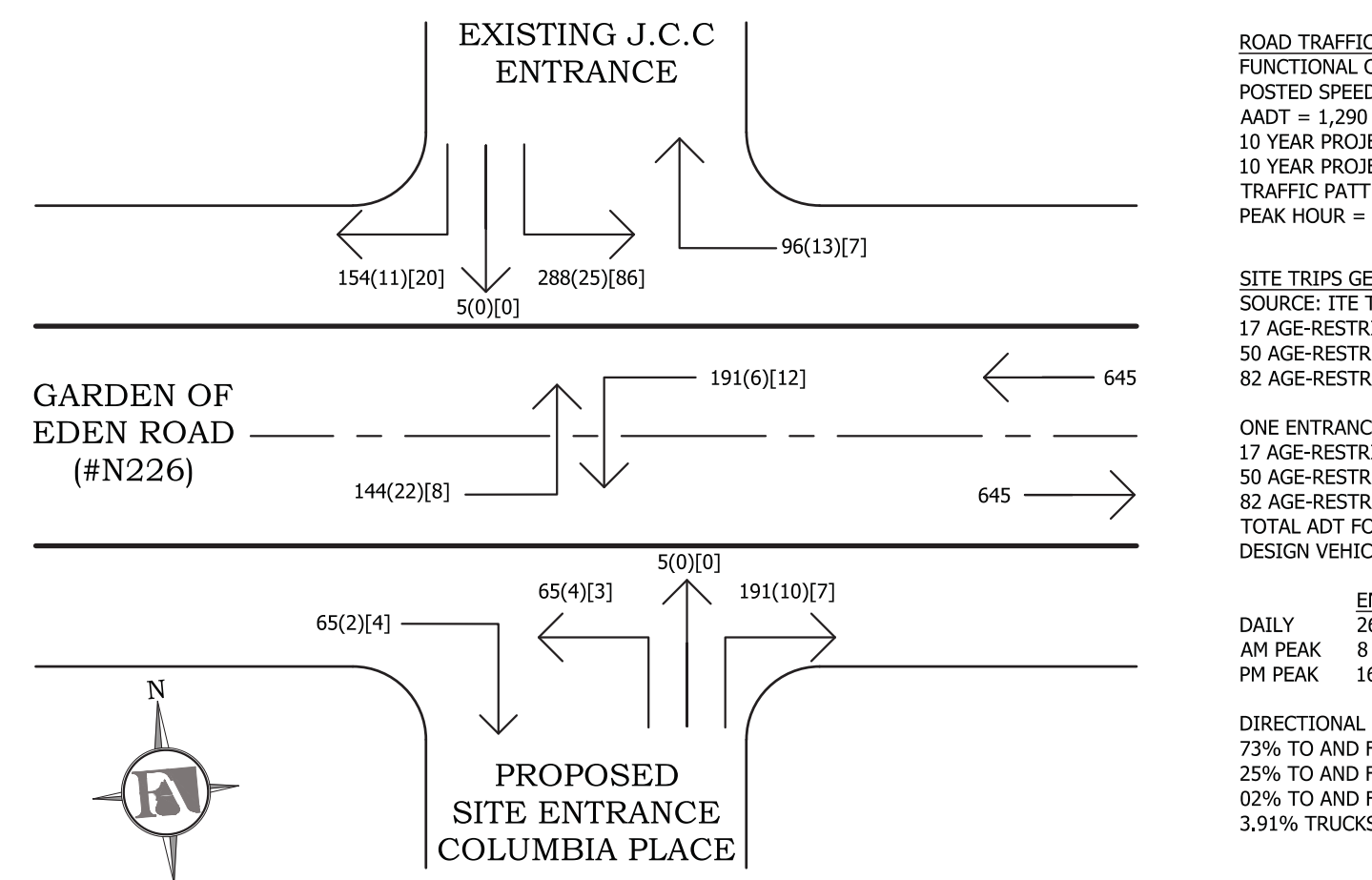
COLUMBIA PLACE

APPENDIX B – PLANS FOR ADJACENT PROJECTS

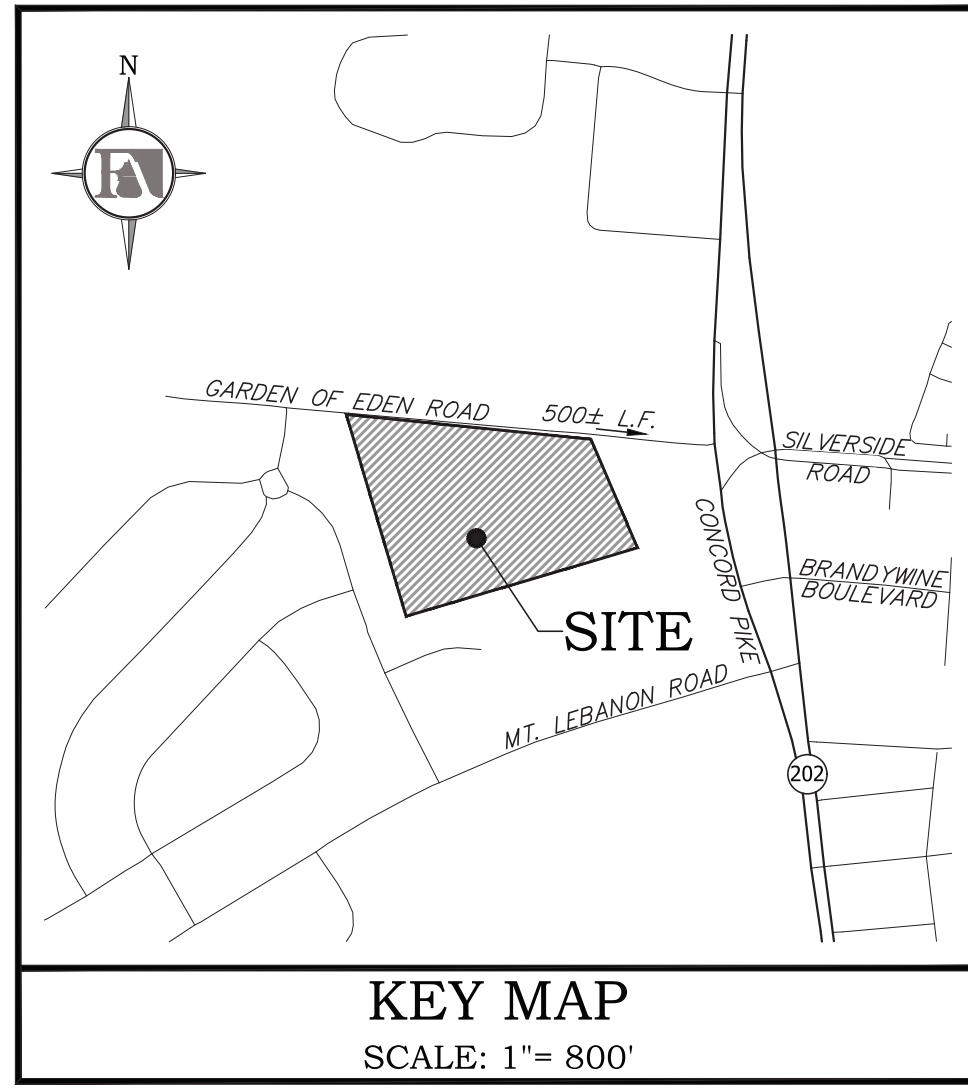
GENERAL NOTES

- 1. ALL ENTRANCES SHALL CONFORM TO THE DELAWARE DEPARTMENT OF TRANSPORTATION'S (DELDOT'S) CURRENT STANDARDS AND REGULATIONS FOR SUBDIVISION STREETS AND STATE HIGHWAY ACCESS AND SHALL BE SUBJECT TO ITS APPROVAL.
2. ALL MATERIALS AND WORKMANSHIP, INCLUDING STORM DRAIN PIPING, INLET, MANHOLE AND END SECTION INSTALLATION, WITHIN THE STATE OF DELAWARE RIGHT-OF-WAY SHALL BE IN ACCORDANCE WITH CURRENT STATE OF DELAWARE DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, SUPPLEMENTAL SPECIFICATIONS, STANDARD CONSTRUCTION DETAILS, SPECIAL PROVISIONS, AND DESIGN GUIDANCE MEMORANDUMS.
3. ALL DISTURBED AREAS WITHIN THE STATE RIGHT-OF-WAY, BUT NOT IN THE PAVEMENT, SHALL BE TOP-SOILED (6" MINIMUM), FERTILIZED, SEEDED AND MULCHED. IF SOIL IS USED NEXT TO SIDEWALK OR SHARED-USE PATH, CONTRACTOR SHALL GRADE TOPSOIL ADJACENT TO THE SIDEWALK OR SHARED-USE PATH PRIOR TO PLACEMENT OF SOIL TO ENSURE THAT SOIL IS PLACED FLUSH OR JUST BELOW EDGE OF SIDEWALK OR SHARED-USE PATH TO AVOID WATER PONDING ON THE SIDEWALK OR SHARED-USE PATH.
4. A 72-HOUR (MINIMUM) NOTICE SHALL BE GIVEN TO THE DELDOT DISTRICT PERMIT SUPERVISOR PRIOR TO STARTING ENTRANCE CONSTRUCTION.
5. MISS UTILITY OF DELAWARE SHALL BE NOTIFIED THREE (3) CONSECUTIVE WORKING DAYS PRIOR TO EXCAVATION, AT 1-800-282-8555.
6. ALL SIGNING, STRIPING AND MAINTENANCE OF TRAFFIC IS THE CONTRACTOR'S RESPONSIBILITY AND SHALL FOLLOW THE GUIDELINES SHOWN IN THE DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (DE MUTCD) FOR STREETS AND HIGHWAYS (LATEST EDITION). THE OWNER OR MAINTENANCE CORPORATION SHALL BE RESPONSIBLE FOR MAINTENANCE OF ALL SIGNS INSTALLED AS PART OF THIS PROJECT.
7. PLAN LOCATION AND DIMENSIONS SHALL BE STRICTLY ADHERED TO UNLESS OTHERWISE DIRECTED BY THE DELDOT DISTRICT PERMIT SUPERVISOR.
8. A COPY OF THE UP TO DATE APPROVED CONSTRUCTION DOCUMENTS AND DELDOT APPROVAL LETTERS SHALL BE MAINTAINED ON THE PROJECT SITE AT ALL TIMES AND BE AVAILABLE FOR INSPECTION BY DELDOT PERSONNEL.
9. EXISTING UTILITIES ARE SHOWN IN ACCORDANCE WITH THE BEST AVAILABLE INFORMATION. COMPLETENESS OR CORRECTNESS THEREOF IS NOT GUARANTEED. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO CONTACT THE UTILITY COMPANIES INVOLVED IN ORDER TO SECURE THE MOST ACCURATE INFORMATION AVAILABLE AS TO UTILITY LOCATION AND ELEVATION. NO CONSTRUCTION AROUND OR ADJACENT TO UTILITIES SHALL BEGIN WITHOUT NOTIFYING THEIR OWNERS AT LEAST 48-HOURS IN ADVANCE. THE CONTRACTOR SHALL TAKE THE NECESSARY PRECAUTIONS TO PROTECT THE EXISTING UTILITIES AND MAINTAIN UNINTERRUPTED SERVICE AND ANY DAMAGE DONE TO THEM DUE TO HIS/HER NEGLIGENCE SHALL BE IMMEDIATELY AND COMPLETELY REPAIRED AT THE CONTRACTOR'S EXPENSE. TO LOCATE EXISTING UTILITIES IN THE FIELD PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL CONTACT MISS UTILITY OF DELAWARE (SEE NOTE #5).
10. SHOULD UTILITY RELOCATION BE REQUIRED, THE DEVELOPER MUST SUBMIT A UTILITY RELOCATION PLAN FOR DELDOT APPROVAL AND A DELDOT UTILITY PERMIT IS ISSUED TO THE UTILITY COMPANY.
11. UPON COMPLETION OF THE CONSTRUCTION OF THE SIDEWALK OR SHARED-USE PATH ACROSS THE PROJECT'S FRONTAGE AND PHYSICAL CONNECTION TO ADJACENT EXISTING FACILITIES, THE DEVELOPER, THE PROPERTY OWNERS OR BOTH ASSOCIATED WITH THIS PROJECT, SHALL BE RESPONSIBLE TO REMOVE ANY EXISTING ROAD TIE-IN CONNECTIONS LOCATED ALONG ADJACENT PROPERTIES, AND RESTORE THE AREA TO GRASS. THESE DISTURBED AREAS SHALL BE TOP-SOILED (6" MINIMUM), FERTILIZED, SEEDED AND MULCHED. SUCH ACTIONS SHALL BE COMPLETED, IN CONFORMANCE WITH DELDOT'S "SHARED-USE PATH AND/OR SIDEWALK TERMINATION POLICY".
12. DELDOT WILL NOT PROVIDE A CERTIFICATE OF OCCUPANCY APPROVAL FOR A COMMERCIAL ENTRANCE TO THE RESPECTIVE LOCAL LAND USE AGENCY UNTIL THE ENTRANCE(S) ARE COMPLETED TO THE SATISFACTION OF THE DEPARTMENT.
13. DESIGN, FABRICATION AND INSTALLATION OF ALL PERMANENT SIGNING SHALL BE AS OUTLINED IN THE DE MUTCD.
14. DESIGN AND INSTALLATION OF ALL PAVEMENT MARKINGS AND STRIPING SHALL BE AS OUTLINED IN THE DE MUTCD. FOR FINAL PERMANENT PAVEMENT MARKINGS:
14.1. EPOXY RESIN PAINT SHALL BE REQUIRED FOR LONG LINE STRIPING.
14.2. THERMO PLASTIC (EXTRUDED OR PREFORMED MATERIAL) WILL BE REQUIRED ON ASPHALT SURFACES, FOR SHORT LINE STRIPING, I.E. SYMBOLS/LEGENDS.
14.3. PERMANENT PAVEMENT MARKING TAPE (PER DELDOT APPROVED MATERIALS LIST) WILL BE REQUIRED ON CONCRETE SURFACES, FOR SHORT LINE STRIPING, I.E. SYMBOLS/LEGENDS.

- 21. REMOVAL OF LONG LINE PAVEMENT STRIPING SHALL BE PERFORMED USING: SHOT OR HYDRO-BLASTING. BREAKAWAY POSTS SHALL BE USED WHEN INSTALLING ALL SIGNS. REFERENCE DELDOT STANDARD CONSTRUCTION DETAIL T-15.
22. CONTRACTOR SHALL SUPPLY MESSAGE BOARDS THAT ARE TO BE PLACED TEN (10) DAYS PRIOR TO CONSTRUCTION ALONG GARDEN OF EDEN ROAD. THE PROPER WORDING ON THE MESSAGE BOARD WILL NEED TO BE COORDINATED WITH THE DISTRICT SAFETY OFFICER.
23. MAINTENANCE OF THE STREETS WITHIN THIS PROPERTY WILL BE THE RESPONSIBILITY OF THE DEVELOPER. THE PROPERTY OWNERS WITHIN THIS SUBDIVISION, OR BOTH, THE STATE OF DELAWARE ASSUMES NO RESPONSIBILITY FOR THE FUTURE MAINTENANCE OF THESE STREETS.
24. THE ENDS OF ALL CURBS SHALL BE TRANSITIONED TO BE FLUSH WITH THE PAVEMENT AT A RATIO OF TWELVE TO ONE (12:1), TAPER CURB PER LOCAL DELDOT DISTRICT FIELD SUPERVISOR (TYPICAL FOR BOTH SIDES OF THE ENTRANCE). ALL APPROACH AND EXIT ENDS OF MEDIAN ISLAND AND CURB SHALL ALSO BE TRANSITIONED FLUSH WITH THE PAVEMENT AT A RATIO OF TWELVE TO ONE (12:1). TRIANGULAR (POPK CHOP) ISLAND CURB SHALL HAVE ALL CORNERS TRANSITIONED FLUSH WITH PAVEMENT AT A RATIO OF FOUR TO ONE (4:1).
25. MAINTENANCE OF ALL LANDSCAPING SHOWN ON THIS PLAN WILL BE THE RESPONSIBILITY OF THE DEVELOPER, OR PROPERTY OWNER AFTER COMPLETION OF THE PROJECT. THE STATE ASSUMES NO RESPONSIBILITY FOR THE FUTURE MAINTENANCE OF THE LANDSCAPING. DELDOT MUST REVIEW AND APPROVE ANY PLANTINGS PROPOSED WITHIN THE RIGHT OF WAY PRIOR TO INSTALLATION. IF ANY PLANTINGS ARE REPLANTED WITHOUT DELDOT'S KNOWLEDGE AND/OR APPROVAL, THEN DELDOT HAS THE RIGHT TO HAVE THE DEVELOPER REMOVE THE PLANTINGS, WITH ALL COSTS BEING PAID FOR BY THE DEVELOPER.
26. VERIFY IF ANY UTILITIES WILL NEED TO BE RELOCATED DUE TO THE ADDITION OF THE SHOULDER, FOR CLEAR ZONE PURPOSES, ALL NEW UTILITIES ARE TO BE A MINIMUM OF 10'-FEET FROM THE EDGE OF TRAVEL LANE OR 5'-FEET FROM THE EDGE OF PAVEMENT.
27. IT IS THE CONTRACTOR'S RESPONSIBILITY TO ENSURE THAT PAVING WITHIN THE STATE OF DELAWARE RIGHT-OF-WAY IS INSTALLED TO THE ELEVATIONS SHOWN AND THAT NO PONDING OF WATER EXISTS AFTER PAVING IS COMPLETE.
28. ALL STORM DRAIN PIPING DESIGNATED AS RCP IS TO BE REINFORCED CONCRETE PIPE, MEETING ASHTO M-170 SPECIFICATIONS. SEE PLANS FOR SPECIFIC CLASS OF PIPE.
29. ALL PROPOSED CLOSED STORMDRAIN SYSTEMS SHALL BE VIDEO INSPECTED AND APPROVED PRIOR TO THE INSTALLATION OF FINAL PAVING. IF REPAIRS ARE NEEDED, THE REPAIRED PIPE SECTIONS WILL NEED TO BE VIDEO INSPECTED AGAIN BEFORE THE REPAIR CAN BE APPROVED.
30. WARM-MIX SHALL BE PLACED IN ACCORDANCE WITH DELDOT SPECIAL PROVISIONS(S) 40100S & 40101A - WMA SUPERPAVE. REINFORCED CONCRETE PIPE SHALL BE INSTALLED IN ACCORDANCE WITH DELDOT SPECIAL PROVISION(S) 601011 & 601012. PIPE VIDEO INSPECTION WILL BE PERFORMED IN ACCORDANCE WITH DELDOT SPECIAL PROVISION 612529.
31. THE DEVELOPER AND EXISTING/FUTURE OWNERS OF NON-STATE MAINTAINED ROADWAYS SHALL ENSURE THAT THE TRAFFIC CONTROL DEVICES ON SAID ROADWAYS OPEN TO PUBLIC TRAVEL ARE IN COMPLIANCE WITH THE DELAWARE MUTCD.
32. THE DEVELOPER SHALL BE REQUIRED TO FURNISH AND PLACE RIGHT-OF-WAY MONUMENTS ON THE DEDICATED SUBDIVISION STREET RIGHTS-OF-WAY IN ACCORDANCE WITH DELDOT'S STANDARDS AND REGULATIONS FOR SUBDIVISION STREETS AND STATE HIGHWAY ACCESS, AND THE REQUIREMENTS OF THE LAND USE AGENCY.
33. CONSTRUCTION WILL NOT BE PERMITTED UNTIL CONSTRUCTION PLANS HAVE BEEN APPROVED, SURETY HAS BEEN RECEIVED AND THE TRAFFIC SIGNAL AGREEMENT IS COMPLETE. AT DELDOT'S DISCRETION, A TEMPORARY CONSTRUCTION ENTRANCE PERMIT MAY BE ISSUED FOR CLEARING, GRUBBING, TEMPORARY ENTRANCE CONSTRUCTION, BULK GRADING AND PERIMETER EROSION AND SEDIMENT CONTROLS UP TO 30 DAYS PRIOR TO PLAN APPROVAL. NO BUILDING CONSTRUCTION WILL BE PERMITTED UNDER A TEMPORARY ENTRANCE CONSTRUCTION PERMIT. IF PLAN APPROVAL IS NOT RECEIVED WITHIN 30 CALENDAR DAYS, ALL CONSTRUCTION ACTIVITIES SHALL BE STOPPED. DELDOT WILL NOT PROVIDE A CIO APPROVAL FOR A COMMERCIAL ENTRANCE TO NCCO UNTIL THE ENTRANCE(S) ARE COMPLETED TO THE SATISFACTION OF THE DEPARTMENT.
34. THE CURB AND SIDEWALK SHALL BE THE RESPONSIBILITY OF THE DEVELOPER, THE PROPERTY OWNERS OR BOTH WITHIN THIS SUBDIVISION. THE STATE OF DELAWARE ASSUMES NO RESPONSIBILITY FOR THE FUTURE MAINTENANCE OF THE SIDEWALK.
35. THE CONTRACTOR SHALL REPLACE, IN-KIND, ANY EXISTING DRAINAGE STRUCTURES DAMAGED AS A RESULT OF CONSTRUCTION.
36. ALL STORMWATER MANAGEMENT AND DRAINAGE FACILITIES ARE TO REMAIN PRIVATELY OWNED AND MAINTAINED.

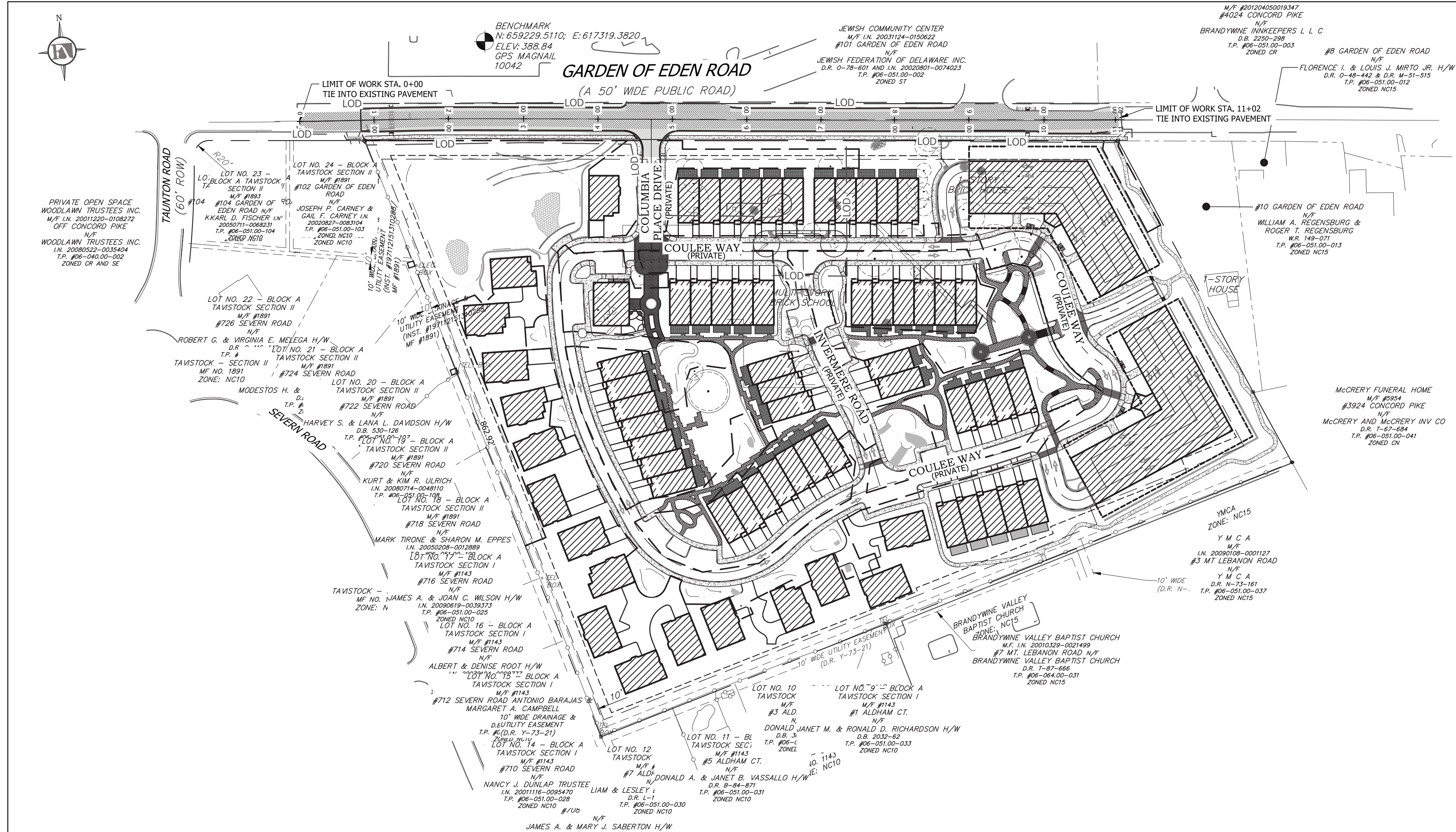


ROAD TRAFFIC DATA: GARDEN OF EDEN ROAD (MAINTENANCE ROAD #N226)
FUNCTIONAL CLASSIFICATION - LOCAL ROAD
POSTED SPEED LIMIT - 20 MPH (SCHOOL)
AADT = 1,290 TRIPS (FROM 2009 DELDOT TRAFFIC SUMMARY)
50 YEAR PROJECTED AADT = 1,116 X 1,290 TRIPS = 1,496 TRIPS
10 YEAR PROJECTED AADT + SITE AADT = 2,018 TRIPS
TRAFFIC PATTERN GROUP 4: K=15.69, D=73.41, TRUCKS=3.91
PEAK HOUR = 15.69% X 2,018 = 317 TRIPS
SITE TRIPS GENERATED:
SOURCE: ITE TRIP GENERATION MANUAL 8TH EDITION
17 AGE-RESTRICTED DETACHED HOMES X 3.71 = 63 TRIPS
50 AGE-RESTRICTED ATTACHED HOMES X 3.48 = 174 TRIPS
82 AGE-RESTRICTED APARTMENTS X 2.52 = 207 TRIPS
TOTAL ADI FOR SUBDIVISION = 522 TRIPS
DESIGN VEHICLE: SU-30
DIRECTIONAL DISTRIBUTION:
73% TO AND FROM THE EAST (381 TRIPS)(16 AM PK)(19 PM PK)
25% TO AND FROM THE WEST (131 TRIPS)(6 AM PK)(7 PM PK)
02% TO AND FROM OPPOSITE I.C.C. ENTRANCE (10 TRIPS)(0 AM PK)(0 PM PK)
3.91% TRUCKS & BUSES X 191 = 8



SITE DATA

Table with columns: CATEGORY, QUANTITY. Includes Developer (REYBOLD VENTURE GROUP XVIII, LLC), Tax Map Parcel No. (06-051-00-014), Net Acreage (14.938+ AC), Proposed Land Use (RESIDENTIAL), Land Use Details (Single Family Homes: 17 units, Townhomes: 50 units, Condos: 82 units), and Parking Requirements (Single Family Homes: 34 provided, 34 required).



EXISTING FEATURES

- EX. RIGHT-OF-WAY LINE
EX. ADJOINER LINE
EX. EASEMENT
EX. SIDEWALK
EX. TREE
EX. TREELINE
EX. CONTOUR
EX. STORM DRAIN & CATCH BASIN
EX. SANITARY SEWER LINE & MANHOLE
EX. WATER MAIN
EX. FIRE HYDRANT
EX. GAS LINE
EX. UTILITY POLE

PROPOSED FEATURES

- PR. MILL & OVERLAY
PR. FULL DEPTH ASPHALT
PR. INTEGRAL P.C.C. CURB & GUTTER, TYPE 3-4
PR. DEPRESSION CURB
PR. PORTLAND CEMENT SIDEWALK
PR. PAVER SIDEWALK
PR. BUILDING
PR. CONCRETE MONUMENT
PR. LIMIT OF DISTURBANCE
PR. BUILDING SETBACK LINE
PR. LANDSCAPE BUFFERYARD
PR. TREELINE
PR. ADDRESS LABEL
PR. SIDEWALK/CURB OPENING

PLAN INDEX

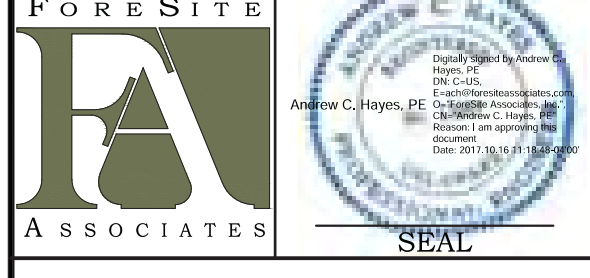
- 1. INDEX SHEET
2. ENTRANCE PLAN
3. GRADES & GEOMETRICS PLAN
4. SIGNING & STRIPING
5. EROSION & SEDIMENT CONTROL
6. ROAD PROFILES
7. STORM PROFILES
8. DETAILS

REVIEWED FOR GENERAL CONFORMITY WITH CURRENT REGULATIONS, STANDARD SPECIFICATIONS AND STANDARD DETAILS. THE DEVELOPER'S ENGINEER IS RESPONSIBLE FOR ACCURACY OF CONTENT.
10/20/2017
SUBDIVISION ENGINEER
DELAWARE DEPARTMENT OF TRANSPORTATION

FORE SITE ASSOCIATES
LAND PLANNING
CIVIL ENGINEERING
WATER RESOURCES ENGINEERING
FORESITE ASSOCIATES INC.
208 DELAWARE STREET
NEW CASTLE, DE 19720
PHONE: 302.351.3421
FAX: 302.351.3456
EMAIL: INFO@FORESITEASSOCIATES.COM

COLUMBIA PLACE AT GARDEN OF EDEN ROAD COMMERCIAL ENTRANCE PLAN REYBOLD VENTURE GROUP XVIII-A, LLC 116 E. SCOTLAND DR. BEAR, DE 19701

Table with 4 columns: #, COMMENT, BY, DATE. Includes entries for PER DELDOT COMMENTS and comments from BNC.

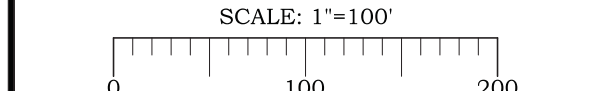


INDEX SHEET

PER DELDOT COMMENTS
COMMERCIAL ENTRANCE PLAN

COLUMBIA PLACE AT GARDEN OF EDEN ROAD

Table with 2 columns: CATEGORY, QUANTITY. Includes Date (02.13.15), Project # (001.17), Surveyed by (BNC), Sheet (1), Created by (MDS), Drawn by (MDS/AZ), Checked by (ACH), and total of 1 of 8 sheets.



DeIDOT
 REVIEWED FOR
 GENERAL
 CONFORMITY
 Oct. 20, 2017



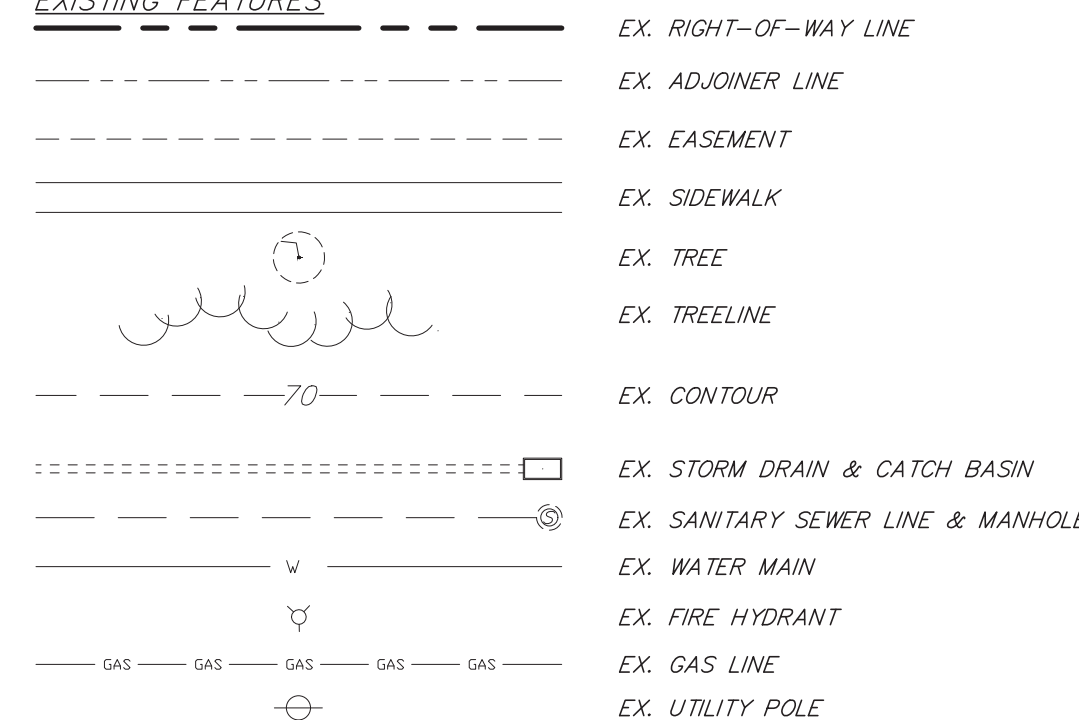
LAND PLANNING
 CIVIL ENGINEERING
 WATER RESOURCES ENGINEERING

FORESITE ASSOCIATES INC.
 208 DELAWARE STREET
 NEW CASTLE, DE 19720
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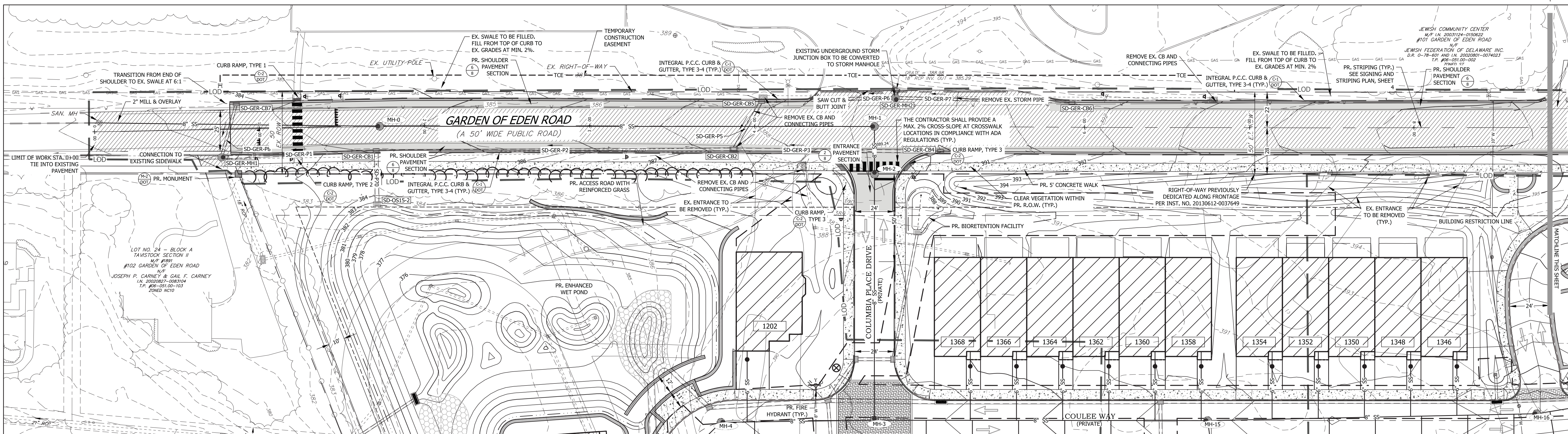
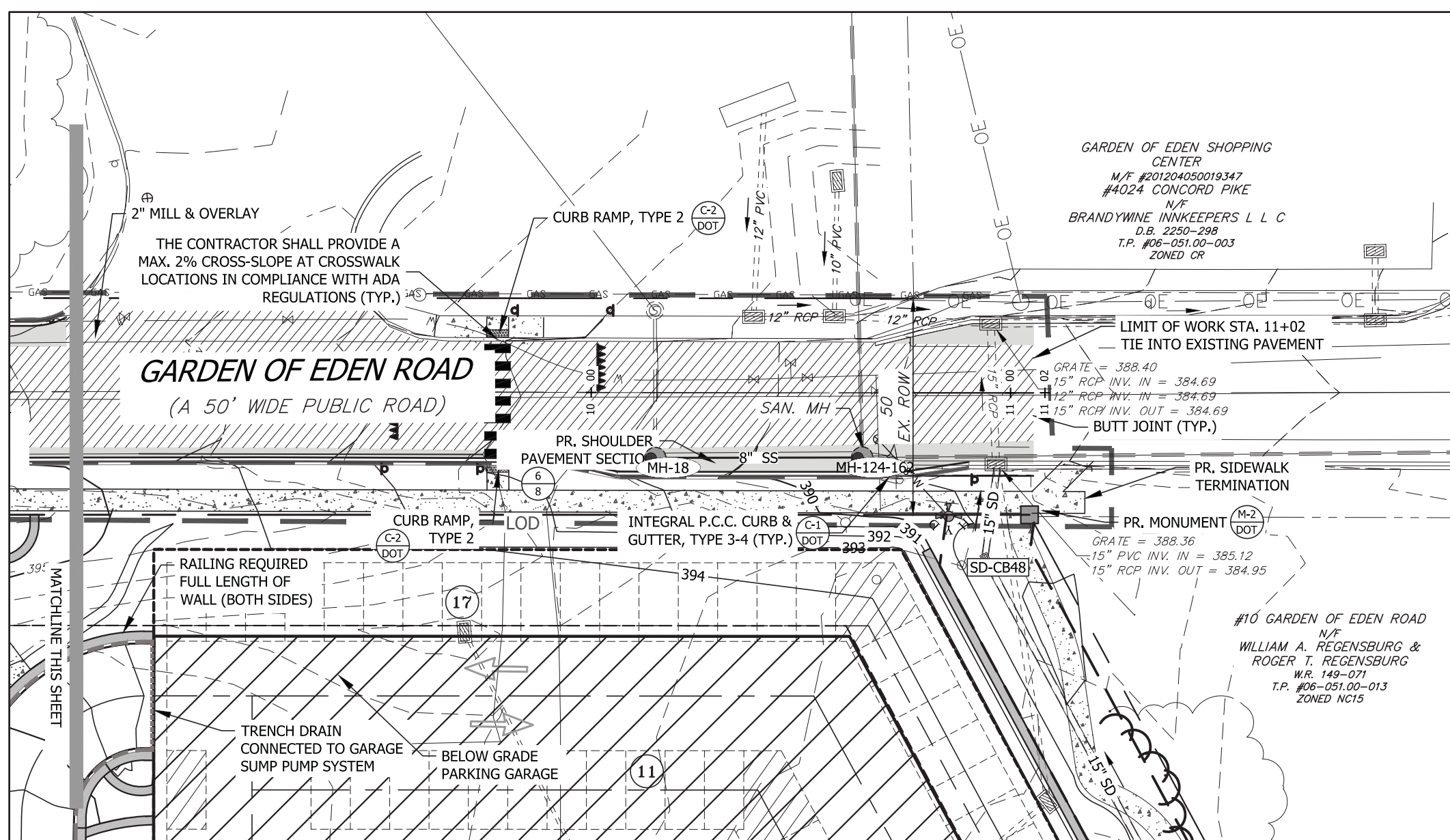
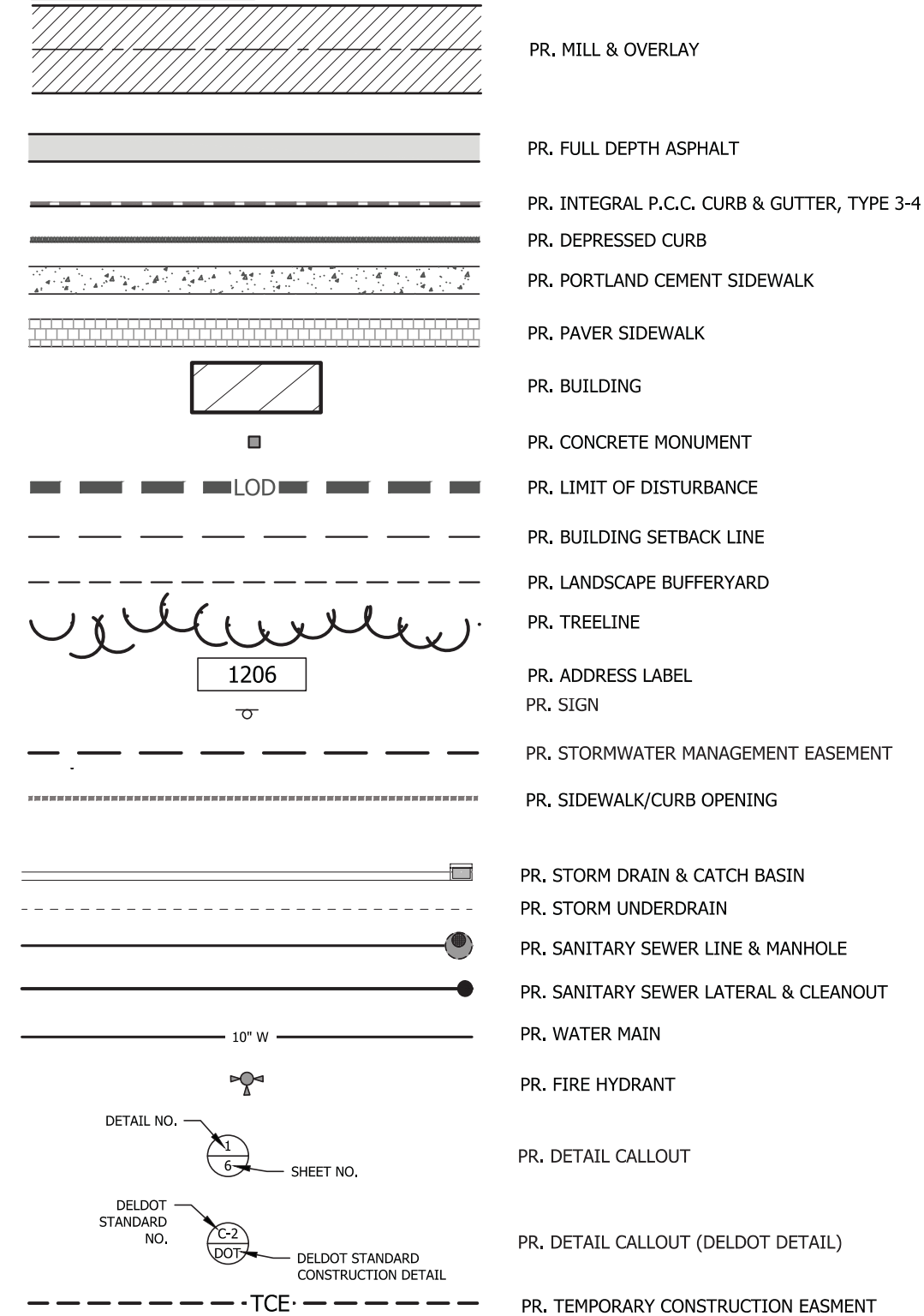
COLUMBIA PLACE AT
 GARDEN OF EDEN ROAD
 COMMERCIAL ENTRANCE PLAN
 REYBOLD VENTURE GROUP XVIII-A, LLC
 116 E. SCOTLAND DR. BEAR, DE 19701

LEGEND

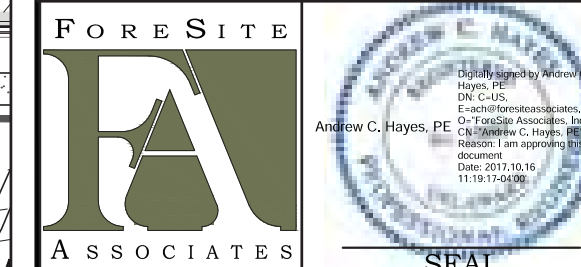
EXISTING FEATURES



PROPOSED FEATURES



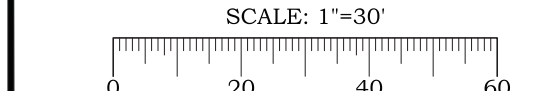
| # | COMMENT | BY | DATE |
|---|---------------------|----|----------|
| 4 | PER DELDOT COMMENTS | AZ | 09.25.17 |
| 3 | PER DELDOT COMMENTS | AZ | 06.15.17 |
| 2 | PER DELDOT COMMENTS | AZ | 12.16.16 |
| 1 | PER DELDOT COMMENTS | AZ | 09.07.16 |



COMMERCIAL ENTRANCE PLAN
 ENTRANCE PLAN

COLUMBIA PLACE AT
 GARDEN OF EDEN ROAD

BRANDYWINE HUNDRED WILMINGTON
 NEW CASTLE COUNTY DELAWARE
 DATE: 02.13.15 PROJECT #: 001.17
 SURVEYED BY: BMC SHEET:
 CREATED BY: MDS 2
 DRAWN BY: MDS/AZ 2 OF 8
 CHECKED BY: ACH



**DelDOT
REVIEWED FOR
GENERAL
CONFORMITY**
Oct. 20, 2017

FORESITE ASSOCIATES
- LAND PLANNING
- CIVIL ENGINEERING
- WATER RESOURCES ENGINEERING
FORESITE ASSOCIATES INC.
208 DELAWARE STREET
NEW CASTLE, DE 19720
PHONE: 302.351.3421
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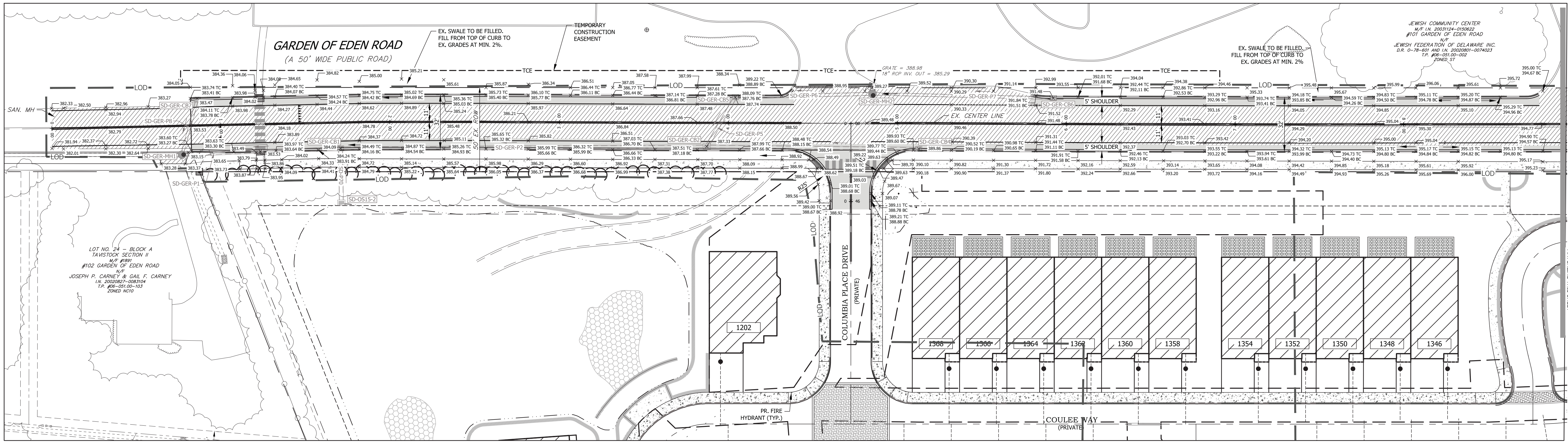
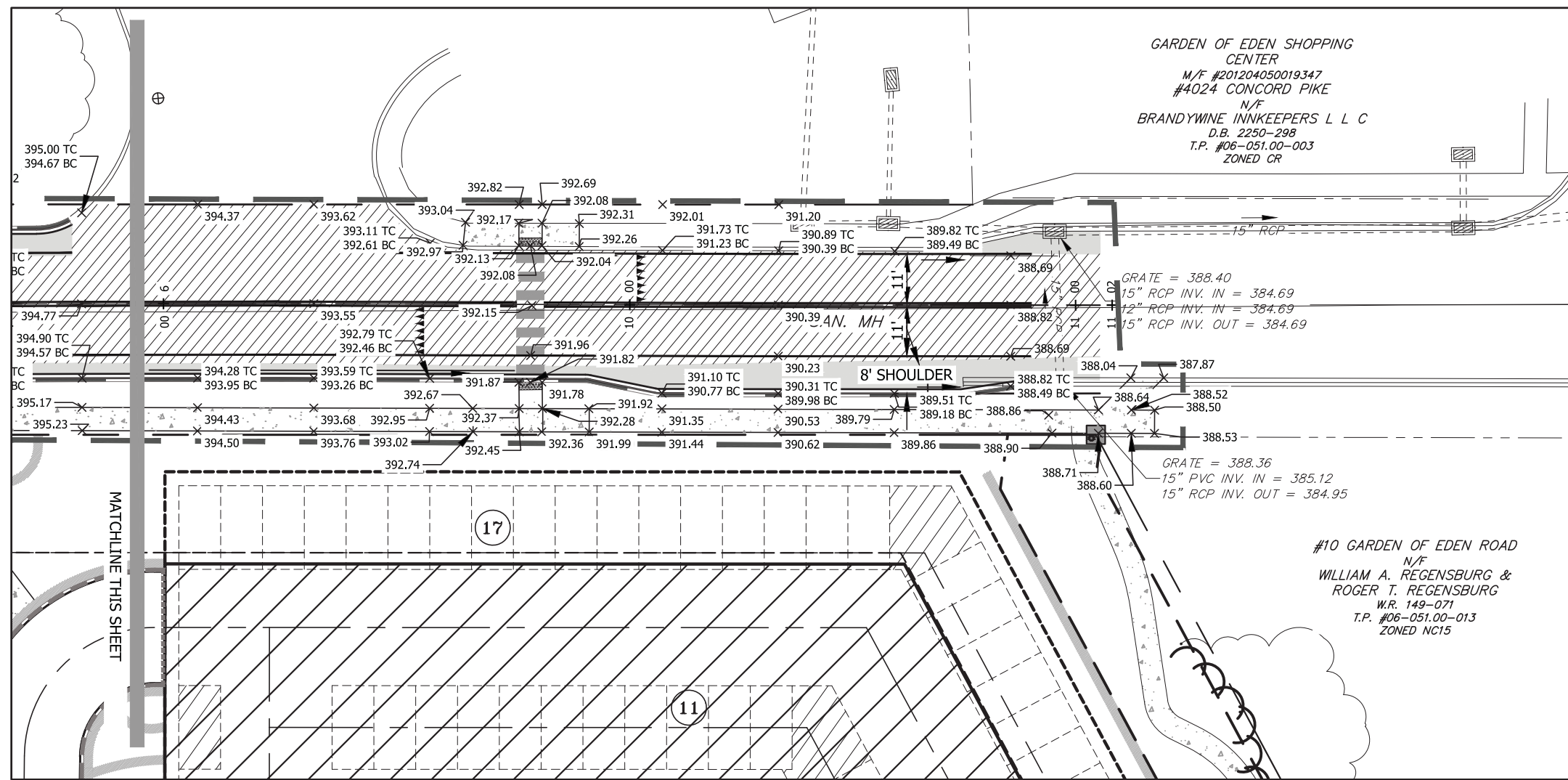
LEGEND

EXISTING FEATURES

--- EX. RIGHT-OF-WAY LINE
--- EX. ADJOINER LINE
--- EX. EASEMENT
--- EX. SIDEWALK
--- EX. TREE
--- EX. TREELINE
--- -70--- EX. CONTOUR
--- - - - - EX. STORM DRAIN & CATCH BASIN
--- - - - - EX. SANITARY SEWER LINE & MANHOLE
--- - - - - EX. WATER MAIN
--- - - - - EX. FIRE HYDRANT
--- - - - - EX. GAS LINE
--- - - - - EX. UTILITY POLE

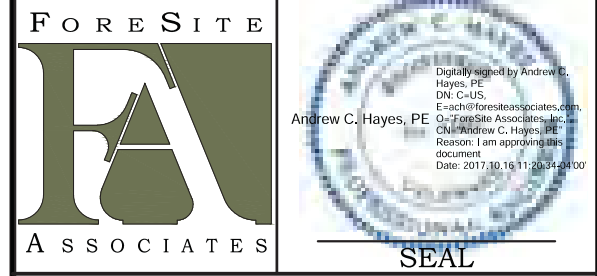
PROPOSED FEATURES

PR. MILL & OVERLAY
PR. FULL DEPTH ASPHALT
PR. INTEGRAL P.C.C. CURB & GUTTER, TYPE 3-4
PR. DEPRESSED CURB
PR. PORTLAND CEMENT SIDEWALK
PR. PAVER SIDEWALK
PR. BUILDING
PR. CONCRETE MONUMENT
PR. LIMIT OF DISTURBANCE
PR. BUILDING SETBACK LINE
PR. LANDSCAPE BUFFERYARD
PR. TREELINE
PR. ADDRESS LABEL
PR. SIGN
PR. STORMWATER MANAGEMENT EASEMENT
PR. SIDEWALK/CURB OPENING
PR. STORM DRAIN & CATCH BASIN
PR. STORM UNDERDRAIN
PR. SANITARY SEWER LINE & MANHOLE
PR. SANITARY SEWER LATERAL & CLEANOUT
PR. WATER MAIN
PR. FIRE HYDRANT
PR. TEMPORARY CONSTRUCTION EASEMENT



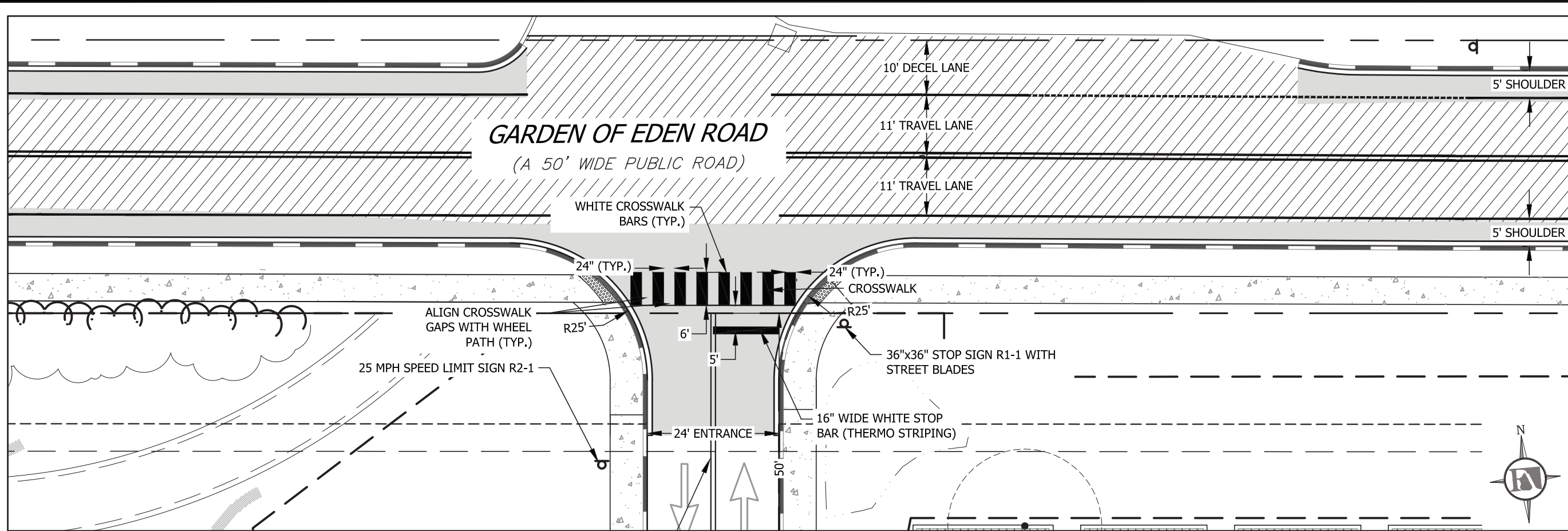
**COLUMBIA PLACE AT
GARDEN OF EDEN ROAD
COMMERCIAL ENTRANCE PLAN
REYBOLD VENTURE GROUP XVIII-A, LLC
116 E. SCOTLAND DR. BEAR, DE 19701**

| | | |
|---|---------------------|------|
| 4 | PER DELDOT COMMENTS | AZ |
| 3 | PER DELDOT COMMENTS | AZ |
| 2 | PER DELDOT COMMENTS | AZ |
| 1 | PER DELDOT COMMENTS | AZ |
| # | COMMENT | DATE |

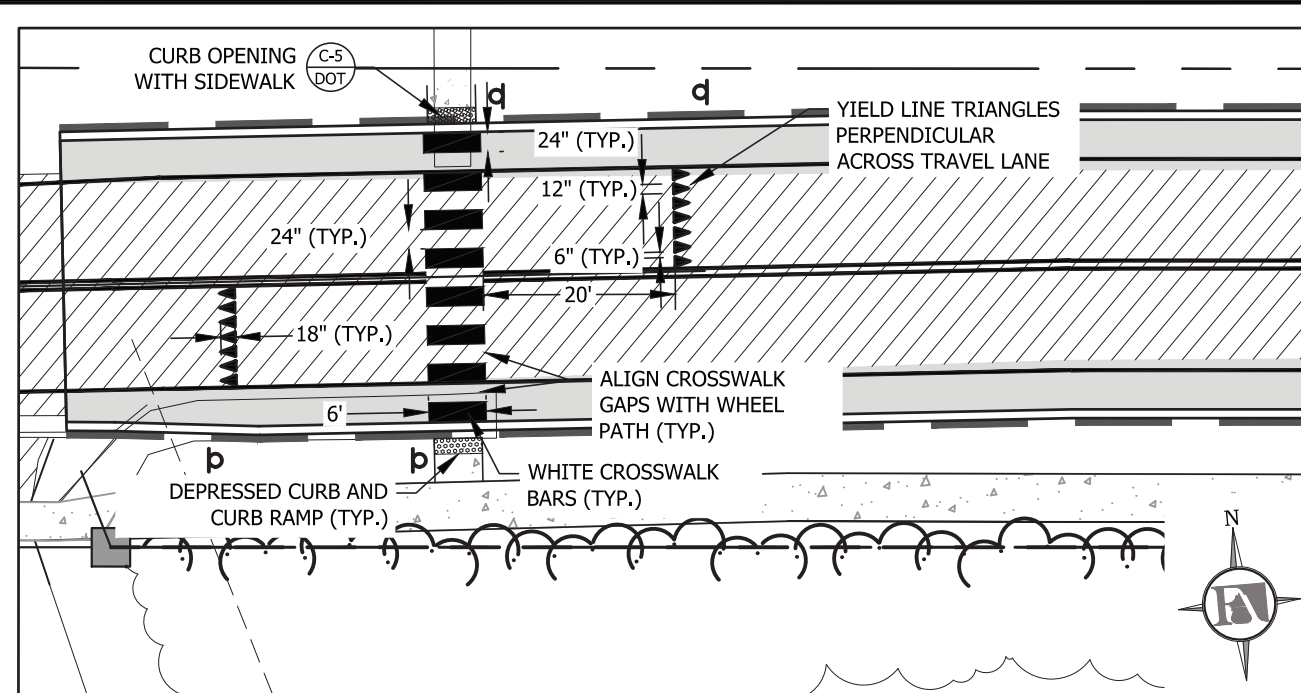


**COMMERCIAL ENTRANCE PLAN
GRADES &
GEOMETRICS PLAN**

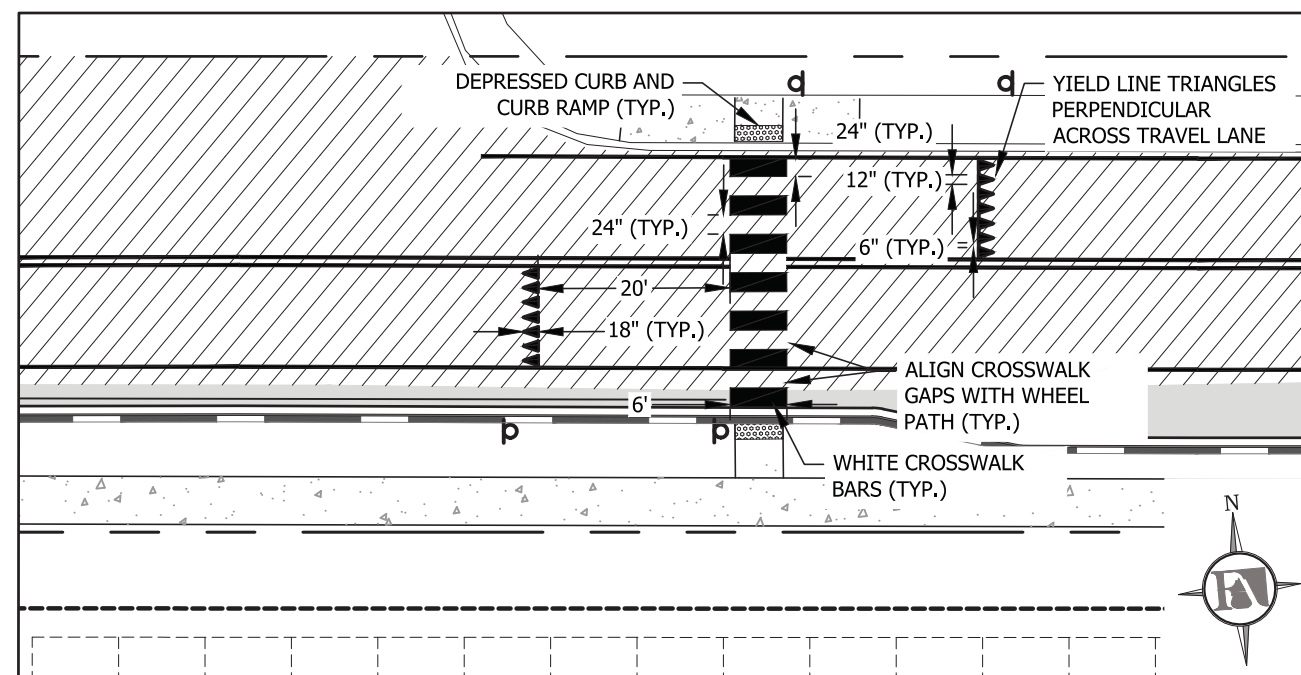
COLUMBIA PLACE AT
GARDEN OF EDEN ROAD
BRANDYWINE HUNDRED NEW CASTLE COUNTY DELAWARE
WILMINGTON DELAWARE
DATE: 02.13.15 PROJECT #: 001.17
SURVEYED BY: MDS SHEET: 3 OF 8
CREATED BY: BMC
DRAWN BY: MDS/AZ
CHECKED BY: ACH
SCALE: 1"=30'



1
4 SITE ENTRANCE STRIPING DETAIL 1"=20'



2
4 CROSSWALK BAR AND YIELD LINE DETAILS 1"=20'



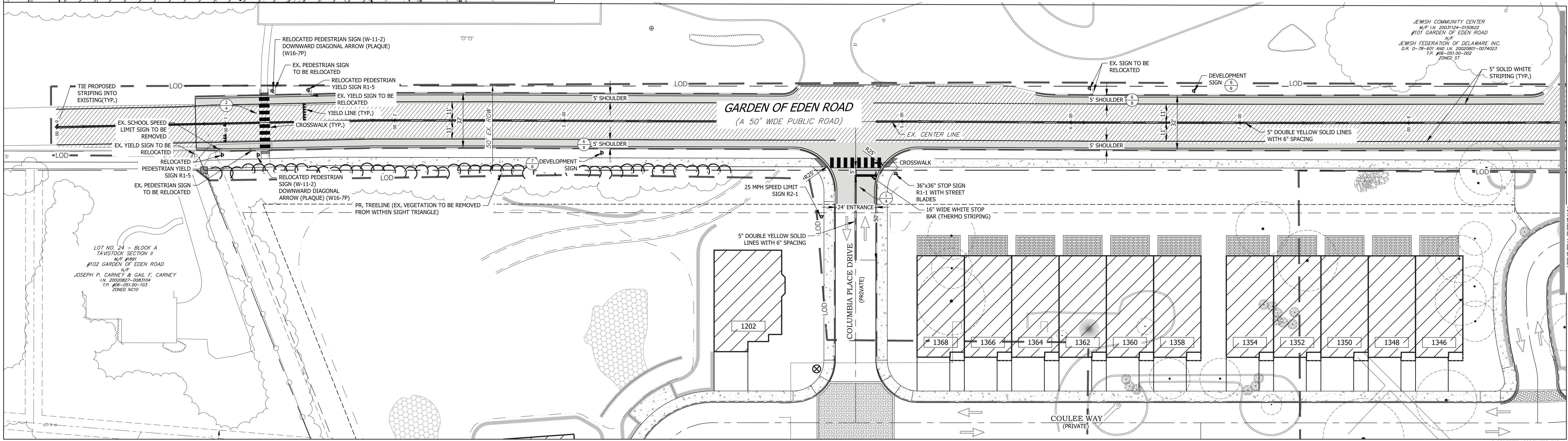
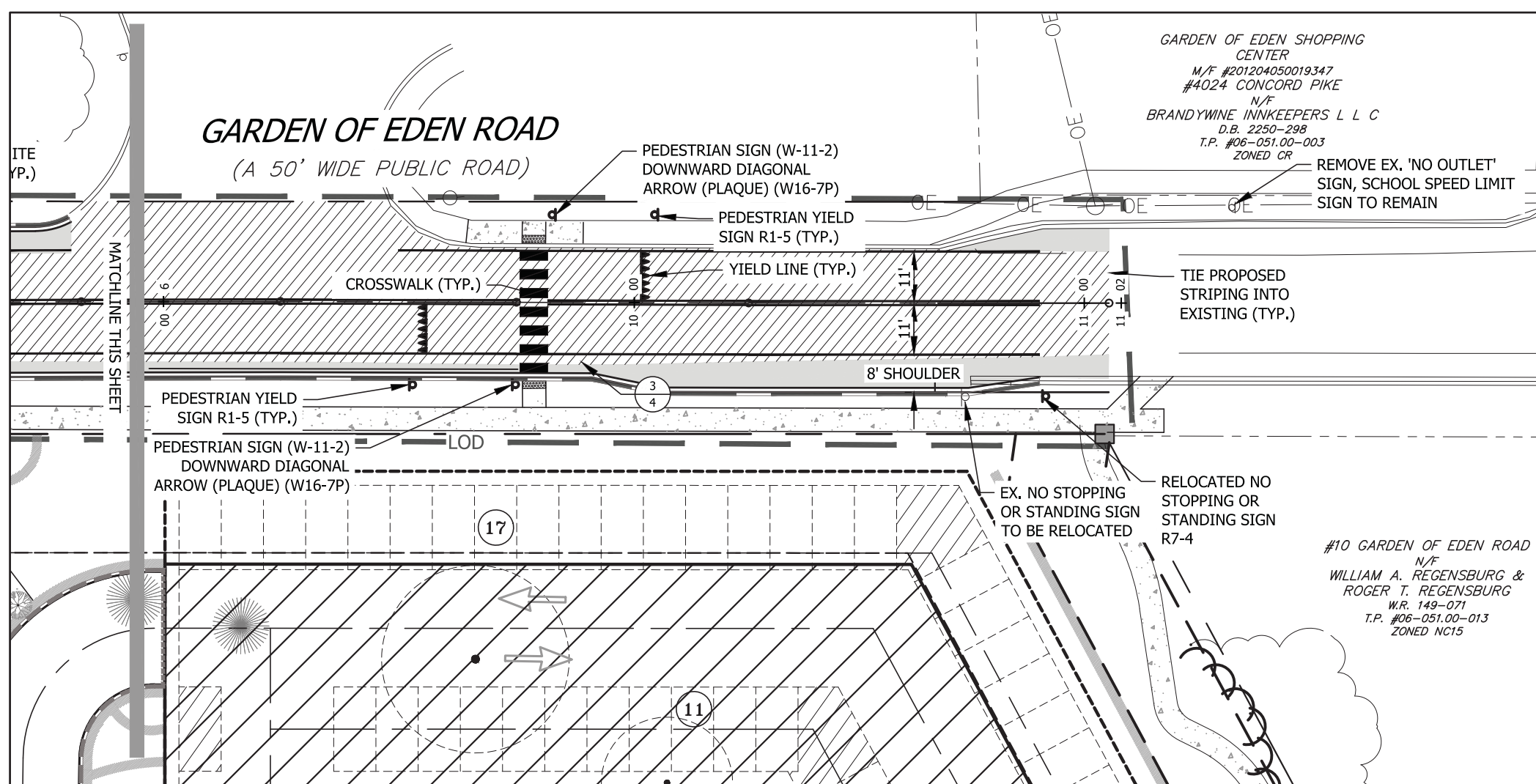
3
4 CROSSWALK BAR AND YIELD LINE DETAILS 1"=20'

SIGNING & STRIPING NOTES

- ALL DISTURBED AREAS WITHIN THE STATE RIGHT-OF-WAY, BUT NOT IN THE PAVEMENT, SHALL BE TOP-SOILED (6" MINIMUM), FERTILIZED, SEEDED AND MULCHED, IF SOIL IS USED NEXT TO SIDEWALK OR SHARED USE PATH, CONTRACTOR SHALL GRADE TOPSOIL ADJACENT TO THE SIDEWALK OR SHARED USE PATH PRIOR TO PLACEMENT OF SOIL TO ENSURE THAT SOIL IS PLACED FLUSH OR JUST BELOW EDGE OF SIDEWALK OR SHARED USE PATH TO AVOID WATER PONDING ON THE SIDEWALK OR SHARED USE PATH.
- ALL SIGNING, STRIPING AND MAINTENANCE OF TRAFFIC IS THE CONTRACTOR'S RESPONSIBILITY AND SHALL FOLLOW THE GUIDELINES SHOWN IN THE DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (DE MUTCD) FOR STREETS AND HIGHWAYS (LATEST EDITION). THE OWNER OR MAINTENANCE CORPORATION SHALL BE RESPONSIBLE FOR MAINTENANCE OF ALL SIGNS INSTALLED AS PART OF THIS PROJECT.
- DESIGN, FABRICATION AND INSTALLATION OF ALL PERMANENT SIGNING SHALL BE AS OUTLINED IN THE DE MUTCD.
- DESIGN AND INSTALLATION OF ALL PAVEMENT MARKINGS AND STRIPING SHALL BE AS OUTLINED IN THE DE MUTCD. FOR FINAL PERMANENT PAVEMENT MARKINGS:
1. EPOXY RESIN PAINT SHALL BE REQUIRED FOR LONG LINE STRIPING.
2. THERMO PLASTIC (EXTRUDED OR PREFORMED MATERIAL) WILL BE REQUIRED ON ASPHALT SURFACES, FOR SHORT LINE STRIPING, I.E. SYMBOLS/LEGENDS.
3. PERMANENT PAVEMENT MARKING TAPE (PER DELDOT APPROVED MATERIALS LIST) WILL BE REQUIRED ON CONCRETE SURFACES, FOR SHORT LINE STRIPING, I.E. SYMBOLS/LEGENDS.
- LANE LINE ERADICATION WILL BE DONE IN ACCORDANCE AND AT THE DIRECTION OF DELDOT'S CANAL DISTRICT PUBLIC WORKS DEPARTMENT. DUE TO EXPERIENCE WITH PAVEMENT FAILURES RESULTING FROM LANE LINE ERADICATION, THE DEPARTMENT WILL REQUIRE THAT ANY PAVEMENT FAILURES WITHIN 3 YEARS OF ENTRANCE CONSTRUCTION RESULTING FROM LANE LINE ERADICATION WILL REQUIRE REPAIR AS DIRECTED BY DELDOT, IN MOST CASES BEING A FULL LANE WIDTH MILL AND OVERLAY.
- BREAKAWAY POSTS SHALL BE USED WHEN INSTALLING ALL SIGNS. REFERENCE DELDOT STANDARD CONSTRUCTION DETAIL T-15.
- CONTRACTOR SHALL PATCH, REPAIR, AND FINISH ALL DAMAGED SURFACES CAUSED BY THE WORK, USING MATERIALS OF THE SAME KIND.

LEGEND

- PR. 5' DOUBLE YELLOW STRIPING
- PR. 5' SOLID WHITE STRIPING
- PR. 10' DOTTED WHITE STRIPING
- PR. CROSSWALK STRIPING
- PR. YIELD LINE STRIPING
- PR. BREAK-AWAY SIGN ASSEMBLY (MUTCD SIGNS BOOK CODE)



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GENERAL
CONFORMITY
Oct. 20, 2017

FORESITE ASSOCIATES

LAND PLANNING
CIVIL ENGINEERING
WATER RESOURCES ENGINEERING

FORESITE ASSOCIATES INC.
208 DELAWARE STREET
NEW CASTLE, DE 19720
PHONE: 302.351.3421
FAX: 302.351.3456
EMAIL: INFO@FORESITEASSOCIATES.COM

**COLUMBIA PLACE AT
GARDEN OF EDEN ROAD
COMMERCIAL ENTRANCE PLAN**
REYBOLD VENTURE GROUP XVIII-A, LLC
116 E. SCOTLAND DR. BEAR, DE 19701

| # | COMMENT | BY | DATE |
|---|---------------------|----|----------|
| 4 | PER DELDOT COMMENTS | AZ | 09.25.17 |
| 3 | PER DELDOT COMMENTS | AZ | 06.15.17 |
| 2 | PER DELDOT COMMENTS | AZ | 12.18.16 |
| 1 | PER DELDOT COMMENTS | AZ | 09.07.16 |

FORESITE ASSOCIATES SEAL

COMMERCIAL ENTRANCE PLAN
SIGNING & STRIPING PLAN

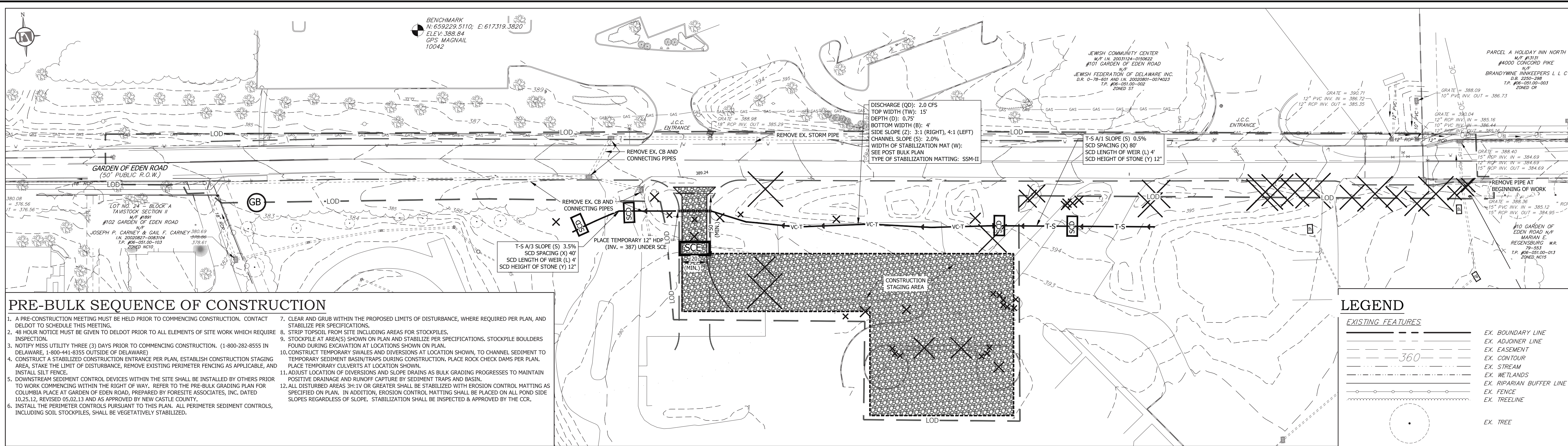
COLUMBIA PLACE AT
GARDEN OF EDEN ROAD

BRANDYWINE HUNDRED WILMINGTON
NEW CASTLE COUNTY DELAWARE

DATE: 02.13.15 PROJECT #: 001.17
SURVEYED BY: BMC SHEET: 4
CREATED BY: MDS
DRAWN BY: MDS/AZ
CHECKED BY: ACH 4 OF 8

SCALE: 1"=30'

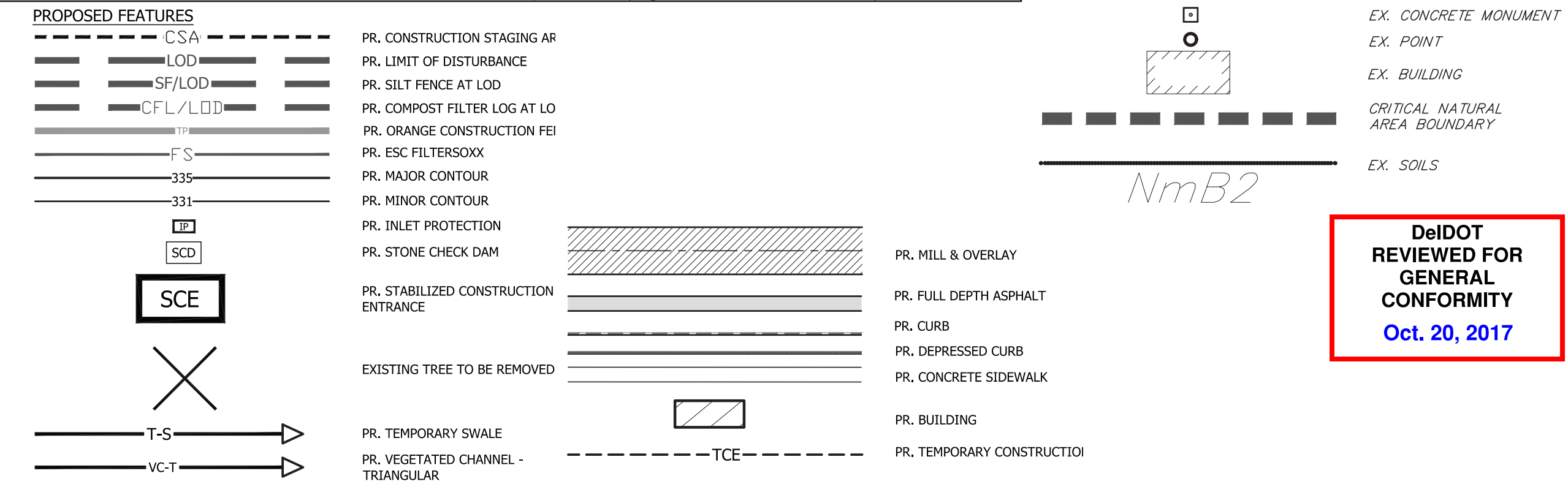
**COLUMBIA PLACE AT
GARDEN OF EDEN ROAD
COMMERCIAL ENTRANCE PLAN
REYBOLD VENTURE GROUP XVIII-A, LLC**
116 E. SCOTLAND DR. BEAR, DE 19701



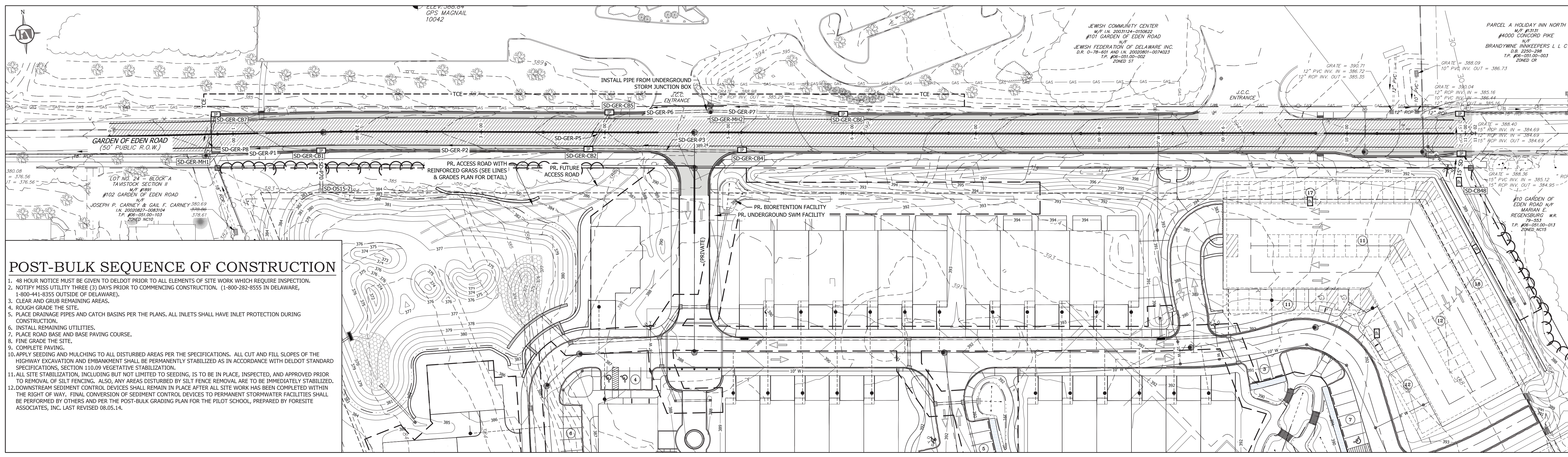
PRE-BULK SEQUENCE OF CONSTRUCTION

1. A PRE-CONSTRUCTION MEETING MUST BE HELD PRIOR TO COMMENCING CONSTRUCTION. CONTACT DELDOT TO SCHEDULE THIS MEETING.
2. 48 HOUR NOTICE MUST BE GIVEN TO DELDOT PRIOR TO ALL ELEMENTS OF SITE WORK WHICH REQUIRE INSPECTION.
3. NOTIFY MISS UTILITY THREE (3) DAYS PRIOR TO COMMENCING CONSTRUCTION. (1-800-282-8555 IN DELAWARE, 1-800-441-8355 OUTSIDE OF DELAWARE)
4. CONSTRUCT A STABILIZED CONSTRUCTION ENTRANCE PER PLAN, ESTABLISH CONSTRUCTION STAGING AREA, STAKE THE LIMIT OF DISTURBANCE, REMOVE EXISTING PERIMETER FENCING AS APPLICABLE, AND INSTALL SILT FENCE.
5. DOWNSCREEN SEDIMENT CONTROL DEVICES WITHIN THE SITE SHALL BE INSTALLED BY OTHERS PRIOR TO WORK COMMENCING WITHIN THE RIGHT OF WAY. REFER TO THE PRE-BULK GRADING PLAN FOR COLUMBIA PLACE AT GARDEN OF EDEN ROAD, PREPARED BY FORESITE ASSOCIATES, INC. DATED 10.25.12, REVISED 05.02.13 AND AS APPROVED BY NEW CASTLE COUNTY.
6. INSTALL THE PERIMETER CONTROLS PURSUANT TO THIS PLAN. ALL PERIMETER SEDIMENT CONTROLS, INCLUDING SOIL STOCKPILES, SHALL BE VEGETATIVELY STABILIZED.
7. CLEAR AND GRUB WITHIN THE PROPOSED LIMITS OF DISTURBANCE, WHERE REQUIRED PER PLAN, AND STABILIZE PER SPECIFICATIONS.
8. STRIP TOPSOIL FROM SITE INCLUDING AREAS FOR STOCKPILES.
9. STOCKPILE AT AREA(S) SHOWN ON PLAN AND STABILIZE PER SPECIFICATIONS. STOCKPILE BOULDERS FOUND DURING EXCAVATION AT LOCATIONS SHOWN ON PLAN.
10. CONSTRUCT TEMPORARY SWALES AND DIVERSIONS AT LOCATION SHOWN, TO CHANNEL SEDIMENT TO TEMPORARY SEDIMENT BASIN/TRAPS DURING CONSTRUCTION. PLACE ROCK CHECK DAMS PER PLAN. PLACE TEMPORARY CULVERTS AT LOCATION SHOWN.
11. ADJUST LOCATION OF DIVERSIONS AND SLOPE DRAINS AS BULK GRADING PROGRESSES TO MAINTAIN POSITIVE DRAINAGE AND RUNOFF CAPTURE BY SEDIMENT TRAPS AND BASIN.
12. ALL DISTURBED AREAS 3H:1V OR GREATER SHALL BE STABILIZED WITH EROSION CONTROL MATTING AS SPECIFIED ON PLAN. IN ADDITION, EROSION CONTROL MATTING SHALL BE PLACED ON ALL POND SIDE SLOPES REGARDLESS OF SLOPE. STABILIZATION SHALL BE INSPECTED & APPROVED BY THE CCR.

PRE-BULK AND DEMO PLAN



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REVIEWED FOR
GENERAL
CONFORMITY
Oct. 20, 2017

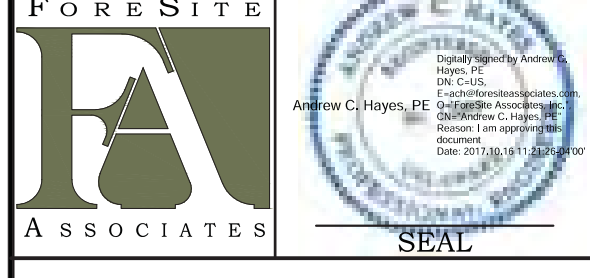


POST-BULK SEQUENCE OF CONSTRUCTION

1. 48 HOUR NOTICE MUST BE GIVEN TO DELDOT PRIOR TO ALL ELEMENTS OF SITE WORK WHICH REQUIRE INSPECTION.
2. NOTIFY MISS UTILITY THREE (3) DAYS PRIOR TO COMMENCING CONSTRUCTION. (1-800-282-8555 IN DELAWARE, 1-800-441-8355 OUTSIDE OF DELAWARE).
3. CLEAR AND GRUB REMAINING AREAS.
4. ROUGH GRADE THE SITE.
5. PLACE DRAINAGE PIPES AND CATCH BASINS PER THE PLANS. ALL INLETS SHALL HAVE INLET PROTECTION DURING CONSTRUCTION.
6. INSTALL REMAINING UTILITIES.
7. PLACE ROAD BASE AND BASE PAVING COURSE.
8. FINE GRADE THE SITE.
9. COMPLETE PAVING.
10. APPLY SEEDING AND MULCHING TO ALL DISTURBED AREAS PER THE SPECIFICATIONS. ALL CUT AND FILL SLOPES OF THE HIGHWAY EXCAVATION AND EMBANKMENT SHALL BE PERMANENTLY STABILIZED AS IN ACCORDANCE WITH DELDOT STANDARD SPECIFICATIONS, SECTION 110.09 VEGETATIVE STABILIZATION.
11. ALL SITE STABILIZATION, INCLUDING BUT NOT LIMITED TO SEEDING, IS TO BE IN PLACE, INSPECTED, AND APPROVED PRIOR TO REMOVAL OF SILT FENCING. ALSO, ANY AREAS DISTURBED BY SILT FENCE REMOVAL ARE TO BE IMMEDIATELY STABILIZED.
12. DOWNSCREEN SEDIMENT CONTROL DEVICES SHALL REMAIN IN PLACE AFTER ALL SITE WORK HAS BEEN COMPLETED WITHIN THE RIGHT OF WAY. FINAL CONVERSION OF SEDIMENT CONTROL DEVICES TO PERMANENT STORMWATER FACILITIES SHALL BE PERFORMED BY OTHERS AND PER THE POST-BULK GRADING PLAN FOR THE PILOT SCHOOL, PREPARED BY FORESITE ASSOCIATES, INC. LAST REVISED 08.05.14.

POST-BULK PLAN

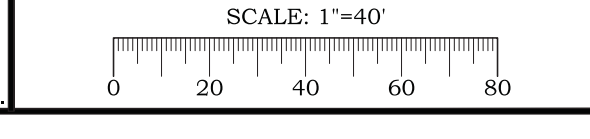
| # | COMMENT | BY | DATE |
|---|---------------------|----|----------|
| 4 | PER DELDOT COMMENTS | AZ | 09.25.17 |
| 3 | PER DELDOT COMMENTS | AZ | 06.15.17 |
| 2 | PER DELDOT COMMENTS | AZ | 12.18.16 |
| 1 | PER DELDOT COMMENTS | AZ | 09.07.16 |

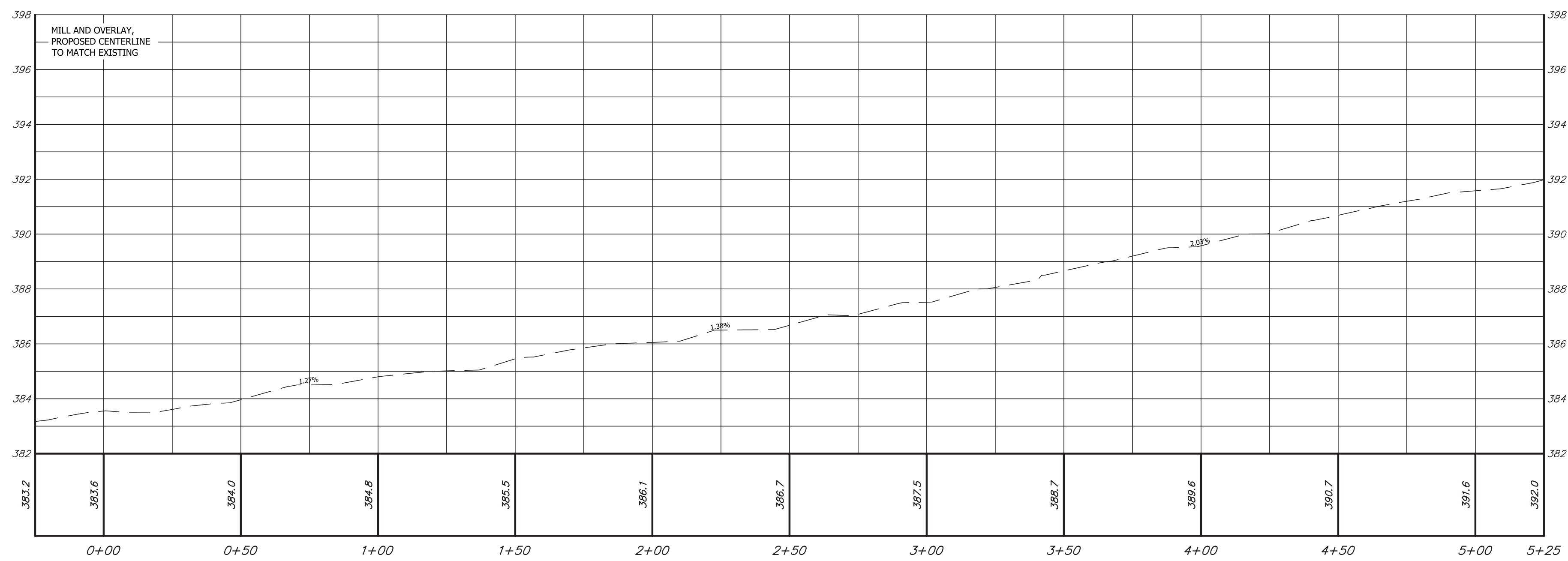


COMMERCIAL ENTRANCE PLAN
EROSION & SEDIMENT CONTROL PLAN

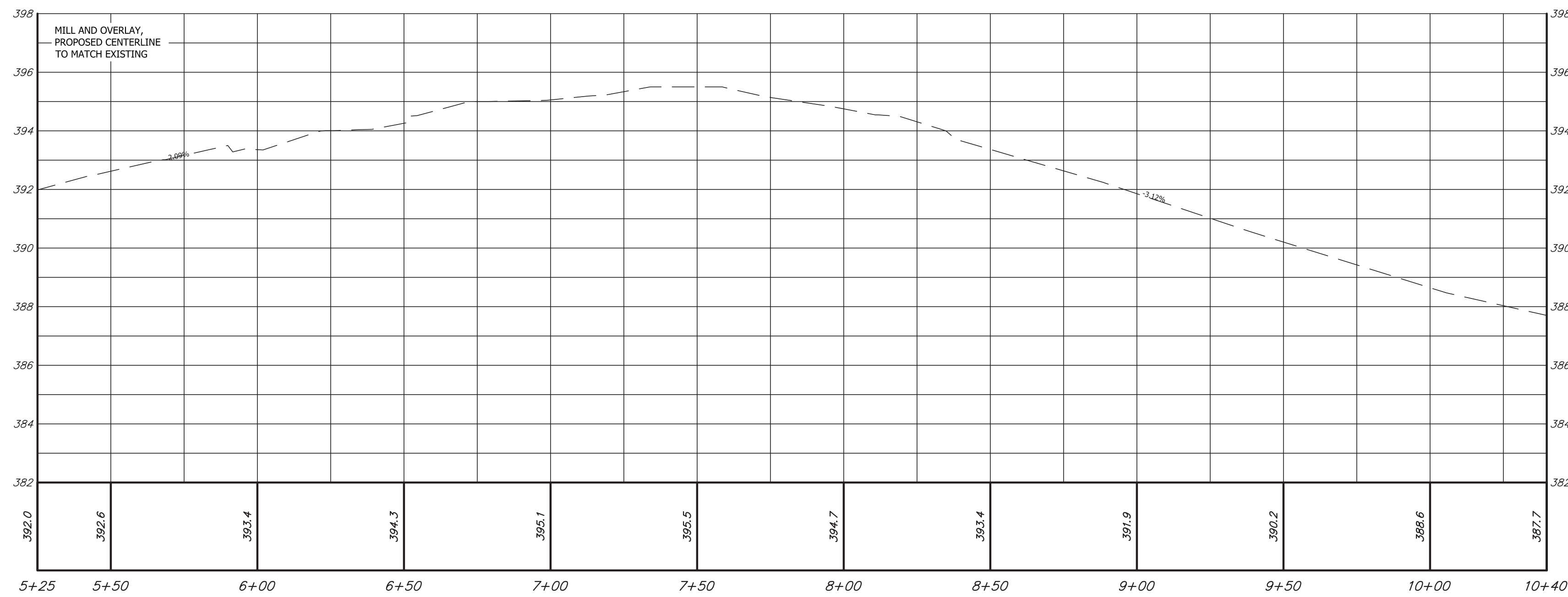
**COLUMBIA PLACE AT
GARDEN OF EDEN ROAD**

| | |
|---|------------------------|
| BRANDYWINE HUNDRED NEW CASTLE COUNTY | WILMINGTON DELAWARE |
| DATE: 02.13.15 | PROJECT #: 001.17 |
| SURVEYED BY: BMC | SHEET: 5 |
| CREATED BY: MDS | |
| DRAWN BY: MDS/AZ | |
| CHECKED BY: ACH | 5 OF 8 |

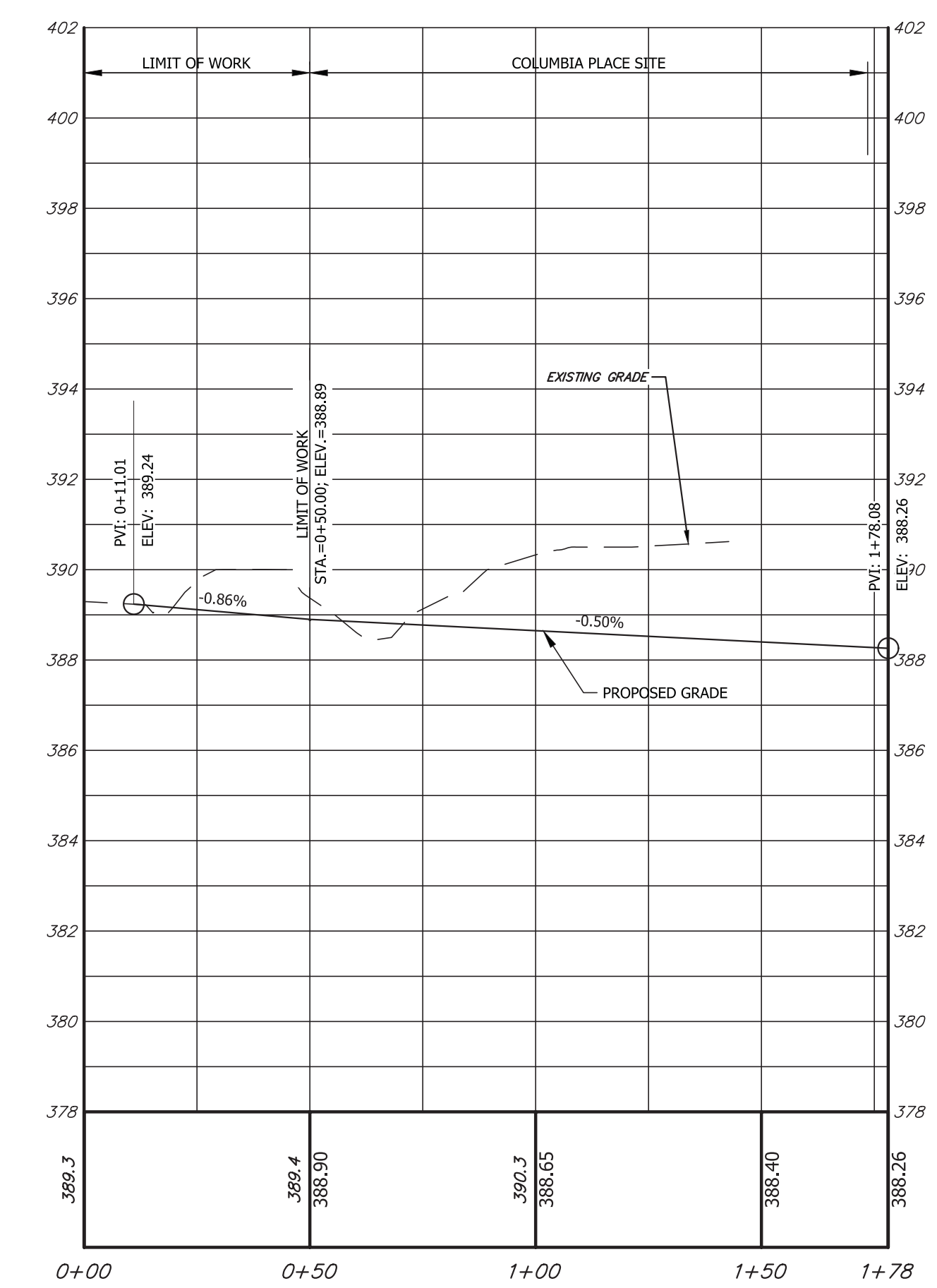




PROFILE VIEW: GARDEN OF EDEN ROAD
 HORZ. SCALE: 1"=30'
 VERT. SCALE: 1"=5'



PROFILE VIEW: GARDEN OF EDEN ROAD
 HORZ. SCALE: 1"=30'
 VERT. SCALE: 1"=5'



PROFILE VIEW: CP DRIVE
 HORZ. SCALE: 1"=30'
 VERT. SCALE: 1"=5'

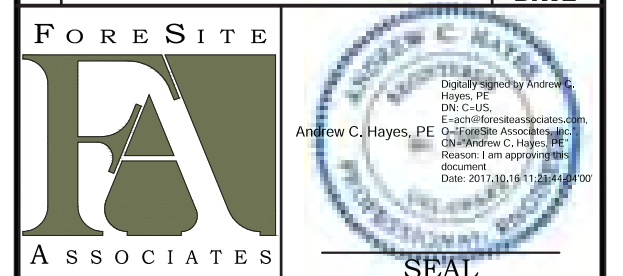
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 REVIEWED FOR
 GENERAL
 CONFORMITY
 Oct. 20, 2017



LAND PLANNING
 - CIVIL ENGINEERING
 - WATER RESOURCES ENGINEERING
 FORESITE ASSOCIATES INC.
 208 DELAWARE STREET
 NEW CASTLE, DE 19720
 PHONE: 302.351.3421
 FAX: 302.351.3456
 EMAIL:
 INFO@FORESITEASSOCIATES.COM

COLUMBIA PLACE AT
 GARDEN OF EDEN ROAD
 COMMERCIAL ENTRANCE PLAN
 REYBOLD VENTURE GROUP XVIII-A, LLC
 116 E. SCOTLAND DR. BEAR, DE 19701

| # | COMMENT | BY | DATE |
|---|---------------------|----|----------|
| 4 | PER DELDOT COMMENTS | AZ | 09.25.17 |
| 3 | PER DELDOT COMMENTS | AZ | 06.15.17 |
| 2 | PER DELDOT COMMENTS | AZ | 12.16.16 |
| 1 | PER DELDOT COMMENTS | AZ | 09.07.16 |



COMMERCIAL ENTRANCE PLAN
 ROAD
 PROFILES

COLUMBIA PLACE AT
 GARDEN OF EDEN ROAD

BRANDYWINE HUNDRED WILMINGTON
 NEW CASTLE COUNTY DELAWARE

DATE: 02.13.15 PROJECT #: 001.17

SURVEYED BY: BSMC SHEET:

CREATED BY: MDS

DRAWN BY: MDS/AZ

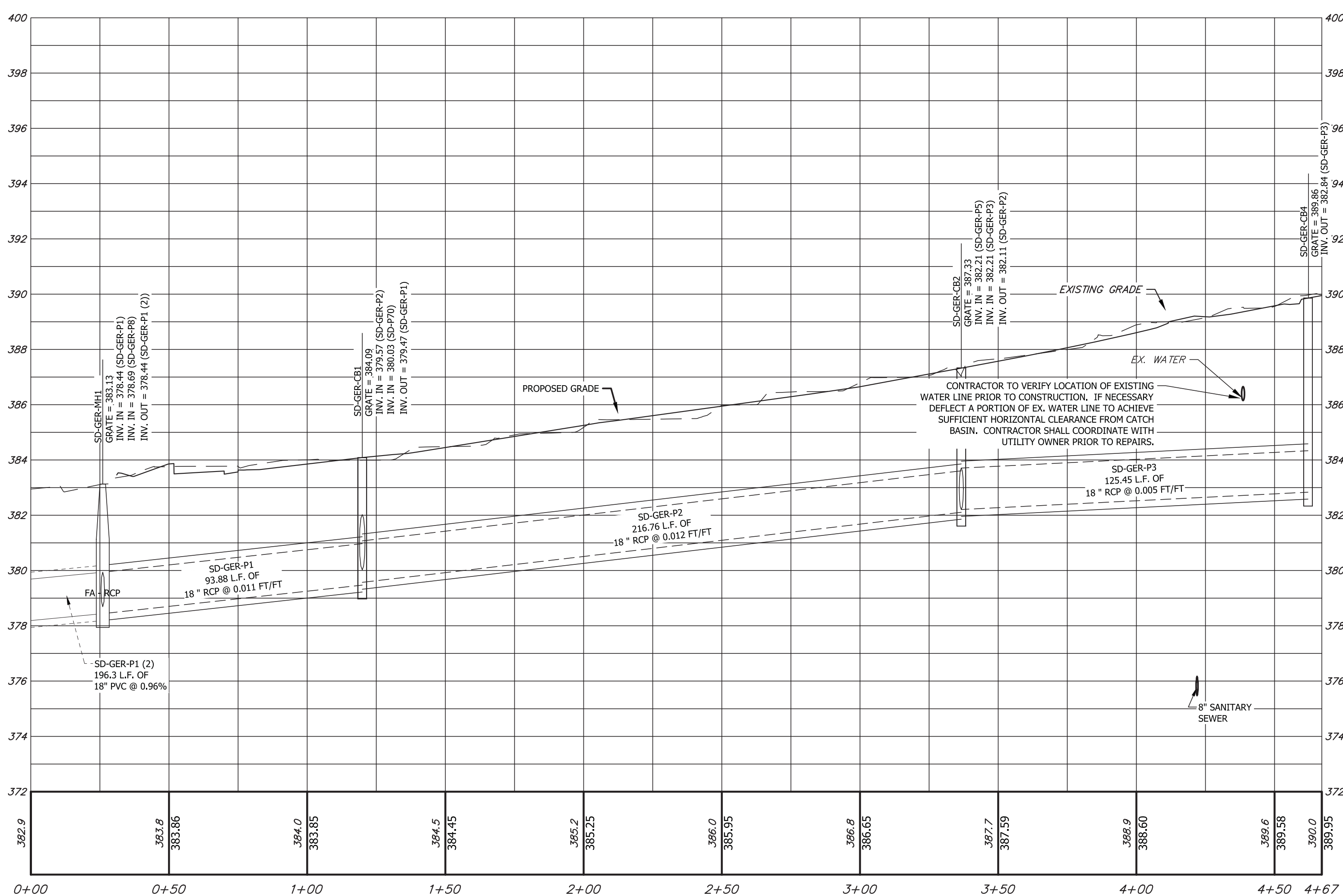
CHECKED BY: ACH

6 OF 8

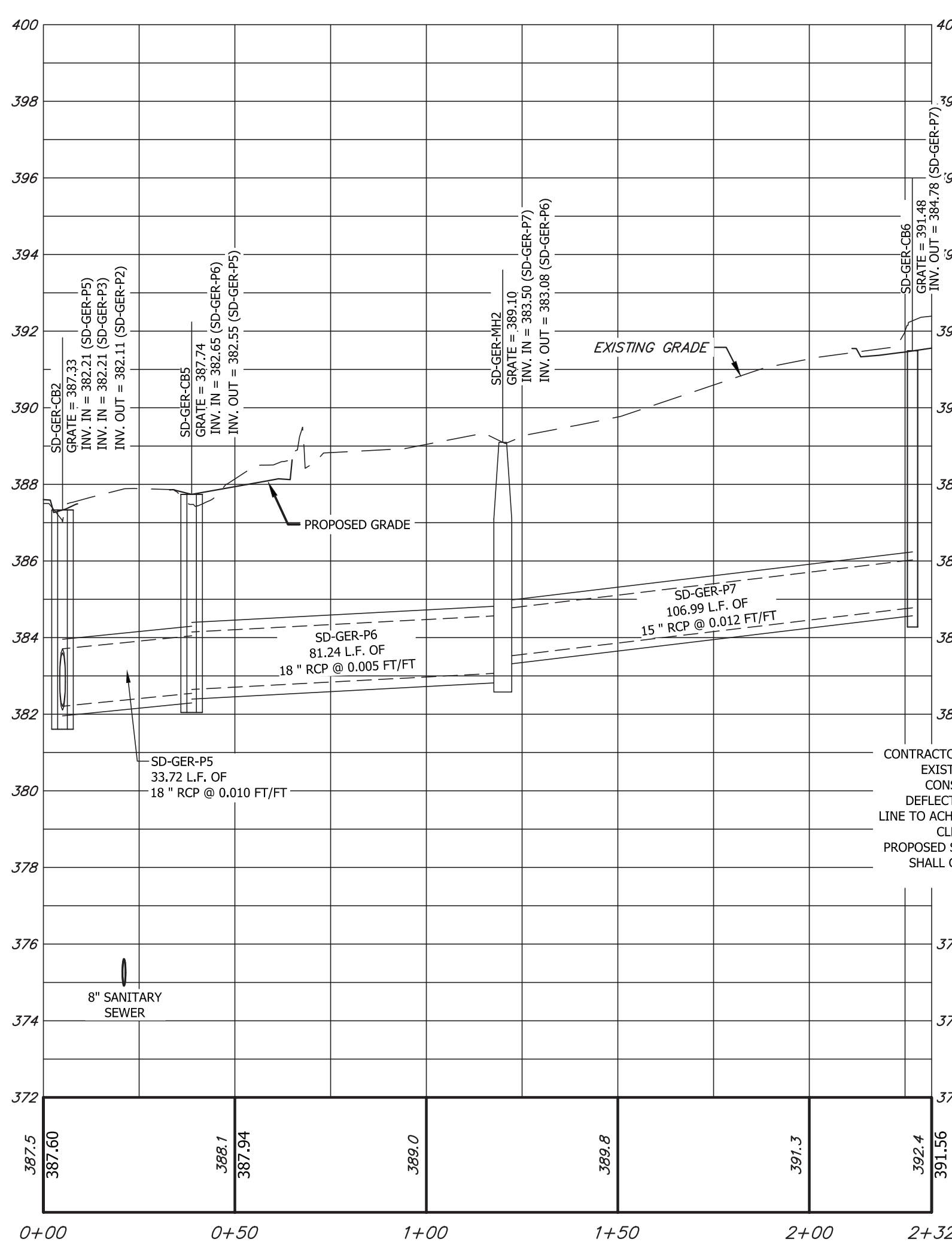
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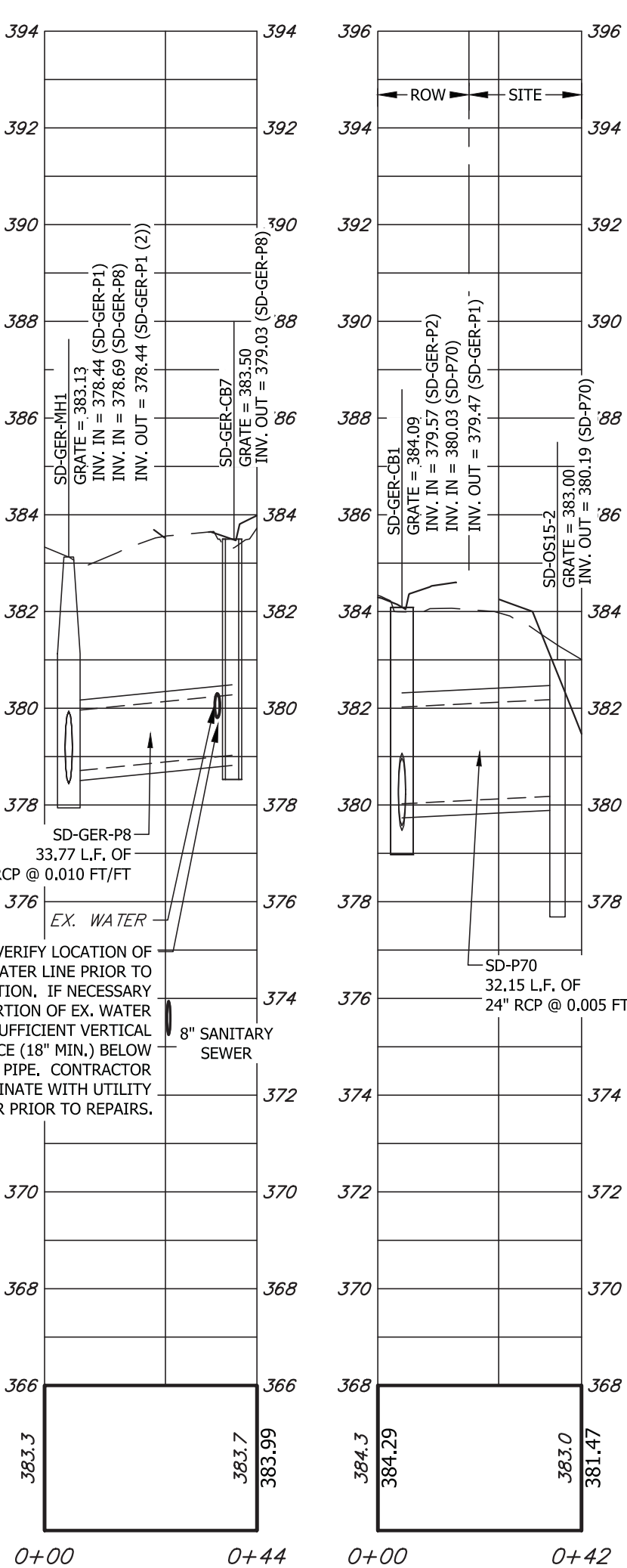
COLUMBIA PLACE AT GARDEN OF EDEN ROAD
COMMERCIAL ENTRANCE PLAN
REYBOLD VENTURE GROUP XVIII-A, LLC
116 E. SCOTLAND DR. BEAR, DE 19701



PROFILE VIEW:
SD-GER-MH1 TO CB4
HORZ. SCALE: 1"=30'
VERT. SCALE: 1"= 3'



PROFILE VIEW:
SD-GER-CB2 TO CB6
HORZ. SCALE: 1"=30'
VERT. SCALE: 1"= 3'



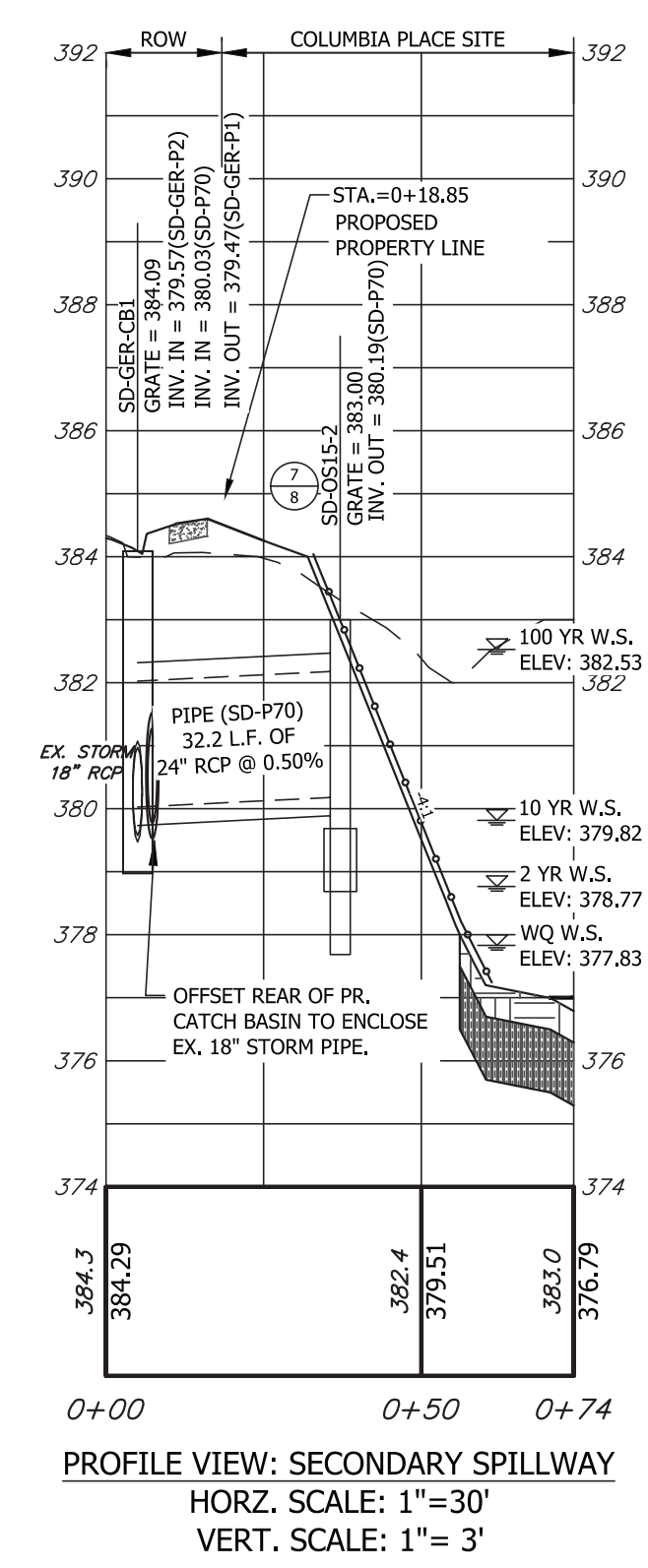
PROFILE VIEW:
SD-GER-MH1 TO CB7
HORZ. SCALE: 1"=30'
VERT. SCALE: 1"= 3'

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GENERAL
CONFORMITY
Oct. 20, 2017

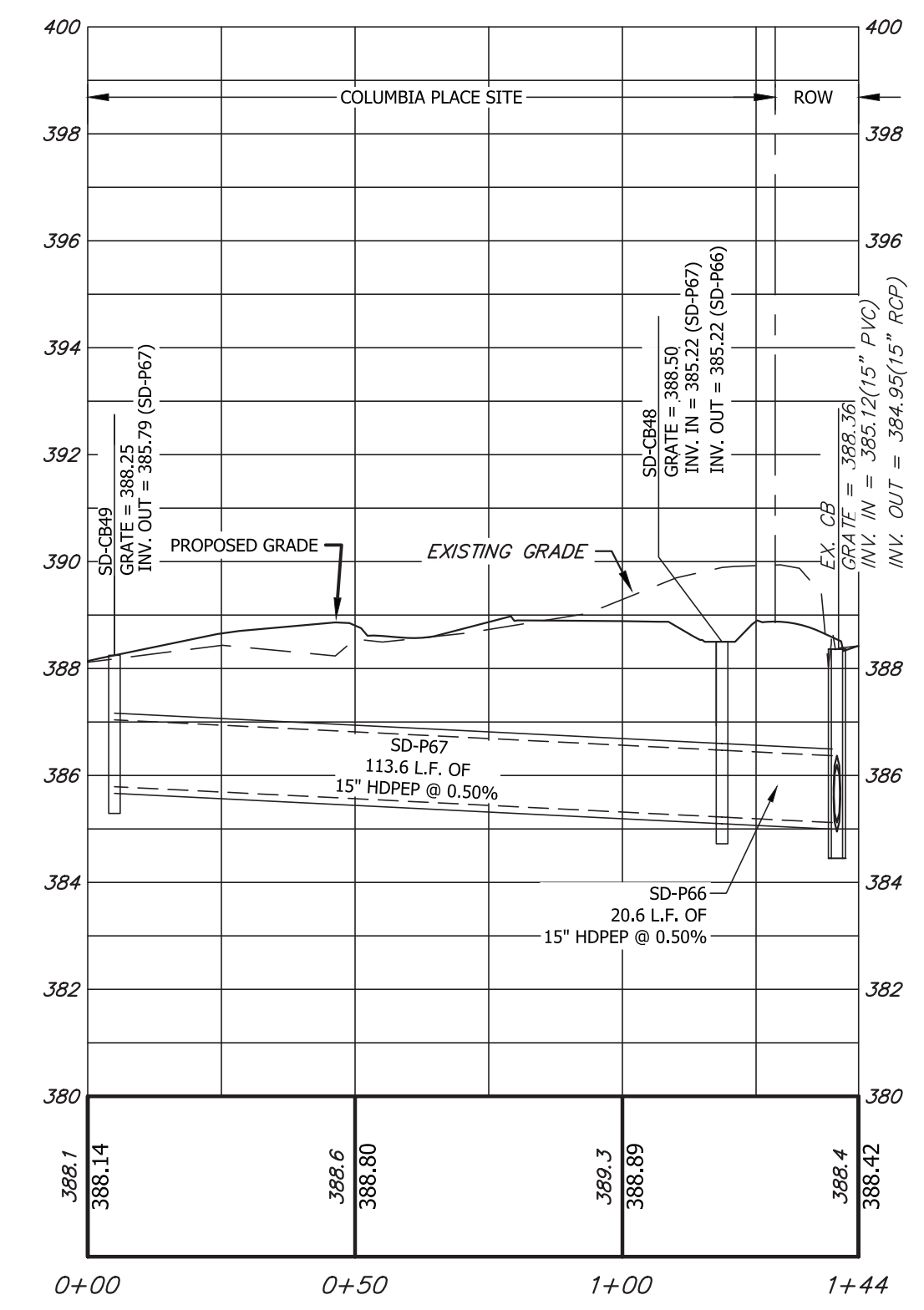
PROFILE VIEW:
SD-GER-CB1 TO OS15-2
HORZ. SCALE: 1"=30'
VERT. SCALE: 1"= 3'

| STORM DRAINAGE STRUCTURE SCHEDULE | | | | | | | |
|-----------------------------------|-------------|----------|---------------|--------------------|------------------------------|----------------|-----------------------|
| NAME | BOX | TOP UNIT | GRATE | TOP OF GRATE (FT.) | INV. IN (FT.) | INV. OUT (FT.) | NOTES |
| SD-GER-CB1 | 30"x48" | TYPE C | TYPE 1 | 384.09 | 379.57 - 18" 380.03 - 24" | 379.47 - 18" | - |
| SD-GER-CB2 | 30"x48" | TYPE C | TYPE 1 | 387.33 | 382.21 - 18" 382.21 - 18" | 382.11 - 18" | - |
| SD-GER-CB4 | 30"x48" | TYPE C | TYPE 1 | 389.86 | 382.84 - 18" | 382.84 - 18" | - |
| SD-GER-CB5 | 30"x48" | TYPE C | TYPE 1 | 387.74 | 382.65 - 18" | 382.55 - 18" | - |
| SD-GER-CB6 | 24"x34" | TYPE C | TYPE 1 | 391.48 | 384.78 - 15" | - | - |
| SD-GER-CB7 | 18"x34" | TYPE C | TYPE 1 | 383.50 | 379.03 - 15" | - | - |
| SD-GER-MH1 | BOX MANHOLE | - | MANHOLE COVER | 383.13 | 378.44 - 18" 378.69 - 15" | 378.44 - 18" | 30"x48" |
| SD-GER-MH2 | BOX MANHOLE | - | MANHOLE COVER | 389.10 | 383.50 - 15" | 383.08 - 18" | 30"x48" |
| SD-OS15-2 | 30"x48" | - | - | 383.00 | 380.19 - 24" | 380.03 - 18" | OMIT TOP UNIT & GRATE |

| STORM DRAINAGE PIPE SCHEDULE | | | | | | | | | |
|------------------------------|------------|------------|---------------|--------------|-------|-----------------|---------------|----------------|-------|
| NAME | FROM | TO | SIZE/MATERIAL | LENGTH (FT.) | CLASS | SLOPE (FT./FT.) | INV. IN (FT.) | INV. OUT (FT.) | NOTES |
| SD-GER-P1 | SD-GER-CB1 | SD-GER-MH1 | 18" RCP | 93.88 | III | 0.011 | 379.47 | 378.44 | - |
| SD-GER-P1 (2) | SD-GER-MH1 | - | 18" RCP | 196.31 | - | 0.010 | 378.44 | 376.56 | - |
| SD-GER-P2 | SD-GER-CB2 | SD-GER-CB1 | 18" RCP | 216.76 | III | 0.012 | 382.11 | 379.57 | - |
| SD-GER-P3 | SD-GER-CB4 | SD-GER-CB2 | 18" RCP | 125.45 | III | 0.005 | 382.84 | 382.21 | - |
| SD-GER-P5 | SD-GER-CB5 | SD-GER-CB2 | 18" RCP | 33.72 | III | 0.010 | 382.55 | 382.21 | - |
| SD-GER-P6 | SD-GER-MH2 | SD-GER-CB5 | 18" RCP | 81.24 | III | 0.005 | 383.08 | 382.65 | - |
| SD-GER-P7 | SD-GER-CB6 | SD-GER-MH2 | 15" RCP | 106.99 | III | 0.012 | 384.78 | 383.50 | - |
| SD-GER-P8 | SD-GER-CB7 | SD-GER-MH1 | 15" RCP | 33.77 | III | 0.010 | 379.03 | 378.69 | - |
| SD-P70 | SD-OS15-2 | SD-GER-CB1 | 24" RCP | 32.15 | III | 0.005 | 380.19 | 380.03 | - |



PROFILE VIEW: SECONDARY SPILLWAY
HORZ. SCALE: 1"=30'
VERT. SCALE: 1"= 3'



PROFILE VIEW:
SD-CB9 TO EX-CB
HORZ. SCALE: 1"=30'
VERT. SCALE: 1"= 3'

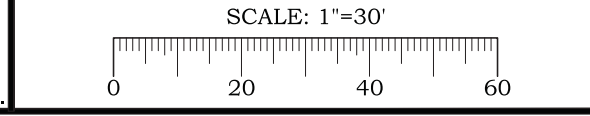
| # | COMMENT | BY | DATE |
|---|---------------------|----|----------|
| 4 | PER DELDOT COMMENTS | AZ | 09.25.17 |
| 3 | PER DELDOT COMMENTS | AZ | 06.18.17 |
| 2 | PER DELDOT COMMENTS | AZ | 12.16.16 |
| 1 | PER DELDOT COMMENTS | AZ | 09.07.16 |

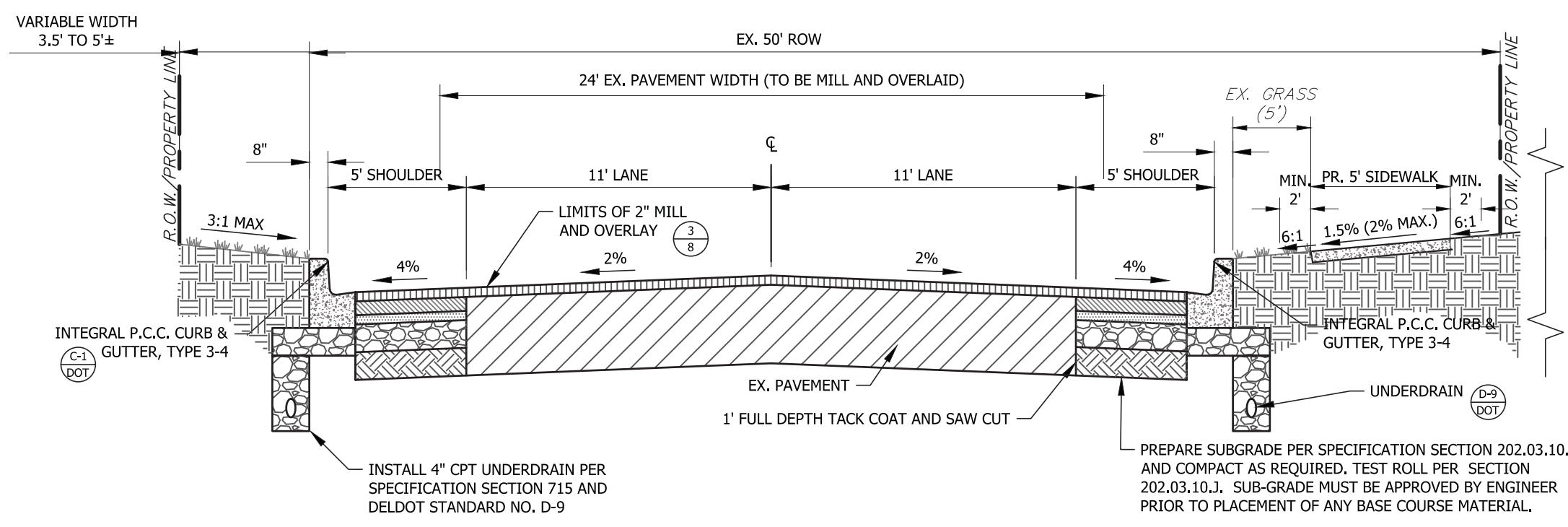
FORE SITE ASSOCIATES
ANDREW C. HOYES, P.E.
SEAL

COMMERCIAL ENTRANCE PLAN
STORM PROFILES

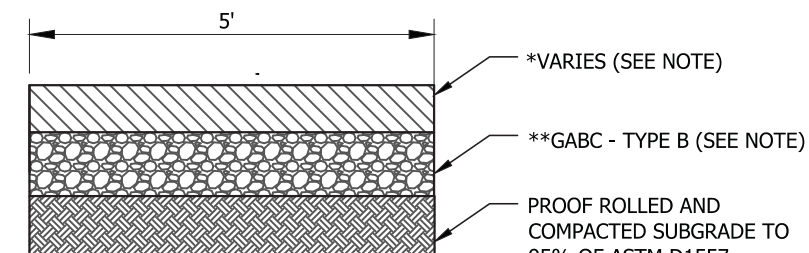
COLUMBIA PLACE AT GARDEN OF EDEN ROAD

| | | | |
|--------------------------------------|---------------------|----------------------|-------------|
| BRANDYWINE HUNDRED NEW CASTLE COUNTY | | WILMINGTON DELAWARE | |
| DATE: 02.13.15 | SURVEYED BY: BMC | PROJECT #: 001.17 | SHEET: 7 |
| CREATED BY: MDS | DRAWN BY: MDS/AZ | CHECKED BY: ACH | 7 OF 8 |

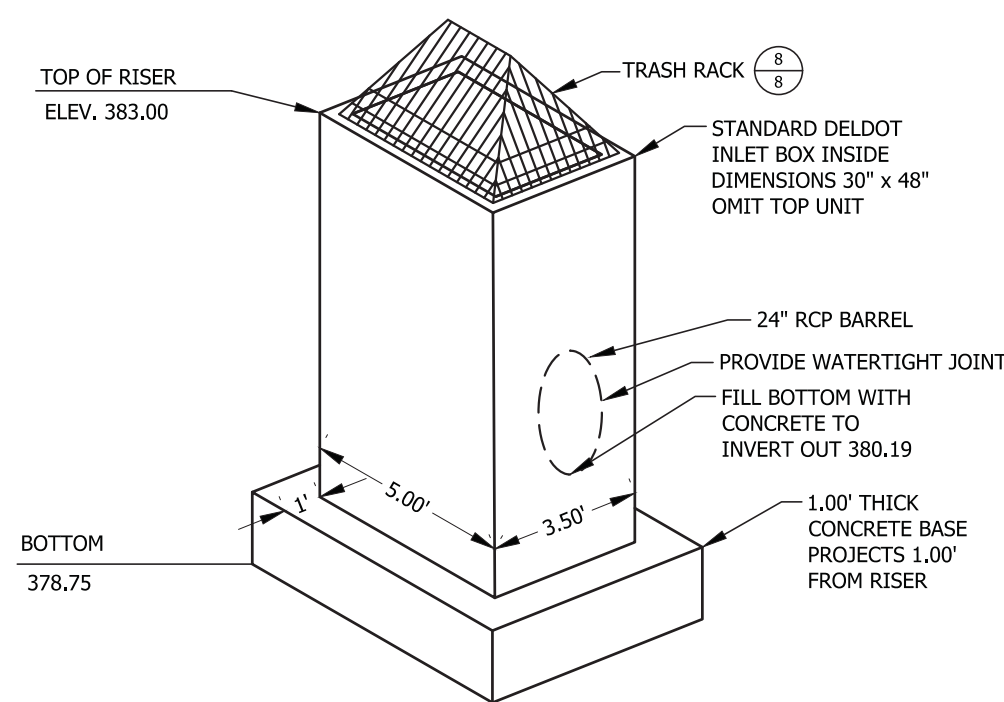




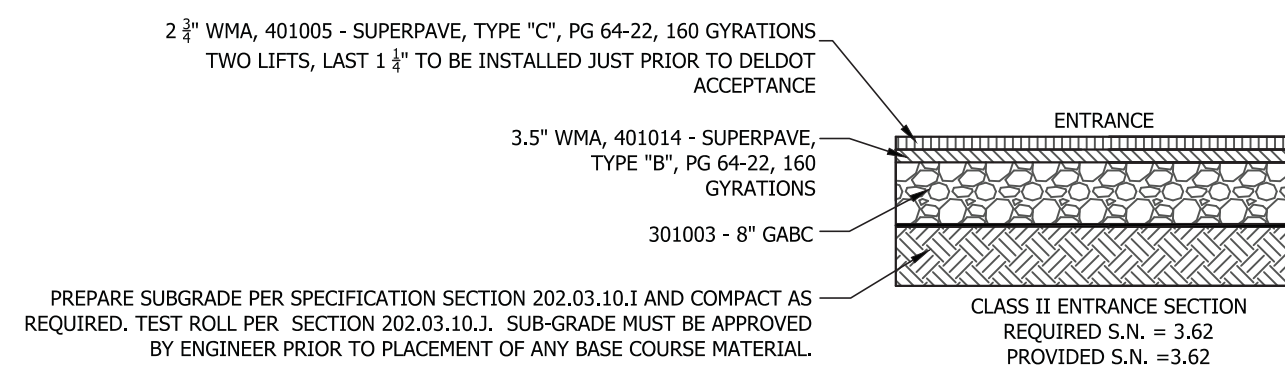
1/8 TYPICAL SECTION - GARDEN OF EDEN ROAD NOT TO SCALE



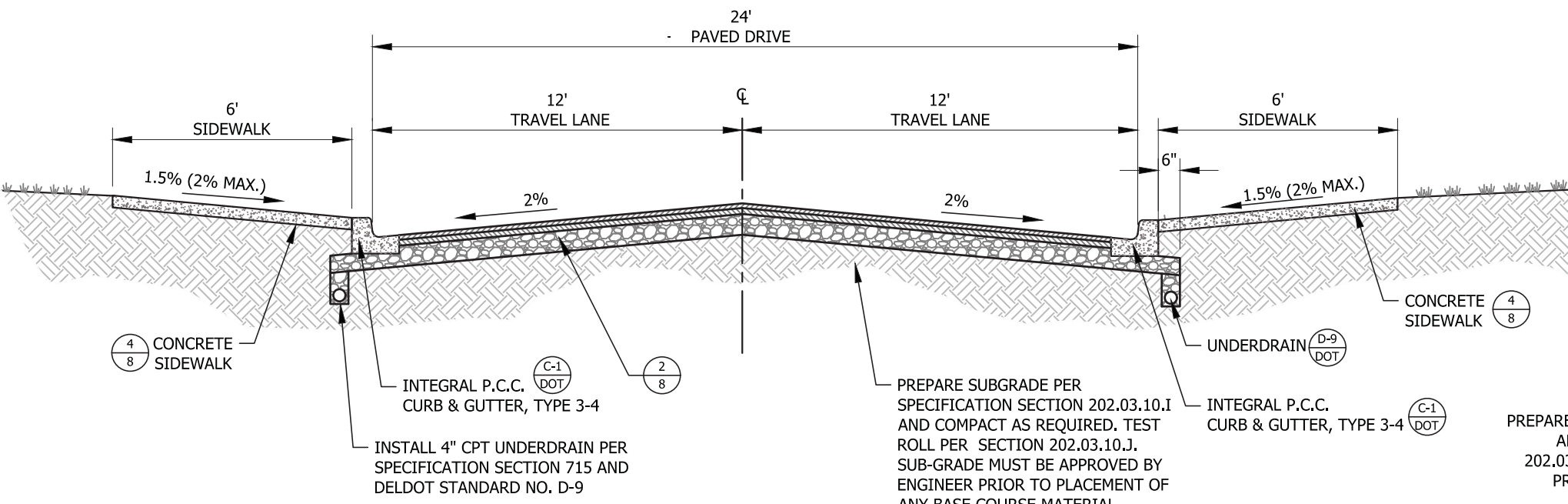
4/8 CONCRETE SIDEWALK SECTION NOT TO SCALE



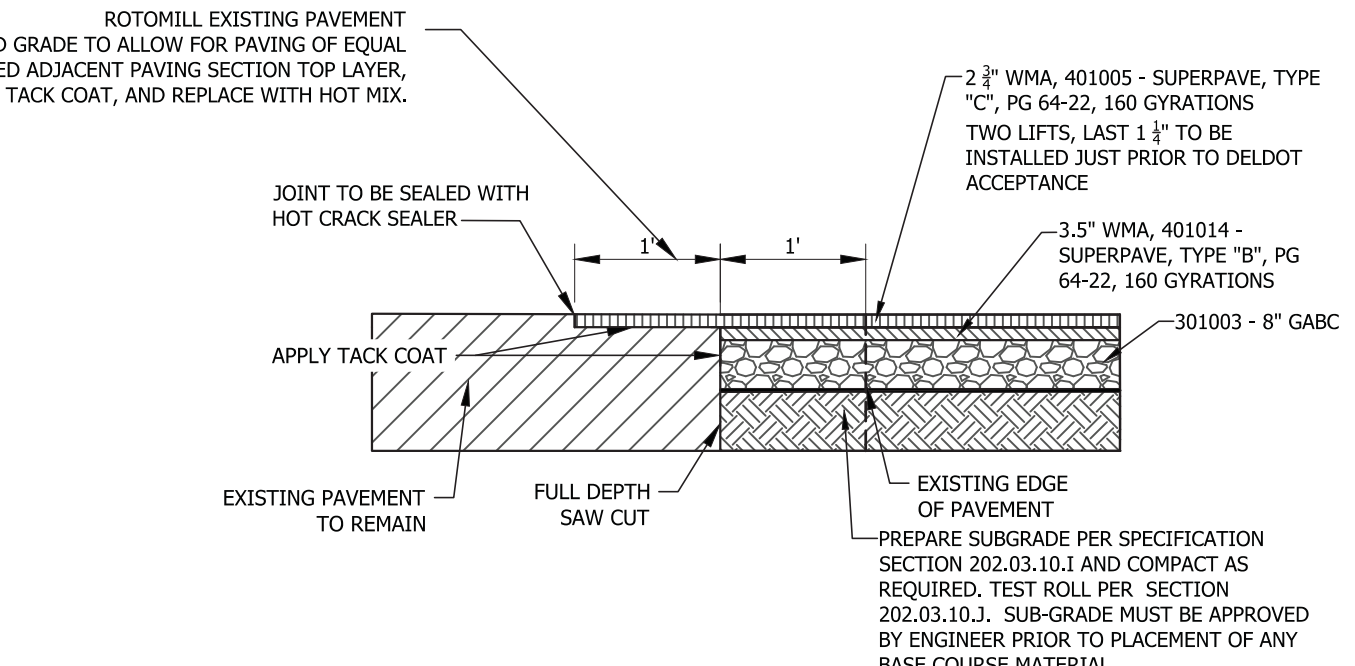
7/8 SECONDARY SPILLWAY OS15-2 SPECIFICATIONS NOT TO SCALE



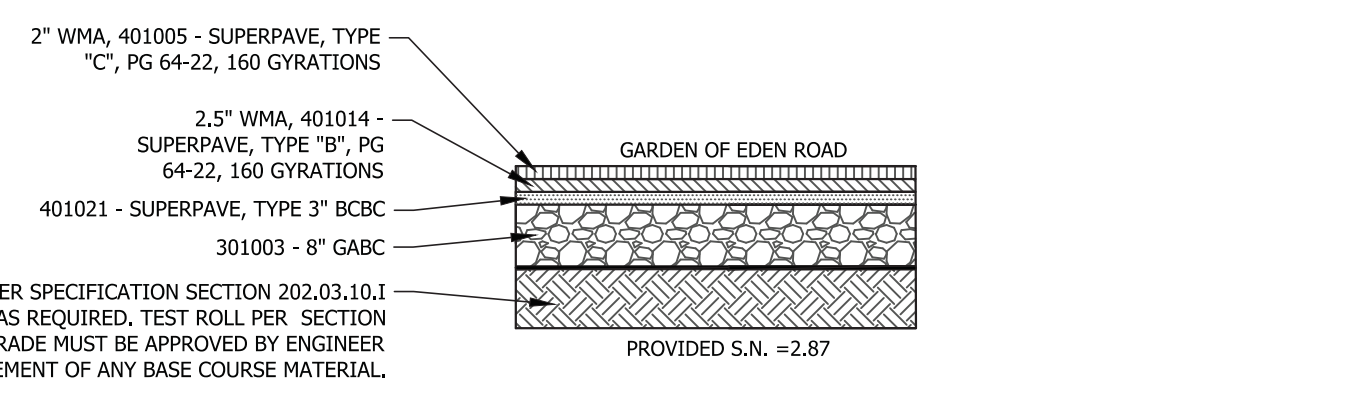
2/8 ENTRANCE PAVEMENT SECTION NOT TO SCALE



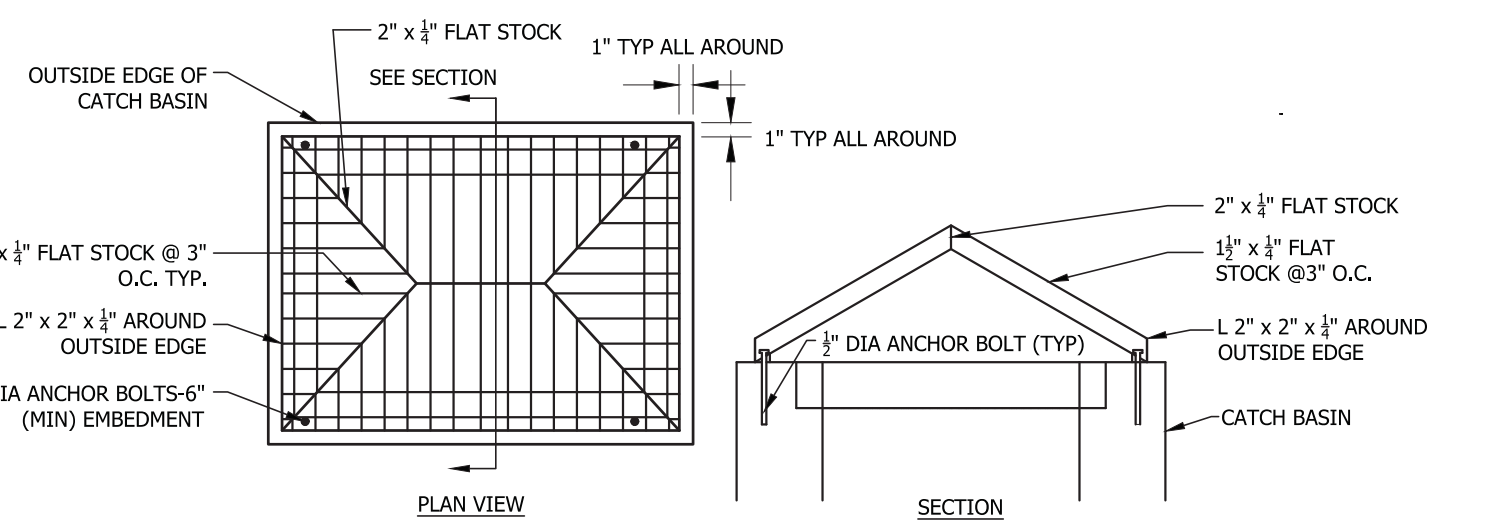
5/8 TYPICAL SECTION - COLUMBIA PLACE DRIVE NOT TO SCALE



3/8 PAVEMENT TIE-IN DETAIL NOT TO SCALE



6/8 GARDEN OF EDEN ROAD SHOULDER PAVEMENT SECTION NOT TO SCALE



8/8 TRASH RACK NOT TO SCALE

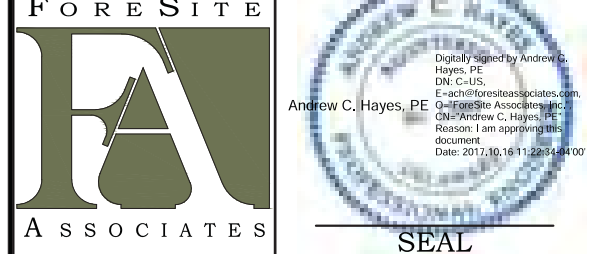
DelDOT
REVIEWED FOR
GENERAL
CONFORMITY
Oct. 20, 2017

MAINTENANCE OF TRAFFIC NOTES

- ALL WORK SHALL BE PERFORMED IN A MANNER THAT WILL REASONABLY PROVIDE THE LEAST PRACTICABLE OBSTRUCTION TO ROAD USERS, INCLUDING VEHICULAR TRAFFIC, BICYCLE TRAFFIC AND PEDESTRIAN TRAFFIC.
- ALL TEMPORARY TRAFFIC CONTROL AND TEMPORARY TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH THE CONTRACT DOCUMENTS, THE LATEST EDITION OF THE MANUAL TITLED "STATE OF DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD)" (HEREINAFTER REFERRED TO AS THE "DELAWARE MUTCD"), CURRENT STATE OF DELAWARE DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, AND SUPPLEMENTAL SPECIFICATIONS, INCLUDING ALL REVISIONS AS OF THE DATE OF THE ENTRANCE PERMIT APPROVAL.
- THE DEPARTMENT RESERVES THE RIGHT TO STOP THE CONTRACTOR'S OPERATIONS, IF, IN THE OPINION OF THE DEPARTMENT'S REPRESENTATIVE, THE CONTRACTOR'S OPERATIONS ARE NOT IN COMPLIANCE WITH THE DELAWARE MUTCD, THE SPECIFICATIONS OR THE PLANS OR IF THE CONTRACTOR'S OPERATIONS ARE DEEMED UNSAFE.
- IF THE CONTRACTOR DESIRES TO DEVIATE FROM THE TEMPORARY TRAFFIC CONTROL PLAN (TTCP) PROVIDED IN THE PLAN SET OR DESIRES CHANGES TO THE PHASING OR SCOPE OF THE TTCP, THE CONTRACTOR SHALL SUBMIT A NEW TTCP TO THE DISTRICT SAFETY OFFICER FOR APPROVAL PRIOR TO THE START OF WORK AT EACH AND EVERY LOCATION. THE TTCP SHALL BE PREPARED, SIGNED AND SEALED BY A PROFESSIONAL ENGINEER REGISTERED IN THE STATE OF DELAWARE AND SHALL BE PREPARED IN ACCORDANCE WITH ALL APPLICABLE DELDOT STANDARDS. THE TTCP SHALL BE SUBMITTED 14 CALENDAR DAYS IN ADVANCE OF STARTING WORK.
- ALL ROADWAY CLOSURES OR LANE CLOSURES BEYOND THOSE SPECIFIED AND APPROVED IN THE PLANS SHALL BE APPROVED BY THE DISTRICT SAFETY OFFICER A MINIMUM OF TWO WEEKS IN ADVANCE OF THE PROPOSED RESTRICTION.
- TEMPORARY TRAFFIC CONTROL DEVICES SHALL BE MAINTAINED IN GOOD CONDITION IN ACCORDANCE WITH THE BROCHURE ENTITLED "QUALITY GUIDELINES FOR TEMPORARY TRAFFIC CONTROL DEVICES", PUBLISHED BY THE AMERICAN TRAFFIC SAFETY SERVICES ASSOCIATION (ATSSA), ANY TEMPORARY TRAFFIC CONTROL DEVICES THAT DO NOT MEET THE QUALITY GUIDELINES SHALL BE REMOVED AND REPLACED WITH ACCEPTABLE DEVICES. FAILURE TO COMPLY WILL RESULT IN WORK STOPPAGE.
- TEMPORARY TRAFFIC CONTROL DEVICES USED ON ALL ROADWAYS OPEN TO THE PUBLIC IN DELAWARE SHALL CONFORM TO THE DELAWARE MUTCD AND SHALL BE IN NEW OR REBURNISHED CONDITION. ALL DEVICES SHALL BE CRASHWORTHY IN ACCORDANCE WITH THE NATIONAL COOPERATIVE HIGHWAY RESEARCH PROGRAM (NCHRP) REPORT 350 AND/OR IN ACCORDANCE WITH THE LATEST EDITION OF THE MANUAL FOR ASSESSING SAFETY HARDWARE (MASH), PUBLISHED BY THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS (AASHTO). THE CONTRACTOR SHALL SUBMIT CERTIFICATION FOR ALL TEMPORARY TRAFFIC CONTROL DEVICES USED SPECIFICALLY ON THIS PROJECT TO THE DISTRICT SAFETY OFFICER AT OR PRIOR TO THE PRE-CONSTRUCTION MEETING. THE CONTRACTOR SHALL NOT BEGIN WORK OR PLACE ANY TEMPORARY TRAFFIC CONTROL DEVICES UNTIL THE CERTIFICATION OF DEVICES HAS BEEN APPROVED BY THE DISTRICT SAFETY OFFICER.
- ANY DEFICIENCIES RELATED TO TEMPORARY TRAFFIC CONTROL THAT ARE REPORTED TO THE CONTRACTOR IN WRITING SHALL BE CORRECTED WITHIN 24 HOURS OR AS DIRECTED BY THE DISTRICT SAFETY OFFICER. CORRECTIVE ACTIONS ON SEVERE DEFICIENCIES SHALL BE TAKEN IMMEDIATELY. FAILURE TO COMPLY WILL RESULT IN THE SUSPENSION OF WORK UNTIL DEVICES ARE BROUGHT BACK INTO COMPLIANCE.
- ACCESS TO ALL BUSINESSES AND RESIDENCES WITHIN THE PROJECT LIMITS SHALL BE MAINTAINED THROUGHOUT THE DURATION OF THIS CONTRACT. ANY TEMPORARY CLOSURE OF A DRIVEWAY OR ENTRANCE FOR TIE-IN PURPOSES SHALL BE COORDINATED WITH THE ENGINEER AND THE PROPERTY OWNER IN ADVANCE OF THE CLOSURE.
- ACCESS TO ALL TRANSIT STOPS LOCATED WITHIN THE PROJECT LIMITS SHALL BE MAINTAINED UNLESS OTHERWISE DIRECTED BY THE PLANS OR THE ENGINEER. MAINTAINING ACCESS TO THE TRANSIT STOP SHALL INCLUDE MAINTAINING AN AREA OF THE TRANSIT VEHICLE TO STOP TO PICK-UP AND DISCHARGE PASSENGERS AND ALSO AN ACCESSIBLE PATH FOR PEDESTRIANS TO SAFELY ACCESS THE TRANSIT STOP.
- THE CONTRACTOR SHALL PROVIDE ALL PROPERTY OWNERS AND RESIDENTS WHO LIVE ADJACENT TO THE WORK ZONE WITH WRITTEN NOTICE, 48 HOURS IN ADVANCE OF THE START OF CONSTRUCTION WORK. THIS NOTIFICATION SHALL INCLUDE THE SCOPE OF WORK, WORKING HOURS, ANTICIPATED START AND COMPLETION DATES; A SUMMARY OF CONSTRUCTION ACTIVITIES WHICH MAY INTERFERE WITH ACCESS TO THE PROPERTY INCLUDING A SCHEDULE AND ACCESS COORDINATION PLAN, CONTRACTOR'S NAME AND ADDRESS AND A DELDOT CONTACT PHONE NUMBER. FAILURE TO GIVE PROPER NOTICE WILL RESULT IN A SUSPENSION OF THE WORK REQUIRING NOTICE, UNTIL PROPER NOTICE IS PROVIDED. THE CONTRACTOR SHALL PROVIDE WRITTEN VERIFICATION TO THE ENGINEER THAT THE PROPERTY OWNERS AND RESIDENTS WERE NOTIFIED.
- THE CONTRACTOR SHALL NOTIFY THE LOCAL 911 CENTER, LOCAL SCHOOLS AND THE DELDOT PUBLIC INFORMATION CENTER OF ALL ROADS AND LANES TO BE CLOSED A MINIMUM OF SEVEN CALENDAR DAYS BEFORE THE CLOSURE.
- THE CONTRACTOR SHALL NOTIFY THE LOCAL 911 CENTER IF ACCESS TO A FIRE HYDRANT IS TEMPORARILY RESTRICTED.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR ENSURING THAT THE TRANSPORTATION MANAGEMENT CENTER IS NOTIFIED EACH AND EVERY DAY WHEN WORK IS BEING PERFORMED IN STATE RIGHT-OF-WAY. THE CONTRACTOR SHALL IDENTIFY THE TYPE OF WORK, ANY LANE(S) OR SHOULDERS CLOSED, THE LENGTH OF TIME FOR WORK, WHEN THE LANE RESTRICTIONS ARE IN PLACE AND WHEN LANE RESTRICTIONS ARE LIFTED, CONTACT PERSON/PHONE NUMBER AND STATE INSPECTOR. THE TRANSPORTATION MANAGEMENT CENTER CAN BE REACHED AT (302) 659-4600.
- AT THE END OF EACH WORKDAY, THE CONTRACTOR SHALL CORRECT ALL VERTICAL DIFFERENCES IN ACCORDANCE WITH TABLE 6G-1 OF THE DELAWARE MUTCD.
- AT THE END OF EACH DAY'S OPERATION AND BEFORE TRAFFIC IS RETURNED TO UNRESTRICTED ROADWAY USE, TEMPORARY PAVEMENT MARKINGS SHALL BE APPLIED IN ACCORDANCE WITH THE DELAWARE MUTCD AND DELDOT'S TEMPORARY PAVEMENT MARKINGS POLICY.
- WHEN SIDE ROADS INTERSECT THE WORK ZONE, ADDITIONAL TRAFFIC CONTROL DEVICES SHALL BE ERECTED INCLUDING PERMANENT WARNING SIGNS.
- ALL STORAGE OF EQUIPMENT AND MATERIAL SHALL COMPLY WITH SECTION 6G.22 OF THE DELAWARE MUTCD.
- ALL FLAGGERS SHALL COMPLY WITH CHAPTER 6E OF THE DELAWARE MUTCD.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING HIS/HER WORK WITH OTHER CONTRACTORS IN THE AREA.
- ALL PERSONS WORKING WITHIN THE STATE RIGHT-OF-WAY SHALL WEAR A MINIMUM OF AN ANSI CLASS II SAFETY VEST MEETING OR EXCEEDING THE ANSI 107-2004 REQUIREMENTS, AS SPECIFIED IN THE DELAWARE MUTCD.
- ALL PAVEMENT MARKINGS THAT ARE NO LONGER IN USE AND CONFLICT WITH TEMPORARY PAVEMENT MARKINGS SHALL BE REMOVED AND COMPLETELY OBLITERATED BY A METHOD APPROVED BY THE ENGINEER. PAINTING OVER THE CONFLICTING PAVEMENT MARKINGS WILL NOT BE ACCEPTED AS A METHOD OF REMOVAL.
- THE CONTRACTOR IS RESPONSIBLE FOR THE MAINTENANCE OF EXISTING PAVEMENT WITHIN THE PROJECT LIMITS FOR THE DURATION OF THE CONTRACT OR AS DIRECTED BY THE ENGINEER.
- ALL ROADWAYS AND ENTRANCES NOT OPEN TO TRAFFIC SHALL BE CLOSED USING TYPE III BARRICADES AND SHALL BE INSTALLED PER THE DELAWARE MUTCD. IF THE ROADWAY OR ENTRANCE IS CLOSED FOR MORE THAN ONE MONTH, THE CONTRACTOR SHALL ERECT PERMANENT BARRICADES AS DIRECTED IN PART 3 OF THE DELAWARE MUTCD.
- WITHIN THE MAINLINE WORK AREA, PERMANENT ADVANCE WARNING SIGNS WITH THE LEGENDS ROAD WORK 1500 FT, ROAD WORK 1000 FT AND ROAD WORK 500 FT SHALL BE INSTALLED IN ADVANCE OF THE WORK AREA IN BOTH DIRECTIONS. AN END ROAD WORK SIGN SHALL BE LOCATED 500 FEET DOWNSTREAM FROM THE WORK AREA. ALL PERMANENT ADVANCE WARNING SIGNS SHALL BE GROUND MOUNTED ON TWO NCHRP-350 OR MASH APPROVED BREAKAWAY POSTS AND SHALL BE MOUNTED IN COMPLIANCE WITH THE DELAWARE MUTCD. PERMANENT ADVANCE WARNING SIGNS SHALL BE MOUNTED AT A HEIGHT OF 7 FEET, MEASURED FROM THE ROADWAY TO THE BOTTOM OF THE SIGN. THE USE OF SKID MOUNTED SIGN SUPPORTS IS NOT ALLOWED UNLESS THE CONTRACTOR CAN DEMONSTRATE THAT A UTILITY CONFLICT EXISTS, WHICH SHALL BE VERIFIED BY THE ENGINEER, OR CONCRETE MEDIANS PREVENT THE INSTALLATION OF THE PERMANENT ADVANCE WARNING SIGNS IN THE APPROPRIATE LOCATION.
- TYPICAL APPLICATIONS PER THE DELAWARE MUTCD SHALL BE INCORPORATED TO ACHIEVE REQUIRED TEMPORARY TRAFFIC CONTROL AND SAFETY REQUIREMENTS. THIS PROJECT IS SUBJECT TO THE FOLLOWING TYPICAL APPLICATIONS UNLESS DIRECTED OTHERWISE BY THE DELDOT DISTRICT SAFETY OFFICER: TYPICAL APPLICATION 10: LANE CLOSURE ON A TWO-LANE ROAD USING FLAGGERS.

**COLUMBIA PLACE AT
 GARDEN OF EDEN ROAD
 COMMERCIAL ENTRANCE PLAN
 REYBOLD VENTURE GROUP XVIII-A, LLC
 116 E. SCOTLAND DR. BEAR, DE 19701**

| # | COMMENT | BY | DATE |
|---|---------------------|----|----------|
| 4 | PER DELDOT COMMENTS | AZ | 09.25.17 |
| 3 | PER DELDOT COMMENTS | AZ | 06.15.17 |
| 2 | PER DELDOT COMMENTS | AZ | 12.16.16 |
| 1 | PER DELDOT COMMENTS | AZ | 09.07.16 |



COMMERCIAL ENTRANCE PLAN

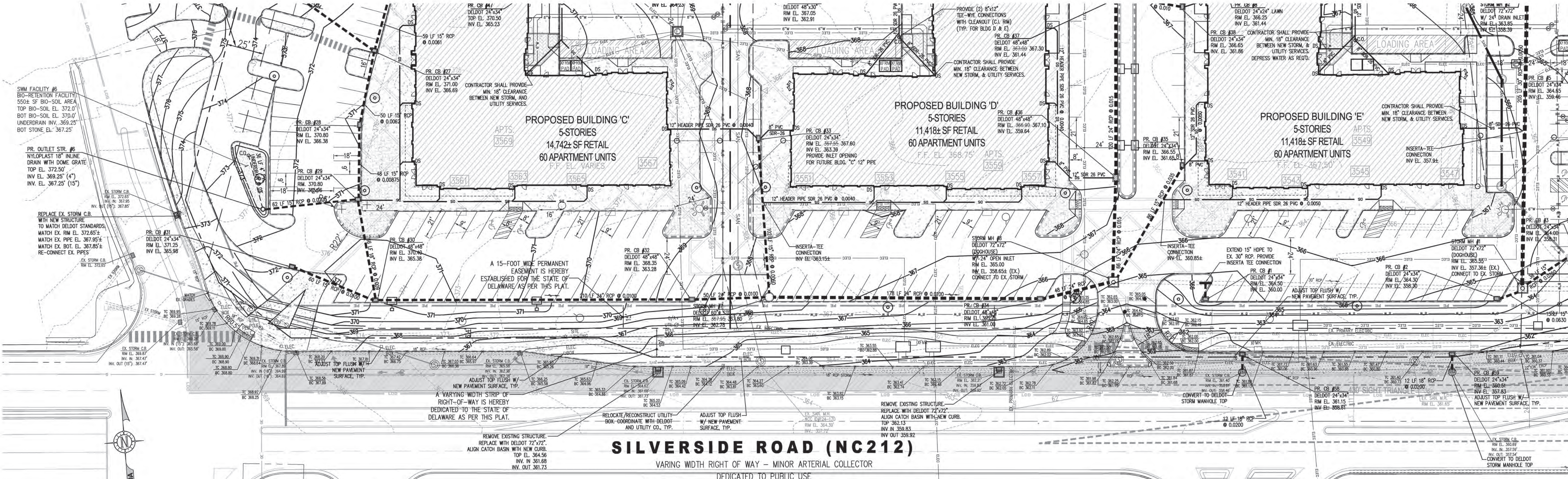
DETAILS

COLUMBIA PLACE AT
GARDEN OF EDEN ROAD

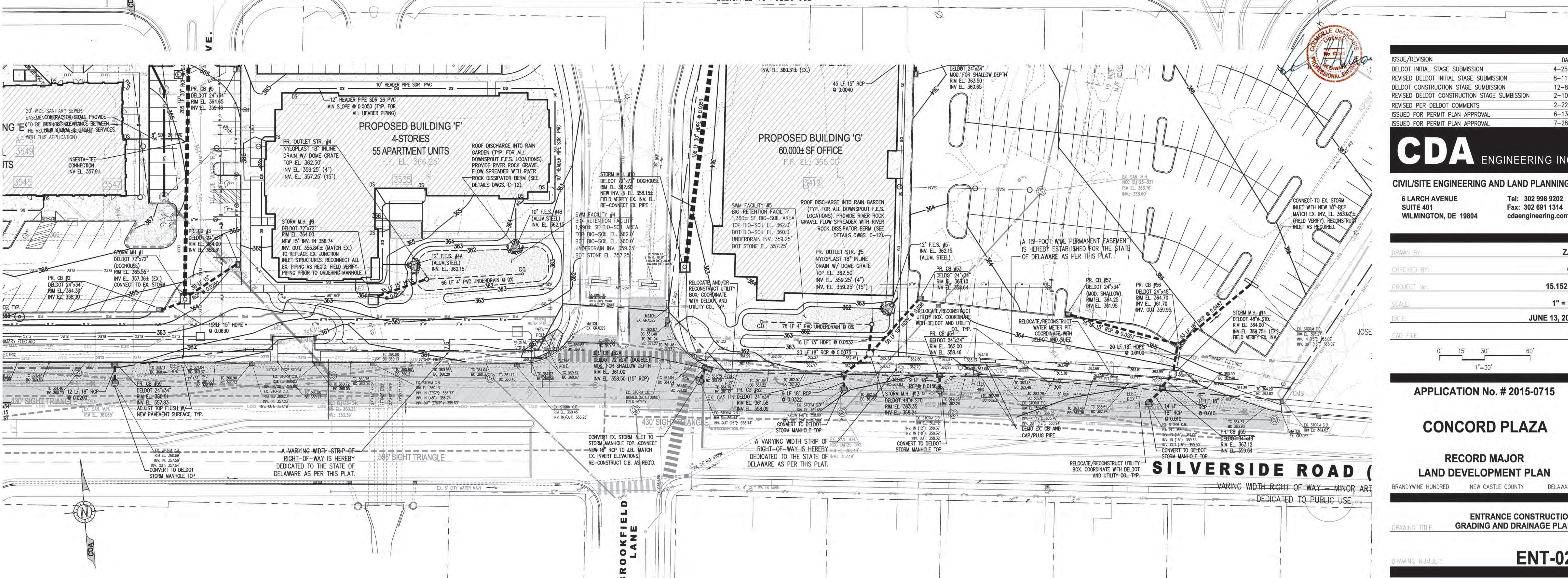
| | |
|---|------------------------|
| BRANDYWINE HUNDRED NEW CASTLE COUNTY | WILMINGTON DELAWARE |
| DATE: 02.13.15 | PROJECT #: 001.17 |
| SURVEYED BY: BMC | SHEET: 8 |
| CREATED BY: MDS/MMH | 8 OF 8 |
| DRAWN BY: MDS/AZ | SCALE: AS NOTED |
| CHECKED BY: ACH | |

CONCORD PLAZA

APPENDIX B – PLANS FOR ADJACENT PROJECTS



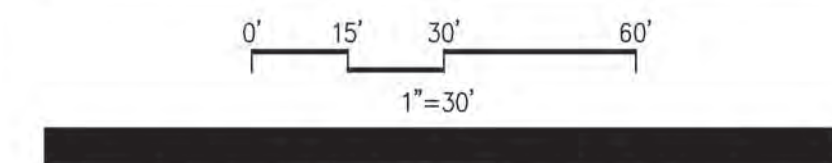
SILVERSIDE ROAD (NC212)
 VARYING WIDTH RIGHT OF WAY - MINOR ARTERIAL COLLECTOR
 DEDICATED TO PUBLIC USE



| ISSUE/REVISION | DATE |
|---|---------|
| DELETED INITIAL STAGE SUBMISSION | 4-25-16 |
| REVISED DELETED INITIAL STAGE SUBMISSION | 8-11-16 |
| DELETED CONSTRUCTION STAGE SUBMISSION | 12-8-16 |
| REVISED DELETED CONSTRUCTION STAGE SUBMISSION | 2-10-17 |
| REVISED PER DELETED COMMENTS | 2-22-17 |
| ISSUED FOR PERMIT PLAN APPROVAL | 6-13-17 |
| ISSUED FOR PERMIT PLAN APPROVAL | 7-28-17 |

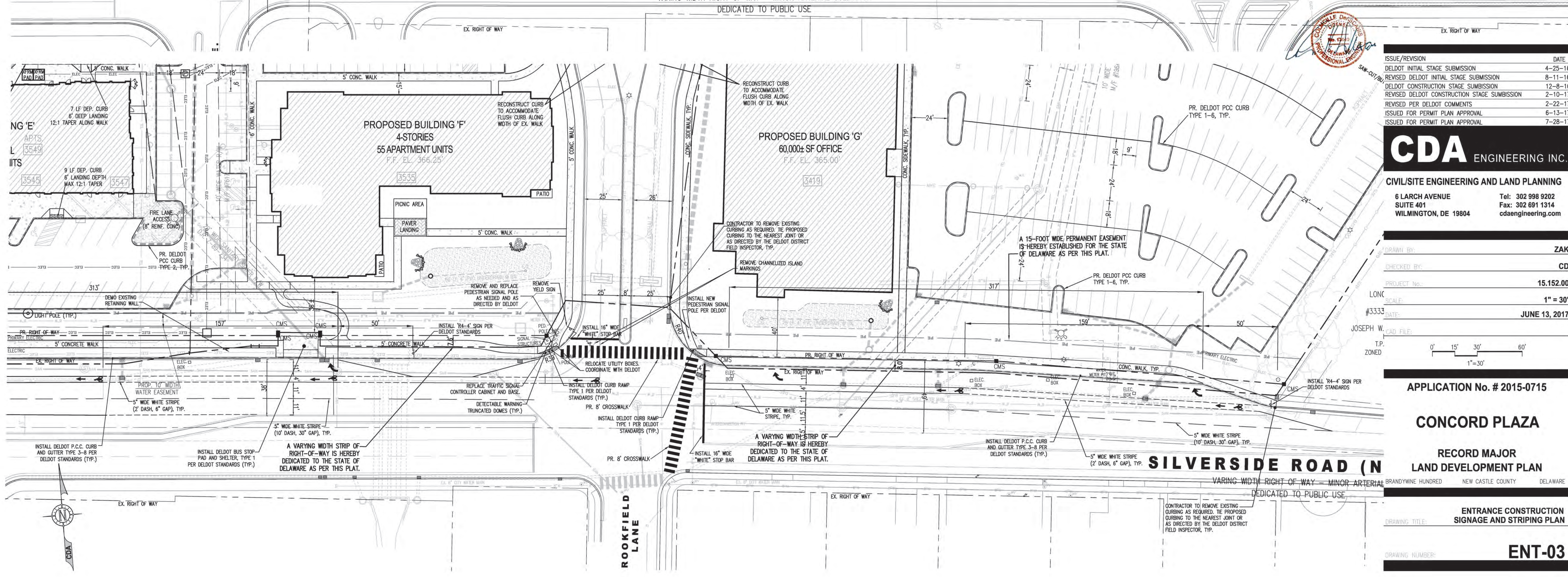
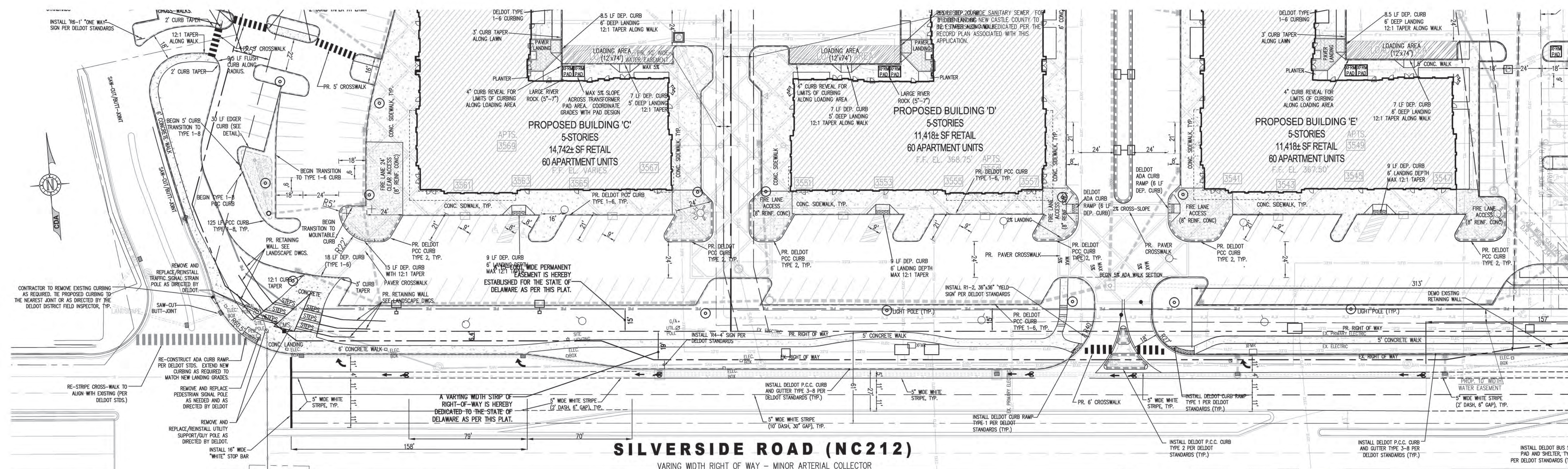
CDA ENGINEERING INC.
 CIVIL/SITE ENGINEERING AND LAND PLANNING
 6 LARCH AVENUE
 SUITE 401
 WILMINGTON, DE 19804
 Tel: 302 998 9202
 Fax: 302 691 1314
 cdaengineering.com

DRAWN BY: ZAK
 CHECKED BY: CD
 PROJECT No.: 15.152.00
 SCALE: 1" = 30'
 DATE: JUNE 13, 2017
 CAD FILE:



APPLICATION No. # 2015-0715

CONCORD PLAZA
 RECORD MAJOR
 LAND DEVELOPMENT PLAN
 BRANDYWINE HUNDRED NEW CASTLE COUNTY DELAWARE
 ENTRANCE CONSTRUCTION
 GRADING AND DRAINAGE PLAN
 DRAWING NUMBER: **ENT-02**



| ISSUE/REVISION | DATE |
|--|---------|
| DELDOT INITIAL STAGE SUBMISSION | 4-25-16 |
| REVISED DELDOT INITIAL STAGE SUBMISSION | 8-11-16 |
| DELDOT CONSTRUCTION STAGE SUBMISSION | 12-8-16 |
| REVISED DELDOT CONSTRUCTION STAGE SUBMISSION | 2-10-17 |
| REVISED PER DELDOT COMMENTS | 2-22-17 |
| ISSUED FOR PERMIT PLAN APPROVAL | 6-13-17 |
| ISSUED FOR PERMIT PLAN APPROVAL | 7-28-17 |

CDA ENGINEERING INC.
CIVIL/SITE ENGINEERING AND LAND PLANNING
6 LARCH AVENUE SUITE 401 WILMINGTON, DE 19804
Tel: 302.998.9202 Fax: 302.691.1314 cdaengineering.com

DRAWN BY: ZAK
CHECKED BY: CD
PROJECT No.: 15.152.00
LONG SCALE: 1" = 30'
DATE: JUNE 13, 2017
#3333
JOSEPH W. T.P. ZONED

APPLICATION No. # 2015-0715
CONCORD PLAZA
RECORD MAJOR LAND DEVELOPMENT PLAN
BRANDYWINE HUNDRED NEW CASTLE COUNTY DELAWARE

ENTRANCE CONSTRUCTION SIGNAGE AND STRIPING PLAN
DRAWING NUMBER: **ENT-03**

MOT NOTES:

- ALL WORK SHALL BE PERFORMED IN A MANNER THAT WILL REASONABLY PROVIDE THE LEAST PRACTICABLE OBSTRUCTION TO ROAD USERS, INCLUDING VEHICULAR TRAFFIC, BICYCLE TRAFFIC AND PEDESTRIAN TRAFFIC.
- ALL TEMPORARY TRAFFIC CONTROL AND TEMPORARY TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH THE CONTRACT DOCUMENTS, THE LATEST EDITION OF THE MANUAL TITLED "STATE OF DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD)" (HEREINAFTER REFERRED TO AS THE "DELAWARE MUTCD"), CURRENT STATE OF DELAWARE DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, AND SUPPLEMENTAL SPECIFICATIONS, INCLUDING ALL REVISIONS AS OF THE DATE OF THE ENTRANCE PERMIT APPROVAL.
- THE DEPARTMENT RESERVES THE RIGHT TO STOP THE CONTRACTOR'S OPERATIONS, IF, IN THE OPINION OF THE DEPARTMENT'S REPRESENTATIVE, THE CONTRACTOR'S OPERATIONS ARE NOT IN COMPLIANCE WITH THE DELAWARE MUTCD, THE SPECIFICATIONS OR THE PLANS OR IF THE CONTRACTOR'S OPERATIONS ARE DEEMED UNSAFE.
- IF THE CONTRACTOR DESIRES TO DEVIATE FROM THE TEMPORARY TRAFFIC CONTROL PLAN (TTCP) PROVIDED IN THE PLAN SET OR DESIRES CHANGES TO THE PHASING OR SCOPE OF THE TTCP, THE CONTRACTOR SHALL SUBMIT A NEW TTCP TO THE DISTRICT SAFETY OFFICER FOR APPROVAL PRIOR TO THE START OF WORK AT EACH AND EVERY LOCATION. THE TTCP SHALL BE PREPARED, SIGNED AND SEALED BY A PROFESSIONAL ENGINEER REGISTERED IN THE STATE OF DELAWARE AND SHALL BE PREPARED IN ACCORDANCE WITH ALL APPLICABLE DELDOT STANDARDS. THE TTCP SHALL BE SUBMITTED 14 CALENDAR DAYS IN ADVANCE OF STARTING WORK.
- ALL ROADWAY CLOSURES OR LANE CLOSURES BEYOND THOSE SPECIFIED AND APPROVED IN THE PLANS SHALL BE APPROVED BY THE DISTRICT SAFETY OFFICER A MINIMUM OF TWO WEEKS IN ADVANCE OF THE PROPOSED RESTRICTION.
- TEMPORARY TRAFFIC CONTROL DEVICES SHALL BE MAINTAINED IN GOOD CONDITION IN ACCORDANCE WITH THE BROCHURE ENTITLED "QUALITY GUIDELINES FOR TEMPORARY TRAFFIC CONTROL DEVICES", PUBLISHED BY THE AMERICAN TRAFFIC SAFETY SERVICES ASSOCIATION (ATSSA). ANY TEMPORARY TRAFFIC CONTROL DEVICES THAT DO NOT MEET THE QUALITY GUIDELINES SHALL BE REMOVED AND REPLACED WITH ACCEPTABLE DEVICES. FAILURE TO COMPLY WILL RESULT IN WORK STOPPAGE.
- TEMPORARY TRAFFIC CONTROL DEVICES USED ON ALL ROADWAYS OPEN TO THE PUBLIC IN DELAWARE SHALL CONFORM TO THE DELAWARE MUTCD AND SHALL BE IN NEW OR REFURBISHED CONDITION. ALL DEVICES SHALL BE CRASHWORTHY IN ACCORDANCE WITH THE NATIONAL COOPERATIVE HIGHWAY RESEARCH PROGRAM (NCHRP) REPORT 350 AND/OR IN ACCORDANCE WITH THE LATEST EDITION OF THE MANUAL FOR ASSESSING SAFETY HARDWARE (MASH), PUBLISHED BY THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS (AASHTO). THE CONTRACTOR SHALL SUBMIT CERTIFICATION FOR ALL TEMPORARY TRAFFIC CONTROL DEVICES USED SPECIFICALLY ON THIS PROJECT TO THE DISTRICT SAFETY OFFICER AT OR PRIOR TO THE PRE-CONSTRUCTION MEETING. THE CONTRACTOR SHALL NOT BEGIN WORK OR PLACE ANY TEMPORARY TRAFFIC CONTROL DEVICES UNTIL THE CERTIFICATION OF DEVICES HAS BEEN APPROVED BY THE DISTRICT SAFETY OFFICER.
- ANY DEFICIENCIES RELATED TO TEMPORARY TRAFFIC CONTROL THAT ARE REPORTED TO THE CONTRACTOR IN WRITING SHALL BE CORRECTED WITHIN 24 HOURS OR AS DIRECTED BY THE DISTRICT SAFETY OFFICER. CORRECTIVE ACTIONS ON SEVERE DEFICIENCIES SHALL BE TAKEN IMMEDIATELY. FAILURE TO COMPLY WILL RESULT IN THE SUSPENSION OF WORK UNTIL DEVICES ARE BROUGHT BACK INTO COMPLIANCE.
- ACCESS TO ALL BUSINESSES AND RESIDENCES WITHIN THE PROJECT LIMITS SHALL BE MAINTAINED THROUGHOUT THE DURATION OF THIS CONTRACT. ANY TEMPORARY CLOSURE OF A DRIVEWAY OR ENTRANCE FOR TIE-IN PURPOSES SHALL BE COORDINATED WITH THE ENGINEER AND THE PROPERTY OWNER IN ADVANCE OF THE CLOSURE.
- ACCESS TO ALL TRANSIT STOPS LOCATED WITHIN THE PROJECT LIMITS SHALL BE MAINTAINED UNLESS OTHERWISE DIRECTED BY THE PLANS OR THE ENGINEER. MAINTAINING ACCESS TO THE TRANSIT STOP SHALL INCLUDE MAINTAINING AN AREA OF THE TRANSIT VEHICLE TO STOP TO PICK-UP AND DISCHARGE PASSENGERS AND ALSO AN ACCESSIBLE PATH FOR PEDESTRIANS TO SAFELY ACCESS THE TRANSIT STOP.
- THE CONTRACTOR SHALL PROVIDE ALL PROPERTY OWNERS AND RESIDENTS WHO LIVE ADJACENT TO THE WORK ZONE WITH WRITTEN NOTICE, 48 HOURS IN ADVANCE OF THE START OF CONSTRUCTION WORK. THIS NOTIFICATION SHALL INCLUDE THE SCOPE OF WORK, WORKING HOURS, ANTICIPATED START AND COMPLETION DATES; A SUMMARY OF CONSTRUCTION ACTIVITIES WHICH MAY INTERFERE WITH ACCESS TO THE PROPERTY INCLUDING A SCHEDULE AND ACCESS COORDINATION PLAN, CONTRACTOR'S NAME AND ADDRESS AND A DELDOT CONTACT PHONE NUMBER. FAILURE TO GIVE PROPER NOTICE WILL RESULT IN A SUSPENSION OF THE WORK REQUIRING NOTICE, UNTIL PROPER NOTICE IS PROVIDED. THE CONTRACTOR SHALL PROVIDE WRITTEN VERIFICATION TO THE ENGINEER THAT THE PROPERTY OWNERS AND RESIDENTS WERE NOTIFIED.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR NOTIFYING THE LOCAL 911 CENTER, LOCAL SCHOOLS AND THE DELDOT PUBLIC INFORMATION CENTER OF ALL ROADS AND LANES TO BE CLOSED A MINIMUM OF SEVEN CALENDAR DAYS BEFORE THE CLOSURE.
- THE CONTRACTOR SHALL NOTIFY THE LOCAL 911 CENTER IF ACCESS TO A FIRE HYDRANT IS TEMPORARILY RESTRICTED.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR ENSURING THAT THE TRANSPORTATION MANAGEMENT CENTER IS NOTIFIED EACH AND EVERY DAY WHEN WORK IS BEING PERFORMED IN STATE RIGHT-OF-WAY. THE CONTRACTOR SHALL IDENTIFY THE TYPE OF WORK, ANY LANE(S) OR SHOULDERS CLOSED, THE LENGTH OF TIME FOR WORK, WHEN THE LANE RESTRICTIONS ARE IN PLACE AND WHEN LANE RESTRICTIONS ARE LIFTED, CONTACT PERSON/PHONE NUMBER AND STATE INSPECTOR. THE TRANSPORTATION MANAGEMENT CENTER CAN BE REACHED AT (302) 659-4600.
- AT THE END OF EACH WORKDAY, THE CONTRACTOR SHALL CORRECT ALL VERTICAL DIFFERENCES IN ACCORDANCE WITH TABLE 60-1 OF THE DELAWARE MUTCD.
- AT THE END OF EACH DAY'S OPERATION AND BEFORE TRAFFIC IS RETURNED TO UNRESTRICTED ROADWAY USE, TEMPORARY PAVEMENT MARKINGS SHALL BE APPLIED IN ACCORDANCE WITH THE DELAWARE MUTCD AND DELDOT'S TEMPORARY PAVEMENT MARKINGS POLICY.
- WHEN SIDE ROADS INTERSECT THE WORK ZONE, ADDITIONAL TRAFFIC CONTROL DEVICES SHALL BE ERRECTED INCLUDING PERMANENT WARNING SIGNS.
- ALL STORAGE OF EQUIPMENT AND MATERIAL SHALL COMPLY WITH SECTION 60.21 OF THE DELAWARE MUTCD.
- ALL FLAGGERS SHALL COMPLY WITH CHAPTER 6E OF THE DELAWARE MUTCD.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING HIS/HER WORK WITH OTHER CONTRACTORS IN THE AREA.
- ALL PERSONS WORKING WITHIN THE STATE RIGHT-OF-WAY SHALL WEAR A MINIMUM OF AN ANSI CLASS II SAFETY VEST MEETING OR EXCEEDING THE ANSI 107-2004 REQUIREMENTS, AS SPECIFIED IN THE DELAWARE MUTCD.
- ALL PAVEMENT MARKINGS THAT ARE NO LONGER IN USE AND CONFLICT WITH TEMPORARY PAVEMENT MARKINGS SHALL BE REMOVED AND COMPLETELY OBLITERATED BY A METHOD APPROVED BY THE ENGINEER. PAINTING OVER THE CONFLICTING PAVEMENT MARKINGS WILL NOT BE ACCEPTED AS A METHOD OF REMOVAL.
- THE CONTRACTOR IS RESPONSIBLE FOR THE MAINTENANCE OF EXISTING PAVEMENT WITHIN THE PROJECT LIMITS FOR THE DURATION OF THE CONTRACT OR AS DIRECTED BY THE ENGINEER.
- ALL ROADWAYS AND ENTRANCES NOT OPEN TO TRAFFIC SHALL BE CLOSED USING TYPE III BARRICADES AND SHALL BE INSTALLED PER THE DELAWARE MUTCD. IF THE ROADWAY OR ENTRANCE IS CLOSED FOR MORE THAN ONE MONTH, THE CONTRACTOR SHALL ERECT PERMANENT BARRICADES AS DIRECTED IN PART 3 OF THE DELAWARE MUTCD.
- WITHIN THE MAINLINE WORK AREA, PERMANENT ADVANCE WARNING SIGNS WITH THE LEGENDS ROAD WORK 1500 FT, ROAD WORK 1000 FT AND ROAD WORK 500 FT SHALL BE INSTALLED IN ADVANCE OF THE WORK AREA IN BOTH DIRECTIONS. AN END ROAD WORK SIGN SHALL BE LOCATED 500 FEET DOWNSTREAM FROM THE WORK AREA. ON INTERSECTING ROADWAYS WITHIN THE PROJECT LIMITS, A ROAD WORK AHEAD SIGN SHALL BE PLACED AT A DISTANCE NOT LESS THAN 500 FEET IN ADVANCE OF THE WORK AREA AND AN END ROAD WORK SIGN SHALL BE LOCATED 500 FEET DOWNSTREAM OF THE WORK AREA. ALL PERMANENT ADVANCE WARNING SIGNS SHALL BE GROUND MOUNTED ON TWO NCHRP-350 OR MASH APPROVED BREAKAWAY POSTS AND SHALL BE MOUNTED IN COMPLIANCE WITH THE DELAWARE MUTCD. PERMANENT ADVANCE WARNING SIGNS SHALL BE MOUNTED AT A HEIGHT OF 7 FEET, MEASURED FROM THE ROADWAY TO THE BOTTOM OF THE SIGN. THE USE OF SKID MOUNTED SIGN SUPPORTS IS NOT ALLOWED UNLESS THE CONTRACTOR CAN DEMONSTRATE THAT A UTILITY CONFLICT EXISTS, WHICH SHALL BE VERIFIED BY THE ENGINEER, OR CONCRETE MEDIANS PREVENT THE INSTALLATION OF THE PERMANENT ADVANCE WARNING SIGNS IN THE APPROPRIATE LOCATION.
- TYPICAL APPLICATIONS PER THE DELAWARE MUTCD SHALL BE INCORPORATED TO ACHIEVE REQUIRED TEMPORARY TRAFFIC CONTROL AND SAFETY REQUIREMENTS. THIS PROJECT IS SUBJECT TO THE FOLLOWING TYPICAL APPLICATIONS UNLESS DIRECTED OTHERWISE BY THE DELDOT DISTRICT SAFETY OFFICER: TYPICAL APPLICATION 10: LANE CLOSURE ON A TWO-LANE ROAD USING FLAGGERS (TA-10) AND TYPICAL APPLICATION 11B: LANE DIVERSION ON A TWO-LANE ROAD WITH LOW FLOW TRAFFIC VOLUMES (TA-11B).
- ANY OPERATIONS REQUIRING THE CLOSURE OF A LANE ON SILVERSIDE ROAD (NC 212) WESTBOUND WILL NEED TO BE PERFORMED BETWEEN THE HOURS OF 8PM AND 6AM.

| Storm Drainage Pipe Schedule | | | | | | | | | |
|------------------------------|--------|-------|------|------|--------|----------|-----------|------------------|--------|
| Pipe # | From | To | Size | Type | Length | Class | Slope (%) | Invert Elevation | |
| | | | | | | | | In | Out |
| 1 | CB 52A | JB | 15 | RCP | 7 | Type IV | 0.857143 | 358.50 | 358.44 |
| 2 | CB 52 | JB | 18 | RCP | 9 | Type III | 1 | 358.09 | 358.00 |
| 3 | MH 13 | CB 52 | 18 | RCP | 20 | Type III | 0.75 | 358.24 | 358.09 |
| 4 | CB 54 | JB | 18 | RCP | 9 | Type III | 1.777778 | 358.48 | 358.32 |
| 5 | CB 55 | JB | 18 | RCP | 14 | Type III | 1 | 359.64 | 359.50 |
| 6 | MH 14 | CB 55 | 18 | RCP | 11 | Type III | 1 | 359.75 | 359.64 |
| 7 | CB 59 | JB | 18 | RCP | 12 | Type IV | 2 | 357.83 | 357.59 |
| 8 | CB 58 | JB | 18 | RCP | 12 | Type V | 1.25 | 358.61 | 358.46 |

STORM PIPE SCHEDULE



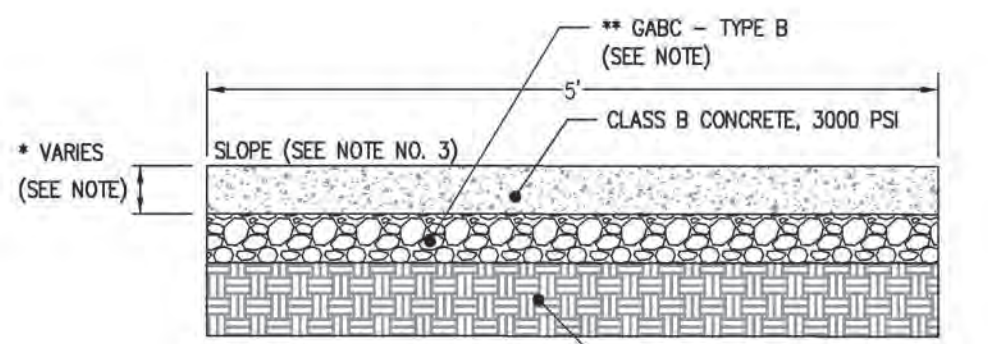
| ISSUE/REVISION | DATE |
|--|---------|
| DELDOT INITIAL STAGE SUBMISSION | 4-25-16 |
| REVISED DELDOT INITIAL STAGE SUBMISSION | 8-11-16 |
| DELDOT CONSTRUCTION STAGE SUBMISSION | 12-8-16 |
| REVISED DELDOT CONSTRUCTION STAGE SUBMISSION | 2-10-17 |
| REVISED PER DELDOT COMMENTS | 2-22-17 |
| ISSUED FOR PERMIT PLAN APPROVAL | 6-13-17 |
| ISSUED FOR PERMIT PLAN APPROVAL | 7-28-17 |

CDA ENGINEERING INC.

CIVIL/SITE ENGINEERING AND LAND PLANNING

6 LARCH AVENUE
SUITE 401
WILMINGTON, DE 19804

Tel: 302 998 9202
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cdaengineering.com

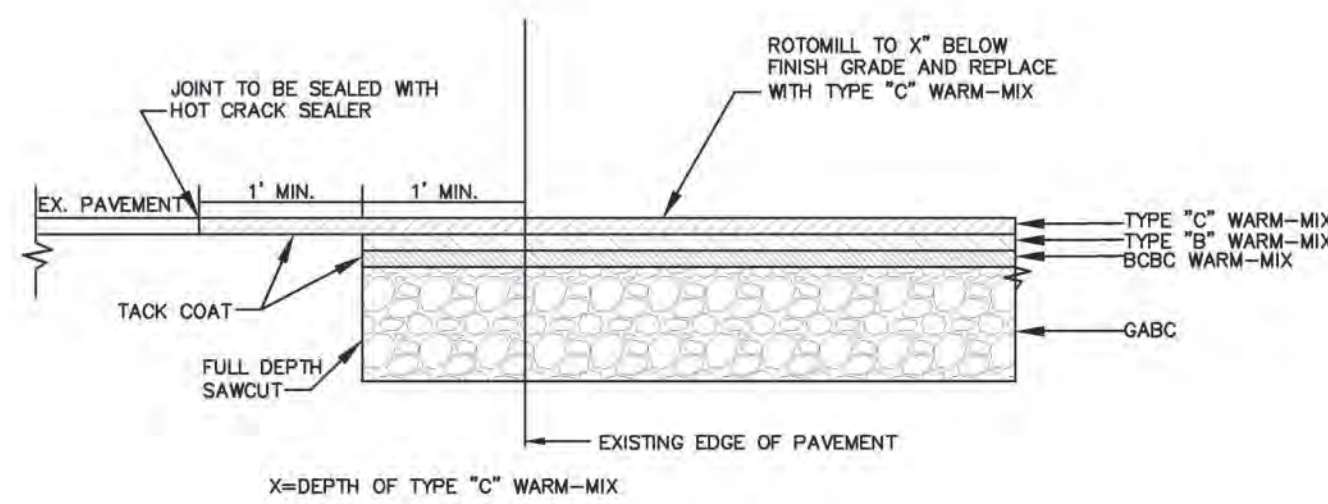


* - STANDARD SIDEWALK SHALL BE 4" THICK. FOR DEPRESSED, DRIVEWAYS, CURB RAMPS, AND TRANSITION AREAS THE SIDEWALK SHALL BE 6" THICK.

** - STANDARD SIDEWALK OF 4" THICKNESS SHALL BE PLACED OVER 4" GABC - TYPE B. SIDEWALK FOR DEPRESSED, DRIVEWAYS, CURB RAMPS, AND TRANSITION AREAS OF 6" THICKNESS SHALL BE PLACED OVER 6" GABC - TYPE B.

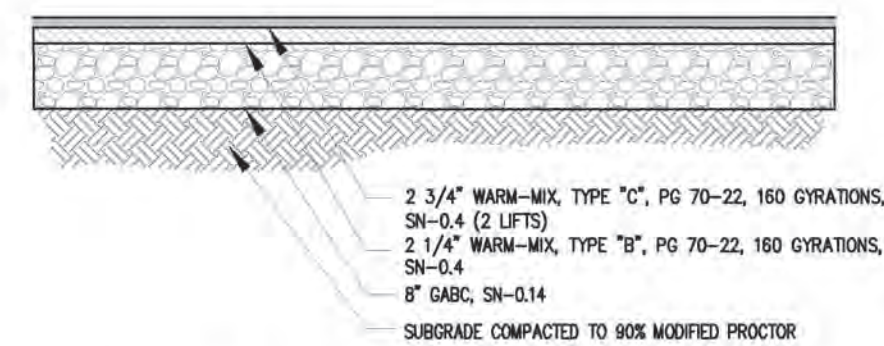
NOTES:
1. MARK IN 5' SQUARES, USE PREMOLDED EXPANSION JOINTS AT INTERVALS NOT GREATER THAN 15'.
2. CONCRETE SIDEWALKS SHALL BE CONSTRUCTED AS PER DELDOT SPECIFICATIONS.
3. SIDEWALK TO BE CONSTRUCTED AT GRADE. SLOPE OF SIDEWALK SHALL BE 1.5% SLOPE WITH A 2% MAX/1% MIN CROSS SLOPE IN ORDER TO ASSURE POSITIVE DRAINAGE.

DETAIL: SIDEWALK WITH IN DELDOT RIGHT OF WAY
NOT TO SCALE



PAVEMENT TIE-IN DETAIL

N.T.S.



PAVEMENT SECTION DETAIL

N.T.S.

APPLICATION No. # 2015-0715

CONCORD PLAZA

RECORD MAJOR LAND DEVELOPMENT PLAN

BRANDYWINE HUNDRED NEW CASTLE COUNTY DELAWARE

ENTRANCE CONSTRUCTION DETAILS

DRAWING TITLE

DRAWING NUMBER

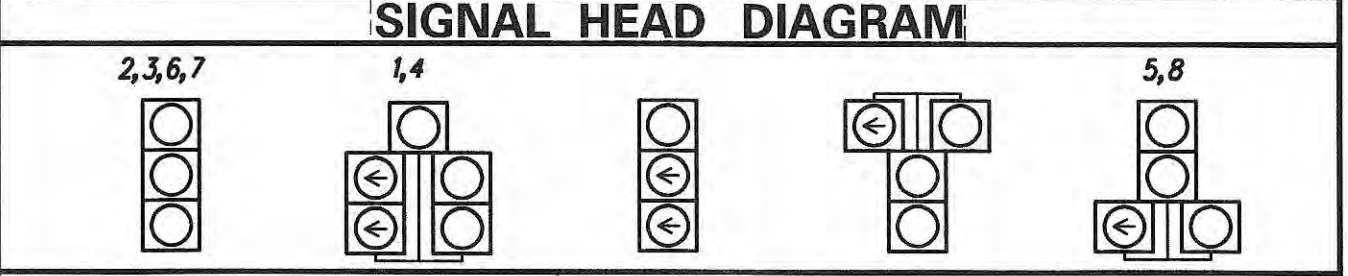
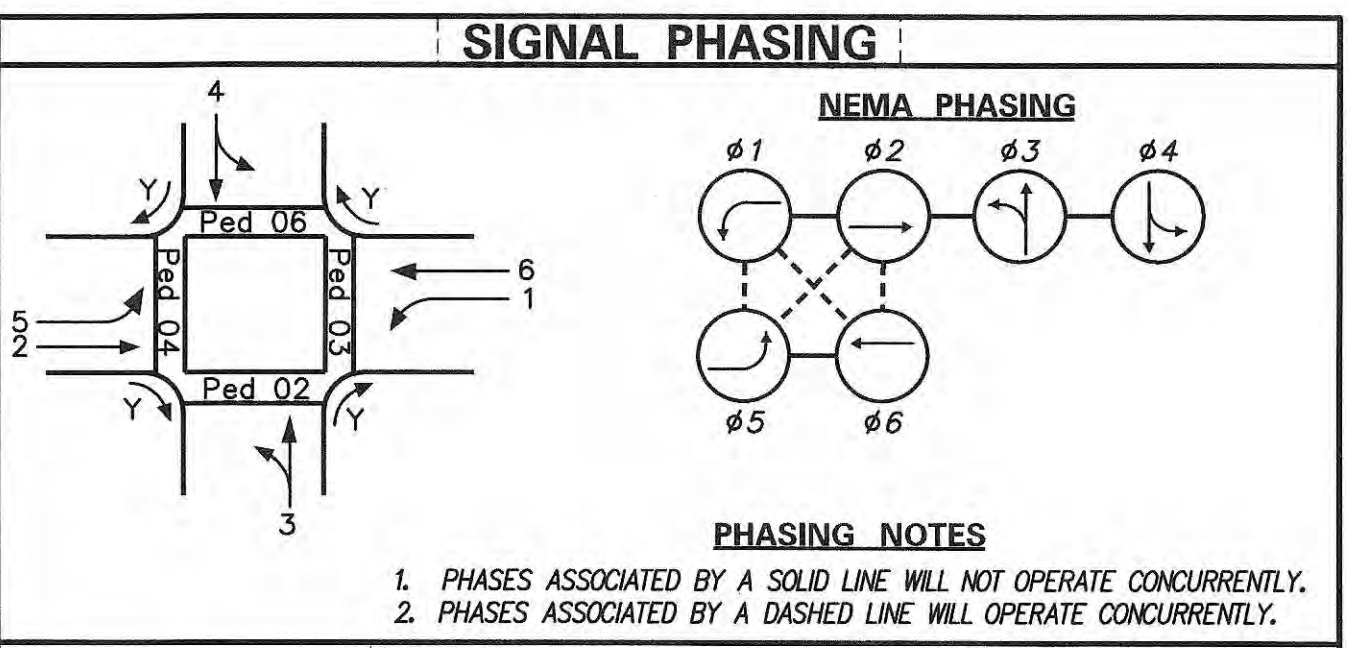
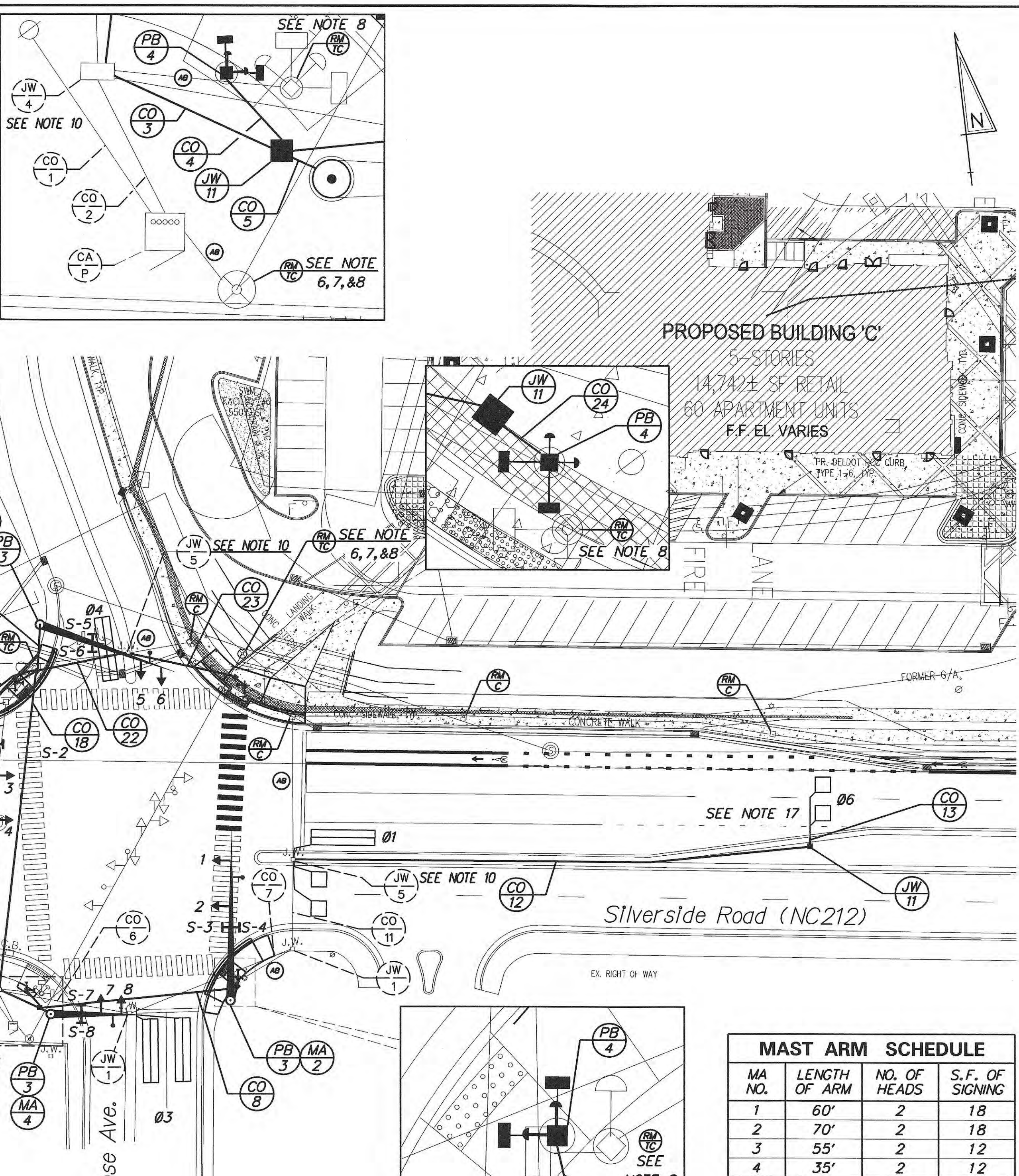
ENT-04

SIGNAL PLANS FOR SILVERSIDE RD AT ROSE AVE

APPENDIX B – PLANS FOR ADJACENT PROJECTS

ADDITIONAL NOTES:

- DOWN GUY EXISTING STRAIN POLES AS NEEDED TO ACCOMMODATE PROPOSED MAST ARM INSTALLATION.
- EXISTING SIGNAL SHALL REMAIN ACTIVE UNTIL NEW SIGNAL IS EXCEPTED.
- THE TRAFFIC CONTRACTOR SHALL REMOVE EXISTING SPAN WIRE, SIGNAL HEADS, WIRING, ELECTRICAL CABLES, SIGNAL POLES, PEDESTRIAN POLES, PEDESTRIAN SIGNAL HEADS AND POLE FOUNDATIONS. PAYMENT SHALL BE UNDER ITEM 748941.
- WHERE APPLICABLE THE CONTRACTOR SHALL REMOVE EXISTING BONDING AND LOOP WIRE FROM EXISTING SAWCUT. THE CONTRACTOR SHALL INSTALL NEW #14/1 LOOP WIRE IN EXISTING SAWCUT IN ACCORDANCE WITH NEW LOOP STANDARDS.
- EXISTING JUNCTION WELL SHALL BE CLEARED OF ALL OLD WIRE AND CLEARED OF DEBRIS AS PART OF THE ADJUST OR REPAIR EXISTING JUNCTION WELL PAY ITEM 83008.
- REMOVE EXISTING JUNCTION WELL FRAME AND LID AND REPLACE WITH NEW PRECAST CONCRETE/POLYMER LID AND FRAME. ADJUST OR REPAIR EXISTING JUNCTION WELL AS NEEDED.
- THE CONTRACTOR SHALL COORDINATE THE REMOVAL OF SIGNAL POLES, POLE BASES, CABINET, AND CABINET BASE WITH TRAFFIC CONSTRUCTION FORCES. AFTER REMOVAL, THE AREA SHALL BE RESTORED TO MATCH EXISTING ADJACENT CONDITIONS.
- THE CONTRACTOR SHALL REMOVE EXISTING ELECTRICAL CABLES, SIGNAL HEADS, AND SPAN WIRES. PROPOSED POLE BASES SUPPORTING POLES WITH PEDESTRIAN PUSH BUTTONS SHALL BE CONSTRUCTED IMMEDIATELY ADJACENT TO THE FLAT (501 OR FLATTER) LANDING AREA OF THE CURB RAMP IN ACCORDANCE WITH CURRENT ADA BEST PRACTICES. THESE POLE BASES SHALL BE FLUSH WITH THE ADJOINING LANDING AREA. THE PEDESTRIAN PUSH BUTTON SHOULD BE INSTALLED AT A HEIGHT OF 42 INCHES TO 48 INCHES ABOVE THE LANDING AREA, AND SHALL BE LOCATED SUCH THAT MAXIMUM REACH DISTANCE DOES NOT EXCEED 10 INCHES FROM THE LANDING AREA TO THE FACE OF THE PUSH BUTTON. PEDESTRIAN SIGNAL HEADS SHOULD BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING, INCLUDING BRACKETS, NOT LESS THAN 7 FEET OR MORE THAN 10 FEET ABOVE LANDING AREA OR SIDEWALK LEVEL.
- PEDESTRIAN SIGNALS SHALL CONTAIN PEDESTRIAN COUNTDOWN DISPLAYS.
- DELDOT TRAFFIC FORCES SHALL INSTALL PUSH BUTTON SO THAT THE FACE OF THE PUSH BUTTON IS PARALLEL WITH THE CROSSING AND INSTALL THE COUNTDOWN DISPLAY ALIGNED WITH THE CROSSING, AS SHOWN.
- DELDOT TRAFFIC SHALL INSTALL COUNTDOWN PEDESTRIAN SIGN (R10-3e) WITH PROPER PEDESTRIAN MOVEMENT ARROW FOR RELEVANT CROSSWALK DETECTION ABOVE EACH PEDESTRIAN PUSH BUTTON OR COUNTDOWN PEDESTRIAN SIGN (R10-3e-2) WITH PROPER PEDESTRIAN MOVEMENT ARROW FOR RELEVANT CROSSWALK DETECTION ON TOP OF EACH PUSH BUTTON HOUSING WHERE TWO PUSH BUTTONS ARE INSTALLED ON ONE POLE. THE FACE OF THE PUSH BUTTON AND SIGN SHALL BE INSTALLED PARALLEL WITH THE CROSSING.
- ADJUST EXISTING MAINLINE PASSAGE DETECTION LOOP TO FOUR (4) SECONDS. CONTRACTOR SHALL RE-OUT NEW LOOP DETECTORS WITH 1/4" CABLES AT 200 FEET FROM STOP BAR AND ABANDON EXISTING LOOPS. ALL HOME-RUN CABLES FOR PROPOSED LOOPS SHALL BE 2/4" CABLE.
- PROPOSED PAVEMENT MARKINGS TO BE COMPLETED BY THE CIVIL CONTRACTOR.
- PROPOSED CURB RAMP ON THE NORTHEAST CORNER TO BE INSTALLED BY CIVIL CONTRACTOR. SEE SHEET TWO FOR PROPOSED RAMP LAYOUTS.
- ALL PROPOSED PAVEMENT MARKINGS SHALL BE THE RESPONSIBILITY OF THE DEVELOPER.



LEGEND

| | | | |
|------|---|------|--|
| (AB) | ABANDON | (OH) | EXISTING OVERHEAD RUN IDENTIFIER (# OF OVERHEAD RUN) |
| (CA) | EXISTING CABINET IDENTIFIER (TYPE OF CABINET) | (OH) | PROPOSED OVERHEAD RUN IDENTIFIER (# OF OVERHEAD RUN) |
| (CB) | PROPOSED CABINET IDENTIFIER (TYPE OF CABINET) | (PB) | EXISTING POLE BASE IDENTIFIER (TYPE OF POLE BASE) |
| (CO) | EXISTING CONDUIT RUN IDENTIFIER (# OF CONDUIT RUN) | (PB) | PROPOSED POLE BASE IDENTIFIER (TYPE OF POLE BASE) |
| (CO) | PROPOSED CONDUIT RUN IDENTIFIER (# OF CONDUIT RUN) | (FL) | EXISTING POLE IDENTIFIER (# OF POLE) |
| (JW) | EXISTING JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL) | (PA) | PROPOSED POLE IDENTIFIER (# OF POLE) |
| (JW) | PROPOSED JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL) | (RM) | REMOVE BY CONTRACTOR |
| (MA) | EXISTING MAST ARM IDENTIFIER (LENGTH OF ARM) | (RM) | REMOVE BY OTHERS |
| (MA) | PROPOSED MAST ARM IDENTIFIER (LENGTH OF ARM) | (RM) | REMOVE BY TRAFFIC CONTRACTOR |

| | EXISTING SYMBOL | PROPOSED SYMBOL |
|------------------------|-----------------|-----------------|
| JUNCTION WELL | J.W. | ■ |
| LOOP DETECTOR, TYPE 1 | □ | □ |
| LOOP DETECTOR, TYPE 2 | □ | □ |
| LUMINAIRE | ⊙ | ⊙ |
| MAST ARM | ⊙ | ⊙ |
| MICROWAVE DETECTION | ⊙ | ⊙ |
| OPTICOM RECEIVER | ⊙ | ⊙ |
| OVERHEAD SIGNING | ⊙ | ⊙ |
| PEDESTRIAN POLE/BASE | ⊙ | ⊙ |
| PEDESTRIAN PUSHBUTTON | ⊙ | ⊙ |
| PEDESTRIAN SIGNAL HEAD | ⊙ | ⊙ |
| RIGHT-OF-WAY | --- | ---R/W--- |
| SERVICE PEDESTAL | ⊙ | ⊙ |
| SIGNAL CABINET | ⊙ | ⊙ |
| SIGNAL HEAD | ⊙ | ⊙ |
| SIGNAL POLE/BASE | ⊙ | ⊙ |
| SPAN INSULATOR | ⊙ | ⊙ |
| SPAN WIRE | ---XX--- | ---XX--- |
| UTILITY POLE | ⊙ | ⊙ |
| VIDEO DETECTION | ⊙ | ⊙ |

CONDUIT RUN SCHEDULE

| CR NO. | NO. OF CONDUITS | SIZE | LENGTH | B/T/O | AMOUNT AND TYPE OF CABLE / WIRE |
|--------|-----------------|------|--------|-------|--|
| *1 | 1 | 2" | -- | -- | (1)#8/2 UF w/Ground |
| *2 | 3 | 4" | 15' | -- | (1)12MM Remove-(8)#18/4 (4)#14/9, Install-(14)#14/2, (8)#14/5, (4)#14/9, (4)#18/4, (1)#6 |
| 3 | 1 | 4" | 20' | T | (4)#14/5, (2)#14/9, (2)#18/4, (1)#6 |
| 4 | 1 | 2.5" | 10' | T | (2)#14/5, (1)#6 |
| 5 | 1 | 3" | 10' | T | (1)#14/9, (1)#18/4, (1)#6 |
| *6 | 1 | 2.5" | 55' | -- | Remove-(2)#18/4 (2)#14/9, Install-(7)#14/2, (1)#6 |
| *7 | 1 | 2.5" | 70' | -- | Remove-(3)#18/4 (2)#14/9, Install-(5)#14/2, (1)#6 |
| 8 | 1 | 4" | 80' | B | (2)#14/5, (1)#14/9, (1)#18/4, (1)#6 |
| 9 | 1 | 3" | 5' | T | (1)#18/4, (1)#14/9, (1)#6 |
| 10 | 1 | 2.5" | 5' | T | (2)#14/5, (1)#6 |
| *11 | 1 | 2.5" | 35' | -- | Remove-(5)#18/4 (1)#14/9, Install-(5)#14/2, (1)#6 |
| 12 | 1 | 4" | 205' | O | (2)#14/2, (1)#6 |
| 13 | 1 | 1.5" | 5' | O | (2)#14/1 |
| *14 | 1 | 2.5" | 70' | -- | (1)12MM Remove-(7)#18/4 (1)#14/9, Install-(6)#14/2, (1)#6 |
| *15 | 1 | 1.5" | 145' | -- | Remove-(1)#18/4, Install-(2)#14/2, (1)#6 |
| 16 | 1 | 4" | 45' | O | (2)#14/2, (1)#6 |
| 17 | 1 | 1.5" | 5' | O | (2)#14/1 |
| 18 | 1 | 4" | 125' | B | (1)#14/2, (4)#14/5, (2)#14/9, (2)#18/4, (1)#6 |
| 19 | 1 | 3" | 20' | T | (1)#14/9, (1)#18/4, (1)#6 |
| 20 | 1 | 3" | 20' | T | (1)#14/9, (1)#18/4, (1)#6 |
| 21 | 1 | 2.5" | 10' | T | (2)#14/5, (1)#6 |
| 22 | 1 | 4" | 35' | B | (1)#14/2, (2)#14/5, (1)#6 |

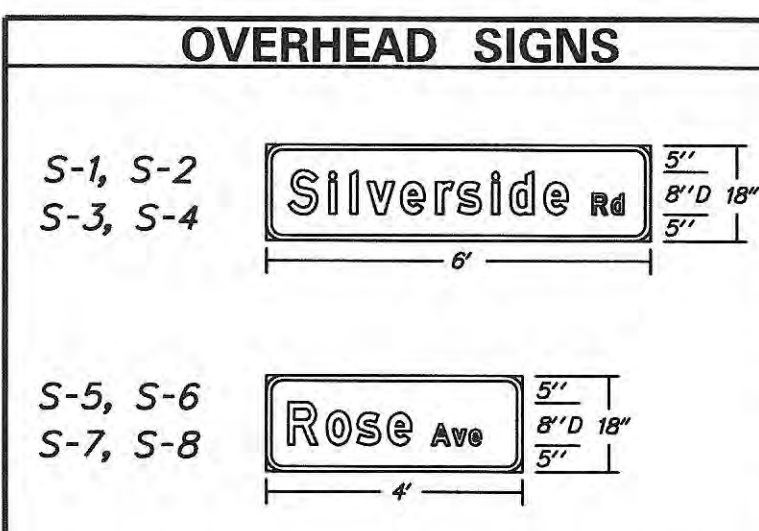
CONDUIT RUN SCHEDULE

| CR NO. | NO. OF CONDUITS | SIZE | LENGTH | B/T/O | AMOUNT AND TYPE OF CABLE / WIRE |
|--------|-----------------|------|--------|-------|-------------------------------------|
| 23 | 1 | 4" | 45' | B | (2)#14/5, (1)#6 |
| 24 | 1 | 2.5" | 5' | T | (2)#14/5, (1)#6 |
| *25 | 1 | 2.5" | 45' | -- | (1)12 MM, Remove-(1)#18/4, (1)#14/9 |

*DENOTES EXISTING

MAST ARM SCHEDULE

| MA NO. | LENGTH OF ARM | NO. OF HEADS | S.F. OF SIGNING |
|--------|---------------|--------------|-----------------|
| 1 | 60' | 2 | 18 |
| 2 | 70' | 2 | 18 |
| 3 | 55' | 2 | 12 |
| 4 | 35' | 2 | 12 |



- GENERAL SIGNAL NOTES**
- DETECTION - 40 M.P.H. - 4 SECONDS PASSAGE TIME AT 200 FEET FROM STOP BAR.
 - LOOP DETECTORS: TYPE #1 - 6' x 6' - TO BE INSTALLED ON MAIN STREET THROUGH MOVEMENTS. TYPE #2 - 8' x 25' - TO BE INSTALLED ON MAIN STREET LEFT TURN MOVEMENTS. TYPE #3 - 8' x 25' - TO BE INSTALLED ON SIDE STREET THROUGH AND LEFT TURN MOVEMENTS.
 - ALL SIGNAL POLES UP RIGHTS WILL BE 21 FEET. SEE MAST ARM SCHEDULE FOR ARM LENGTHS.
 - CO #1 IS EXISTING.
 - ALL SIGNAL EQUIPMENT REMOVED FROM A PROJECT IS TO BE RETURNED TO DELDOT TRAFFIC - DOVER, DELAWARE.
 - POLE BASES, CABINET BASE AND CONDUIT JUNCTION WELLS TO BE REMOVED IN ACCORDANCE WITH SECTION 201 AND 202 OF THE STANDARD SPECIFICATIONS OR AS DIRECTED BY ENGINEER. EXISTING CONDUIT IS TO BE ABANDONED.
 - ALL GALVANIZED CONDUIT (GRC) SHALL BE REAMED AND THREADED. ALL GRC SHALL BE THREADED TOGETHER WITH APPROVED COUPLINGS. SET SCREW, BOLTED, AND COMPRESSION FITTING ARE NOT ACCEPTABLE.
 - ALL UNDERGROUND AND OVERHEAD UTILITIES SHOWN ON THESE PLANS ARE SCHEMATIC ONLY AND MAY NOT BE COMPLETE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR NOTIFYING MISS UTILITY, AND/OR THE APPROPRIATE UTILITY PRIOR TO THE BEGINNING OF CONSTRUCTION FOR THE UTILITY MARKOUTS. IF THE CONTRACTOR PERCEIVES THAT A CONFLICT BETWEEN UTILITIES AND THE TRAFFIC SIGNAL WILL OCCUR, THE CONTRACTOR SHALL NOTIFY DELDOT TRAFFIC IMMEDIATELY BEFORE CONSTRUCTION.

RECOMMENDED DATE: 6/13/17

RECOMMENDED DATE: _____

RECOMMENDED DATE: _____

APPROVED TRAFFIC ENGINEER DATE: 6/13/17

APPROVED FOR INSTALLATION CHIEF TRAFFIC ENGINEER DATE: 6/13/17

DELAWARE DEPARTMENT OF TRANSPORTATION

CONCORD PLAZA

SCALE: 0 30 60 90 FEET

CONTRACT: _____ PERMIT NO. **N504**

COUNTY: _____ DESIGNED BY: Sol Moineddin (RK&K)

NEW CASTLE: _____ CHECKED BY: Jason Roshid (RK&K)

SIGNAL PLAN

Silverside Rd. at Rose Ave.

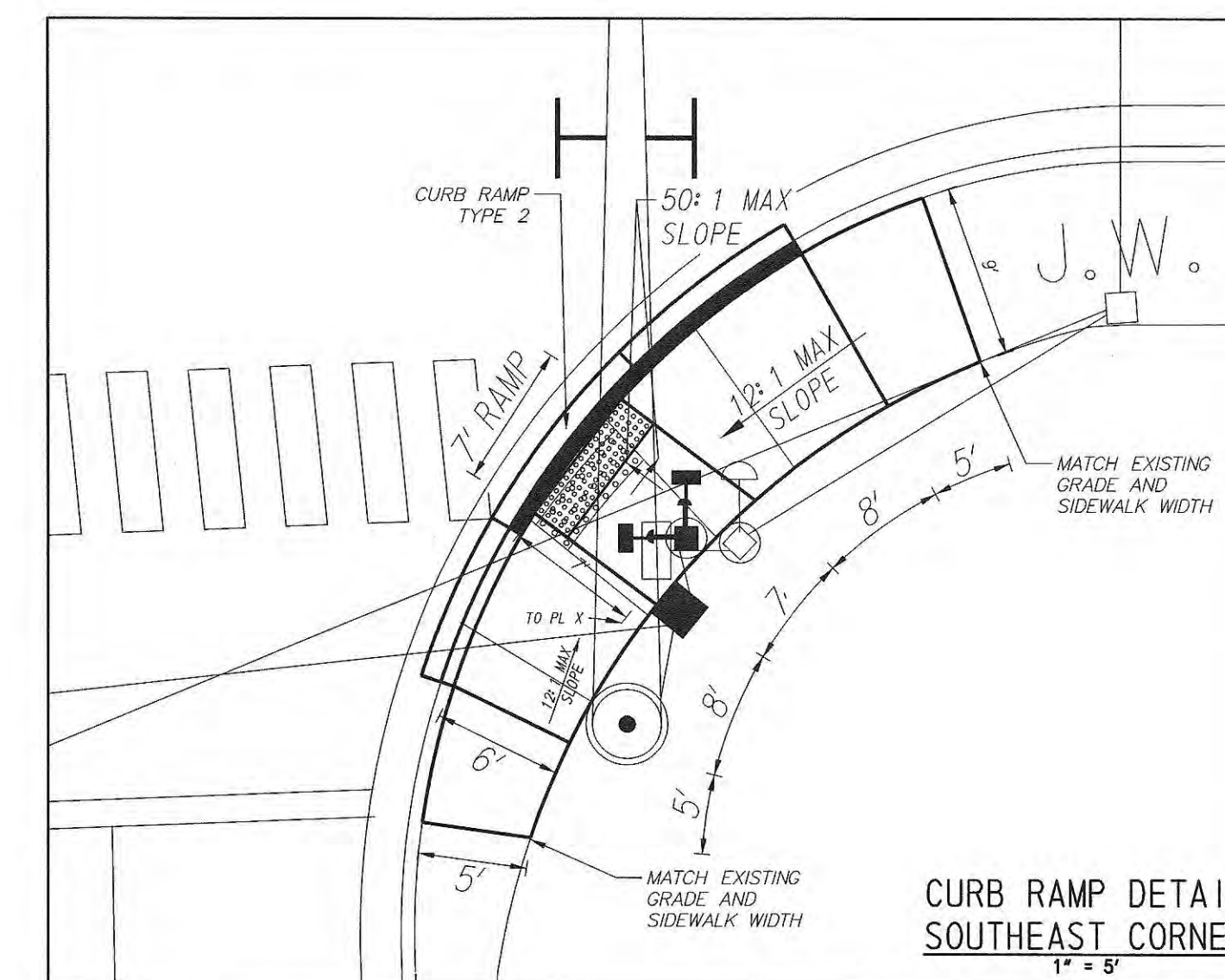
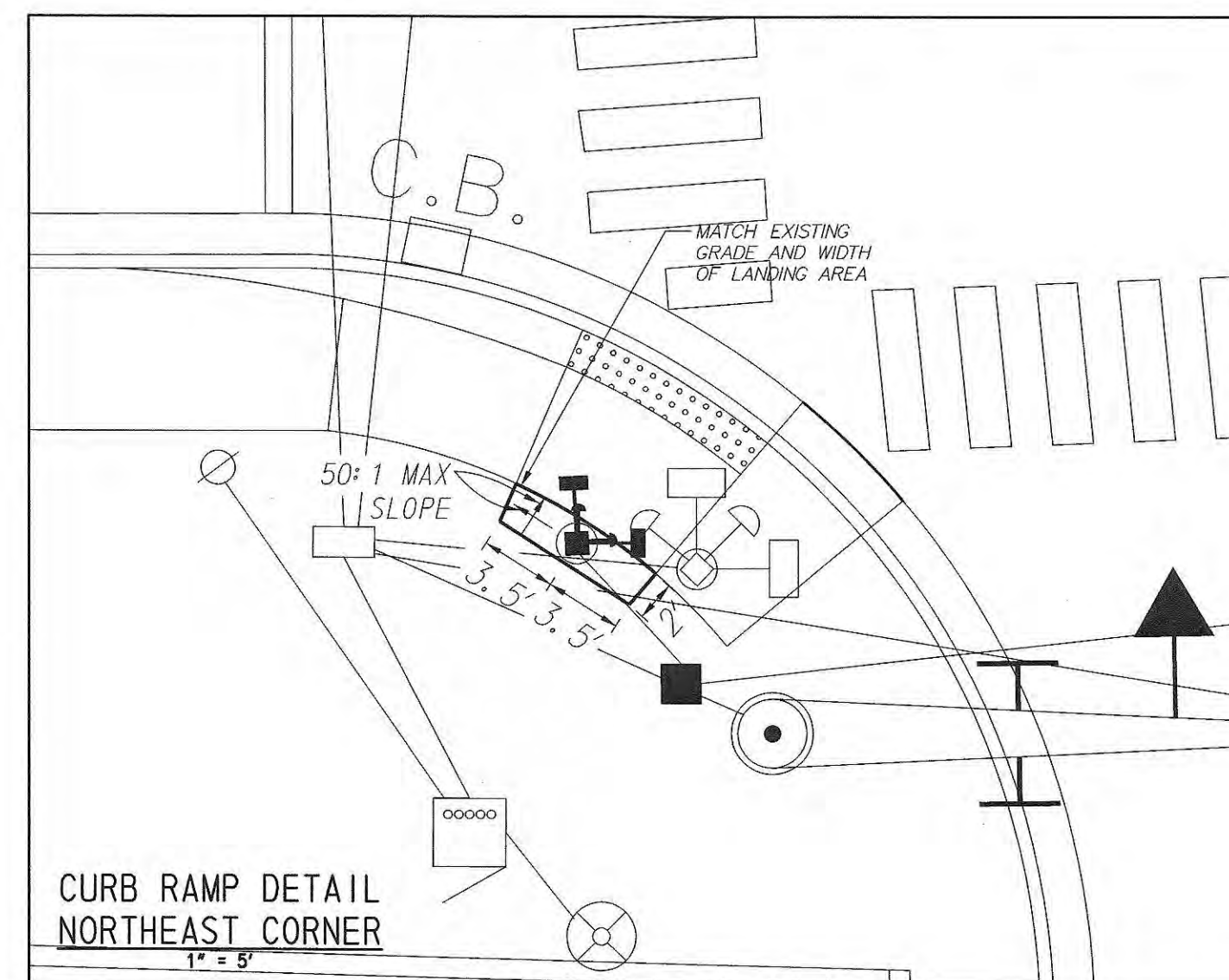
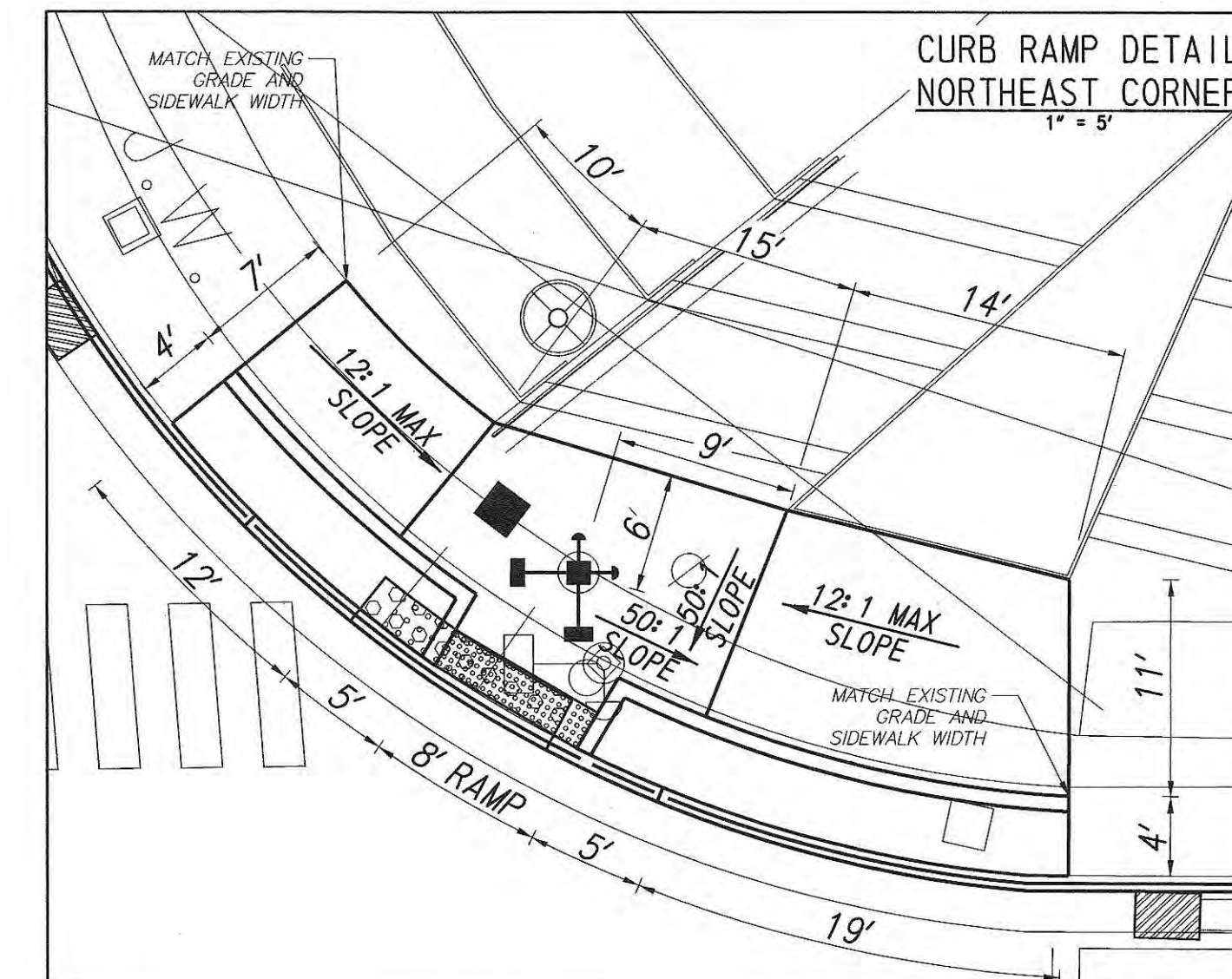
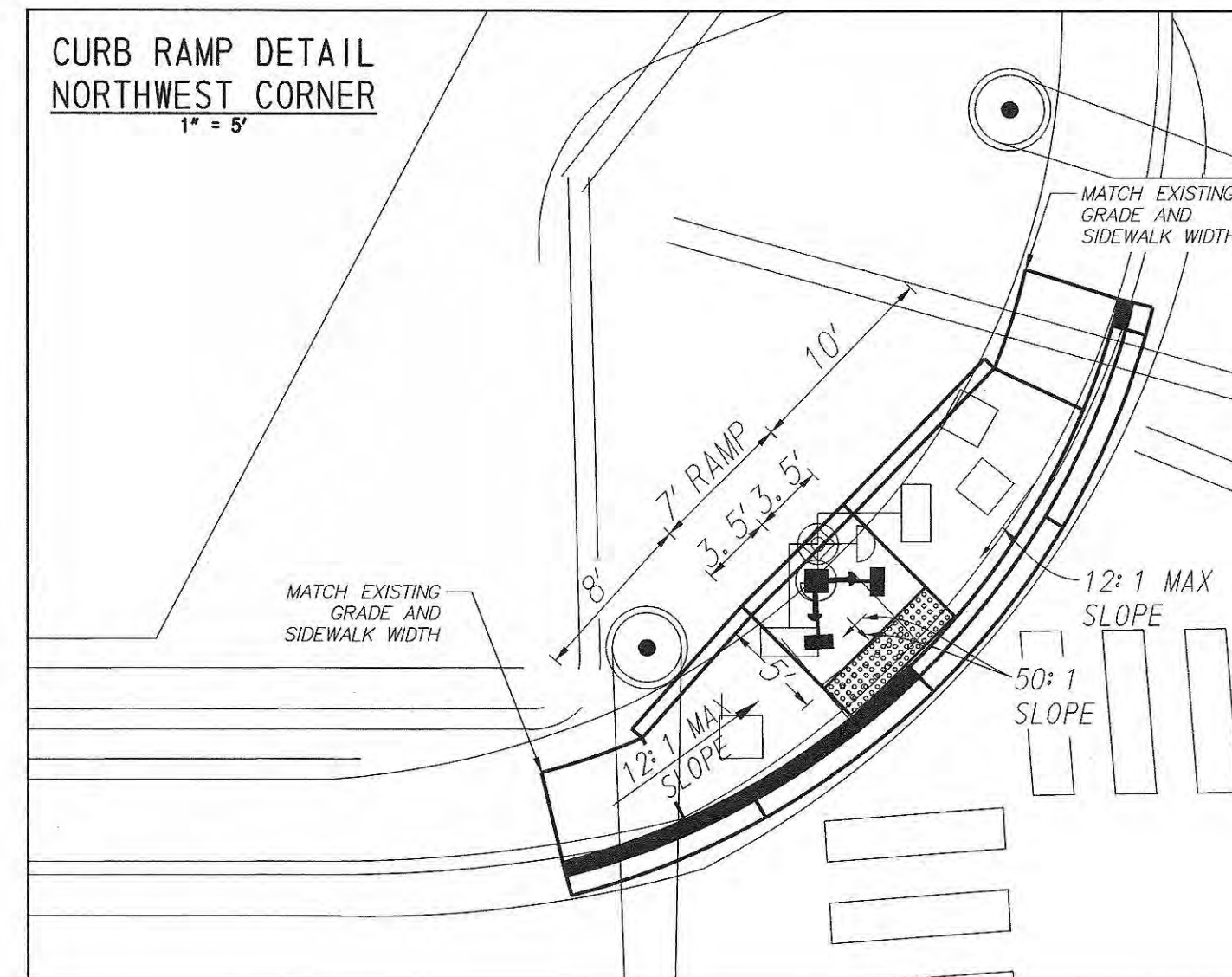
SHEET NO. 1

TOTAL SHTS. 2

BA SILVERSIDE ROAD\CAD\NS04.DGN

ADDITIONAL NOTES:

24. ACCEPTABLE WORK HOURS FOR THIS PROJECT ARE 8:00PM TO 5:00AM.
25. FOR TRAFFIC CONTROL ELEMENTS THE CONTRACTOR SHALL COMPLY WITH ALL GUIDELINES AND REQUIREMENT AS OUTLINED IN PART 6 OF "THE DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (DE MUTCD). ANY DEVIATION OR SPECIAL REQUEST SHALL BE APPROVED BY MANAGER SAFETY OFFICER.
26. AS SHOWN IN PART 6H OF THE DE MUTCD THE FOLLOWING TYPICAL APPLICATION SHALL BE UTILIZED FOR CONSTRUCTION OF THE NEEDED FACILITY - TA-13, TA-21, TAZ1A, TA-33, TA-37, AND TA-47A. ADDITIONAL CASE MAY BE REQUIRED BASED ON FIELD CONDITIONS.
27. FOR THE CONSTRUCTION OF THE NORTHEAST CORNER COORDINATION WITH THE DEVELOPER CONTRACTOR SHALL OCCUR. COORDINATION OF PEDESTRIAN MOT, LANE CLOSURES, AND OTHER ROADWAY IMPACTS SHOULD BE MINIMIZED WITH PROPER COORDINATION.
28. THE TEMPORARY PEDESTRIAN PATH SHALL BE A MINIMUM OF 4 FEET WIDE WHEN TIE-ING INTO ADJACENT SIDEWALK (5 FEET IS PREFERRED).
29. ACCEPTABLE MATERIAL FOR TEMPORARY PEDESTRIAN PATHS SHALL INCLUDE CONCRETE, HOT-MIX, COMPACTED MILLINGS, OR PLYWOOD WALKWAY STRUCTURE IN ACCORDANCE WITH SECTION 743 OF THE DELDOT "STANDARD SPECIFICATIONS," DATED AUGUST 2001 AND THE DELDOT "STANDARD CONSTRUCTION DETAILS," DATED 2014, INCLUDING ALL REVISIONS UP TO THE DATE OF ADVERTISEMENT.
30. STONE OR GRADED AGGREGATE BASE COURSE SHALL NOT BE USED FOR TEMPORARY PEDESTRIAN PATHS.
31. DURING ANY SIDEWALK CLOSURE, THE CONTRACTOR SHALL PROVIDE A FLAGGER EXCLUSIVELY FOR PEDESTRIAN MOT. AT THE END OF EACH WORK DAY A VIABLE ROUTE FOR PEDESTRIANS MUST BE AVAILABLE USING ACCEPTABLE PEDESTRIAN MATERIALS. THE ALTERNATE PEDESTRIAN ROUTE MUST BE APPROVED BY DELDOT TRAFFIC SAFETY.
32. FOR THE SOUTHEAST CORNER THE CONTRACTOR MAY CONSTRUCT PROPOSED SIDEWALK CONNECTION, TO MAINTAIN PEDESTRIAN ACCESS. A TEMPORARY PEDESTRIAN PATH WITH PEDESTRIAN CHANNELIZING BARRICADES ON EITHER SIDE OF THE PATH, LOCATED BEHIND THE IMPACTED SIDEWALK AND PEDESTRIAN RAMP CONSTRUCTION. THE ALTERNATE PEDESTRIAN ROUTE MUST BE APPROVED BY DELDOT TRAFFIC SAFETY.
33. FOR THE CONSTRUCTION OF THE NORTHWEST CORNER THE CONTRACTOR SHALL UTILIZE A PEDESTRIAN DETOUR TO ALLOW ACCESS TO PROPOSED DART BUS STOP LOCATED BETWEEN THE INTERSECTION OF ROSE AVE AND BROOKFIELD LN. LOCATED ON THE EASTBOUND SIDE OF SILVERSIDE RD.



SIGNAL PHASING

SIGNAL HEAD DIAGRAM

LEGEND

| | | | |
|------|---|------|--|
| (AB) | ABANDON | (OR) | EXISTING OVERHEAD RUN IDENTIFIER (# OF OVERHEAD RUN) |
| (CA) | EXISTING CABINET IDENTIFIER (TYPE OF CABINET) | (OP) | PROPOSED OVERHEAD RUN IDENTIFIER (# OF OVERHEAD RUN) |
| (CB) | PROPOSED CABINET IDENTIFIER (TYPE OF CABINET) | (PB) | EXISTING POLE BASE IDENTIFIER (TYPE OF POLE BASE) |
| (CO) | EXISTING CONDUIT RUN IDENTIFIER (# OF CONDUIT RUN) | (PB) | PROPOSED POLE BASE IDENTIFIER (TYPE OF POLE BASE) |
| (CO) | PROPOSED CONDUIT RUN IDENTIFIER (# OF CONDUIT RUN) | (PL) | EXISTING POLE IDENTIFIER (# OF POLE) |
| (JW) | EXISTING JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL) | (PL) | PROPOSED POLE IDENTIFIER (# OF POLE) |
| (JW) | PROPOSED JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL) | (RM) | REMOVE BY CONTRACTOR |
| (MA) | EXISTING MAST ARM IDENTIFIER (LENGTH OF ARM) | (RM) | REMOVE BY OTHERS |
| (MA) | PROPOSED MAST ARM IDENTIFIER (LENGTH OF ARM) | (RM) | REMOVE BY TRAFFIC CONTRACTOR |

| | EXISTING SYMBOL | PROPOSED SYMBOL |
|------------------------|-----------------|-----------------|
| JUNCTION WELL | J.W. | ■ |
| LOOP DETECTOR, TYPE 1 | □ | □ |
| LOOP DETECTOR, TYPE 2 | □ | □ |
| LUMINAIRE | ○ | ○ |
| MAST ARM | — | — |
| MICROWAVE DETECTION | ○ | ○ |
| OPTICOM RECEIVER | ○ | ○ |
| OVERHEAD SIGNING | — | — |
| PEDESTRIAN POLE/BASE | ○ | ○ |
| PEDESTRIAN PUSHBUTTON | ○ | ○ |
| PEDESTRIAN SIGNAL HEAD | ○ | ○ |
| RIGHT-OF-WAY | — | — R/W |
| SERVICE PEDESTAL | □ | □ |
| SIGNAL CABINET | □ | □ |
| SIGNAL HEAD | ○ | ○ |
| SIGNAL POLE/BASE | ○ | ○ |
| SPAN INSULATOR | ○ | ○ |
| SPAN WIRE | — XX — | — XX — |
| UTILITY POLE | ○ | ○ |
| VIDEO DETECTION | ○ | ○ |

GENERAL SIGNAL NOTES

B:\SILVERSIDE ROAD\CADD\NS04_2.DGN

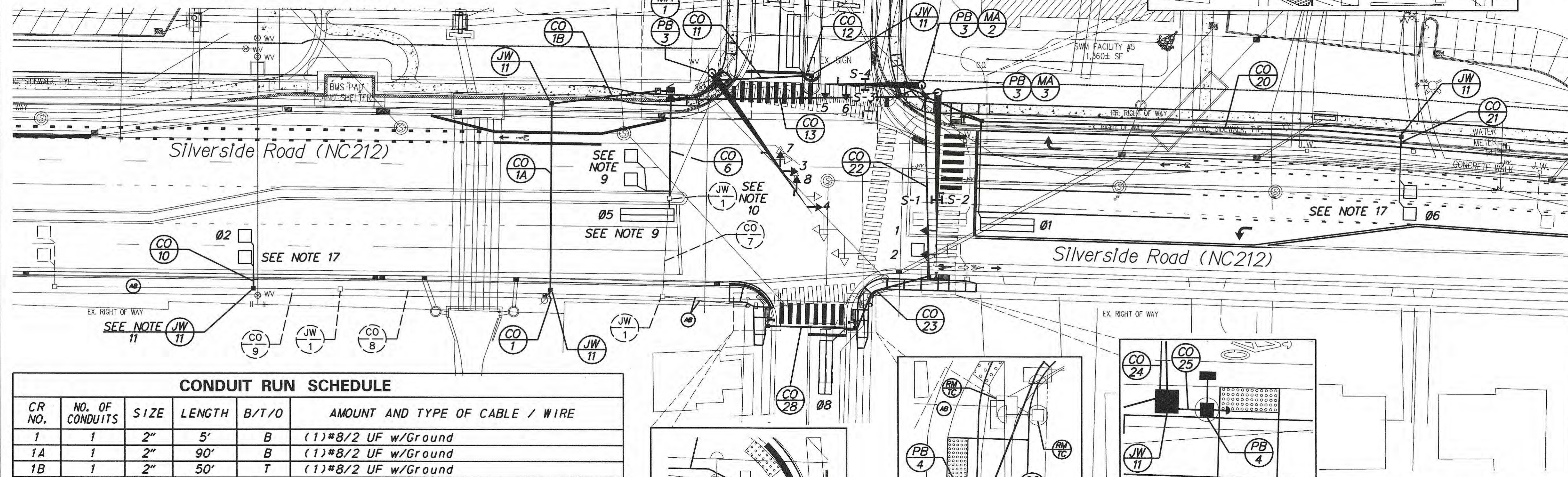
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|--|-------------------------------|---|--|---|
| RECOMMENDED DATE: 6.13.17 | RECOMMENDED _____ DATE: _____ | RECOMMENDED _____ DATE: _____ | APPROVED TRAFFIC ENGINEER DATE: 6/13/17 | APPROVED FOR INSTALLATION CHIEF TRAFFIC ENGINEER DATE: 6/13/17 |
| ADDENDUM / REVISIONS | | | NONE | |
| DELAWARE DEPARTMENT OF TRANSPORTATION | | CONCORD PLAZA | | SIGNAL PLAN Silverside Rd. at Rose Ave. |
| | | CONTRACT PERMIT NO. N504 | | SHEET NO. 2 |
| | | COUNTY NEW CASTLE CHECKED BY: Jason Rashid (RK&K) | | TOTAL SHTS. 2 |

SIGNAL PLANS FOR SILVERSIDE RD AT BROOKFIELD LN

APPENDIX B – PLANS FOR ADJACENT PROJECTS

ADDITIONAL NOTES:

- DOWN GUY EXISTING STRAIN POLES AS NEED TO ACCOMMODATE PROPOSED MAST ARM INSTALLATION.
- EXISTING SIGNAL SHALL REMAIN ACTIVE UNTIL NEW SIGNAL IS EXCEPTED.
- THE TRAFFIC CONTRACTOR SHALL REMOVE EXISTING SPAN WIRE, SIGNAL HEADS, WIRING, ELECTRICAL CABLE, SIGNAL POLES, PEDESTRIAN POLES, PEDESTRIAN SIGNAL HEADS AND POLE FOUNDATIONS. PAYMENT SHALL BE UNDER ITEM 74694L.
- WHERE APPLICABLE THE CONTRACTOR SHALL REMOVE EXISTING BONDING AND LOOP WIRE FROM EXISTING SAWCUT. THE CONTRACTOR SHALL INSTALL NEW #14/1 LOOP WIRE IN EXISTING SAWCUT IN ACCORDANCE WITH NEW LOOP STANDARDS.
- EXISTING JUNCTION WELL SHALL BE CLEARED OF ALL OLD WIRE AND CLEARED OF DEBRIS AS PART OF THE ADJUST OR REPAIR EXISTING JUNCTION WELL PAY ITEM 8300B.
- INTERCEPT EXISTING PATHWAY WITH NEW TYPE 11 JUNCTION WELL FOR PASSAGE LOOP LOCATION ADJUSTMENT.
- THE CONTRACTOR SHALL COORDINATE THE REMOVAL OF SIGNAL POLES, POLE BASES, CABINET, AND CABINET BASE WITH TRAFFIC CONSTRUCTION FORCES. AFTER REMOVAL, THE AREA SHALL BE RESTORED TO MATCH EXISTING ADJACENT CONDITIONS.
- PROPOSED POLE BASES SUPPORTING POLES WITH PEDESTRIAN PUSH BUTTONS SHALL BE CONSTRUCTED IMMEDIATELY ADJACENT TO THE FLAT (SLOPE OR FLATTER) LANDING AREA OF THE CURB RAMP IN ACCORDANCE WITH CURRENT ADA BEST PRACTICES. THESE POLE BASES SHALL BE FLUSH WITH THE ADJOINING LANDING AREA. THE PEDESTRIAN PUSH BUTTON SHOULD BE INSTALLED AT A HEIGHT OF 42 INCHES TO 48 INCHES ABOVE THE LANDING AREA, AND SHALL BE LOCATED SUCH THAT MAXIMUM REACH DISTANCE DOES NOT EXCEED 10 INCHES FROM THE LANDING AREA TO THE FACE OF THE PUSH BUTTON. PEDESTRIAN SIGNAL HEADS SHOULD BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING, INCLUDING BRACKETS, NOT LESS THAN 7 FEET OR MORE THAN 10 FEET ABOVE LANDING AREA OR SIDEWALK LEVEL.
- PEDESTRIAN SIGNALS SHALL CONTAIN PEDESTRIAN COUNTDOWN DISPLAYS.
- DELDOT TRAFFIC FORCES SHALL INSTALL PUSH BUTTON SO THAT THE FACE OF THE PUSH BUTTON IS PARALLEL WITH THE CROSSING AND INSTALL THE COUNTDOWN DISPLAY ALIGNED WITH THE CROSSING, AS SHOWN.
- DELDOT TRAFFIC SHALL INSTALL COUNTDOWN PEDESTRIAN SIGN (R10-3c) WITH PROPER PEDESTRIAN MOVEMENT ARROW FOR RELEVANT CROSSWALK DETECTION ABOVE EACH PEDESTRIAN PUSH BUTTON OR COUNTDOWN PEDESTRIAN SIGN (R10-3c-DE) WITH PROPER PEDESTRIAN MOVEMENT ARROW FOR RELEVANT CROSSWALK DETECTION ON TOP OF EACH PUSH BUTTON HOUSING WHERE TWO PUSH BUTTONS ARE INSTALLED ON ONE POLE. THE FACE OF THE PUSH BUTTON AND SIGN SHALL BE INSTALLED PARALLEL WITH THE CROSSING.
- ADJUST EXISTING MAINLINE PASSAGE DETECTION LOOP TO FOUR (4) SECONDS CONTRACTOR SHALL RE-CUT NEW LOOP DETECTORS WITH 1/4" CABLES AT 200 FEET FROM STOP BAR AND ABANDONED EXISTING LOOPS. ALL HOME-RUN CABLES FOR PROPOSED LOOPS SHALL BE 2/4" CABLE.
- PROPOSED PAVEMENT MARKINGS TO BE COMPLETED BY THE CIVIL CONTRACTOR.
- PROPOSED CURB RAMP ON THE NORTHEAST CORNER TO BE INSTALLED BY CIVIL CONTRACTOR. SEE SHEET TWO FOR PROPOSED RAMP LAYOUTS.
- ALL PROPOSED PAVEMENT MARKINGS SHALL BE THE RESPONSIBILITY OF THE DEVELOPER.

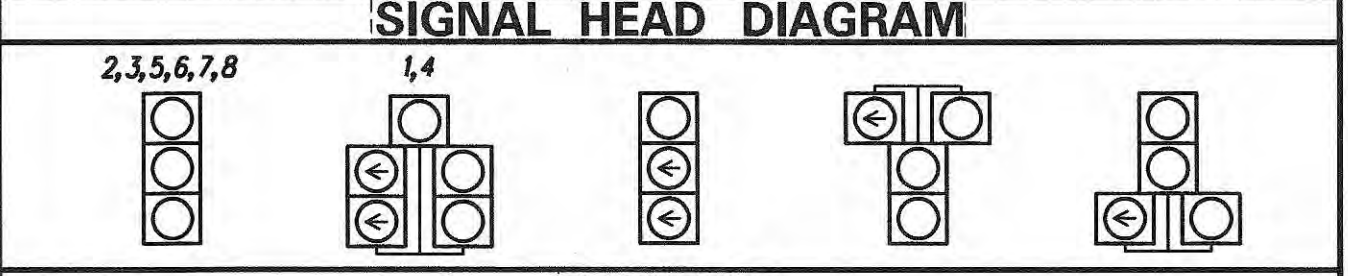
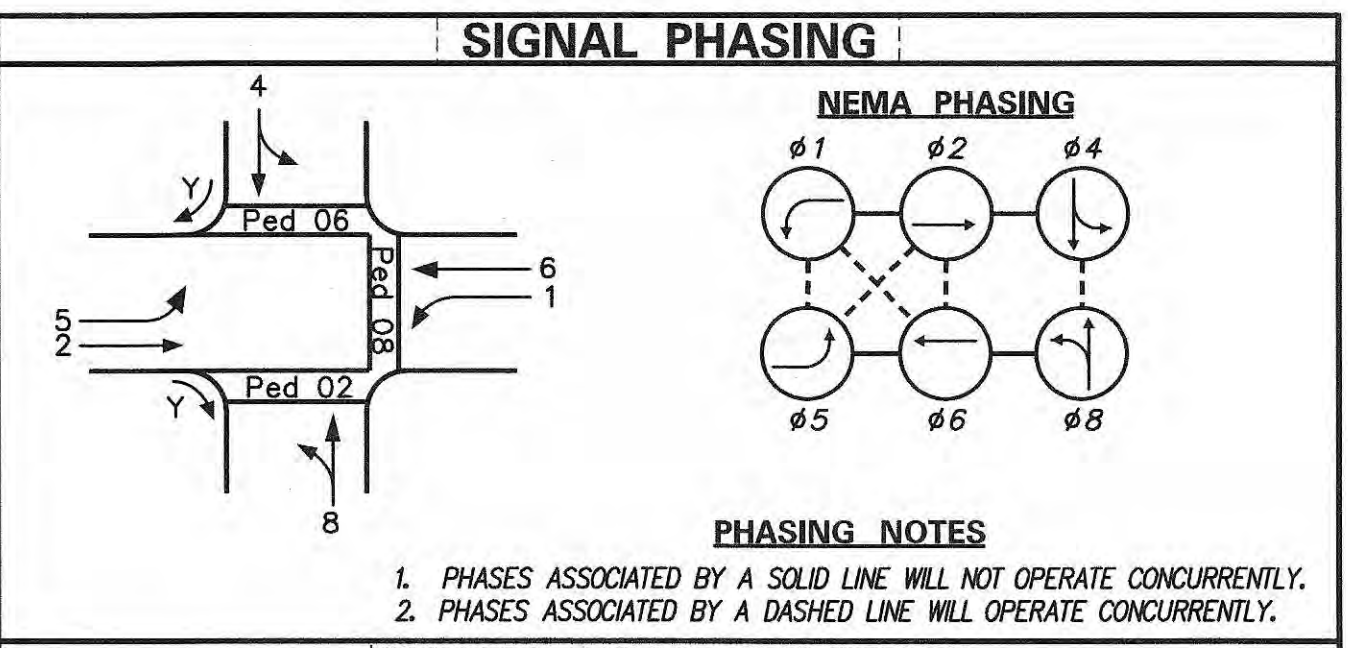
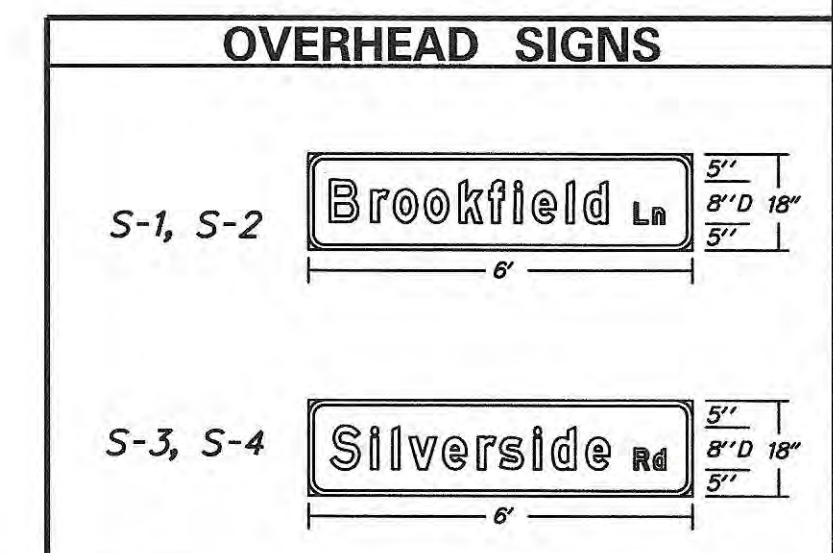


| CONDUIT RUN SCHEDULE | | | | | |
|----------------------|-----------------|------|--------|-------|---|
| CR NO. | NO. OF CONDUITS | SIZE | LENGTH | B/T/O | AMOUNT AND TYPE OF CABLE / WIRE |
| 1 | 1 | 2" | 5' | B | (1)#8/2 UF w/Ground |
| 1A | 1 | 2" | 90' | B | (1)#8/2 UF w/Ground |
| 1B | 1 | 2" | 50' | T | (1)#8/2 UF w/Ground |
| 1C | 1 | 2" | 5' | T | (1)#8/2 UF w/Ground |
| 2 | 4 | 4" | 5' | T | (10)#14/2, (6)#14/5, (4)#14/9, (4)#18/4 |
| 3 | 1 | 4" | 30' | T | (1)#14/2, (1)#14/5, (1)#14/9, (1)#18/4 |
| 4 | 1 | 3" | 10' | T | (1)#14/9, (1)#18/4 |
| 5 | 1 | 2.5" | 5' | T | (1)#14/5 |
| 6 | 1 | 4" | 45' | B | (5)#14/2 |
| *7 | 1 | 2.5" | 45' | -- | Remove-(2)#14/9, (4)#18/4, Install-(2)#14/2 |
| *8 | 1 | 2.5" | 152' | -- | Remove-(2)#18/4, Install-(2)#14/2 |
| *9 | 1 | 2.5" | 40' | -- | Remove-(2)#18/4, Install-(2)#14/2 |
| 10 | 1 | 1.5" | 5' | O | (2)#14/2 |
| 11 | 1 | 4" | 45' | B | (1)#14/2 |
| 12 | 1 | 1.5" | 5' | T | (1)#14/1 |
| 13 | 2 | 4" | 70' | B | (5)#14/2, (5)#14/5, (2)#14/9, (2)#18/4 |
| 14 | 1 | 2.5" | 15' | T | (1)#14/5 |
| 15 | 1 | 3" | 10' | T | (1)#14/9, (1)#18/4 |
| 16 | 1 | 3" | 10' | T | (1)#14/9, (1)#18/4 |
| 17 | 1 | 4" | 30' | T | (3)#14/2, (1)#14/5 |
| 18 | 1 | 2.5" | 10' | T | (1)#14/5 |
| 19 | 1 | 1.5" | 5' | T | (1)#14/1 |
| 20 | 1 | 4" | 195' | T | (2)#14/2 |

| CONDUIT RUN SCHEDULE | | | | | |
|----------------------|-----------------|------|--------|-------|---------------------------------|
| CR NO. | NO. OF CONDUITS | SIZE | LENGTH | B/T/O | AMOUNT AND TYPE OF CABLE / WIRE |
| 21 | 1 | 1.5" | 5' | T | (2)#14/1 |
| 22 | 1 | 4" | 85' | T | (2)#14/2, (3)#14/5 |
| 23 | 1 | 4" | 45' | T | (1)#14/2, (2)#14/5 |
| 24 | 1 | 1.5" | 5' | T | (1)#14/1 |
| 25 | 1 | 2.5" | 5' | T | (1)#14/5 |
| 26 | 1 | 2.5" | 5' | T | (1)#14/5 |
| 27 | 1 | 1.5" | 5' | T | (1)#14/1 |
| 28 | 1 | 4" | 40' | B | (1)#14/5 |
| 29 | 1 | 2.5" | 5' | T | (1)#14/5 |

*DENOTES EXISTING

| MAST ARM SCHEDULE | | | |
|-------------------|---------------|--------------|-----------------|
| MA NO. | LENGTH OF ARM | NO. OF HEADS | S.F. OF SIGNING |
| 1 | 80' | 4 | -- |
| 2 | 45' | 2 | 18 |
| 3 | 80' | 2 | 18 |



| LEGEND | | | |
|--------|---|------|--|
| (AB) | ABANDON | (OP) | EXISTING OVERHEAD RUN IDENTIFIER (# OF OVERHEAD RUN) |
| (CA) | EXISTING CABINET IDENTIFIER (TYPE OF CABINET) | (OP) | PROPOSED OVERHEAD RUN IDENTIFIER (# OF OVERHEAD RUN) |
| (CA) | PROPOSED CABINET IDENTIFIER (TYPE OF CABINET) | (PB) | EXISTING POLE BASE IDENTIFIER (TYPE OF POLE BASE) |
| (CO) | EXISTING CONDUIT RUN IDENTIFIER (# OF CONDUIT RUN) | (PB) | PROPOSED POLE BASE IDENTIFIER (TYPE OF POLE BASE) |
| (CO) | PROPOSED CONDUIT RUN IDENTIFIER (# OF CONDUIT RUN) | (PL) | EXISTING POLE IDENTIFIER (# OF POLE) |
| (JW) | EXISTING JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL) | (PL) | PROPOSED POLE IDENTIFIER (# OF POLE) |
| (JW) | PROPOSED JUNCTION WELL IDENTIFIER (TYPE OF JUNCTION WELL) | (RM) | REMOVE BY CONTRACTOR |
| (MA) | EXISTING MAST ARM IDENTIFIER (LENGTH OF ARM) | (RM) | REMOVE BY OTHERS |
| (MA) | PROPOSED MAST ARM IDENTIFIER (LENGTH OF ARM) | (RM) | REMOVE BY TRAFFIC CONTRACTOR |

| | EXISTING SYMBOL | PROPOSED SYMBOL |
|------------------------|-----------------|-----------------|
| JUNCTION WELL | J.W. | ■ |
| LOOP DETECTOR, TYPE 1 | □ | □ |
| LOOP DETECTOR, TYPE 2 | □ | □ |
| LUMINAIRE | ↗ | ↗ |
| MAST ARM | ↗ | ↗ |
| MICROWAVE DETECTION | ↗ | ↗ |
| OPTICOM RECEIVER | ○ | ○ |
| OVERHEAD SIGNING | ↑ | ↑ |
| PEDESTRIAN POLE/BASE | ○ | ○ |
| PEDESTRIAN PUSHBUTTON | → | → |
| PEDESTRIAN SIGNAL HEAD | → | → |
| RIGHT-OF-WAY | --- | ---R/W--- |
| SERVICE PEDESTAL | □ | □ |
| SIGNAL CABINET | □ | □ |
| SIGNAL HEAD | → | → |
| SIGNAL POLE/BASE | ○ | ○ |
| SPAN INSULATOR | ◆ | ◆ |
| SPAN WIRE | --- | --- |
| UTILITY POLE | ○ | ○ |
| VIDEO DETECTION | ↗ | ↗ |

- GENERAL SIGNAL NOTES**
- DETECTION - 40 M.P.H. - 4 SECONDS PASSAGE TIME AT 200 FEET FROM STOP BAR.
 - LOOP DETECTORS: TYPE #1 - 6' x 6' - TO BE INSTALLED ON MAIN STREET THROUGH MOVEMENTS. TYPE #2 - 8' x 25' - TO BE INSTALLED ON MAIN STREET LEFT TURN MOVEMENTS. TYPE #3 - 8' x 25' - TO BE INSTALLED ON SIDE STREET THROUGH AND LEFT TURN MOVEMENTS.
 - ALL SIGNAL POLES WILL BE MAST ARMS.
 - ALL SIGNAL EQUIPMENT REMOVED FROM A PROJECT IS TO BE RETURNED TO DELDOT TRAFFIC - DOVER, DELAWARE.
 - POLE BASES, CABINET BASE AND CONDUIT JUNCTION WELLS TO BE REMOVED IN ACCORDANCE WITH SECTION 201 AND 202 OF THE STANDARD SPECIFICATIONS OR AS DIRECTED BY ENGINEER. EXISTING CONDUIT IS TO BE ABANDONED.
 - ALL GALVANIZED CONDUIT (GRC) SHALL BE REAMED AND THREADED. ALL GRC SHALL BE THREADED TOGETHER WITH APPROVED COUPLINGS, SET SCREW, BOLTED, AND COMPRESSION FITTING ARE NOT ACCEPTABLE.
 - ALL UNDERGROUND AND OVERHEAD UTILITIES SHOWN ON THESE PLANS ARE SCHEMATIC ONLY AND MAY NOT BE COMPLETE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR NOTIFYING MISS UTILITY AND/OR THE APPROPRIATE UTILITY PRIOR TO THE BEGINNING OF CONSTRUCTION FOR THE UTILITY MARKOUTS. IF THE CONTRACTOR PERCEIVES THAT A CONFLICT BETWEEN UTILITIES AND THE TRAFFIC SIGNAL WILL OCCUR, THE CONTRACTOR SHALL NOTIFY DELDOT TRAFFIC IMMEDIATELY BEFORE CONSTRUCTION.

RECOMMENDED _____ DATE: 6.13.17 RECOMMENDED _____ DATE: _____ RECOMMENDED _____ DATE: _____ APPROVED TRAFFIC ENGINEER _____ DATE: 6/13/17 APPROVED FOR INSTALLATION CHIEF TRAFFIC ENGINEER _____ DATE: 6/13/17

ADDENDUM / REVISIONS

SCALE 0 30 60 90 FEET

DELAWARE DEPARTMENT OF TRANSPORTATION

CONCORD PLAZA

CONTRACT PERMIT NO. **N047P**

COUNTY DESIGNED BY: Sol Moinuddin (RK&K)

NEW CASTLE CHECKED BY: Jason Rashid (RK&K)

SIGNAL PLAN

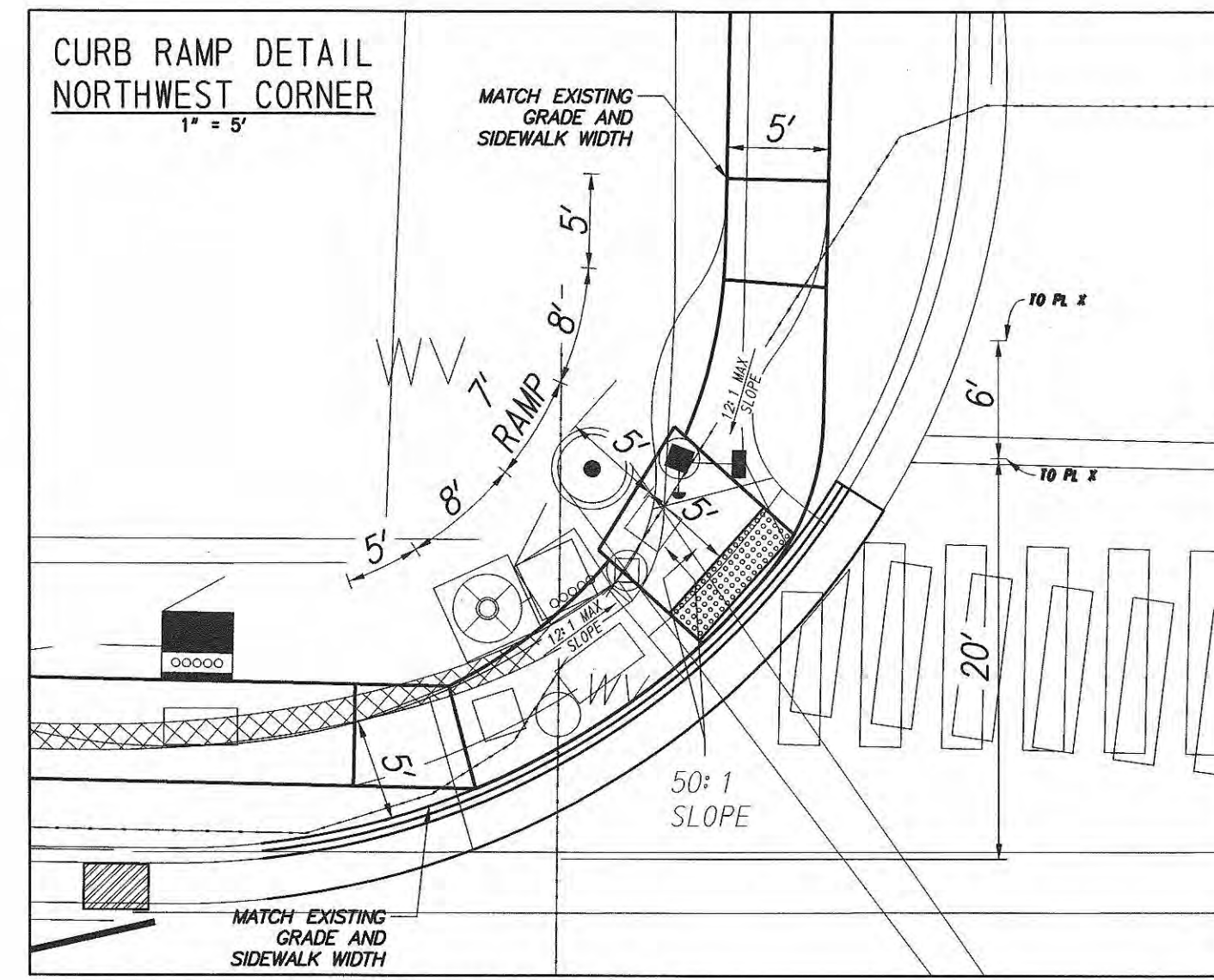
Silverside Rd. at Brookfield Ln.

SHEET NO. 1

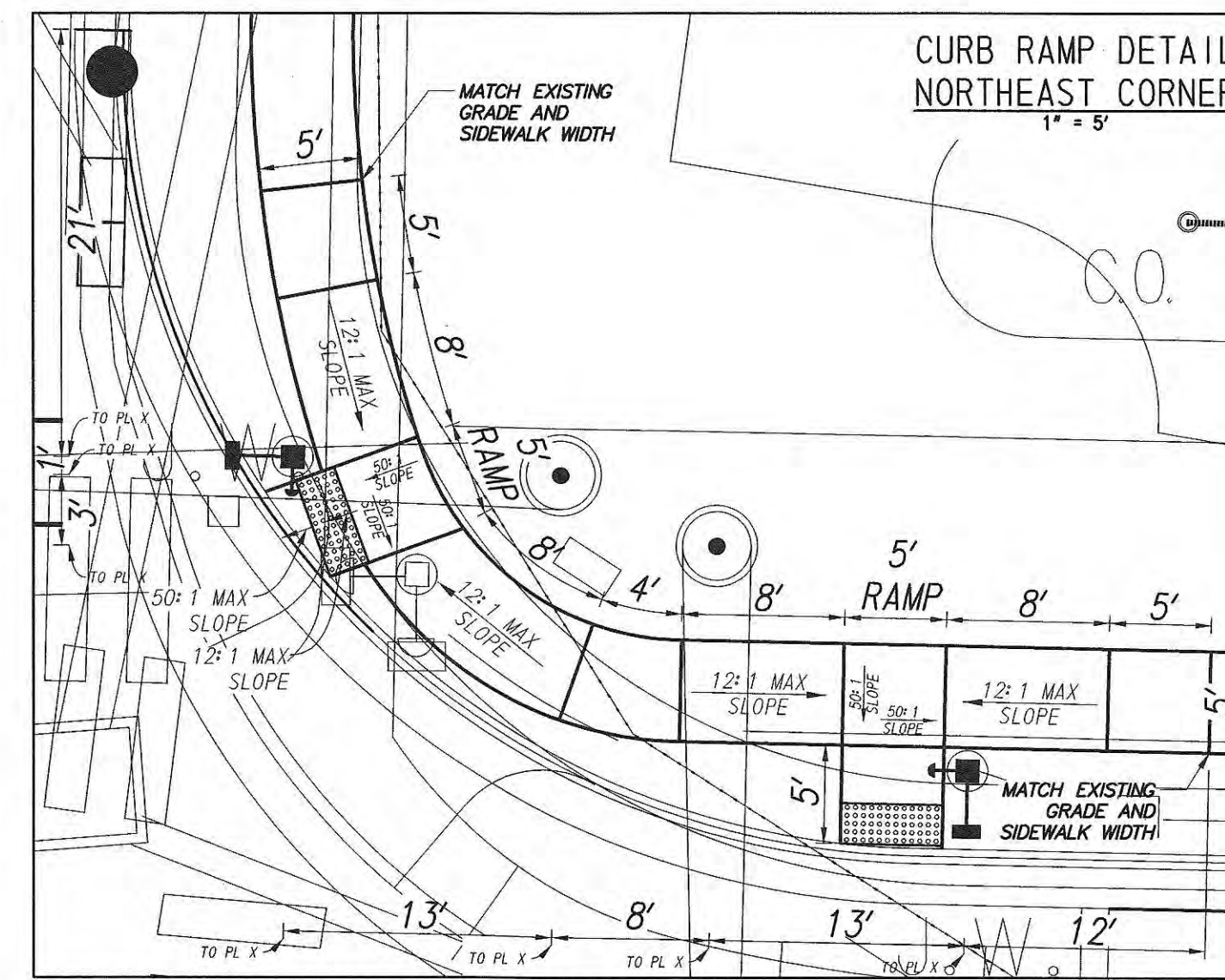
TOTAL SHTS. 2

ADDITIONAL NOTES:

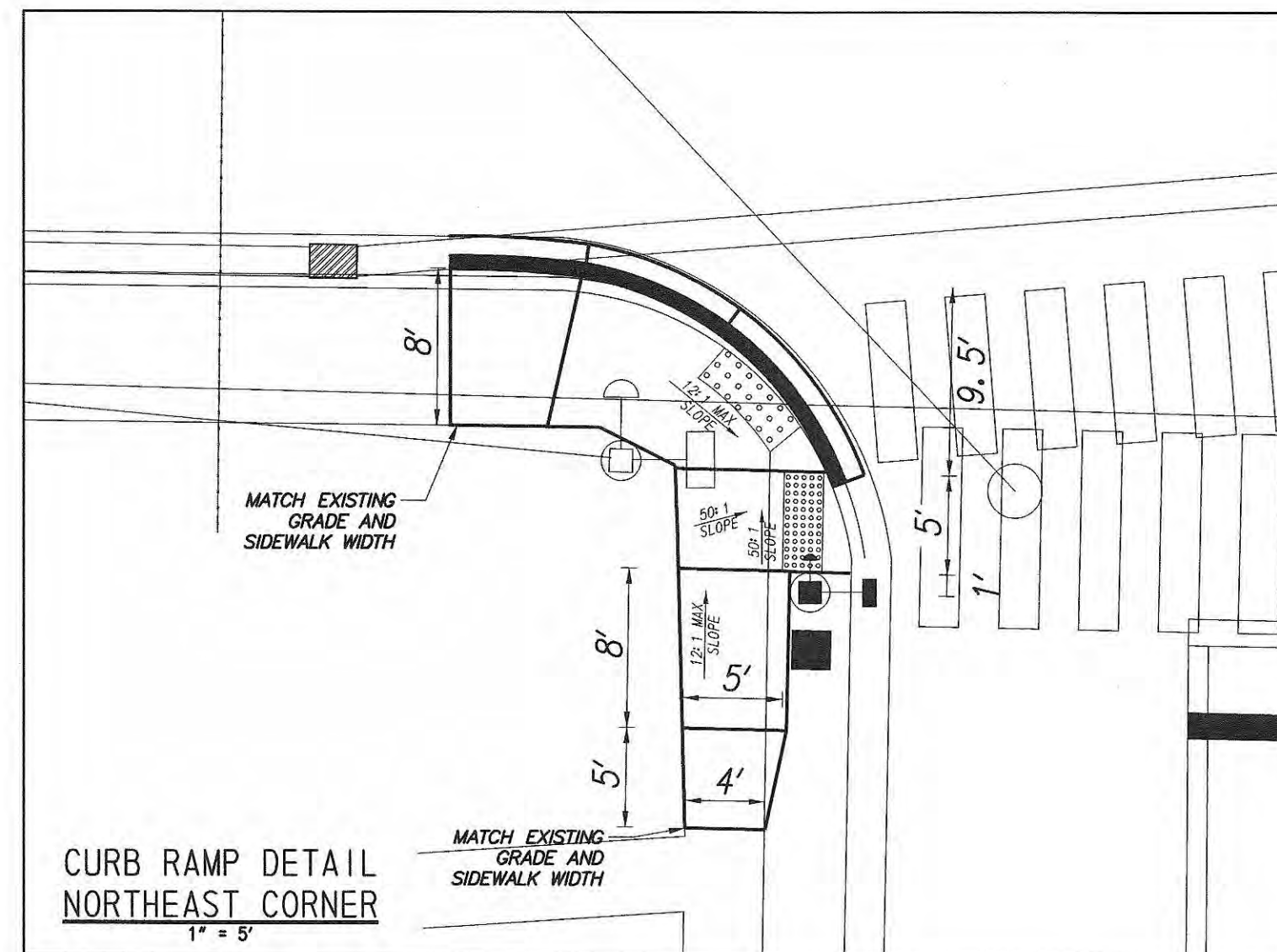
23. ACCEPTABLE WORK HOURS FOR THIS PROJECT ARE 8:00PM TO 5:00AM.
24. FOR TRAFFIC CONTROL ELEMENTS THE CONTRACTOR SHALL COMPLY WITH ALL GUIDELINES AND REQUIREMENT AS OUTLINED IN PART 6 OF "THE DELAWARE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES" (DE MUTCD). ANY DEVIATION OR SPECIAL REQUEST SHALL BE APPROVED BY MANAGER SAFETY OFFICER.
25. AS SHOWN IN PART 6H OF THE DE MUTCD THE FOLLOWING TYPICAL APPLICATION SHALL BE UTILIZED FOR CONSTRUCTION OF THE NEEDED FACILITY - TA-13, TA-21, TA21A, TA-33, TA-37, AND TA-47A. ADDITIONAL CASE MAY BE REQUIRED BASED ON FIELD CONDITIONS.
26. FOR THE CONSTRUCTION OF THE NORTHEAST AND NORTH WEST CORNER COORDINATION WITH THE DEVELOPER CONTRACTOR SHALL OCCUR. COORDINATION OF PEDESTRIAN MOT, LANE CLOSURES, AND OTHER ROADWAY IMPACTS SHOULD BE MINIMIZED WITH PROPER COORDINATION.
27. ACCEPTABLE MATERIAL FOR TEMPORARY PEDESTRIAN PATHS SHALL INCLUDE CONCRETE, HOT-MIX, COMPACTED MILLINGS, OR PLYWOOD WALKWAY STRUCTURE IN ACCORDANCE WITH SECTION 743 OF THE DELDOT "STANDARD SPECIFICATIONS," DATED AUGUST 2001 AND THE DELDOT "STANDARD CONSTRUCTION DETAILS," DATED 2014, INCLUDING ALL REVISIONS UP TO THE DATE OF ADVERTISEMENT.
28. STONE OR GRADED AGGREGATE BASE COURSE SHALL NOT BE USED FOR TEMPORARY PEDESTRIAN PATHS.
29. DURING ANY SIDEWALK CLOSURE, THE CONTRACTOR SHALL PROVIDE A FLAGGER EXCLUSIVELY FOR PEDESTRIAN MOT. AT THE END OF EACH WORK DAY A VIABLE ROUTE FOR PEDESTRIANS MUST BE AVAILABLE USING ACCEPTABLE PEDESTRIAN MATERIALS. THE ALTERNATE PEDESTRIAN ROUTE MUST BE APPROVED BY DELDOT TRAFFIC SAFETY.
30. TO MAINTAIN PEDESTRIAN ACCESS THE CONTRACTOR MAY CONSTRUCT PROPOSED SIDEWALK CONNECTION. A TEMPORARY PEDESTRIAN PATH WITH PEDESTRIAN CHANNELIZING BARRICADES ON EITHER SIDE OF THE PATH, LOCATED BEHIND THE IMPACTED SIDEWALK AND PEDESTRIAN RAMP CONSTRUCTION, THE ALTERNATE PEDESTRIAN ROUTE MUST BE APPROVED BY DELDOT TRAFFIC SAFETY.



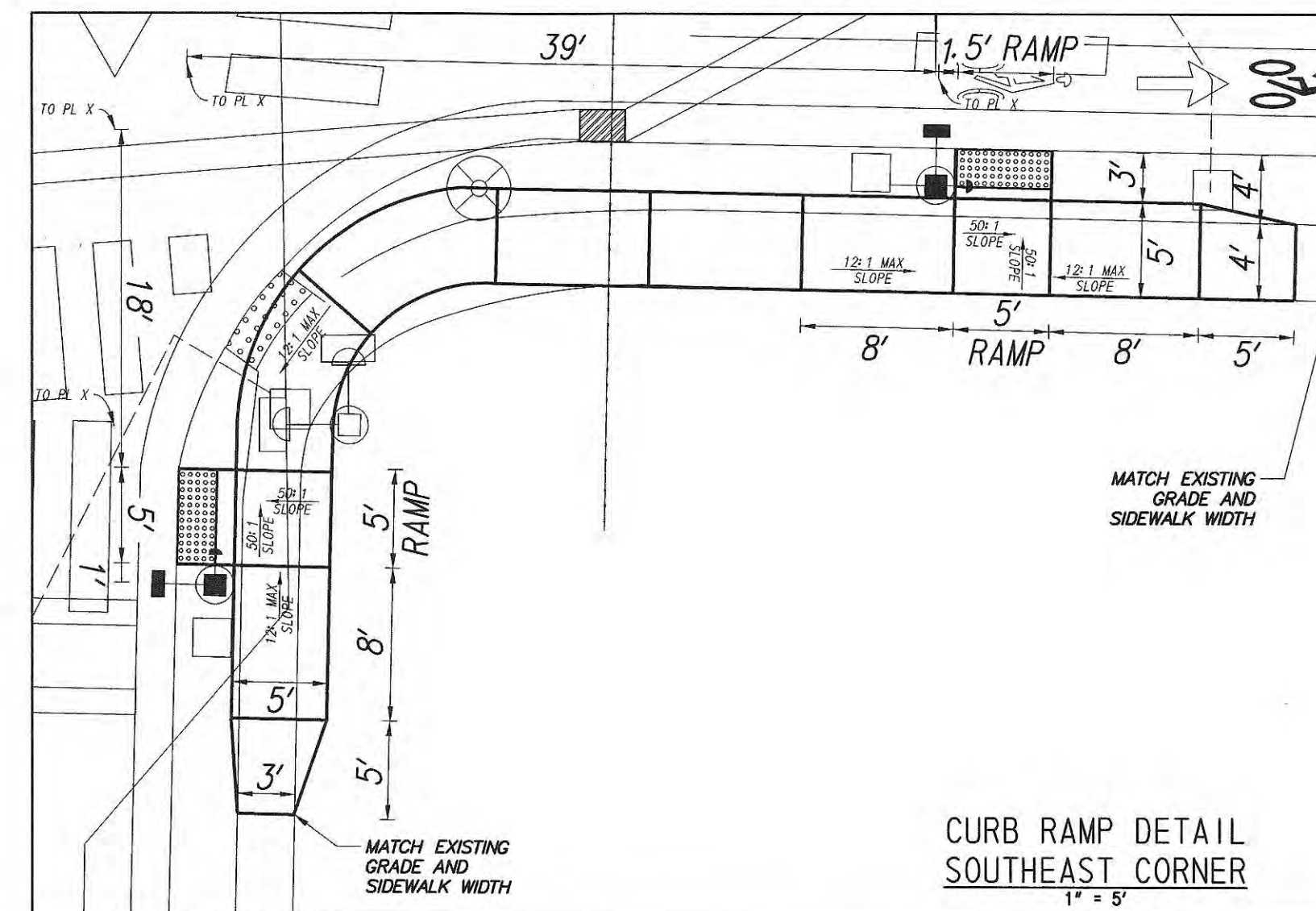
DEVELOPER CONTRACTOR TO BUILD RAMP



DEVELOPER CONTRACTOR TO BUILD RAMP



CURB RAMP DETAIL NORTHEAST CORNER



CURB RAMP DETAIL SOUTHEAST CORNER

SIGNAL PHASING

SIGNAL HEAD DIAGRAM

LEGEND

| | | | |
|------|---|-------|--|
| (AB) | ABANDON | (OPL) | EXISTING OVERHEAD RUN IDENTIFIER (# OF OVERHEAD RUN) |
| (CA) | EXISTING CABINET IDENTIFIER (TYPE OF CABINET) | (OP) | PROPOSED OVERHEAD RUN IDENTIFIER (# OF OVERHEAD RUN) |
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| (MA) | PROPOSED MAST ARM IDENTIFIER (LENGTH OF ARM) | (RM) | REMOVE BY TRAFFIC CONTRACTOR |

| | EXISTING SYMBOL | PROPOSED SYMBOL |
|------------------------|-----------------|-----------------|
| JUNCTION WELL | J.W. | ■ |
| LOOP DETECTOR, TYPE 1 | □ | □ |
| LOOP DETECTOR, TYPE 2 | □ | □ |
| LUMINAIRE | ⬇ | ⬇ |
| MAST ARM | ⬇ | ⬇ |
| MICROWAVE DETECTION | ⬇ | ⬇ |
| OPTICOM RECEIVER | ○ | ○ |
| OVERHEAD SIGNING | ⬇ | ⬇ |
| PEDESTRIAN POLE/BASE | ⬇ | ⬇ |
| PEDESTRIAN PUSHBUTTON | ⬇ | ⬇ |
| PEDESTRIAN SIGNAL HEAD | ⬇ | ⬇ |
| RIGHT-OF-WAY | --- | R/W |
| SERVICE PEDESTAL | □ | □ |
| SIGNAL CABINET | □ | □ |
| SIGNAL HEAD | ⬇ | ⬇ |
| SIGNAL POLE/BASE | ⬇ | ⬇ |
| SPAN INSULATOR | ⬇ | ⬇ |
| SPAN WIRE | XX | XX |
| UTILITY POLE | ⬇ | ⬇ |
| VIDEO DETECTION | ⬇ | ⬇ |

GENERAL SIGNAL NOTES

BRASILVERSIDE ROAD\CADD\ND047P_2.DGN

| | | | | | | | |
|---|-------------------------------|-----------------------------------|--|---|------------|---------------------------------|----------------------------------|
| RECOMMENDED DATE: 6.13.17 | RECOMMENDED _____ DATE: _____ | RECOMMENDED _____ DATE: _____ | APPROVED TRAFFIC ENGINEER DATE: 6/13/17 | APPROVED FOR INSTALLATION CHIEF TRAFFIC ENGINEER DATE: 6/13/17 | | | |
| ADDENDUM / REVISIONS | | | NONE | CONCORD PLAZA | | | |
| | | | | | | | |
| DELAWARE DEPARTMENT OF TRANSPORTATION | CONTRACT | PERMIT NO. | N047P | SIGNAL PLAN | SHEET NO. | | |
| | COUNTY | DESIGNED BY: Sol Moiruddin (RK&K) | | | NEW CASTLE | CHECKED BY: Jason Rashid (RK&K) | 2 |
| | | | | | | | Silverside Rd. at Brookfield Ln. |
| | | | | | 2 | | |

3701 CONCORD PIKE

APPENDIX B – PLANS FOR ADJACENT PROJECTS



GENERAL NOTES

- (*Last revised December 8, 2017)
1 All entrances shall conform to the Delaware Department of Transportation's (DelDOT's) current Development Coordination Manual (DCM) and shall be subject to its approval.
2 All materials and workmanship within the State of Delaware right-of-way shall be in accordance with current State of Delaware Department of Transportation Standard Specifications for Road and Bridge Construction, Supplemental Specifications, Standard Construction Details, Special Provisions and Design Guidance Memorandums.
3 All disturbed areas within the State right-of-way, but not in the pavement, shall be top-soiled (6" minimum), fertilized, seeded and mulched. If sod is used next to sidewalk or shared-use path, contractor shall grade upslope adjacent to the sidewalk or shared-use path prior to placement of sod to ensure that sod is placed flush or just below edge of sidewalk or shared-use path to avoid water ponding on the sidewalk or shared-use path.
4 A 72-hour (minimum) notice shall be given to the DelDOT district permit supervisor prior to starting entrance construction.
5 Miss Utility of Delaware shall be notified three (3) consecutive working days prior to excavation, at 1-800-282-8555.
6 All signing, striping and maintenance of traffic is the contractor's responsibility and shall follow the guidelines shown in the Delaware Manual on Uniform Traffic Control Devices (DE MUTCD) for Streets and Highways (latest edition). The owner or maintenance corporation shall be responsible for maintenance of all signs installed as part of this project.
7 Plan location and dimensions shall be strictly adhered to unless otherwise directed by the DelDOT Inspector.
8 A copy of the up to date approved construction documents and DelDOT approval letters shall be maintained on the project site at all times and be available for inspection by DelDOT personnel.
9 Existing utilities are shown in accordance with the best available information. Completeness or correctness thereof is not guaranteed. It shall be the contractor's responsibility to contact the utility companies involved in order to secure the most accurate information available as to utility location and elevation. No construction around or adjacent to utilities shall begin without notifying their owners at least 48-hours in advance. The contractor shall take the necessary precautions to protect the existing utilities and maintain uninterrupted service and any damage done to them due to his/her negligence shall be immediately and completely repaired at the contractor's expense. To locate existing utilities in the field prior to construction, the contractor shall contact Miss Utility of Delaware (See Note #5).
10 Should utility relocation be required, the developer must submit a utility relocation plan for DelDOT review, along with correspondence from the utility companies stating preliminary approval to the relocation and design of the utilities prior to the DelDOT pre-construction meeting. No physical construction can occur until the utility plans are approved, the individual utility companies issue final approval, and a DelDOT utility permit is issued to the utility company.
11 Upon completion of the construction of the sidewalk or shared-use path across this project's frontage and physical connection to adjacent existing facilities, the developer, the property owners or both associated with this project, shall be responsible to remove any existing road tie-in connections located along adjacent properties, and restore the area to grass. These disturbed areas shall be top-soiled (6" minimum), fertilized, seeded and mulched. Such actions shall be completed at DelDOT's discretion, and in conformance with DelDOT's "Shared-Use Path and/or Sidewalk Termination Policy".
12 DelDOT will not provide the respective local land use agency with a 'no objection to the issuance of the Certificate of Occupancy notice' until the entrance(s) are completed to the satisfaction of the Department.

- 13 Design, fabrication and installation of all permanent signing shall be as outlined in the latest version of the DE MUTCD.
14 Design and installation of all pavement markings and striping shall be as outlined in the latest version of the DE MUTCD. For final permanent pavement markings:
a) Epoxy resin paint shall be required for long line striping.
b) Thermo plastic (extruded or preformed material) will be required on Asphalt surfaces, for short line striping, i.e. symbols/legends.
c) Permanent pavement marking tape (per DelDOT approved materials list) will be required on Concrete surfaces, for short line striping, i.e. symbols/legends.
15 Removal of long line pavement striping shall be performed using: Shot, Sand or Hydro-Blasting.
16 Breakaway posts shall be used when installing all signs. Reference DelDOT Standard Construction Detail T-15.
17 No daytime lane closures will be allowed along Route 202 between May 1st and October 1st. Summer time lane shifts will be subject to the approval of the Traffic Safety Officer based on potential traffic impacts.
18 Contractor shall supply message boards that are to be placed ten (10) days prior to construction along Route 202. The message board must remain in place five (5) days after work has started. The proper wording on the message board will need to be coordinated with the District Safety Officer.
20 Maintenance of the streets within this subdivision will be the responsibility of the developer, the property owners within this subdivision, or both. The State of Delaware assumes no responsibility for the future maintenance of these streets.
21 Where curb type and/or height is being varied, taper curb per DelDOT Inspector (Typical for both sides of the entrance).

- 22 A double yellow centerline will be required along the paved portion of Prospect Avenue. Striping of the centerline shall be 5-inch wide line separated by a 6-inch space.
23 Maintenance of all landscaping shown on this plan will be the responsibility of the Developer, the Homeowners Association or both. The State assumes no responsibility for the future maintenance of the landscaping. DelDOT must review and approve any plantings proposed within the right of way prior to installation. If any plantings are planted without DelDOT's knowledge and/or approval, then DelDOT has the right to have the Developer remove the plantings, with all costs being paid for by the Developer.
24 Verify if any utilities will need to be relocated due to the addition of the shoulder. For clear zone purposes, all utilities to be a minimum of 10-feet from the edge of travel lane and 5-feet from the edge of pavement. Any utility that does not meet this requirement shall be relocated.
25 All paving within the State of Delaware right-of-way is installed to the elevations shown and no ponding of water exists after paving is complete.
26 All storm drain piping designated as RCP is to be reinforced concrete pipe, meeting AASHTO M-170 specifications. See plans for specific class of pipe.
27 All proposed closed stormdrain systems shall be video inspected, repaired as necessary and approved prior to the installation of final paving. If repairs are needed, the repaired pipe sections will need to be video inspected again before the repair can be approved.
28 Driveways will not be permitted to be placed at drainage inlet locations.
29 The developer and existing/future owner of non-state-maintained roadways shall ensure that the traffic control devices on said roadways open to public travel are in compliance with the latest version of the Delaware MUTCD.
30 For information on obtaining a Utility Permit in New Castle County contact M&O-Canal District-Public Works at (302) 326-4523.
31 For information on getting approval for proposed outdoor advertising in New Castle County contact M&O-Canal District-Public Works at (302) 326-4523.
32 All subgrade shall be compacted to 95% of the maximum standard T-99 dry density.

- ADDITIONAL NOTES
33. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO ENSURE THERE IS SUFFICIENT COVER ON ALL PIPING DURING CONSTRUCTION TO PREVENT FAILURE OF PIPES.
34. ALL PROPOSED UNDERDRAIN UTILITIES SHALL BE MANHOLE WITH 6-INCH (MIN) METALLIC MANHOLE TAPES OF APPROPRIATE COLOR AND APPROXIMATE TO UTILITY SHOWN. RESEARCH IT AND SHALL BE INSTALLED PER THE MANUFACTURER'S RECOMMENDATIONS.
35. ALL UTILITY EXCAVATION SHALL BE KEPT DRY AT ALL TIMES.
36. NO DEBRIS SHALL BE BURIED ON SITE.
37. PROVIDE EROSION CONTROL MATTING ON ALL SLOPES 3:1 OR STEEPER.
38. WHERE APPLICABLE INLET BOXES SHALL BE CAST WITH 6" HOLES TO ACCEPT UNDERDRAIN. THE UNDERDRAIN SHALL ENTER THE INLET BOX AS NEAR AS POSSIBLE TO THE PRESCRIBED DEPTH. ANY SPACE BETWEEN THE INLET WALL AND THE PIPE SHALL BE FILLED WITH NON-SHRINK GROUT CONFORMING TO ASTM C1107. UNDERDRAIN SHALL ENTER THE INLET BOX A MINIMUM OF 2" FROM ANY INTERIOR CORNER OF THE INLET BOX AND A MINIMUM OF 4" FROM ANY STORM DRAIN PIPE ENTERING THE SAME SIDE OF THE INLET.
39. THE SHARED USE PATH CURB RAMP INSTALLATION SHALL BE UNDERDRAIN PER DELDOT'S STANDARD DETAIL T-15. PLEASE NOTE THAT THE INSTALLATION OF THIS UNDERDRAIN AS SHOWN ON THE DETAIL IS NO LONGER REQUIRED BY DELDOT.
40. ALL STEEL USED IN CATCH BASINS SHALL BE 80 KSI.
41. PIPE BEDDING USED FOR THE INSTALLATION OF SANITARY SEWER SHALL MEET THE REQUIREMENTS INCLUDED IN THE NEW CASTLE COUNTY STANDARD SPECIFICATIONS FOR CONSTRUCTION AND AMENDMENTS. ALL OTHER PIPE BEDDING SHALL MEET THE REQUIREMENTS OF CLASS 'C'.

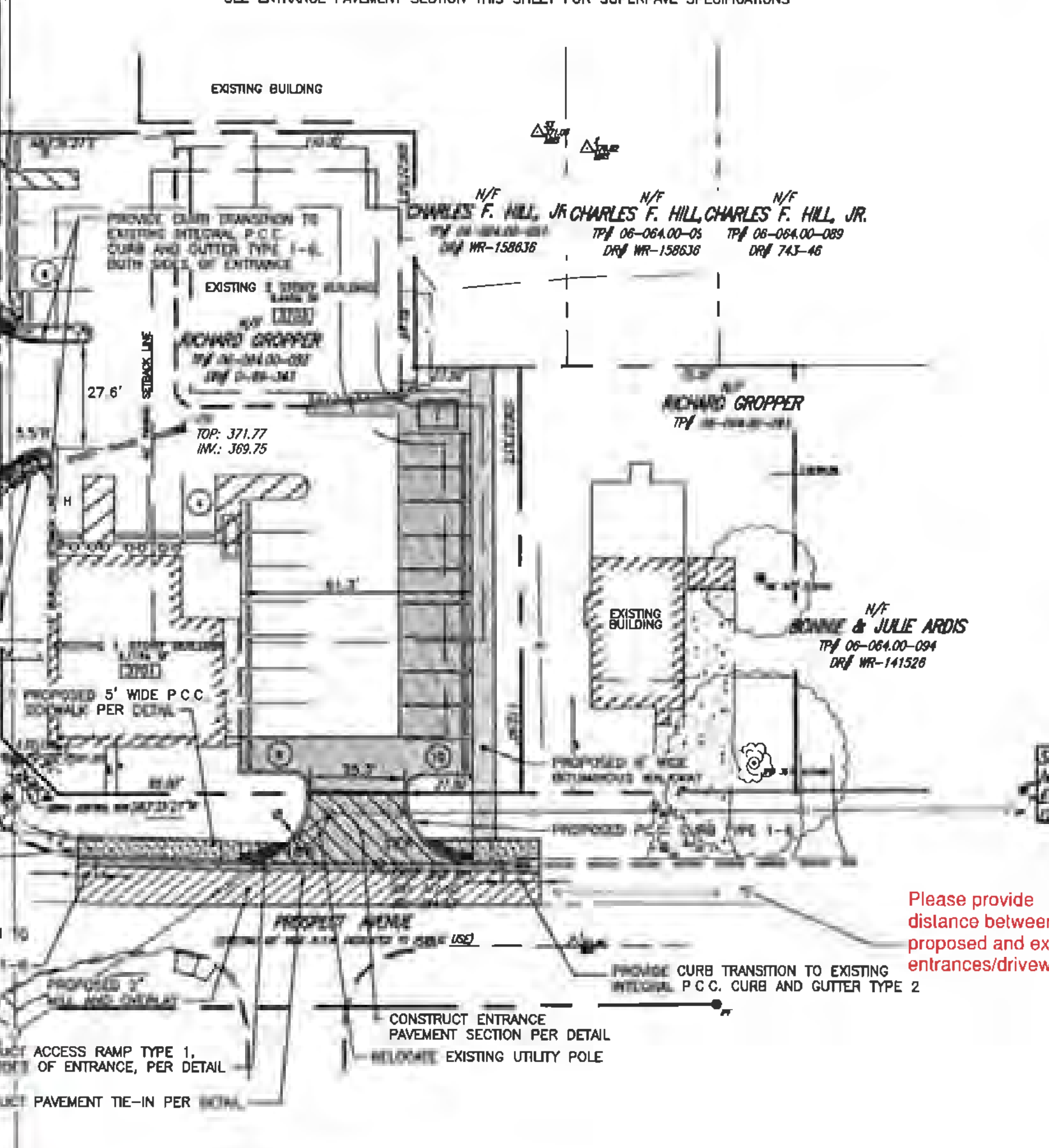
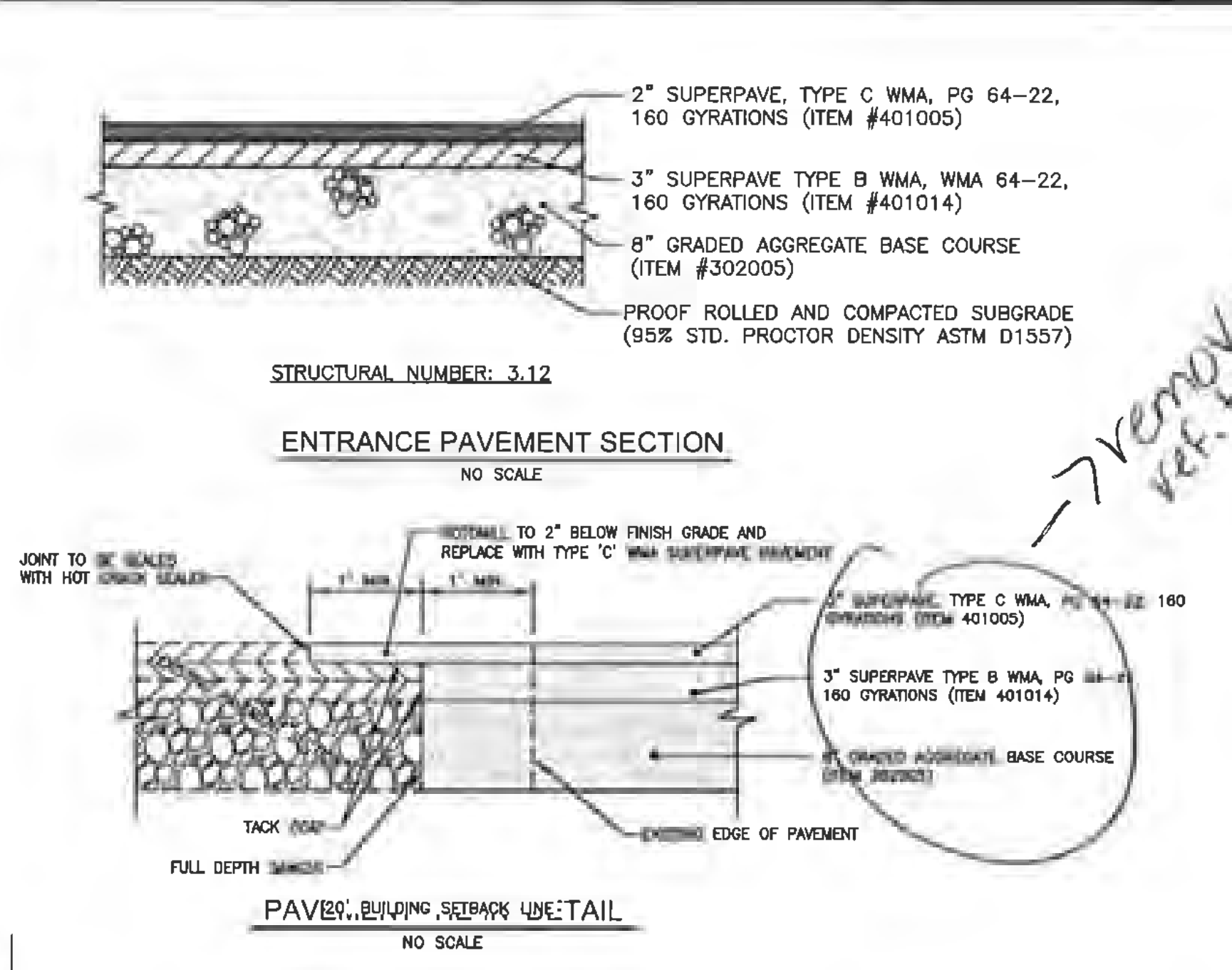
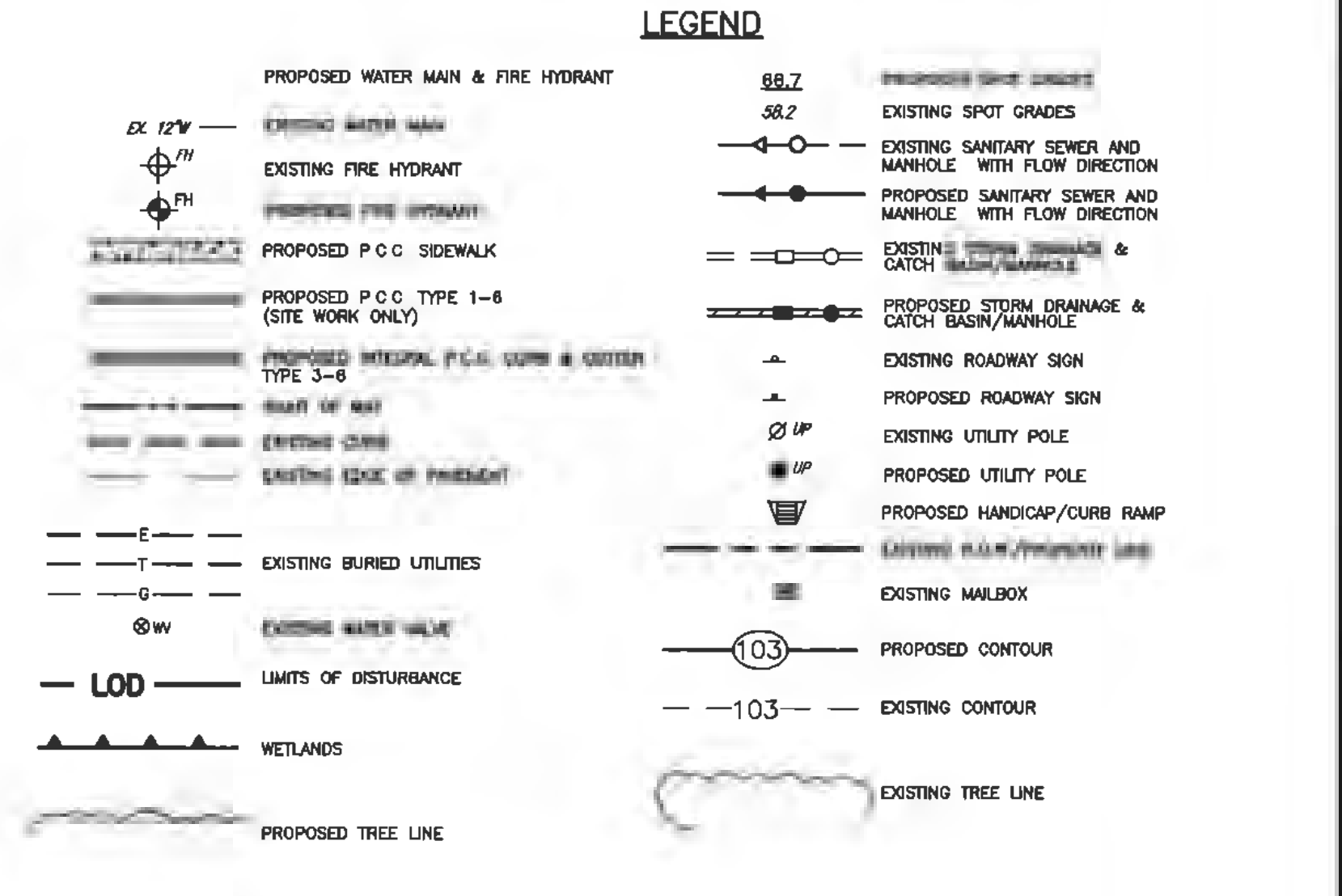
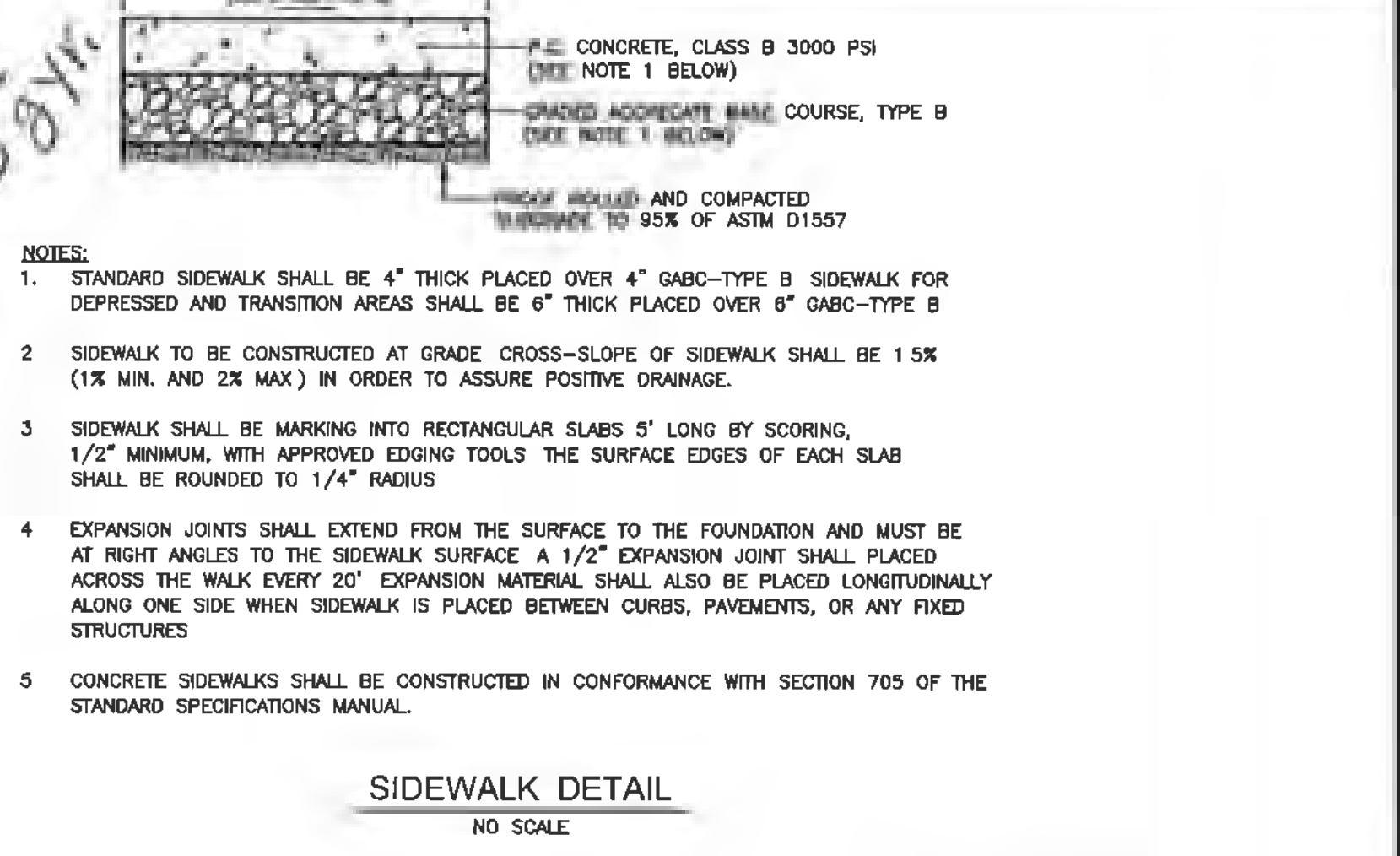


Table with columns: COUNTY (NEW CASTLE), SHEET No. (CD-01), TOTAL SHEETS (02). Includes disclaimer text.



811 Know what's below. Call before you dig. MISS UTILITY PHONE 1-800-282-8555. PROTECT YOURSELF, GIVE THREE WORKING DAYS NOTICE.

SHEET INDEX table with columns: SHEET, DESCRIPTION, COUNTY. Includes LAYOUT PLAN CD-01 and GRADING/ STRIPING PLAN CD-02.

TRAFFIC GENERATION - CONCORD PIKE & PROSPECT AVE. Includes table with columns: LOCATION, TRIP GENERATION, TRIP RATE, etc.

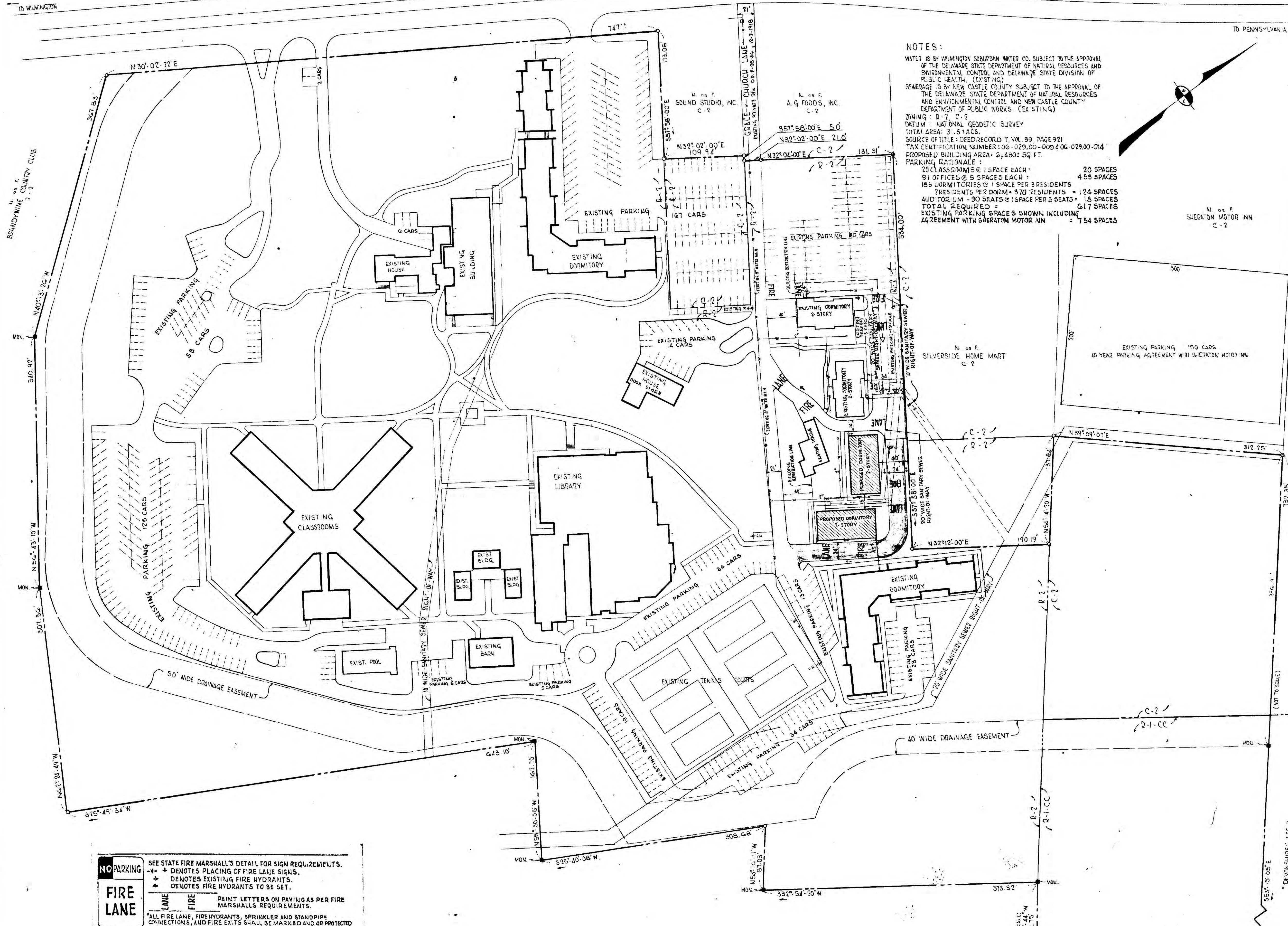
ENTRANCE PLAN-LAYOUT 3701 CONCORD PIKE BRANDYWINE HUNDRED - NEW CASTLE COUNTY DELAWARE. Includes OWNER: RICHARD GROPPER, ADDRESS: 733 BURNLEY DRIVE WILMINGTON, DE 19803, SCALE: 1"=30', DATE: 2-20-18.

PERMIT NO., CELEBRATING Landmark Science & Engineering, REVISIONS table, and CD-01 label.

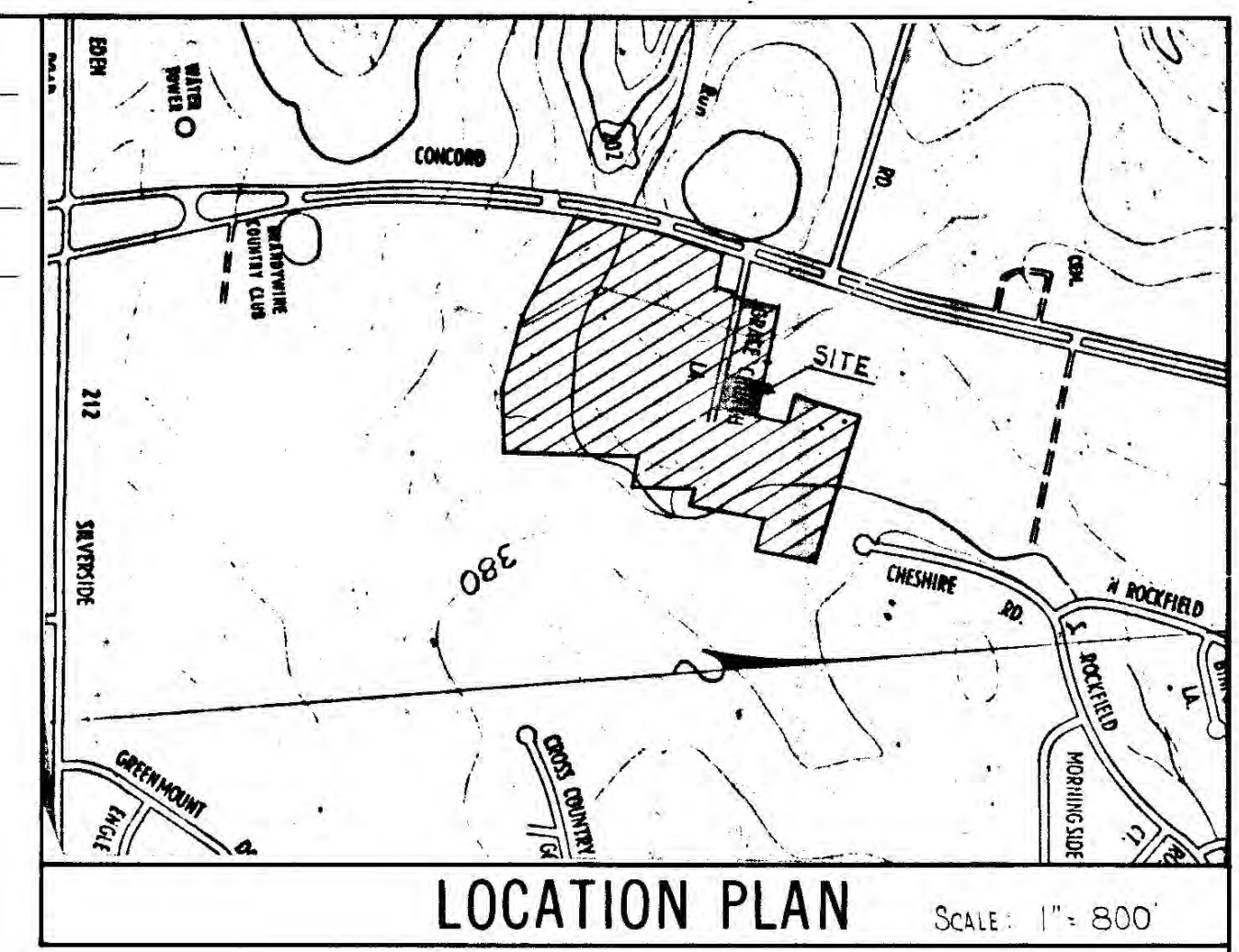
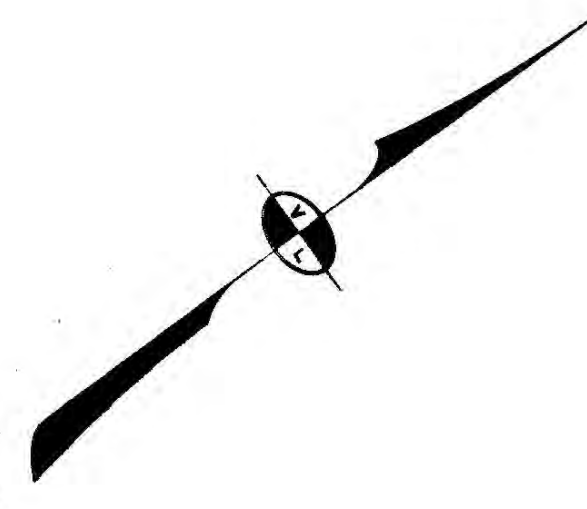
DEED RESEARCH FOR PARCEL # 0602900009

APPENDIX B – PLANS FOR ADJACENT PROJECTS

CONCORD TURNPIKE U.S. RTE. # 202



NOTES:
 WATER IS BY WILMINGTON SUBURBAN WATER CO. SUBJECT TO THE APPROVAL OF THE DELAWARE STATE DEPARTMENT OF NATURAL RESOURCES AND ENVIRONMENTAL CONTROL AND DELAWARE STATE DIVISION OF PUBLIC HEALTH. (EXISTING)
 SEWERAGE IS BY NEW CASTLE COUNTY SUBJECT TO THE APPROVAL OF THE DELAWARE STATE DEPARTMENT OF NATURAL RESOURCES AND ENVIRONMENTAL CONTROL AND NEW CASTLE COUNTY DEPARTMENT OF PUBLIC WORKS. (EXISTING)
 ZONING: R-7, C-2
 DATUM: NATIONAL GEODETIC SURVEY
 TOTAL AREA: 31.5 AC.
 SOURCE OF TITLE: DEED RECORD T, VOL. 89, PAGE 921
 TAX CERTIFICATION NUMBER: 06-029.00-009 & 06-029.00-014
 PROPOSED BUILDING AREA: 6,480 SQ. FT.
PARKING RATIONALE:
 20 CLASSROOMS @ 1 SPACE EACH = 20 SPACES
 91 OFFICES @ 5 SPACES EACH = 455 SPACES
 185 DORMITORIES @ 1 SPACE PER 3 RESIDENTS = 61 SPACES
 7 RESIDENTS PER DORM = 370 RESIDENTS = 124 SPACES
 AUDITORIUM - 90 SEATS @ 1 SPACE PER 5 SEATS = 18 SPACES
 TOTAL REQUIRED = 617 SPACES
 EXISTING PARKING SPACES SHOWN INCLUDING AGREEMENT WITH SHERATON MOTOR INN = 754 SPACES



ANDREW A. BUSHKO
 We, BERNARD J. DANLEY hereby certify that I am the owners of the property shown on this plan, that the subdivision plan thereof was made at the direction of William W. Dailey, Jr. and I acknowledge the same to be his act and plan and desire the same to be recorded as such according to law, and in accordance with the subdivision and land development regulations of New Castle County, and furthermore, that we voluntarily agree to subdivide and develop the land in accordance with the concepts shown on the approved District Plan.

Bernard J. Danley, Andrew A. Bushko

I, WILLIAM W. DAILEY, JR. hereby certify that I am a Professional land surveyor in the state of Delaware and that the plan shown and described hereon, consisting of 1 (one) sheet represents a survey made by VanDemark & Lynch, Inc., and is true and correct to the accuracy required by accepted surveying standards and practices and by the New Castle County Subdivision and Land Development Regulations to the extent that it describes the bearings and distances of subdivided lands, and that the existing monuments shown hereon actually exist and that their positions are accurately shown.

DEC 7 1982
 Approved 1/27 Date 1982 By William W. Dailey, Jr. Professional Land Surveyor
 Approved 1/27 Date 1982 By William W. Dailey, Jr. Planning Director for Department of Planning of New Castle County.
 Recorded: 1/27 Date 1982 in the office of the Recorder of Deeds in and for New Castle County. Microfilm No. 0360

"Lines and Grades No. 41881 of the Department of Public Works assumes no change in grade and shall not permit any construction or change of grade or contour without further formal approval of lines and grades."

NOTE:
 THIS PLAN SUPERSEDES IN PART THE RECORD MINOR LAND DEVELOPMENT PLAN OF BRANDYWINE COLLEGE, DATED JAN. 28, 1981 AND REVISED 2/23/81 AND 3/10/81 AND RECORDED ON MAR. 11, 1981 IN THE OFFICE OF THE RECORDER OF DEEDS IN AND FOR NEW CASTLE COUNTY, STATE OF DELAWARE. MICROFILM NO. 5897

RECORD MINOR LAND DEVELOPMENT PLAN BRANDYWINE COLLEGE CONCORD TURNPIKE & GRACE CHURCH LANE

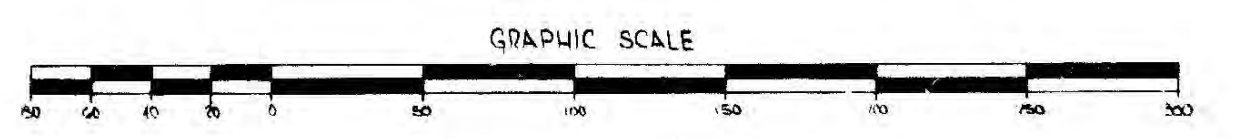
BRANDYWINE HUNDRED NEW CASTLE COUNTY DELAWARE
 SCALE: 1" = 60' DATE: DEC. 7, 1981
 SLD: 81-297 REV. 1-12-82

NO PARKING
FIRE LANE

SEE STATE FIRE MARSHALL'S DETAIL FOR SIGN REQUIREMENTS.
 DENOTES PLACING OF FIRE LANE SIGNS.
 DENOTES EXISTING FIRE HYDRANTS.
 DENOTES FIRE HYDRANTS TO BE SET.

PAINT LETTERS ON PAVING AS PER FIRE MARSHALL'S REQUIREMENTS.

ALL FIRE LANE, FIRE HYDRANTS, SPRINKLER AND STANDPIPE CONNECTIONS, AND FIRE EXITS SHALL BE MARKED AND/OR PROTECTED IN ACCORDANCE WITH THE STANDARD FOR COMPLIANCE WITH STATE DEPARTMENT OF PUBLIC WORKS.

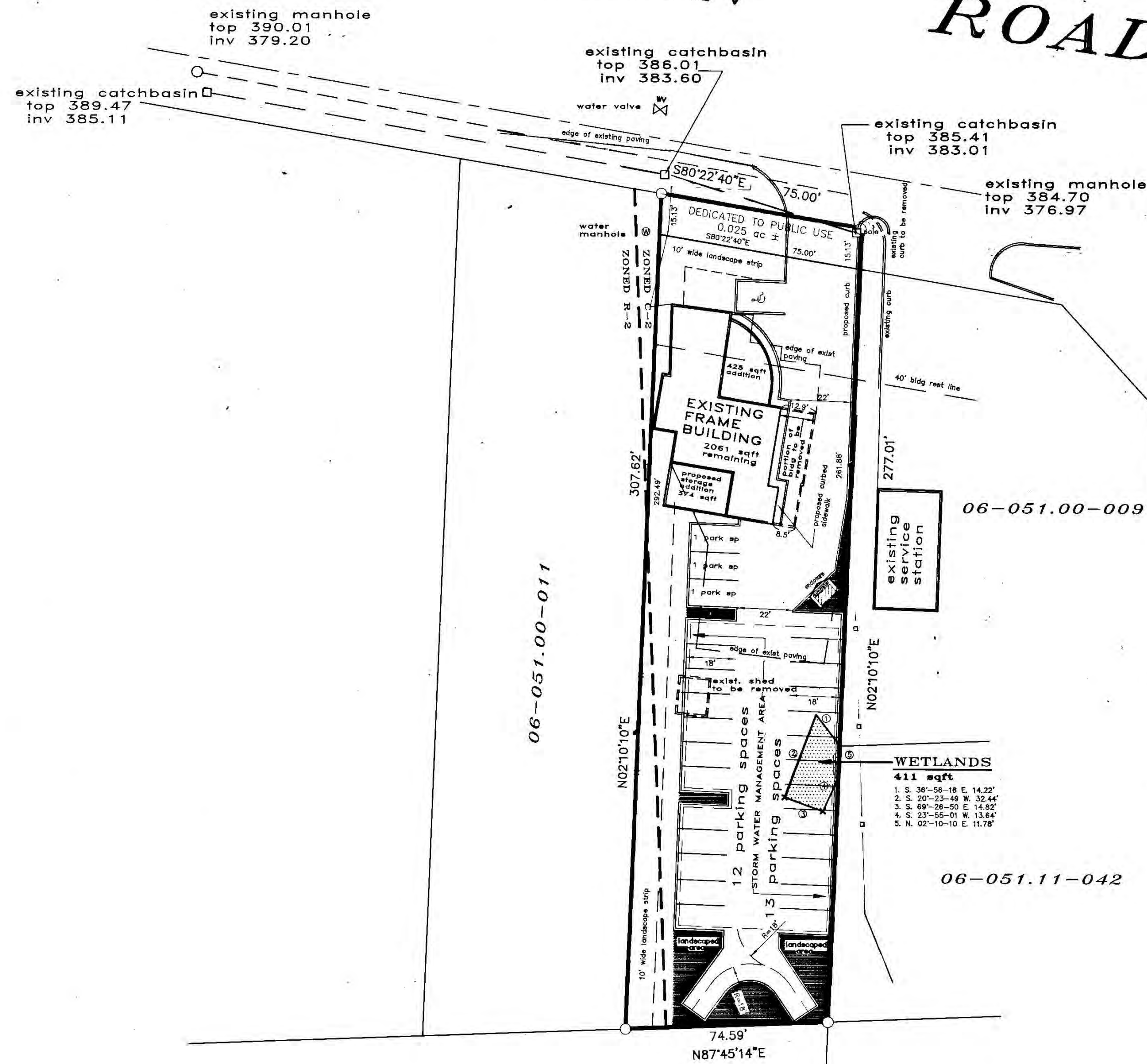


DEED RESEARCH FOR PARCEL # 0605100010

APPENDIX B – PLANS FOR ADJACENT PROJECTS

GARDEN OF EDEN ROAD

CONCORD TURNPIKE



MAINTENANCE
ALL COMMON FACILITIES INCLUDING, BUT NOT LIMITED TO, PAVED AREAS, SIDEWALKS, CURBING, LANDSCAPING, OPEN SPACE, STORMWATER MANAGEMENT AREAS AND/OR DRAINAGE FACILITIES SHALL BE KEPT IN GOOD REPAIR AND MAINTAINED IN A SAFE AND SANITARY CONDITION IN ACCORDANCE WITH THE PROVISIONS OF SECTION 20-70(C) AND (D) OF THE NEW CASTLE COUNTY CODE.

FIRE PROTECTION
ALL FIRE LANES, FIRE HYDRANTS, SPRINKLER AND STANDPIPE CONNECTIONS AND EXITS SHALL BE MARKED AND/OR PROTECTED IN ACCORDANCE WITH THE STANDARD FOR COMPLIANCE WITH NEW CASTLE COUNTY ORDINANCE 71-103, AND DELAWARE STATE FIRE PREVENTION REGULATION #14.

STORMWATER MANAGEMENT
DRAINAGE, STORMWATER MANAGEMENT, AND EROSION AND SEDIMENTATION CONTROL SHALL BE IN ACCORDANCE WITH THE DELAWARE SEDIMENT AND STORMWATER REGULATIONS AND THE NEW CASTLE COUNTY DRAINAGE CODE.

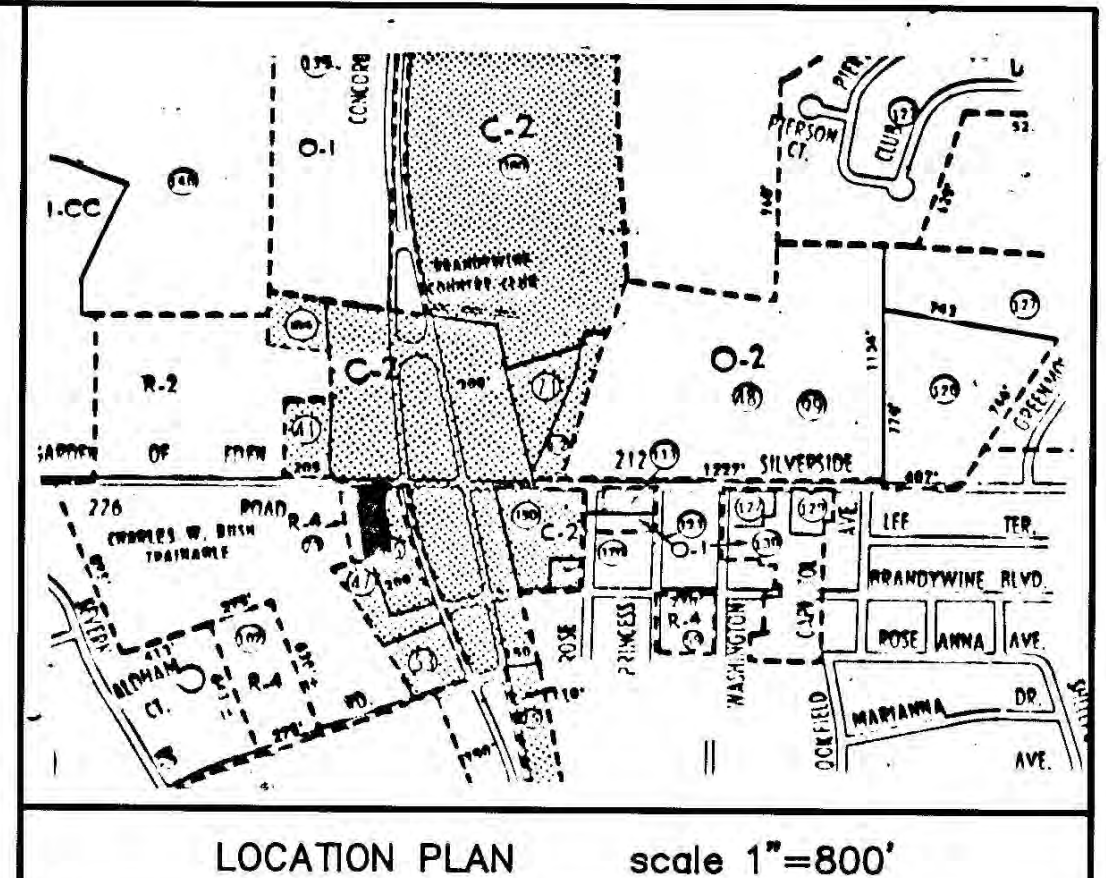
OWNERS
SHEK HUNG TSANG & LIAO YA YU
c/o Hunan Restaurant
8 Brammar Plaza Shopping Center
Wilmington, DE 19810

TAX PARCEL NUMBER
06-051.00-010

SOURCE OF TITLE
Deed Record 1398-123

AREAS

| | |
|--------------------------|-------------------|
| BUILDING COVER | 0.065 ac ± |
| PARKING & ROADWAY | 0.282 ac ± |
| OPEN | 0.137 ac ± |
| DEDICATED TO PUBLIC USES | 0.025 ac ± |
| TOTAL | 0.509 ac ± |



FLOOR AREA OF PROPOSED RESTAURANT
2870 sqft or 0.065 ac ±

PURPOSE
THE PURPOSE OF THIS PLAN IS TO SHOW THE PROPOSED ADDITIONS TO THE EXISTING BUILDING (THE NEW USE OF THE BUILDING WILL BE A RESTAURANT) AND THE PROPOSED PARKING.

COUNCILMATIC DISTRICT
3

MODIFIED GRID NUMBER
102/372

ZONING
C-2 & R-2

PARKING
1 space per 100 sqft, restaurant use
1st floor = 2860 sqft
29 required parking spaces
29 provided parking spaces (including 1 handicapped parking space.)

NOTE: 2nd floor to be converted to storage attic 7' in height.

WETLANDS
The wetland boundaries depicted on this plan were delineated by JAMES C. McCULLEY, IV, ENVIRONMENTAL CONSULTANTS, INC. with the procedure set forth in the FEDERAL MANUAL FOR IDENTIFYING AND DELINEATING JURISDICTIONAL WETLANDS, dated January 10, 1989, or as later amended. The Wetland Investigation Report for this site is dated August 17, 1994.

SETBACK
The setback of the building is made non-conforming by the dedication of the right-of-way by this plan.

LANDSCAPING
A landscape plan prepared by ERIC J. STURM, RLA, ASLA last dated 4/5/95 or as later amended and approved in writing by the Department of Planning is hereby considered to be a part of this plan.

SEWER
NEW CASTLE COUNTY subject to the approval of the Delaware State Department of Natural Resources and Environmental Control and the New Castle County Department of Public Works.
At the time of approval of this plan, sewer capacity existed to accommodate the anticipated flows generated by this additional development; however, New Castle County shall not be committed to provide sewer service until a sewer agreement between the developer and New Castle County is executed.

WATER
CITY OF WILMINGTON subject to the approval of the Delaware State Department of Natural Resources and Environmental Control and the Delaware State Division of Public Health.

STEEP SLOPES
There are no steep slopes on this site as defined by Section 23-115(b)(c) of the New Castle County Code.

DEBRIS DISPOSAL
No debris will be buried on this site.

DATUM
N.G.V.D.

TREE PRESERVATION
Developer shall preserve all trees on this site except where necessary to construct buildings, parking, accessways, recreational facilities, and utilities. Specific species of plant materials as designed on this plan or the Landscape Plan (if such a Plan is an integral part of this Plan) shall be preserved and properly protected during construction. In the case of utility rights of way and easements, any disturbed area shall be replanted so as to achieve a recurrence of the natural vegetation.

SANITARY SEWER
A 40-foot wide easement, 20-foot on each side of the centerline of the pipe, shall be created where a sanitary sewer exists outside of the limits of the dedicated highway right of way. Where pipes are installed in privately maintained paved areas such as parking lots, private streets, or driveways, this easement shall be 20-foot wide, 10-foot on each side of the centerline of the pipe.

CERTIFICATION OF ACCURACY
I, CARMINE F. CASPER, P.E. HEREBY CERTIFY THAT I AM A REGISTERED PROFESSIONAL ENGINEER WITH A BACKGROUND IN CIVIL ENGINEERING IN THE STATE OF DELAWARE, AND THAT THE PLAN SHOWN AND DESCRIBED HEREON, CONSISTING OF ONE (1) SHEET, REPRESENTS A SURVEY MADE BY HOWARD L. ROBERTSON, INC., AND IS TRUE AND CORRECT TO THE ACCURACY REQUIRED BY ACCEPTED SURVEYING STANDARDS AND PRACTICES AND BY THE NEW CASTLE COUNTY SUBDIVISION AND LAND DEVELOPMENT REGULATIONS TO THE EXTENT THAT IT DESCRIBES THE BEARINGS AND DISTANCES OF SUBDIVIDED LANDS, AND THAT THE EXISTING MONUMENTS SHOWN HEREON ACTUALLY EXIST AND THAT THEIR POSITIONS ARE ACCURATELY SHOWN.

4/5/95
DATE
CARMINE F. CASPER, P.E. (C.E.)

CERTIFICATION OF OWNERSHIP
WE, SHEK HUNG TSANG & LIAO YA YU, HEREBY CERTIFY THAT WE ARE THE OWNERS OF THE PROPERTY SHOWN ON THIS PLAN, AND THE SUBDIVISION PLAN THEREOF WAS MADE AT OUR DIRECTION, THAT WE ACKNOWLEDGE THE SAME TO BE OUR ACT AND PLAN AND DESIRE THE SAME TO BE RECORDED AS SUCH ACCORDING TO LAW, AND IN ACCORDANCE WITH THE SUBDIVISION AND LAND DEVELOPMENT REGULATIONS OF NEW CASTLE COUNTY, AND THAT ALL STREETS SHOWN AND NOT HERETOFORE DEDICATED ARE HEREBY DEDICATED TO THE PUBLIC USE, AND THAT ALL PROPOSED STREET MONUMENTS SHOWN HEREON WILL BE SET AT THE LOCATIONS SHOWN.

4-5-95
DATE
SHEK HUNG TSANG

4/5/95
DATE
LIAO YA YU

LINE AND GRADES
Lines and Grades No. 233-94 of the Department of Public Works assumes no change of grade or contour without formal approval of Lines and Grades.

RECORDATION
Recorded 4-17-95 in the Office of the Recorder of Deeds in and for New Castle County in Microfilm No. 12385

CERTIFICATION OF PLAN APPROVAL
APPROVED 4-12-95 BY [Signature] PLANNING DIRECTOR
FOR DEPARTMENT OF PLANNING OF NEW CASTLE COUNTY
APPROVED 4-12-95 BY [Signature] PLANNING DIRECTOR
FOR COUNTY COUNCIL OF NEW CASTLE COUNTY

| | |
|--------------|------------------------------|
| 6 | |
| 5 | |
| 4 | |
| 3 | |
| 2 | PER NCC PLANNING 8/9/95 |
| 1 | PER NCC PUBLIC WORKS 1/27/95 |
| REVISIONS | |
| SHEET 1 OF 1 | |

Application No. 941179(S)

RECORD MINOR LAND DEVELOPMENT PLAN

PROPERTY OF SHEK HUNG TSANG & LIAO YA YU
4 GARDEN OF EDEN ROAD

N.C.C. TAX PARCEL # 06-051.00-010 BLOCK MAP # D-2-N
BRANDYWINE HUNDRED NEW CASTLE COUNTY DELAWARE

SCALE: 1"=30' DATE: 12/13/94

HOWARD L. ROBERTSON, INC.
Registered Professional Engineers and Land Surveyors
801 BRANDYWINE BOULEVARD
WILMINGTON, DELAWARE 19809

APPENDIX C

CRASH SUMMARIES

Corridor-wide Crash Analysis

| | SUMMARY | | | ENVIRONMENTAL FACTORS | | | MANNER OF IMPACT | | | | CONTRIBUTING CIRCUMSTANCE | | | |
|----------------|-------------|------------|-------------------|-----------------------|------------|-----------|------------------|------------|------------|-----------|---------------------------|---------------------|----------------------|------------|
| | Total | Fatalities | Personal Injuries | Dry | Daylight | Alcohol | Rear-end | Angle | Sideswipe | Other | Driver Inattention | Following Too Close | Failure to Yield ROW | Other |
| US 202 | 447 | 1 | 54 | 364 | 337 | 9 | 244 | 108 | 55 | 40 | 106 | 103 | 39 | 199 |
| Silverside | 90 | 0 | 14 | 77 | 69 | 4 | 38 | 33 | 12 | 7 | 16 | 14 | 12 | 48 |
| Mt. Lebanon | 20 | 0 | 3 | 14 | 15 | 0 | 11 | 5 | 1 | 3 | 3 | 4 | 1 | 12 |
| Garden of Eden | 15 | 0 | 1 | 10 | 12 | 1 | 9 | 4 | 0 | 2 | 2 | 4 | 4 | 5 |
| Total | 572 | 1 | 72 | 465 | 433 | 14 | 302 | 150 | 68 | 52 | 127 | 125 | 56 | 264 |
| Percent | 100% | 0% | 13% | 81% | 76% | 2% | 53% | 26% | 12% | 9% | 22% | 22% | 10% | 46% |

Source: DelDOT Crash Summaries for a 5-year period between 12/19/2012 and 12/19/2017

Bicycle & Pedestrian Crashes

| # | Date | Primary Street | Closest Side Street | Lighting | Severity | Contributing Factors |
|---|------------|----------------|---------------------|--------------------|-------------------------|---|
| 1 | 3/1/2008 | Silverside Rd | Rose Avenue | Dark - No Lighting | Injury - Incapacitating | Pedestrian hit crossing on unmarked leg of crosswalk. |
| 2 | 1/10/2010 | US 202 | Righter Pkwy | Dark - Lighting | Injury - Incapacitating | Bicyclist hit crossing against traffic signal. |
| 3 | 12/20/2010 | Silverside Rd | 100' east of US 202 | Dark - No Lighting | Injury | Pedestrian hit crossing away from crosswalk after exiting bus. |
| 4 | 4/5/2012 | US 202 | Rocky Run Pkwy | Daylight | No injury | Bicyclist riding on sidewalk and approaching intersection flipped over handlebars while braking. |
| 5 | 8/27/2014 | US 202 | Rocky Run Pkwy | Daylight | Fatality | Pedestrian hit crossing Route 202. No other details provided. |
| 6 | 8/21/2016 | US 202 | Brandywine Blvd | Daylight | Injury | Bicyclist hit by vehicle turning right on red. Bicyclist under the influence and riding the wrong way. |
| 7 | 11/24/2016 | Silverside Rd | Brookfield Ln | Dark - No Lighting | No injury | Pedestrian hit by left turning vehicle while crossing in crosswalk. Dark conditions were cited as a factor. |

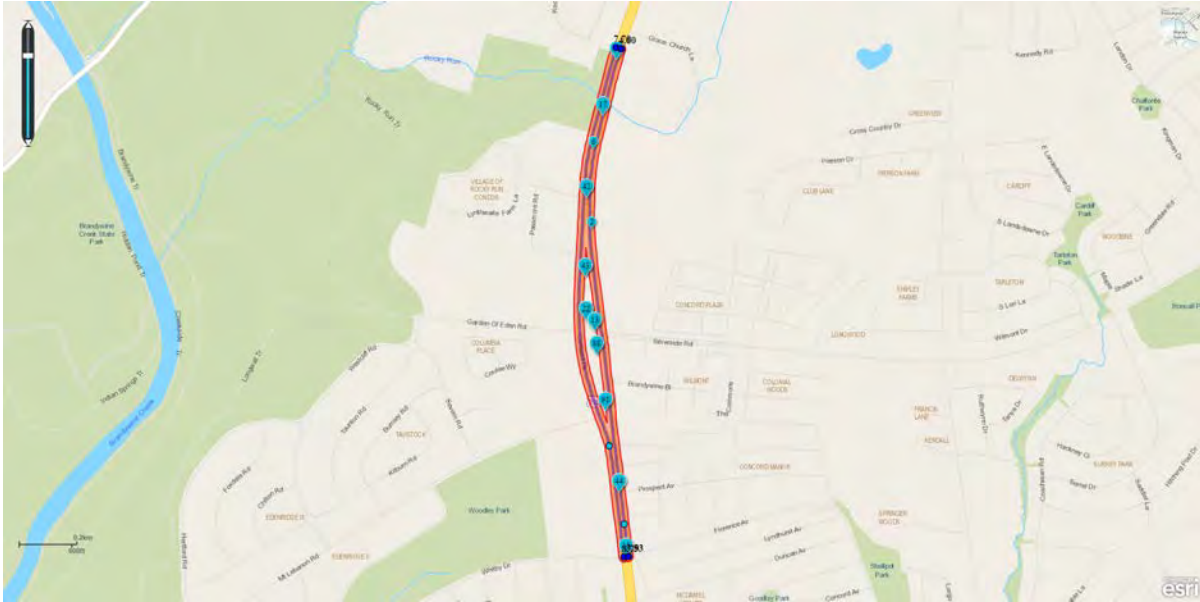
Source: *DeIDOT Crash Summaries for a 10-year period between 12/19/2007 and 12/19/2017*

US 202, FROM WHITBY DR TO ROCKY RUN PKWY

APPENDIX C – CRASH SUMMARIES

Delaware Crash Analysis Reporting System

Crash Study Time Period: 12/19/2012 - 12/19/2017
Query Type: CrashBufferTool
Description: Crash Summary US 202 Whitby Drive to Rockey Run Pwky
Study Requested By: John Federico- Urban Eng
Study Generated By: TDTSKLG
Number of Crashes: 447
Includes Non-Reportable Crashes: N
Study Code:



Disclaimer for CARS: Crash data and associated police reports are intended for DeIDOT use only and shall not be transmitted, copied, distributed or provided to any entity other than DeIDOT unless written approval is received from the DeIDOT Legal Section. Police reports are the property of the Delaware State Police.

State of Delaware Crash Study

| Summary | | Classification | | | Manner of Impact | | |
|--|--------------|-----------------|--------------|--------------------|--------------------------------------|--------------|--------------------|
| | # of Crashes | | # of Crashes | % of Total Crashes | | # of Crashes | % of Total Crashes |
| Total Crashes | 447 | Non-Reportable | 0 | 0.00% | Front to rear | 244 | 54.59% |
| Fatal Crashes | 1 | Reportable | 392 | 87.70% | Front to front | 4 | 0.89% |
| Total Alcohol-Related Crashes | 9 | Personal Injury | 54 | 12.08% | Angle | 108 | 24.16% |
| Total Non Alcohol-Related Crashes | 438 | Fatality | 1 | 0.22% | Sideswipe, same direction | 54 | 12.08% |
| Total Fatalities | 1 | Total | 447 | | Sideswipe, opposite direction | 1 | 0.22% |
| Total Pedestrian Fatalities | 1 | | | | Rear to side | 0 | 0.00% |
| Total Pedestrian Injuries | 0 | | | | Rear to rear | 0 | 0.00% |
| Total Pedestrian Crashes | 0 | | | | Other | 3 | 0.67% |
| Total Motorcycle Crashes | 2 | | | | Unknown | 5 | 1.12% |
| Total Pedalcyclist Crashes | 1 | | | | Not a collision between two vehicles | 28 | 6.26% |
| | | | | | Total | 447 | |

Alcohol Related Crashes By Classification

| | Non-reportable | Reportable | Personal Injury | Fatality | Total |
|---------------------|----------------|------------|-----------------|----------|------------|
| Alcohol Related | 0 | 6 | 3 | 0 | 9 |
| Non-Alcohol Related | 0 | 386 | 51 | 1 | 438 |
| Total | 0 | 392 | 54 | 1 | 447 |

Manner of Impact By Classification

| | Non-Reportable | Reportable | Personal Injury | Fatality | Total |
|--------------------------------------|----------------|------------|-----------------|----------|------------|
| Front to rear | 0 | 217 | 27 | 0 | 244 |
| Front to front | 0 | 2 | 2 | 0 | 4 |
| Angle | 0 | 92 | 16 | 0 | 108 |
| Sideswipe, same direction | 0 | 52 | 2 | 0 | 54 |
| Sideswipe, opposite direction | 0 | 1 | 0 | 0 | 1 |
| Rear to side | 0 | 0 | 0 | 0 | 0 |
| Rear to rear | 0 | 0 | 0 | 0 | 0 |
| Other | 0 | 2 | 1 | 0 | 3 |
| Unknown | 0 | 5 | 0 | 0 | 5 |
| Not a collision between two vehicles | 0 | 21 | 6 | 1 | 28 |
| Total | 0 | 392 | 54 | 1 | 447 |

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| Day Of Week | | |
|--------------|--------------|--------------------|
| | # of Crashes | % of Total Crashes |
| Sunday | 39 | 8.72% |
| Monday | 53 | 11.86% |
| Tuesday | 61 | 13.65% |
| Wednesday | 63 | 14.09% |
| Thursday | 84 | 18.79% |
| Friday | 74 | 16.55% |
| Saturday | 73 | 16.33% |
| Total | 447 | |

| Time Of Day (AM) | | |
|------------------|--------------|--------------------|
| | # of Crashes | % of Total Crashes |
| 00:00 - 00:59 | 6 | 1.34% |
| 01:00 - 01:59 | 0 | 0.00% |
| 02:00 - 02:59 | 0 | 0.00% |
| 03:00 - 03:59 | 2 | 0.45% |
| 04:00 - 04:59 | 0 | 0.00% |
| 05:00 - 05:59 | 1 | 0.22% |
| 06:00 - 06:59 | 7 | 1.57% |
| 07:00 - 07:59 | 10 | 2.24% |
| 08:00 - 08:59 | 21 | 4.70% |
| 09:00 - 09:59 | 18 | 4.03% |
| 10:00 - 10:59 | 23 | 5.15% |
| 11:00 - 11:59 | 26 | 5.82% |
| Total | 114 | |

| Time Of Day (PM) | | |
|------------------|--------------|--------------------|
| | # of Crashes | % of Total Crashes |
| 12:00 - 12:59 | 52 | 11.63% |
| 13:00 - 13:59 | 50 | 11.19% |
| 14:00 - 14:59 | 38 | 8.50% |
| 15:00 - 15:59 | 41 | 9.17% |
| 16:00 - 16:59 | 24 | 5.37% |
| 17:00 - 17:59 | 36 | 8.05% |
| 18:00 - 18:59 | 36 | 8.05% |
| 19:00 - 19:59 | 18 | 4.03% |
| 20:00 - 20:59 | 11 | 2.46% |
| 21:00 - 21:59 | 14 | 3.13% |
| 22:00 - 22:59 | 10 | 2.24% |
| 23:00 - 23:59 | 3 | 0.67% |
| Total | 333 | |
| Unknown Time | 0 | |

| Surface Conditions | | |
|--------------------------|--------------|--------------------|
| | # of Crashes | % of Total Crashes |
| Dry | 364 | 81.43% |
| Wet | 75 | 16.78% |
| Snow | 4 | 0.89% |
| Ice/Frost | 2 | 0.45% |
| Sand | 0 | 0.00% |
| Water (standing, moving) | 2 | 0.45% |
| Slush | 0 | 0.00% |
| Oil | 0 | 0.00% |
| Mud, Dirt, Gravel | 0 | 0.00% |
| Other | 0 | 0.00% |
| Unknown | 0 | 0.00% |
| Total | 447 | |

| Lighting Conditions | | |
|-----------------------|--------------|--------------------|
| | # of Crashes | % of Total Crashes |
| Daylight | 337 | 75.39% |
| Dawn | 3 | 0.67% |
| Dusk | 8 | 1.79% |
| Dark-Lighted | 83 | 18.57% |
| Dark-Not Lighted | 15 | 3.36% |
| Dark-Unknown Lighting | 1 | 0.22% |
| Other | 0 | 0.00% |
| Unknown | 0 | 0.00% |
| Total | 447 | |

| Weather Conditions | | |
|--|--------------|--------------------|
| | # of Crashes | % of Total Crashes |
| Clear | 327 | 73.15% |
| Cloudy | 64 | 14.32% |
| Fog, Smog, Smoke | 0 | 0.00% |
| Rain | 50 | 11.19% |
| Sleet, Hail (freezing rain or drizzle) | 2 | 0.45% |
| Snow | 6 | 1.34% |
| Blowing Snow | 1 | 0.22% |
| Severe Crosswinds | 0 | 0.00% |
| Blowing Sand, Soil, Dirt | 0 | 0.00% |
| Other | 0 | 0.00% |
| Unknown | 0 | 0.00% |
| Total | 450 | |

Disclaimer for CARS: Crash data and associated police reports are intended for DelDOT use only and shall not be transmitted, copied, distributed or provided to any entity other than DelDOT unless written approval is received from the DelDOT Legal Section. Police reports are the property of the Delaware State Police.

| First Harmful Event | | |
|---|--------------|--------------------|
| | # of Crashes | % of Total Crashes |
| Overturn/Rollover, Non-Collision | 0 | 0.00% |
| Fire/Explosion, Non-Collision | 0 | 0.00% |
| Immersion, Non-Collision | 0 | 0.00% |
| Jackknife, Non-Collision | 0 | 0.00% |
| Cargo/Equipment Loss or Shift, Non-Collision | 0 | 0.00% |
| Fell/Jumped From Motor Vehicle, Non-Collision | 0 | 0.00% |
| Thrown or Falling Object, Non-Collision | 0 | 0.00% |
| Other Non-Collision, Non-Collision | 0 | 0.00% |
| Pedestrian, Collision With Person, Motor Vehicle, or Non-Fixed | 1 | 0.22% |
| Pedalcycle, Collision With Person, Motor Vehicle, or Non-Fixed | 0 | 0.00% |
| Railway Vehicle (train, engine), Collision With Person, Motor Vehicle, or Non-Fixed | 0 | 0.00% |
| Animal, Collision With Person, Motor Vehicle, or Non-Fixed | 10 | 2.24% |
| Motor Vehicle in Transport, Collision With Person, Motor Vehicle, or Non-Fixed | 406 | 90.83% |
| Legally Parked Motor Vehicle, Collision With Person, Motor Vehicle, or Non-Fixed | 3 | 0.67% |
| Struck by Anything Set in Motion by Vehicle, Collision With Person, Motor Vehicle, or Non-Fixed | 0 | 0.00% |
| Work Zone / Maintenance Equipment, Collision With Person, Motor Vehicle, or Non-Fixed | 0 | 0.00% |
| Other Non-Fixed Object, Collision With Person, Motor Vehicle, or Non-Fixed | 1 | 0.22% |
| Impact Attenuator/Crash Cushion, Collision With Fixed Object | 0 | 0.00% |
| Bridge Overhead Structure, Collision With Fixed Object | 0 | 0.00% |
| Bridge Pier or Support, Collision With Fixed Object | 0 | 0.00% |
| Bridge Rail, Collision With Fixed Object | 0 | 0.00% |
| Cable Barrier, Collision With Fixed Object | 0 | 0.00% |
| Culvert, Collision With Fixed Object | 0 | 0.00% |
| Curb, Collision With Fixed Object | 2 | 0.45% |
| Ditch, Collision With Fixed Object | 0 | 0.00% |
| Embankment, Collision With Fixed Object | 0 | 0.00% |
| Guardrail Face, Collision With Fixed Object | 1 | 0.22% |
| Guardrail End, Collision With Fixed Object | 1 | 0.22% |
| | 0 | 0.00% |

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| | | |
|---|------------|-------|
| Concrete Traffic Barrier, Collision With Fixed Object | | |
| Other Traffic Barrier, Collision With Fixed Object | 0 | 0.00% |
| Tree (standing), Collision With Fixed Object | 0 | 0.00% |
| Utility Pole, Collision With Fixed Object | 0 | 0.00% |
| Light Support, Collision With Fixed Object | 0 | 0.00% |
| Traffic Sign Support, Collision With Fixed Object | 1 | 0.22% |
| Overhead Sign Support, Collision With Fixed Object | 0 | 0.00% |
| Traffic Signal Support, Collision With Fixed Object | 0 | 0.00% |
| Fence, Collision With Fixed Object | 0 | 0.00% |
| Mailbox, Collision With Fixed Object | 0 | 0.00% |
| Other Post, Pole or Support, Collision With Fixed Object | 3 | 0.67% |
| Other Fixed Object (wall, building, tunnel, etc.), Collision With Fixed Object | 2 | 0.45% |
| Illegally Parked Motor Vehicle, Collision With Person, Vehicle, Or Object Not Fixed | 0 | 0.00% |
| Stopped Motor Vehicle, Collision With Person, Vehicle, Or Object Not Fixed | 15 | 3.36% |
| Unknown, Collision With Fixed Object | 0 | 0.00% |
| Total | 436 | |

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| Primary Contributing Circumstance | | |
|--|--------------|--------------------|
| | # of Crashes | % of Total Crashes |
| Speeding | 2 | 0.45% |
| Failed to yield right of way | 39 | 8.72% |
| Passed Stop Sign | 1 | 0.22% |
| Disregard Traffic Signal | 30 | 6.71% |
| Wrong side or wrong way | 4 | 0.89% |
| Improper passing | 3 | 0.67% |
| Improper lane change | 44 | 9.84% |
| Following too close | 103 | 23.04% |
| Made improper turn | 8 | 1.79% |
| Driving under the influence | 5 | 1.12% |
| Driver inattention, distraction, or fatigue | 106 | 23.71% |
| Driving in a careless or reckless manner | 24 | 5.37% |
| Driving in an aggressive manner | 3 | 0.67% |
| Improper backing | 1 | 0.22% |
| Other improper driving | 3 | 0.67% |
| Mechanical defects | 3 | 0.67% |
| Animal in Roadway - Deer | 11 | 2.46% |
| Animal in Roadway - Other Animal | 2 | 0.45% |
| Other environmental circumstances - weather, glare | 3 | 0.67% |
| Roadway circumstances - debris, holes, work zone, | 1 | 0.22% |
| Other | 13 | 2.91% |
| Unknown | 37 | 8.28% |
| Total | 446 | |

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| Driver Action | | |
|--|--------------|--------------------|
| | # of Drivers | % of Total Crashes |
| No Contributing Action | 481 | 52.86% |
| Failed to yield right of way | 36 | 3.96% |
| Ran Red Light | 32 | 3.52% |
| Ran Stop Sign | 1 | 0.11% |
| Disregard other traffic sign | 2 | 0.22% |
| Disregard other road markings | 0 | 0.00% |
| Exceeded authorized speed limit | 0 | 0.00% |
| Driving too fast for conditions | 2 | 0.22% |
| Made an improper turn | 9 | 0.99% |
| Improper backing | 1 | 0.11% |
| Wrong side or wrong way | 3 | 0.33% |
| Followed too closely | 114 | 12.53% |
| Failure to keep in proper lane | 41 | 4.51% |
| Ran off roadway | 2 | 0.22% |
| Operating vehicle in erratic, reckless, careless, negligent or aggressive manner | 82 | 9.01% |
| Swerving or avoiding due to wind, slippery surface, vehicle, object, non-motorist in roadway, etc. | 0 | 0.00% |
| Over-correcting/over-steering | 2 | 0.22% |
| Improper Passing | 6 | 0.66% |
| Other Contributing Action | 52 | 5.71% |
| Unknown | 44 | 4.84% |
| Total | 910 | |

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| CTY | RD | MP | C-MP | DIR | COMP/HQ# | Date | Time | Day | Fat | Inj | AL | LC | WC | SC | FHE | PC | Class | MOI |
|------|--------|------|------|-----|------------|------------|-------|-----|-----|-----|----|----|------------|----|-----|----|-------|-----|
| 2013 | | | | | | | | | | | | | | | | | | |
| N | 110670 | 0 | 0.00 | 1 | 0113000841 | 01/04/2013 | 08:50 | 6 | 0 | 0 | N | 01 | 02 | 01 | 13 | 99 | 02 | 03 |
| N | 00004 | 1.89 | 8.04 | 2 | 0113003212 | 01/12/2013 | 09:40 | 7 | 0 | 0 | N | 01 | 02 | 02 | 13 | 11 | 02 | 01 |
| N | 00004 | 2.39 | 8.54 | 2 | 0113005189 | 01/19/2013 | 10:23 | 7 | 0 | 0 | N | 01 | 01 | 01 | 13 | 13 | 02 | 01 |
| N | 00004 | 2.21 | 8.36 | 2 | 0113008591 | 01/31/2013 | 14:11 | 5 | 0 | 0 | N | 01 | 01 | 01 | 13 | 02 | 02 | 03 |
| N | 00004 | 3.89 | 3.89 | 1 | 0113009337 | 02/02/2013 | 12:00 | 7 | 0 | 0 | N | 01 | 01 | 01 | 13 | 08 | 02 | 01 |
| N | 00004 | 4.25 | 4.25 | 1 | 0113011513 | 02/10/2013 | 17:33 | 1 | 0 | 0 | N | 04 | 01 - 01 | 01 | 34 | 11 | 02 | 00 |
| N | 00004 | 4.57 | 4.57 | 1 | 0113011554 | 02/10/2013 | 21:19 | 1 | 0 | 0 | N | 04 | 01 | 01 | 13 | 12 | 02 | 04 |
| N | 00004 | 4.17 | 4.17 | 1 | 0113012836 | 02/15/2013 | 12:53 | 6 | 0 | 0 | N | 01 | 01 | 01 | 13 | 08 | 02 | 01 |
| N | 00004 | 4.5 | 4.50 | 1 | 0113012970 | 02/15/2013 | 19:18 | 6 | 0 | 0 | N | 05 | 01 | 01 | 42 | 08 | 02 | 01 |
| N | 00004 | 3.86 | 3.86 | 1 | 0113014468 | 02/21/2013 | 15:02 | 5 | 0 | 0 | N | 01 | 01 | 01 | 13 | 07 | 02 | 04 |
| N | 00004 | 4.5 | 4.50 | 1 | 0113015510 | 02/25/2013 | 11:06 | 2 | 0 | 0 | N | 01 | 01 | 01 | 13 | 18 | 02 | 01 |
| N | 00004 | 3.82 | 3.82 | 1 | 0113015532 | 02/25/2013 | 12:49 | 2 | 0 | 0 | N | 01 | 01 | 01 | 13 | 08 | 02 | 01 |
| N | | | | | 0113017264 | 03/03/2013 | 14:35 | 1 | 0 | 1 | N | 01 | 01 | 01 | 13 | 08 | 03 | 01 |
| N | 00004 | 3.81 | 3.81 | 1 | 0113018633 | 03/08/2013 | 13:59 | 6 | 0 | 0 | N | 01 | 02 | 01 | 13 | 12 | 02 | 04 |
| N | 00004 | 2.05 | 8.20 | 2 | 0113019231 | 03/10/2013 | 12:32 | 1 | 0 | 0 | N | 01 | 01 | 01 | 13 | 08 | 02 | 01 |
| N | 00004 | 3.97 | 3.97 | 1 | 0113020229 | 03/14/2013 | 08:17 | 5 | 0 | 0 | N | 01 | 01 | 01 | 13 | 07 | 02 | 03 |
| N | 00004 | 1.89 | 8.04 | 2 | 0113020437 | 03/14/2013 | 21:51 | 5 | 0 | 0 | N | 04 | 01 | 01 | 13 | 11 | 02 | 01 |
| N | 00004 | 3.96 | 3.96 | 1 | 0113020879 | 03/16/2013 | 12:26 | 7 | 0 | 0 | N | 01 | 02 | 01 | 13 | 11 | 02 | 01 |
| N | 00004 | 4.57 | 4.57 | 1 | 0113021701 | 03/19/2013 | 12:13 | 3 | 0 | 0 | N | 01 | 01 | 01 | 13 | 88 | 02 | 01 |
| N | 00004 | 4.51 | 4.51 | 1 | 0113022856 | 03/23/2013 | 13:46 | 7 | 0 | 0 | N | 01 | 01 | 01 | 13 | 11 | 02 | 01 |
| N | 00004 | 2.15 | 8.30 | 2 | 0113023463 | 03/25/2013 | 17:19 | 2 | 0 | 0 | N | 03 | 06 | 02 | 24 | 02 | 02 | 00 |
| N | 00004 | 3.87 | 3.87 | 1 | 0113023601 | 03/26/2013 | 12:17 | 3 | 0 | 2 | N | 01 | 01 | 01 | 13 | 08 | 03 | 01 |
| N | 00004 | 1.89 | 8.04 | 2 | 0113025185 | 04/01/2013 | 11:03 | 2 | 0 | 0 | N | 01 | 01 | 01 | 13 | 02 | 02 | 03 |
| N | 00004 | 3.88 | 3.88 | 1 | 0113026720 | 04/06/2013 | 21:40 | 7 | 0 | 0 | N | 04 | 01 | 01 | 13 | 11 | 02 | 01 |
| N | 00004 | 4.11 | 4.11 | 1 | 0113028623 | 04/13/2013 | 13:10 | 7 | 0 | 0 | N | 01 | 01 | 01 | 13 | 11 | 02 | 01 |
| N | 00004 | 2.35 | 8.50 | 2 | 0113028718 | 04/13/2013 | 18:56 | 7 | 0 | 1 | N | 01 | 01 | 01 | 13 | 11 | 03 | 01 |
| N | 00004 | 4.19 | 4.19 | 1 | 0113029453 | 04/16/2013 | 13:43 | 3 | 0 | 0 | N | 01 | 01 | 01 | 13 | 11 | 02 | 01 |
| N | 00004 | 2.04 | 8.19 | 2 | 0113029981 | 04/18/2013 | 08:24 | 5 | 0 | 0 | N | 01 | 02 | 01 | 13 | 08 | 02 | 01 |
| N | 00004 | 4.45 | 4.45 | 1 | 0113032489 | 04/26/2013 | 18:41 | 6 | 0 | 0 | N | 01 | 01 | 01 | 13 | 07 | 02 | 04 |
| N | 00004 | 4.57 | 4.57 | 1 | 0113033237 | 04/29/2013 | 15:18 | 2 | 0 | 0 | N | 01 | 04 | 02 | 13 | 08 | 02 | 01 |
| N | 00004 | 2.08 | 8.23 | 2 | 0113033974 | 05/02/2013 | 08:29 | 5 | 0 | 0 | N | 01 | 01 | 01 | 13 | 11 | 02 | 01 |
| N | 00004 | 4.25 | 4.25 | 1 | 0113035277 | 05/06/2013 | 14:07 | 2 | 0 | 0 | N | 01 | 01 | 01 | 13 | 02 | 02 | 03 |
| N | 00004 | 2.16 | 8.31 | 2 | 0113035514 | 05/07/2013 | 12:35 | 3 | 0 | 0 | N | 01 | 02 | 01 | 13 | 08 | 02 | 01 |
| N | 00004 | 3.81 | 3.81 | 1 | 0113038133 | 05/16/2013 | 10:35 | 5 | 0 | 0 | N | 01 | 02 | 01 | 13 | 07 | 02 | 01 |
| N | 00004 | 2.03 | 8.18 | 2 | 0113039103 | 05/19/2013 | 16:41 | 1 | 0 | 0 | N | 01 | 04 | 02 | 13 | 04 | 02 | 03 |
| N | 00004 | 2.21 | 8.36 | 2 | 0113039145 | 05/19/2013 | 18:53 | 1 | 0 | 0 | N | 01 | 04 | 02 | 13 | 88 | 02 | 01 |
| N | 00004 | 3.81 | 3.81 | 1 | 0113042193 | 05/30/2013 | 12:04 | 5 | 0 | 0 | N | 01 | 01 | 01 | 13 | 08 | 02 | 01 |
| N | 110480 | 0.03 | 0.03 | 1 | 0113042316 | 05/30/2013 | 17:05 | 5 | 0 | 0 | N | 01 | 01 | 01 | 13 | 04 | 02 | 03 |
| N | 00004 | 3.8 | 3.80 | 1 | 0113044415 | 06/06/2013 | 08:45 | 5 | 0 | 0 | N | 01 | 01 | 01 | 13 | 11 | 02 | 01 |
| N | 110480 | 0.03 | 0.03 | 1 | 0113044740 | 06/07/2013 | 09:09 | 6 | 0 | 0 | N | 01 | 04 | 02 | 13 | 12 | 02 | 03 |

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| CTY | RD | MP | C-MP | DIR | COMP/HQ# | Date | Time | Day | Fat | Inj | AL | LC | WC | SC | FHE | PC | Class | MOI |
|-------------|--------|------|------|-----|------------|------------|-------|-----|-----|-----|----|----|------------|----|-----|----|-------|-----|
| 2013 | | | | | | | | | | | | | | | | | | |
| N | 00004 | 2.36 | 8.51 | 2 | 0113044742 | 06/07/2013 | 09:21 | 6 | 0 | 0 | N | 01 | 04 | 02 | 13 | 12 | 02 | 01 |
| N | 00004 | 2.35 | 8.50 | 2 | 0113046177 | 06/11/2013 | 17:02 | 3 | 0 | 0 | N | 01 | 01 | 01 | 42 | 99 | 02 | 01 |
| N | 00004 | 4.02 | 4.02 | 1 | 0113047313 | 06/15/2013 | 12:08 | 7 | 0 | 5 | N | 01 | 01 | 01 | 13 | 02 | 03 | 03 |
| N | 00004 | 4.09 | 4.09 | 1 | 0113054850 | 07/10/2013 | 11:45 | 4 | 0 | 0 | N | 01 | 02 | 01 | 13 | 12 | 02 | 01 |
| N | 00004 | 3.91 | 3.91 | 1 | 0113055226 | 07/11/2013 | 16:16 | 5 | 0 | 0 | N | 01 | 01 | 01 | 13 | 11 | 02 | 01 |
| N | | | | | 0113057903 | 07/20/2013 | 12:20 | 7 | 0 | 2 | N | 01 | 01 | 01 | 13 | 02 | 03 | 03 |
| N | | | | | 0113059297 | 07/25/2013 | 10:14 | 5 | 0 | 0 | N | 01 | 02 | 01 | 13 | 04 | 02 | 03 |
| N | 00212 | 0 | 0.00 | 3 | 0113060541 | 07/29/2013 | 10:18 | 2 | 0 | 0 | N | 01 | 01 | 01 | 13 | 08 | 02 | 01 |
| N | 00004 | 3.64 | 3.64 | 1 | 0113064191 | 08/10/2013 | 18:53 | 7 | 0 | 0 | N | 01 | 01 | 01 | 13 | 02 | 02 | 03 |
| N | | | | | 0113064654 | 08/12/2013 | 12:30 | 2 | 0 | 0 | N | 01 | 01 | 01 | 13 | 07 | 02 | 04 |
| N | 00004 | 2.35 | 8.50 | 2 | 0113065523 | 08/14/2013 | 23:31 | 4 | 0 | 0 | N | 04 | 01 | 01 | 13 | 08 | 02 | 01 |
| N | 00004 | 4.07 | 4.07 | 1 | 0113065927 | 08/16/2013 | 12:26 | 6 | 0 | 0 | N | 01 | 01 | 01 | 13 | 02 | 02 | 03 |
| N | 110480 | 0 | 0.00 | 2 | 0113066087 | 08/16/2013 | 20:20 | 6 | 0 | 0 | N | 04 | 01 | 01 | 13 | 99 | 02 | 01 |
| N | 00004 | 3.8 | 3.80 | 1 | 0113070145 | 08/30/2013 | 12:05 | 6 | 0 | 0 | N | 01 | 02 | 01 | 13 | 11 | 02 | 01 |
| N | 00004 | 3.5 | 3.50 | 1 | 0113071587 | 09/04/2013 | 13:07 | 4 | 0 | 0 | N | 01 | 01 | 01 | 13 | 11 | 02 | 01 |
| N | 00004 | 3.86 | 3.86 | 1 | 0113073233 | 09/10/2013 | 09:06 | 3 | 0 | 0 | N | 01 | 02 | 01 | 13 | 04 | 02 | 03 |
| N | 00004 | 4.57 | 4.57 | 1 | 0113074495 | 09/14/2013 | 21:07 | 7 | 0 | 0 | N | 04 | 01 | 01 | 13 | 09 | 02 | 03 |
| N | 00004 | 2.5 | 8.65 | 2 | 0113075060 | 09/17/2013 | 08:14 | 3 | 0 | 0 | N | 01 | 01 | 01 | 13 | 08 | 02 | 01 |
| N | 00004 | 3.6 | 3.60 | 1 | 0113077499 | 09/25/2013 | 15:17 | 4 | 0 | 0 | N | 01 | 01 | 01 | 13 | 08 | 02 | 01 |
| N | 00004 | 3.99 | 3.99 | 1 | 0113078284 | 09/28/2013 | 09:25 | 7 | 0 | 0 | N | 01 | 02 | 01 | 13 | 02 | 02 | 03 |
| N | 00004 | 3.85 | 3.85 | 1 | 0113083690 | 10/17/2013 | 14:52 | 5 | 0 | 0 | N | 01 | 02 | 01 | 13 | 11 | 02 | 01 |
| N | 00004 | 1.6 | 7.75 | 2 | 0113085389 | 10/23/2013 | 16:53 | 4 | 0 | 0 | N | 01 | 01 | 01 | 27 | 88 | 02 | 00 |
| N | 00004 | 3.86 | 3.86 | 1 | 0113085815 | 10/25/2013 | 09:03 | 6 | 0 | 1 | N | 01 | 01 | 01 | 13 | 08 | 03 | 01 |
| N | 00004 | 2.18 | 8.33 | 2 | 0113087031 | 10/29/2013 | 15:07 | 3 | 0 | 0 | N | 01 | 01 | 01 | 13 | 99 | 02 | 03 |
| N | 00004 | 1.92 | 8.07 | 2 | 0113088839 | 11/04/2013 | 17:46 | 2 | 0 | 0 | N | 04 | 01 | 01 | 13 | 11 | 02 | 01 |
| N | 00004 | 2.21 | 8.36 | 2 | 0113089657 | 11/07/2013 | 17:50 | 5 | 0 | 0 | N | 05 | 01 | 02 | 13 | 02 | 02 | 04 |
| N | 00004 | 4.06 | 4.06 | 1 | 0113089684 | 11/07/2013 | 19:11 | 5 | 0 | 0 | N | 04 | 01 | 01 | 13 | 11 | 02 | 01 |
| N | 00004 | 2.08 | 8.23 | 2 | 0113089968 | 11/08/2013 | 19:54 | 6 | 0 | 0 | N | 05 | 01 | 01 | 13 | 08 | 02 | 01 |
| N | 00004 | 3.75 | 3.75 | 1 | 0113090116 | 11/09/2013 | 12:02 | 7 | 0 | 2 | N | 01 | 01 | 01 | 13 | 08 | 03 | 01 |
| N | 00004 | 2.35 | 8.50 | 2 | 0113090428 | 11/10/2013 | 18:27 | 1 | 0 | 0 | N | 04 | 01 | 01 | 13 | 07 | 02 | 04 |
| N | 00004 | 1.89 | 8.04 | 2 | 0113092866 | 11/19/2013 | 17:24 | 3 | 0 | 0 | N | 03 | 01 | 01 | 13 | 08 | 02 | 01 |
| N | 00004 | 3.63 | 3.63 | 1 | 0113093723 | 11/22/2013 | 16:52 | 6 | 0 | 0 | N | 04 | 02 - 04 | 02 | 13 | 08 | 02 | 01 |
| N | | | | | 0113094399 | 11/25/2013 | 09:44 | 2 | 0 | 0 | N | 01 | 01 | 01 | 13 | 04 | 02 | 03 |
| N | | | | | 0113094789 | 11/26/2013 | 16:52 | 3 | 0 | 0 | N | 04 | 04 | 02 | 13 | 08 | 02 | 01 |
| N | 00004 | 2.64 | 8.79 | 2 | 0113097681 | 12/07/2013 | 00:28 | 7 | 0 | 2 | Y | 05 | 04 | 02 | 13 | 10 | 03 | 01 |
| N | 00227 | 1.41 | 1.41 | 3 | 0113098339 | 12/08/2013 | 12:43 | 1 | 0 | 0 | N | 01 | 02 - 07 | 03 | 13 | 19 | 02 | 03 |
| N | 00004 | 3.87 | 3.87 | 1 | 0113102373 | 12/22/2013 | 12:08 | 1 | 0 | 0 | N | 01 | 04 | 02 | 13 | 04 | 02 | 03 |
| N | 00004 | 2.03 | 8.18 | 2 | 0113103387 | 12/26/2013 | 10:58 | 5 | 0 | 0 | N | 01 | 01 | 01 | 13 | 11 | 02 | 01 |
| N | 00004 | 3.8 | 3.80 | 1 | 0113103668 | 12/27/2013 | 12:15 | 6 | 0 | 0 | N | 01 | 01 | 01 | 42 | 11 | 02 | 03 |
| 2014 | | | | | | | | | | | | | | | | | | |
| N | 00004 | 2.28 | 8.43 | 2 | 0114001141 | 01/04/2014 | 17:36 | 7 | 0 | 0 | N | 04 | 01 | 01 | 13 | 07 | 02 | 04 |

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| CTY | RD | MP | C-MP | DIR | COMP/HQ# | Date | Time | Day | Fat | Inj | AL | LC | WC | SC | FHE | PC | Class | MOI | |
|------|-------|------|------|-----|------------|------------|-------|-----|-----|-----|----|----|----|----|-----|----|-------|-----|--|
| 2014 | | | | | | | | | | | | | | | | | | | |
| N | 00004 | 2.07 | 8.22 | 2 | 0114002348 | 01/09/2014 | 07:54 | 5 | 0 | 0 | N | 01 | 01 | 01 | 13 | 11 | 02 | 01 | |
| N | 00004 | 2.04 | 8.19 | 2 | 0114002352 | 01/09/2014 | 08:11 | 5 | 0 | 0 | N | 01 | 01 | 01 | 13 | 08 | 02 | 01 | |
| N | 00004 | 2.65 | 8.80 | 2 | 0114006134 | 01/22/2014 | 21:16 | 4 | 0 | 0 | N | 04 | 01 | 01 | 13 | 99 | 02 | 01 | |
| N | 00004 | 2.66 | 8.81 | 2 | 0114006344 | 01/23/2014 | 16:46 | 5 | 0 | 0 | N | 03 | 01 | 01 | 13 | 08 | 02 | 01 | |
| N | 00004 | 4.58 | 4.58 | 1 | 0114006368 | 01/23/2014 | 18:01 | 5 | 0 | 0 | N | 04 | 01 | 04 | 13 | 02 | 02 | 03 | |
| N | 00004 | 3.5 | 3.50 | 1 | 0114007087 | 01/26/2014 | 15:15 | 1 | 0 | 1 | N | 01 | 06 | 01 | 13 | 02 | 03 | 00 | |
| N | 00004 | 3.79 | 3.79 | 1 | 0114007193 | 01/27/2014 | 07:08 | 2 | 0 | 0 | N | 01 | 02 | 02 | 13 | 16 | 02 | 01 | |
| N | 00004 | 3.93 | 3.93 | 1 | 0114009627 | 02/05/2014 | 15:43 | 4 | 0 | 0 | N | 01 | 02 | 02 | 13 | 05 | 02 | 05 | |
| N | 00004 | 2.05 | 8.20 | 2 | 0114013421 | 02/19/2014 | 18:32 | 4 | 0 | 0 | N | 06 | 01 | 01 | 13 | 07 | 02 | 03 | |
| N | 00004 | 2.01 | 8.16 | 2 | 0114013884 | 02/21/2014 | 11:44 | 6 | 0 | 0 | N | 01 | 02 | 01 | 13 | 11 | 02 | 01 | |
| N | 00004 | 3.97 | 3.97 | 1 | 0114015228 | 02/26/2014 | 12:11 | 4 | 0 | 0 | N | 01 | 01 | 02 | 13 | 04 | 02 | 03 | |
| N | 00004 | 1.57 | 7.72 | 2 | 0114015965 | 02/28/2014 | 20:19 | 6 | 0 | 0 | N | 04 | 01 | 01 | 13 | 11 | 02 | 04 | |
| N | 00004 | 3.79 | 3.79 | 1 | 0114016943 | 03/04/2014 | 15:51 | 3 | 0 | 0 | N | 01 | 01 | 01 | 13 | 11 | 02 | 03 | |
| N | 00004 | 3.8 | 3.80 | 1 | 0114017074 | 03/05/2014 | 08:36 | 4 | 0 | 0 | N | 01 | 01 | 01 | 13 | 11 | 02 | 01 | |
| N | 00004 | 4.25 | 4.25 | 1 | 0114017347 | 03/06/2014 | 14:28 | 5 | 0 | 0 | N | 01 | 01 | 01 | 13 | 88 | 02 | 01 | |
| N | 00004 | 2.11 | 8.26 | 2 | 0114017536 | 03/07/2014 | 10:01 | 6 | 0 | 0 | N | 01 | 02 | 01 | 13 | 02 | 02 | 01 | |
| N | 00004 | 3.95 | 3.95 | 1 | 0114019000 | 03/12/2014 | 19:24 | 4 | 0 | 0 | N | 04 | 04 | 02 | 13 | 08 | 02 | 01 | |
| N | 00004 | 3.96 | 3.96 | 1 | 0114019497 | 03/14/2014 | 14:56 | 6 | 0 | 0 | N | 01 | 01 | 01 | 13 | 04 | 02 | 03 | |
| N | 00004 | 4.24 | 4.24 | 1 | 0114020747 | 03/19/2014 | 10:52 | 4 | 0 | 0 | N | 01 | 01 | 01 | 13 | 11 | 02 | 01 | |
| N | 00004 | 4.25 | 4.25 | 1 | 0114025320 | 04/05/2014 | 13:27 | 7 | 0 | 2 | N | 01 | 01 | 01 | 42 | 11 | 03 | 01 | |
| N | 00004 | 1.56 | 7.71 | 2 | 0114025842 | 04/07/2014 | 14:59 | 2 | 0 | 0 | N | 01 | 04 | 02 | 13 | 99 | 02 | 01 | |
| N | 00004 | 3.74 | 3.74 | 1 | 0114028098 | 04/15/2014 | 08:17 | 3 | 0 | 0 | N | 01 | 02 | 01 | 13 | 02 | 02 | 04 | |
| N | 00004 | 3.64 | 3.64 | 1 | 0114028699 | 04/17/2014 | 10:36 | 5 | 0 | 2 | N | 01 | 01 | 01 | 13 | 99 | 03 | 03 | |
| N | 00004 | 2.2 | 8.35 | 2 | 0114028770 | 04/17/2014 | 13:56 | 5 | 0 | 0 | N | 01 | 01 | 01 | 13 | 02 | 02 | 03 | |
| N | 00004 | 3.64 | 3.64 | 1 | 0114030140 | 04/22/2014 | 18:13 | 3 | 0 | 0 | N | 01 | 01 | 01 | 13 | 04 | 02 | 03 | |
| N | 00004 | 3.87 | 3.87 | 1 | 0114030931 | 04/25/2014 | 17:21 | 6 | 0 | 0 | N | 01 | 02 | 01 | 13 | 04 | 02 | 03 | |
| N | 00004 | 3.97 | 3.97 | 1 | 0114031775 | 04/28/2014 | 19:08 | 2 | 0 | 1 | N | 01 | 04 | 02 | 13 | 04 | 03 | 03 | |
| N | | | | | 0114032330 | 04/30/2014 | 20:39 | 4 | 0 | 1 | N | 04 | 04 | 02 | 13 | 04 | 03 | 03 | |
| N | 00004 | 2.28 | 8.43 | 2 | 0114032840 | 05/02/2014 | 13:17 | 6 | 0 | 0 | N | 01 | 01 | 01 | 13 | 07 | 02 | 04 | |
| N | 00004 | 4.32 | 4.32 | 1 | 0114034939 | 05/09/2014 | 21:25 | 6 | 0 | 0 | N | 05 | 01 | 01 | 13 | 11 | 02 | 01 | |
| N | 00004 | 3.94 | 3.94 | 1 | 0114035410 | 05/11/2014 | 11:58 | 1 | 0 | 0 | N | 01 | 01 | 01 | 13 | 11 | 02 | 01 | |
| N | 00004 | 4.1 | 4.10 | 1 | 0114035443 | 05/11/2014 | 12:56 | 1 | 0 | 0 | N | 01 | 01 | 01 | 13 | 07 | 02 | 04 | |
| N | 00004 | 3.63 | 3.63 | 1 | 0114039081 | 05/23/2014 | 13:41 | 6 | 0 | 0 | N | 01 | 01 | 01 | 13 | 08 | 02 | 01 | |
| N | 00004 | 3.66 | 3.66 | 1 | 0114039356 | 05/24/2014 | 11:39 | 7 | 0 | 0 | N | 01 | 02 | 01 | 13 | 11 | 02 | 01 | |
| N | 00004 | 1.56 | 7.71 | 2 | 0114042505 | 06/04/2014 | 13:22 | 4 | 0 | 0 | N | 01 | 01 | 01 | 13 | 11 | 02 | 01 | |
| N | 00004 | 4.05 | 4.05 | 1 | 0114045700 | 06/14/2014 | 13:30 | 7 | 0 | 0 | N | 01 | 01 | 01 | 13 | 08 | 02 | 01 | |
| N | 00004 | 3.69 | 3.69 | 1 | 0114046843 | 06/18/2014 | 11:27 | 4 | 0 | 1 | N | 01 | 01 | 01 | 13 | 08 | 03 | 01 | |
| N | 00004 | 2.03 | 8.18 | 2 | 0114050439 | 06/30/2014 | 08:51 | 2 | 0 | 1 | N | 01 | 01 | 01 | 13 | 11 | 03 | 01 | |
| N | 00227 | 1.41 | 1.41 | 3 | 0114050633 | 06/30/2014 | 18:36 | 2 | 0 | 1 | N | 01 | 01 | 01 | 40 | 88 | 03 | 00 | |
| N | 00004 | 4.38 | 4.38 | 1 | 0114053492 | 07/09/2014 | 16:29 | 4 | 0 | 0 | N | 01 | 02 | 01 | 13 | 18 | 02 | 01 | |

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| CTY | RD | MP | C-MP | DIR | COMP/HQ# | Date | Time | Day | Fat | Inj | AL | LC | WC | SC | FHE | PC | Class | MOI | |
|------|-------|------|------|-----|------------|------------|-------|-----|-----|-----|----|----|----|----|-----|----|-------|-----|--|
| 2014 | | | | | | | | | | | | | | | | | | | |
| N | 00004 | 2.12 | 8.27 | 2 | 0114058866 | 07/27/2014 | 14:13 | 1 | 0 | 4 | N | 01 | 01 | 01 | 13 | 08 | 03 | 01 | |
| N | 00004 | 1.88 | 8.03 | 2 | 0114058884 | 07/27/2014 | 15:09 | 1 | 0 | 0 | N | 01 | 01 | 01 | 13 | 99 | 02 | 01 | |
| N | 00004 | 2.24 | 8.39 | 2 | 0114060643 | 08/02/2014 | 12:24 | 7 | 0 | 0 | N | 01 | 01 | 01 | 13 | 99 | 02 | 03 | |
| N | 00004 | 1.56 | 7.71 | 2 | 0114061240 | 08/04/2014 | 15:56 | 2 | 0 | 0 | N | 01 | 01 | 01 | 13 | 08 | 02 | 01 | |
| N | 00004 | 2.51 | 8.66 | 2 | 0114062770 | 08/09/2014 | 14:35 | 7 | 0 | 0 | N | 01 | 01 | 01 | 13 | 11 | 02 | 01 | |
| N | 00004 | 3.63 | 3.63 | 1 | 0114062966 | 08/10/2014 | 07:57 | 1 | 0 | 0 | N | 01 | 01 | 01 | 13 | 08 | 02 | 01 | |
| N | 00004 | 2.35 | 8.50 | 2 | 0114064669 | 08/15/2014 | 16:59 | 6 | 0 | 0 | N | 01 | 01 | 01 | 13 | 12 | 02 | 01 | |
| N | 00004 | 3.8 | 3.80 | 1 | 0114065252 | 08/17/2014 | 14:22 | 1 | 0 | 0 | N | 01 | 01 | 01 | 13 | 12 | 02 | 03 | |
| N | 00004 | 4.58 | 4.58 | 1 | 0114067138 | 08/23/2014 | 12:30 | 7 | 0 | 0 | N | 01 | 01 | 01 | 13 | 02 | 02 | 03 | |
| N | | | | | 0114068240 | 08/27/2014 | 08:07 | 4 | 1 | 0 | N | 01 | 01 | 01 | 09 | 21 | 04 | 00 | |
| N | 00004 | 2.14 | 8.29 | 2 | 0114069768 | 09/01/2014 | 15:27 | 2 | 0 | 2 | N | 01 | 02 | 01 | 13 | 99 | 03 | 01 | |
| N | 00004 | 4.13 | 4.13 | 1 | 0114071316 | 09/06/2014 | 15:41 | 7 | 0 | 0 | N | 01 | 02 | 01 | 13 | 11 | 02 | 01 | |
| N | 00004 | 3.51 | 3.51 | 1 | 0114071598 | 09/07/2014 | 15:02 | 1 | 0 | 0 | N | 01 | 01 | 01 | 39 | 11 | 02 | 00 | |
| N | 00004 | 2.02 | 8.17 | 2 | 0114073243 | 09/13/2014 | 14:13 | 7 | 0 | 0 | N | 01 | 04 | 02 | 13 | 11 | 02 | 01 | |
| N | 00004 | 4.52 | 4.52 | 1 | 0114073482 | 09/14/2014 | 13:21 | 1 | 0 | 0 | N | 01 | 01 | 01 | 13 | 11 | 02 | 01 | |
| N | 00004 | 1.72 | 7.87 | 2 | 0114074124 | 09/16/2014 | 17:09 | 3 | 0 | 1 | N | 01 | 01 | 01 | 13 | 11 | 03 | 01 | |
| N | 00004 | 3.81 | 3.81 | 1 | 0114074967 | 09/19/2014 | 17:37 | 6 | 0 | 0 | N | 01 | 01 | 01 | 13 | 11 | 02 | 01 | |
| N | 00004 | 3.79 | 3.79 | 1 | 0114074990 | 09/19/2014 | 18:19 | 6 | 0 | 0 | N | 01 | 01 | 01 | 13 | 07 | 02 | 03 | |
| N | 00004 | 3.97 | 3.97 | 1 | 0114076703 | 09/25/2014 | 22:12 | 5 | 0 | 1 | N | 04 | 01 | 01 | 13 | 04 | 03 | 03 | |
| N | 00004 | 2.1 | 8.25 | 2 | 0114077011 | 09/26/2014 | 19:06 | 6 | 0 | 0 | N | 04 | 01 | 01 | 13 | 09 | 02 | 03 | |
| N | 00004 | 2.35 | 8.50 | 2 | 0114077839 | 09/29/2014 | 16:42 | 2 | 0 | 0 | N | 01 | 04 | 02 | 13 | 08 | 02 | 01 | |
| N | 00212 | 0.01 | 0.01 | 3 | 0114079235 | 10/04/2014 | 11:17 | 7 | 0 | 0 | N | 01 | 01 | 01 | 13 | 02 | 02 | 03 | |
| N | 00004 | 1.85 | 8.00 | 2 | 0114080662 | 10/09/2014 | 17:12 | 5 | 0 | 0 | N | 01 | 01 | 01 | 13 | 20 | 02 | 00 | |
| N | 00004 | 2.28 | 8.43 | 2 | 0114082428 | 10/15/2014 | 19:37 | 4 | 0 | 0 | N | 04 | 04 | 02 | 13 | 08 | 02 | 01 | |
| N | 00004 | 3.85 | 3.85 | 1 | 0114084711 | 10/23/2014 | 12:04 | 5 | 0 | 0 | N | 01 | 01 | 01 | 13 | 08 | 02 | 01 | |
| N | 00004 | 2.18 | 8.33 | 2 | 0114085713 | 10/26/2014 | 20:48 | 1 | 0 | 0 | N | 05 | 01 | 01 | 13 | 07 | 02 | 04 | |
| N | 00004 | 1.74 | 7.89 | 2 | 0114087881 | 11/03/2014 | 06:38 | 2 | 0 | 0 | N | 02 | 01 | 01 | 12 | 17 | 02 | 00 | |
| N | 00004 | 4.2 | 4.20 | 1 | 0114088310 | 11/04/2014 | 15:48 | 3 | 0 | 0 | N | 01 | 01 | 01 | 13 | 08 | 02 | 01 | |
| N | 00004 | 2.21 | 8.36 | 2 | 0114089161 | 11/07/2014 | 14:29 | 6 | 0 | 0 | N | 01 | 01 | 01 | 13 | 11 | 02 | 04 | |
| N | 00004 | 4.57 | 4.57 | 1 | 0114090341 | 11/11/2014 | 18:28 | 3 | 0 | 0 | N | 04 | 01 | 01 | 13 | 07 | 02 | 03 | |
| N | 00004 | 4.13 | 4.13 | 1 | 0114090564 | 11/12/2014 | 13:01 | 4 | 0 | 0 | N | 01 | 01 | 01 | 13 | 08 | 02 | 01 | |
| N | | | | | 0114090603 | 11/12/2014 | 14:30 | 4 | 0 | 0 | N | 01 | 01 | 01 | 13 | 99 | 02 | 01 | |
| N | 00004 | 4.42 | 4.42 | 1 | 0114091191 | 11/14/2014 | 12:14 | 6 | 0 | 0 | N | 01 | 02 | 01 | 13 | 99 | 02 | 04 | |
| N | 00004 | 3.98 | 3.98 | 1 | 0114092114 | 11/17/2014 | 17:26 | 2 | 0 | 0 | N | 04 | 04 | 02 | 13 | 04 | 02 | 03 | |
| N | 00004 | 4.18 | 4.18 | 1 | 0114092131 | 11/17/2014 | 18:05 | 2 | 0 | 0 | N | 05 | 04 | 02 | 13 | 08 | 02 | 01 | |
| N | 00004 | 3.5 | 3.50 | 1 | 0114093844 | 11/23/2014 | 16:00 | 1 | 0 | 0 | N | 01 | 01 | 01 | 13 | 99 | 02 | 01 | |
| N | 00004 | 2.32 | 8.47 | 2 | 0114094494 | 11/25/2014 | 20:39 | 3 | 0 | 1 | N | 04 | 01 | 01 | 13 | 11 | 03 | 03 | |
| N | 00004 | 3.69 | 3.69 | 1 | 0114096232 | 12/02/2014 | 12:23 | 3 | 0 | 0 | N | 01 | 02 | 02 | 13 | 04 | 02 | 03 | |
| N | 00004 | 4.57 | 4.57 | 1 | 0114096698 | 12/04/2014 | 06:03 | 5 | 0 | 1 | N | 01 | 01 | 01 | 40 | 88 | 03 | 00 | |
| N | 00004 | 1.88 | 8.03 | 2 | 0114097230 | 12/05/2014 | 17:29 | 6 | 0 | 0 | N | 04 | 04 | 02 | 13 | 99 | 02 | 01 | |

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| CTY | RD | MP | C-MP | DIR | COMP/HQ# | Date | Time | Day | Fat | Inj | AL | LC | WC | SC | FHE | PC | Class | MOI |
|-------------|-------|------|------|-----|------------|------------|-------|-----|-----|-----|----|----|----|----|-----|----|-------|-----|
| 2014 | | | | | | | | | | | | | | | | | | |
| N | 00212 | 4.56 | 9.07 | 1 | 0114098471 | 12/10/2014 | 12:50 | 4 | 0 | 1 | N | 01 | 01 | 01 | 13 | 04 | 03 | 03 |
| N | 00004 | 1.57 | 7.72 | 2 | 0114098614 | 12/10/2014 | 20:47 | 4 | 0 | 0 | N | 04 | 01 | 01 | 12 | 17 | 02 | 00 |
| N | 00004 | 3.79 | 3.79 | 1 | 0114098877 | 12/11/2014 | 17:54 | 5 | 0 | 0 | N | 04 | 02 | 02 | 13 | 08 | 02 | 01 |
| N | 00004 | 4.09 | 4.09 | 1 | 0114101265 | 12/20/2014 | 00:26 | 7 | 0 | 0 | N | 04 | 01 | 01 | 39 | 07 | 02 | 03 |
| N | 00004 | 2.31 | 8.46 | 2 | 0114101920 | 12/22/2014 | 12:10 | 2 | 0 | 0 | N | 01 | 01 | 01 | 13 | 08 | 02 | 01 |
| N | 00004 | 4.58 | 4.58 | 1 | 0114102305 | 12/23/2014 | 14:46 | 3 | 0 | 0 | N | 01 | 04 | 02 | 13 | 11 | 02 | 03 |
| N | 00004 | 4.01 | 4.01 | 1 | 0114102335 | 12/23/2014 | 15:24 | 3 | 0 | 0 | Y | 01 | 04 | 02 | 13 | 12 | 02 | 01 |

| | | | | | | | | | | | | | | | | | | |
|-------------|-------|------|------|---|------------|------------|-------|---|---|---|---|----|------------|----|----|----|----|----|
| 2015 | | | | | | | | | | | | | | | | | | |
| N | 00004 | 2.22 | 8.37 | 2 | 0115001091 | 01/05/2015 | 15:11 | 2 | 0 | 0 | N | 01 | 01 | 01 | 13 | 07 | 02 | 03 |
| N | 00004 | 2.16 | 8.31 | 2 | 0115001702 | 01/07/2015 | 15:21 | 4 | 0 | 0 | N | 01 | 01 | 03 | 13 | 08 | 02 | 01 |
| N | 00004 | 4.57 | 4.57 | 1 | 0115001769 | 01/07/2015 | 18:20 | 4 | 0 | 0 | N | 04 | 01 | 01 | 13 | 07 | 02 | 04 |
| N | 00004 | 2.13 | 8.28 | 2 | 0115001811 | 01/08/2015 | 18:11 | 5 | 0 | 0 | N | 04 | 01 | 01 | 13 | 08 | 02 | 01 |
| N | 00004 | 2.1 | 8.25 | 2 | 0115003849 | 01/15/2015 | 15:21 | 5 | 0 | 0 | N | 01 | 01 | 01 | 13 | 11 | 02 | 04 |
| N | 00004 | 4.58 | 4.58 | 1 | 0115005386 | 01/20/2015 | 18:36 | 3 | 0 | 0 | N | 04 | 01 | 01 | 13 | 11 | 02 | 01 |
| N | | | | | 0115006260 | 01/24/2015 | 03:42 | 7 | 0 | 0 | Y | 05 | 04 - 06 | 02 | 13 | 12 | 02 | 00 |
| N | 00004 | 1.59 | 7.74 | 2 | 0115010151 | 02/07/2015 | 19:38 | 7 | 0 | 2 | Y | 04 | 01 | 01 | 13 | 10 | 03 | 01 |
| N | | | | | 0115010634 | 02/09/2015 | 22:02 | 2 | 0 | 0 | N | 04 | 02 | 02 | 13 | 14 | 02 | 01 |
| N | 00004 | 2.52 | 8.67 | 2 | 0115012897 | 02/17/2015 | 18:19 | 3 | 0 | 1 | N | 04 | 01 | 01 | 13 | 02 | 03 | 03 |
| N | 00004 | 2.21 | 8.36 | 2 | 0115013750 | 02/20/2015 | 17:09 | 6 | 0 | 0 | N | 01 | 01 | 01 | 13 | 11 | 02 | 03 |
| N | 00004 | 2.03 | 8.18 | 2 | 0115014229 | 02/21/2015 | 21:57 | 7 | 0 | 0 | N | 04 | 05 | 03 | 13 | 08 | 02 | 01 |
| N | 00004 | 2.36 | 8.51 | 2 | 0115014665 | 02/23/2015 | 13:08 | 2 | 0 | 0 | N | 01 | 01 | 01 | 13 | 02 | 02 | 03 |
| N | 00004 | 2.64 | 8.79 | 2 | 0115015119 | 02/25/2015 | 03:52 | 4 | 0 | 0 | N | 04 | 01 | 01 | 13 | 11 | 02 | 01 |
| N | 00004 | 3.87 | 3.87 | 1 | 0115015593 | 02/26/2015 | 18:37 | 5 | 0 | 1 | N | 04 | 01 | 01 | 17 | 88 | 03 | 00 |
| N | 00004 | 3.5 | 3.50 | 1 | 0115016016 | 02/28/2015 | 13:02 | 7 | 0 | 0 | N | 01 | 01 | 01 | 13 | 08 | 02 | 01 |
| N | 00004 | 1.77 | 7.92 | 2 | 0115016285 | 03/01/2015 | 13:09 | 1 | 0 | 0 | N | 01 | 05 | 04 | 28 | 19 | 02 | 00 |
| N | 00004 | 2.16 | 8.31 | 2 | 0115017728 | 03/06/2015 | 11:46 | 6 | 0 | 0 | N | 01 | 01 | 02 | 13 | 12 | 02 | 01 |
| N | 00004 | 4.04 | 4.04 | 1 | 0115017780 | 03/06/2015 | 14:42 | 6 | 0 | 0 | N | 01 | 01 | 02 | 13 | 02 | 02 | 04 |
| N | 00004 | 3.78 | 3.78 | 1 | 0115017865 | 03/06/2015 | 18:34 | 6 | 0 | 0 | N | 04 | 02 | 01 | 13 | 11 | 02 | 01 |
| N | 00004 | 3.79 | 3.79 | 1 | 0115020080 | 03/14/2015 | 19:40 | 7 | 0 | 0 | N | 05 | 04 | 02 | 13 | 11 | 02 | 01 |
| N | 00004 | 3.64 | 3.64 | 1 | 0115022651 | 03/24/2015 | 16:47 | 3 | 0 | 0 | N | 01 | 01 | 01 | 13 | 04 | 02 | 03 |
| N | 00004 | 1.87 | 8.02 | 2 | 0115025990 | 04/06/2015 | 16:20 | 2 | 0 | 0 | N | 01 | 01 | 01 | 42 | 11 | 02 | 01 |
| N | 00004 | 3.63 | 3.63 | 1 | 0115026600 | 04/09/2015 | 11:46 | 5 | 0 | 0 | N | 01 | 02 | 01 | 13 | 08 | 02 | 01 |
| N | 00004 | 3.95 | 3.95 | 1 | 0115026655 | 04/09/2015 | 15:34 | 5 | 0 | 0 | N | 01 | 02 | 01 | 13 | 11 | 02 | 01 |
| N | 00004 | 2.21 | 8.36 | 2 | 0115026913 | 04/10/2015 | 14:36 | 6 | 0 | 0 | N | 01 | 01 | 01 | 13 | 88 | 02 | 03 |
| N | 00004 | 3.79 | 3.79 | 1 | 0115026965 | 04/10/2015 | 17:02 | 6 | 0 | 0 | N | 01 | 02 | 01 | 13 | 08 | 02 | 01 |
| N | 00004 | 4.55 | 4.55 | 1 | 0115029011 | 04/18/2015 | 11:29 | 7 | 0 | 0 | N | 01 | 01 | 01 | 13 | 08 | 02 | 01 |
| N | 00004 | 2.3 | 8.45 | 2 | 0115033628 | 05/05/2015 | 12:45 | 3 | 0 | 0 | N | 01 | 01 | 01 | 13 | 07 | 02 | 04 |
| N | 00004 | 3.82 | 3.82 | 1 | 0115036134 | 05/13/2015 | 15:00 | 4 | 0 | 0 | N | 01 | 01 | 01 | 13 | 99 | 02 | 04 |
| N | 00004 | 2.14 | 8.29 | 2 | 0115041297 | 05/30/2015 | 22:10 | 7 | 0 | 0 | N | 04 | 01 | 01 | 13 | 01 | 02 | 04 |
| N | 00004 | 3.91 | 3.91 | 1 | 0115041791 | 06/01/2015 | 14:49 | 2 | 0 | 0 | N | 01 | 01 | 01 | 13 | 09 | 02 | 04 |
| N | 00004 | 3.5 | 3.50 | 1 | 0115042066 | 06/02/2015 | 10:40 | 3 | 0 | 1 | N | 01 | 02 | 01 | 42 | 11 | 03 | 01 |

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| CTY | RD | MP | C-MP | DIR | COMP/HQ# | Date | Time | Day | Fat | Inj | AL | LC | WC | SC | FHE | PC | Class | MOI | |
|------|-------|------|------|-----|------------|------------|-------|-----|-----|-----|----|----|----|----|-----|----|-------|-----|--|
| 2015 | | | | | | | | | | | | | | | | | | | |
| N | 00004 | 3.98 | 3.98 | 1 | 0115042879 | 06/04/2015 | 15:16 | 5 | 0 | 0 | N | 01 | 02 | 01 | 13 | 07 | 02 | 04 | |
| N | 00004 | 3.96 | 3.96 | 1 | 0115043375 | 06/06/2015 | 10:49 | 7 | 0 | 0 | N | 01 | 01 | 01 | 13 | 99 | 02 | 04 | |
| N | 00004 | 3.97 | 3.97 | 1 | 0115044272 | 06/09/2015 | 13:44 | 3 | 0 | 0 | N | 01 | 01 | 01 | 13 | 99 | 02 | 04 | |
| N | 00004 | 1.78 | 7.93 | 2 | 0115045542 | 06/13/2015 | 10:49 | 7 | 0 | 0 | N | 01 | 01 | 01 | 13 | 11 | 02 | 01 | |
| N | 00004 | 2.03 | 8.18 | 2 | 0115045610 | 06/13/2015 | 14:20 | 7 | 0 | 0 | N | 01 | 02 | 01 | 13 | 08 | 02 | 01 | |
| N | 00004 | 3.84 | 3.84 | 1 | 0115046653 | 06/16/2015 | 12:06 | 3 | 0 | 0 | N | 01 | 02 | 01 | 13 | 11 | 02 | 01 | |
| N | 00004 | 3.75 | 3.75 | 1 | 0115047243 | 06/18/2015 | 13:09 | 5 | 0 | 0 | N | 01 | 01 | 01 | 13 | 08 | 02 | 01 | |
| N | 00004 | 2.35 | 8.50 | 2 | 0115047866 | 06/20/2015 | 12:46 | 7 | 0 | 0 | N | 01 | 01 | 01 | 13 | 08 | 02 | 01 | |
| N | 00004 | 2.36 | 8.51 | 2 | 0115048970 | 06/23/2015 | 20:21 | 3 | 0 | 0 | N | 05 | 04 | 02 | 13 | 88 | 02 | 03 | |
| N | 00004 | 1.89 | 8.04 | 2 | 0115051088 | 06/30/2015 | 13:44 | 3 | 0 | 0 | N | 01 | 01 | 01 | 13 | 11 | 02 | 01 | |
| N | 00004 | 3.86 | 3.86 | 1 | 0115051318 | 07/01/2015 | 08:03 | 4 | 0 | 1 | N | 01 | 01 | 01 | 13 | 11 | 03 | 03 | |
| N | 00004 | 3.97 | 3.97 | 1 | 0115055747 | 07/15/2015 | 16:41 | 4 | 0 | 0 | N | 04 | 04 | 02 | 13 | 08 | 02 | 01 | |
| N | 00004 | 3.91 | 3.91 | 1 | 0115056101 | 07/16/2015 | 22:20 | 5 | 0 | 0 | Y | 01 | 01 | 01 | 13 | 02 | 02 | 03 | |
| N | 00004 | 2.17 | 8.32 | 2 | 0115059112 | 07/26/2015 | 14:09 | 1 | 0 | 0 | N | 01 | 01 | 01 | 13 | 02 | 02 | 03 | |
| N | 00004 | 3.79 | 3.79 | 1 | 0115061616 | 08/03/2015 | 16:36 | 2 | 0 | 0 | N | 01 | 01 | 01 | 13 | 11 | 02 | 01 | |
| N | 00004 | 3.81 | 3.81 | 1 | 0115064334 | 08/12/2015 | 12:15 | 4 | 0 | 3 | N | 01 | 01 | 01 | 13 | 88 | 03 | 01 | |
| N | 00004 | 3.95 | 3.95 | 1 | 0115067150 | 08/21/2015 | 10:24 | 6 | 0 | 0 | N | 01 | 01 | 01 | 13 | 07 | 02 | 03 | |
| N | 00004 | 1.57 | 7.72 | 2 | 0115067202 | 08/21/2015 | 12:51 | 6 | 0 | 0 | N | 01 | 01 | 01 | 13 | 08 | 02 | 01 | |
| N | 00004 | 3.95 | 3.95 | 1 | 0115071192 | 09/03/2015 | 05:59 | 5 | 0 | 0 | N | 02 | 01 | 01 | 13 | 13 | 02 | 01 | |
| N | 00004 | 2.18 | 8.33 | 2 | 0115072707 | 09/07/2015 | 19:55 | 2 | 0 | 0 | N | 04 | 01 | 01 | 13 | 08 | 02 | 01 | |
| N | 00004 | 3.61 | 3.61 | 1 | 0115073023 | 09/08/2015 | 18:04 | 3 | 0 | 0 | N | 01 | 01 | 01 | 13 | 08 | 02 | 01 | |
| N | 00004 | 3.64 | 3.64 | 1 | 0115073178 | 09/09/2015 | 07:42 | 4 | 0 | 0 | N | 01 | 01 | 01 | 13 | 07 | 02 | 01 | |
| N | 00004 | 4.25 | 4.25 | 1 | 0115073312 | 09/09/2015 | 15:07 | 4 | 0 | 0 | N | 01 | 02 | 01 | 13 | 11 | 02 | 04 | |
| N | 00004 | 2.35 | 8.50 | 2 | 0115073623 | 09/10/2015 | 13:50 | 5 | 0 | 2 | N | 01 | 04 | 02 | 13 | 11 | 03 | 01 | |
| N | 00004 | 4.57 | 4.57 | 1 | 0115075031 | 09/15/2015 | 10:05 | 3 | 0 | 1 | N | 01 | 01 | 01 | 13 | 08 | 03 | 01 | |
| N | 00004 | 3.99 | 3.99 | 1 | 0115075683 | 09/17/2015 | 09:45 | 5 | 0 | 0 | N | 01 | 01 | 01 | 42 | 08 | 02 | 01 | |
| N | 00004 | 3.86 | 3.86 | 1 | 0115077317 | 09/22/2015 | 15:03 | 3 | 0 | 0 | N | 01 | 01 | 01 | 13 | 08 | 02 | 01 | |
| N | 00004 | 3.81 | 3.81 | 1 | 0115077367 | 09/22/2015 | 16:55 | 3 | 0 | 0 | N | 01 | 02 | 01 | 13 | 11 | 02 | 01 | |
| N | 00004 | 1.57 | 7.72 | 2 | 0115079512 | 09/29/2015 | 18:35 | 3 | 0 | 0 | N | 03 | 02 | 01 | 13 | 04 | 02 | 03 | |
| N | 00004 | 4.25 | 4.25 | 1 | 0115080320 | 10/02/2015 | 11:34 | 6 | 0 | 0 | N | 01 | 04 | 02 | 13 | 11 | 02 | 01 | |
| N | 00226 | 0.33 | 0.33 | 3 | 0115080368 | 10/02/2015 | 14:31 | 6 | 0 | 0 | N | 01 | 04 | 02 | 13 | 08 | 02 | 01 | |
| N | 00004 | 2.29 | 8.44 | 2 | 0115080442 | 10/02/2015 | 17:09 | 6 | 0 | 0 | N | 01 | 04 | 02 | 13 | 07 | 02 | 03 | |
| N | 00004 | 2.66 | 8.81 | 2 | 0115081433 | 10/05/2015 | 19:23 | 2 | 0 | 0 | N | 05 | 01 | 01 | 42 | 11 | 02 | 01 | |
| N | 00212 | 4.59 | 9.10 | 4 | 0115083789 | 10/13/2015 | 13:28 | 3 | 0 | 0 | N | 01 | 02 | 01 | 13 | 07 | 02 | 04 | |
| N | 00004 | 2.03 | 8.18 | 2 | 0115084113 | 10/14/2015 | 11:59 | 4 | 0 | 0 | N | 01 | 02 | 01 | 13 | 88 | 02 | 03 | |
| N | 00004 | 3.8 | 3.80 | 1 | 0115084525 | 10/15/2015 | 16:47 | 5 | 0 | 1 | N | 01 | 01 | 01 | 13 | 11 | 03 | 01 | |
| N | 00004 | 4 | 4.00 | 1 | 0115084761 | 10/16/2015 | 13:00 | 6 | 0 | 0 | N | 01 | 01 | 01 | 13 | 07 | 02 | 04 | |
| N | 00004 | 2.17 | 8.32 | 2 | 0115088443 | 10/28/2015 | 18:20 | 4 | 0 | 0 | N | 04 | 04 | 06 | 13 | 08 | 02 | 01 | |
| N | 00004 | 2.29 | 8.44 | 2 | 0115088627 | 10/29/2015 | 08:38 | 5 | 0 | 0 | N | 01 | 01 | 02 | 13 | 04 | 02 | 03 | |
| N | 00004 | 3.81 | 3.81 | 1 | 0115088710 | 10/29/2015 | 13:01 | 5 | 0 | 2 | N | 01 | 01 | 01 | 13 | 10 | 03 | 03 | |

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| CTY | RD | MP | C-MP | DIR | COMP/HQ# | Date | Time | Day | Fat | Inj | AL | LC | WC | SC | FHE | PC | Class | MOI |
|-------------|-------|------|------|-----|------------|------------|-------|-----|-----|-----|----|----|----|----|-----|----|-------|-----|
| 2015 | | | | | | | | | | | | | | | | | | |
| N | 00004 | 1.57 | 7.72 | 2 | 0115089446 | 10/31/2015 | 17:36 | 7 | 0 | 1 | N | 04 | 01 | 01 | 13 | 10 | 03 | 03 |
| N | 00004 | 1.85 | 8.00 | 2 | 0115091994 | 11/08/2015 | 15:21 | 1 | 0 | 0 | N | 01 | 01 | 01 | 13 | 02 | 02 | 04 |
| N | 00212 | 4.59 | 9.10 | 4 | 0115092033 | 11/08/2015 | 18:01 | 1 | 0 | 0 | N | 04 | 01 | 01 | 13 | 04 | 02 | 03 |
| N | 00004 | 1.88 | 8.03 | 2 | 0115095929 | 11/20/2015 | 18:28 | 6 | 0 | 1 | N | 04 | 01 | 01 | 13 | 08 | 03 | 01 |
| N | 00004 | 2.16 | 8.31 | 2 | 0115096520 | 11/22/2015 | 21:37 | 1 | 0 | 0 | N | 04 | 01 | 01 | 13 | 01 | 02 | 01 |
| N | 00004 | 3.88 | 3.88 | 1 | 0115102738 | 12/12/2015 | 12:49 | 7 | 0 | 0 | N | 01 | 01 | 01 | 13 | 08 | 02 | 01 |
| N | 00004 | 2.52 | 8.67 | 2 | 0115103021 | 12/13/2015 | 14:49 | 1 | 0 | 0 | N | 01 | 01 | 01 | 13 | 09 | 02 | 02 |
| N | 00004 | 1.83 | 7.98 | 2 | 0115103366 | 12/14/2015 | 17:57 | 2 | 0 | 0 | N | 04 | 04 | 02 | 13 | 11 | 02 | 04 |
| N | 00004 | 1.89 | 8.04 | 2 | 0115104126 | 12/16/2015 | 19:45 | 4 | 0 | 0 | N | 04 | 01 | 01 | 13 | 11 | 02 | 01 |
| N | 00004 | 4.25 | 4.25 | 1 | 0115104133 | 12/16/2015 | 19:56 | 4 | 0 | 0 | N | 04 | 01 | 01 | 13 | 11 | 02 | 00 |
| N | 00004 | 2.03 | 8.18 | 2 | 0115105416 | 12/20/2015 | 17:48 | 1 | 0 | 0 | N | 04 | 01 | 01 | 13 | 08 | 02 | 01 |
| N | 00004 | 3.67 | 3.67 | 1 | 0115105599 | 12/21/2015 | 11:45 | 2 | 0 | 0 | N | 01 | 01 | 01 | 13 | 07 | 02 | 04 |
| N | 00004 | 4.34 | 4.34 | 1 | 0115105958 | 12/22/2015 | 14:06 | 3 | 0 | 0 | N | 01 | 04 | 02 | 13 | 11 | 02 | 01 |
| N | 00004 | 1.88 | 8.03 | 2 | 0115106246 | 12/23/2015 | 10:55 | 4 | 0 | 0 | N | 01 | 04 | 02 | 13 | 06 | 02 | 04 |
| N | 00004 | 2.51 | 8.66 | 2 | 0115106326 | 12/23/2015 | 15:12 | 4 | 0 | 0 | N | 01 | 04 | 02 | 13 | 08 | 02 | 01 |
| N | 00004 | 4.5 | 4.50 | 1 | 0115106610 | 12/24/2015 | 10:30 | 5 | 0 | 0 | N | 01 | 04 | 02 | 13 | 99 | 02 | 01 |
| N | 00004 | 1.57 | 7.72 | 2 | 0115106648 | 12/24/2015 | 12:13 | 5 | 0 | 0 | N | 01 | 01 | 02 | 13 | 99 | 02 | 01 |

| | | | | | | | | | | | | | | | | | | |
|-------------|--------|------|------|---|------------|------------|-------|---|---|---|---|----|----|----|----|----|----|----|
| 2016 | | | | | | | | | | | | | | | | | | |
| N | 00004 | 4.45 | 4.45 | 1 | 0116000402 | 01/02/2016 | 13:00 | 7 | 0 | 0 | N | 01 | 01 | 01 | 13 | 08 | 02 | 01 |
| N | 00004 | 3.88 | 3.88 | 1 | 0116001884 | 01/07/2016 | 12:05 | 5 | 0 | 1 | N | 01 | 01 | 01 | 13 | 05 | 03 | 02 |
| N | 00004 | 2.05 | 8.20 | 2 | 0116002515 | 01/09/2016 | 18:08 | 7 | 0 | 0 | N | 04 | 01 | 01 | 13 | 08 | 02 | 01 |
| N | 00004 | 2.36 | 8.51 | 2 | 0116004932 | 01/17/2016 | 15:14 | 1 | 0 | 0 | N | 01 | 06 | 02 | 13 | 12 | 02 | 01 |
| N | 00004 | 4.09 | 4.09 | 1 | 0116006566 | 01/22/2016 | 18:11 | 6 | 0 | 0 | N | 05 | 06 | 02 | 13 | 11 | 02 | 01 |
| N | 00004 | 4.25 | 4.25 | 1 | 0116009595 | 02/01/2016 | 21:31 | 2 | 0 | 0 | N | 04 | 04 | 02 | 13 | 04 | 02 | 03 |
| N | 00004 | 2.34 | 8.49 | 2 | 0116010438 | 02/04/2016 | 09:33 | 5 | 0 | 0 | N | 01 | 01 | 01 | 13 | 07 | 02 | 04 |
| N | 00004 | 4.25 | 4.25 | 1 | 0116010522 | 02/04/2016 | 15:05 | 5 | 0 | 0 | N | 01 | 01 | 01 | 13 | 11 | 02 | 01 |
| N | 00004 | 4.38 | 4.38 | 1 | 0116013415 | 02/13/2016 | 13:13 | 7 | 0 | 0 | N | 01 | 01 | 01 | 13 | 11 | 02 | 01 |
| N | 00004 | 4.39 | 4.39 | 1 | 0116013434 | 02/13/2016 | 14:00 | 7 | 0 | 0 | N | 01 | 01 | 01 | 13 | 08 | 02 | 01 |
| N | 110480 | 0.03 | 0.03 | 1 | 0116013759 | 02/14/2016 | 14:53 | 1 | 0 | 0 | N | 01 | 01 | 01 | 13 | 04 | 02 | 03 |
| N | 00212 | 4.59 | 9.10 | 4 | 0116014512 | 02/16/2016 | 18:19 | 3 | 0 | 0 | N | 04 | 01 | 01 | 13 | 08 | 02 | 01 |
| N | 00004 | 3.64 | 3.64 | 1 | 0116015031 | 02/18/2016 | 12:06 | 5 | 0 | 0 | N | 01 | 01 | 01 | 13 | 04 | 02 | 03 |
| N | 00004 | 3.96 | 3.96 | 1 | 0116016745 | 02/23/2016 | 15:52 | 3 | 0 | 0 | N | 01 | 04 | 02 | 13 | 08 | 02 | 01 |
| N | 111000 | 0 | 0.00 | 1 | 0116016794 | 02/23/2016 | 17:23 | 3 | 0 | 0 | N | 05 | 04 | 02 | 13 | 99 | 02 | 01 |
| N | 00004 | 4.25 | 4.25 | 1 | 0116016829 | 02/23/2016 | 18:57 | 3 | 0 | 0 | N | 05 | 04 | 02 | 13 | 19 | 02 | 03 |
| N | 00004 | 4.43 | 4.43 | 1 | 0116018606 | 02/29/2016 | 06:46 | 2 | 0 | 2 | N | 01 | 01 | 01 | 13 | 11 | 03 | 02 |
| N | 00004 | 3.87 | 3.87 | 1 | 0116025001 | 03/19/2016 | 12:08 | 7 | 0 | 0 | N | 01 | 02 | 01 | 13 | 08 | 02 | 01 |
| N | 00004 | 2.02 | 8.17 | 2 | 0116026579 | 03/24/2016 | 11:59 | 5 | 0 | 0 | N | 01 | 01 | 01 | 13 | 11 | 02 | 01 |
| N | 00004 | 2.21 | 8.36 | 2 | 0116026592 | 03/24/2016 | 12:29 | 5 | 0 | 0 | N | 01 | 01 | 01 | 13 | 02 | 02 | 03 |
| N | | | | | 0116028533 | 03/30/2016 | 12:25 | 4 | 0 | 0 | N | 01 | 01 | 01 | 13 | 99 | 02 | 99 |
| N | 00004 | 3.64 | 3.64 | 1 | 0116029077 | 04/01/2016 | 15:23 | 6 | 0 | 0 | N | 01 | 01 | 01 | 13 | 02 | 02 | 03 |
| N | 00212 | 4.59 | 9.10 | 4 | 0116029225 | 04/01/2016 | 23:42 | 6 | 0 | 0 | Y | 04 | 01 | 01 | 13 | 10 | 02 | 01 |

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| CTY | RD | MP | C-MP | DIR | COMP/HQ# | Date | Time | Day | Fat | Inj | AL | LC | WC | SC | FHE | PC | Class | MOI | |
|------|--------|------|------|-----|------------|------------|-------|-----|-----|-----|----|----|----|----|-----|----|-------|-----|--|
| 2016 | | | | | | | | | | | | | | | | | | | |
| N | 00004 | 3.82 | 3.82 | 1 | 0116029939 | 04/04/2016 | 06:53 | 2 | 0 | 0 | N | 01 | 01 | 01 | 13 | 06 | 02 | 03 | |
| N | 00004 | 3.97 | 3.97 | 1 | 0116030091 | 04/04/2016 | 13:06 | 2 | 0 | 0 | N | 01 | 02 | 01 | 42 | 99 | 02 | 01 | |
| N | 00004 | 2.04 | 8.19 | 2 | 0116034021 | 04/17/2016 | 13:37 | 1 | 0 | 0 | N | 01 | 01 | 01 | 13 | 99 | 02 | 03 | |
| N | 00004 | 3.86 | 3.86 | 1 | 0116035227 | 04/21/2016 | 12:17 | 5 | 0 | 0 | N | 01 | 02 | 01 | 13 | 11 | 02 | 01 | |
| N | 00004 | 3.97 | 3.97 | 1 | 0116035366 | 04/21/2016 | 20:26 | 5 | 0 | 0 | N | 04 | 01 | 01 | 13 | 11 | 02 | 01 | |
| N | 00004 | 3.84 | 3.84 | 1 | 0116040159 | 05/07/2016 | 11:29 | 7 | 0 | 0 | N | 01 | 02 | 02 | 13 | 02 | 02 | 03 | |
| N | 00004 | 2.33 | 8.48 | 2 | 0116042036 | 05/13/2016 | 09:18 | 6 | 0 | 0 | N | 01 | 04 | 02 | 13 | 11 | 02 | 01 | |
| N | | | | | 0116042681 | 05/15/2016 | 09:30 | 1 | 0 | 0 | N | 01 | 02 | 01 | 13 | 11 | 02 | 88 | |
| N | 00004 | 1.56 | 7.71 | 2 | 0116046535 | 05/25/2016 | 12:05 | 4 | 0 | 0 | N | 01 | 02 | 01 | 13 | 99 | 02 | 01 | |
| N | 00004 | 1.75 | 7.90 | 2 | 0116046578 | 05/25/2016 | 13:34 | 4 | 0 | 0 | N | 01 | 01 | 01 | 42 | 11 | 02 | 01 | |
| N | 110480 | 0.03 | 0.03 | 1 | 0116046673 | 05/25/2016 | 16:46 | 4 | 0 | 0 | N | 01 | 01 | 01 | 13 | 11 | 02 | 03 | |
| N | | | | | 0116048003 | 05/28/2016 | 15:44 | 7 | 0 | 0 | Y | 01 | 01 | 01 | 13 | 13 | 02 | 00 | |
| N | 00004 | 2.29 | 8.44 | 2 | 0116050892 | 06/04/2016 | 16:31 | 7 | 0 | 0 | N | 01 | 01 | 01 | 13 | 07 | 02 | 03 | |
| N | 00004 | 1.64 | 7.79 | 2 | 0116052504 | 06/08/2016 | 13:50 | 4 | 0 | 0 | N | 01 | 02 | 01 | 13 | 99 | 02 | 01 | |
| N | 00004 | 3.64 | 3.64 | 1 | 0116056613 | 06/18/2016 | 09:53 | 7 | 0 | 0 | N | 01 | 01 | 01 | 13 | 16 | 02 | 01 | |
| N | 00004 | 3.94 | 3.94 | 1 | 0116058119 | 06/22/2016 | 07:12 | 4 | 0 | 0 | N | 01 | 01 | 01 | 13 | 08 | 02 | 01 | |
| N | 00004 | 3.64 | 3.64 | 1 | 0116058152 | 06/22/2016 | 09:11 | 4 | 0 | 1 | N | 01 | 01 | 01 | 13 | 12 | 03 | 01 | |
| N | 00004 | 2.41 | 8.56 | 2 | 0116059550 | 06/25/2016 | 16:10 | 7 | 0 | 0 | N | 01 | 01 | 01 | 13 | 08 | 02 | 01 | |
| N | 00004 | 3.51 | 3.51 | 1 | 0116060166 | 06/27/2016 | 10:38 | 2 | 0 | 0 | N | 01 | 01 | 01 | 13 | 15 | 02 | 03 | |
| N | 00004 | 4.57 | 4.57 | 1 | 0116064266 | 07/07/2016 | 21:30 | 5 | 0 | 0 | N | 04 | 01 | 01 | 13 | 99 | 02 | 01 | |
| N | 00227 | 1.41 | 1.41 | 3 | 0116066382 | 07/13/2016 | 15:32 | 4 | 0 | 1 | N | 01 | 04 | 02 | 13 | 04 | 03 | 03 | |
| N | 00004 | 2.66 | 8.81 | 2 | 0116071311 | 07/18/2016 | 14:30 | 2 | 0 | 0 | N | 01 | 01 | 01 | 13 | 02 | 02 | 03 | |
| N | 00004 | 3.95 | 3.95 | 1 | 0116076276 | 08/08/2016 | 14:54 | 2 | 0 | 0 | N | 01 | 01 | 01 | 13 | 07 | 02 | 04 | |
| N | 00004 | 3.64 | 3.64 | 1 | 0116076835 | 08/09/2016 | 22:47 | 3 | 0 | 0 | N | 04 | 01 | 02 | 13 | 04 | 02 | 03 | |
| N | 00004 | 3.78 | 3.78 | 1 | 0116079138 | 08/15/2016 | 14:56 | 2 | 0 | 0 | N | 01 | 01 | 01 | 13 | 11 | 02 | 01 | |
| N | 00004 | 4.58 | 4.58 | 1 | 0116080091 | 08/17/2016 | 17:00 | 4 | 0 | 0 | N | 01 | 01 | 01 | 13 | 09 | 02 | 03 | |
| N | 00004 | 3.87 | 3.87 | 1 | 0116081721 | 08/21/2016 | 18:19 | 1 | 0 | 1 | Y | 01 | 04 | 02 | 43 | 05 | 03 | 00 | |
| N | 00004 | 2.28 | 8.43 | 2 | 0116085682 | 08/31/2016 | 15:49 | 4 | 0 | 0 | N | 01 | 01 | 01 | 13 | 04 | 02 | 03 | |
| N | 00004 | 3.5 | 3.50 | 1 | 0116090864 | 09/13/2016 | 17:52 | 3 | 0 | 0 | N | 01 | 01 | 01 | 13 | 08 | 02 | 01 | |
| N | 00004 | 3.99 | 3.99 | 1 | 0116091951 | 09/16/2016 | 00:00 | 6 | 0 | 0 | N | 01 | 01 | 01 | 13 | 02 | 02 | 01 | |
| N | 00004 | 3.93 | 3.93 | 1 | 0116092059 | 09/16/2016 | 14:49 | 6 | 0 | 0 | N | 01 | 01 | 01 | 13 | 02 | 02 | 03 | |
| N | | | | | 0116093426 | 09/17/2016 | 16:30 | 7 | 0 | 0 | N | 01 | 01 | 01 | 13 | 99 | 02 | 99 | |
| N | 00004 | 3.87 | 3.87 | 1 | 0116094309 | 09/22/2016 | 17:58 | 5 | 0 | 0 | N | 01 | 01 | 01 | 13 | 08 | 02 | 01 | |
| N | 00004 | 3.95 | 3.95 | 1 | 0116094582 | 09/23/2016 | 13:01 | 6 | 0 | 0 | N | 01 | 01 | 01 | 13 | 08 | 02 | 01 | |
| N | 00004 | 4.25 | 4.25 | 1 | 0116094993 | 09/24/2016 | 13:57 | 7 | 0 | 0 | N | 01 | 02 | 01 | 13 | 11 | 02 | 01 | |
| N | | | | | 0116095381 | 09/25/2016 | 15:41 | 1 | 0 | 0 | N | 01 | 01 | 01 | 14 | 99 | 02 | 99 | |
| N | 00004 | 2.21 | 8.36 | 2 | 0116095938 | 09/27/2016 | 09:58 | 3 | 0 | 0 | N | 01 | 02 | 01 | 13 | 08 | 02 | 01 | |
| N | 00004 | 3.87 | 3.87 | 1 | 0116096628 | 09/29/2016 | 08:12 | 5 | 0 | 0 | N | 04 | 04 | 02 | 13 | 04 | 02 | 03 | |
| N | 00004 | 3.78 | 3.78 | 1 | 0116099397 | 10/06/2016 | 15:42 | 5 | 0 | 0 | N | 01 | 01 | 01 | 13 | 07 | 02 | 03 | |
| N | 00004 | 3.96 | 3.96 | 1 | 0116101189 | 10/11/2016 | 14:12 | 3 | 0 | 0 | N | 01 | 01 | 01 | 13 | 09 | 02 | 04 | |

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| CTY | RD | MP | C-MP | DIR | COMP/HQ# | Date | Time | Day | Fat | Inj | AL | LC | WC | SC | FHE | PC | Class | MOI |
|-----|----|----|------|-----|----------|------|------|-----|-----|-----|----|----|----|----|-----|----|-------|-----|
|-----|----|----|------|-----|----------|------|------|-----|-----|-----|----|----|----|----|-----|----|-------|-----|

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|---|-------|------|------|---|------------|------------|-------|---|---|---|---|----|----|----|----|----|----|----|
| N | | | | | 0116102824 | 10/14/2016 | 17:42 | 6 | 0 | 0 | N | 01 | 01 | 01 | 14 | 99 | 02 | 99 |
| N | 00004 | 2.42 | 8.57 | 2 | 0116103645 | 10/18/2016 | 07:33 | 3 | 0 | 1 | N | 01 | 01 | 01 | 24 | 07 | 03 | 00 |
| N | 00004 | 2.22 | 8.37 | 2 | 0116104407 | 10/20/2016 | 09:21 | 5 | 0 | 0 | N | 01 | 01 | 01 | 13 | 11 | 02 | 03 |
| N | 00004 | 4.07 | 4.07 | 1 | 0116106050 | 10/24/2016 | 17:25 | 2 | 0 | 0 | N | 01 | 01 | 01 | 13 | 07 | 02 | 04 |
| N | 00004 | 2.51 | 8.66 | 2 | 0116106899 | 10/26/2016 | 22:43 | 4 | 0 | 0 | N | 04 | 01 | 01 | 42 | 08 | 02 | 01 |
| N | 00004 | 2.26 | 8.41 | 2 | 0116109369 | 11/02/2016 | 16:26 | 4 | 0 | 0 | N | 01 | 01 | 01 | 13 | 02 | 02 | 01 |
| N | 00004 | 3.81 | 3.81 | 1 | 0116109405 | 11/02/2016 | 00:00 | 4 | 0 | 0 | N | 01 | 01 | 01 | 13 | 15 | 02 | 04 |
| N | 00004 | 2.66 | 8.81 | 2 | 0116109676 | 11/03/2016 | 13:19 | 5 | 0 | 0 | N | 01 | 04 | 02 | 13 | 02 | 02 | 03 |
| N | 00004 | 2.66 | 8.81 | 2 | 0116109687 | 11/03/2016 | 13:58 | 5 | 0 | 0 | N | 01 | 04 | 02 | 13 | 07 | 02 | 04 |
| N | 00004 | 3.5 | 3.50 | 1 | 0116110021 | 11/04/2016 | 13:33 | 6 | 0 | 0 | N | 01 | 01 | 01 | 13 | 07 | 02 | 04 |
| N | 00004 | 2.03 | 8.18 | 2 | 0116111759 | 11/09/2016 | 08:10 | 4 | 0 | 0 | N | 01 | 04 | 02 | 13 | 08 | 02 | 01 |
| N | 00226 | 0.33 | 0.33 | 3 | 0116112312 | 11/10/2016 | 15:12 | 5 | 0 | 0 | N | 01 | 01 | 01 | 13 | 08 | 02 | 01 |
| N | 00004 | 1.87 | 8.02 | 2 | 0116112394 | 11/10/2016 | 19:07 | 5 | 0 | 0 | N | 04 | 01 | 01 | 12 | 17 | 02 | 00 |
| N | 00004 | 3.9 | 3.90 | 1 | 0116112587 | 11/11/2016 | 11:46 | 6 | 0 | 0 | N | 01 | 01 | 01 | 13 | 11 | 02 | 01 |
| N | 00004 | 1.87 | 8.02 | 2 | 0116114695 | 11/17/2016 | 12:20 | 5 | 0 | 0 | N | 01 | 01 | 01 | 13 | 11 | 02 | 04 |
| N | 00004 | 2.32 | 8.47 | 2 | 0116116023 | 11/20/2016 | 15:23 | 1 | 0 | 0 | N | 01 | 01 | 01 | 13 | 08 | 02 | 01 |
| N | 00004 | 2.37 | 8.52 | 2 | 0116116302 | 11/21/2016 | 13:14 | 2 | 0 | 0 | N | 01 | 01 | 01 | 13 | 08 | 02 | 01 |
| N | 00004 | 3.5 | 3.50 | 1 | 0116116473 | 11/21/2016 | 17:54 | 2 | 0 | 0 | N | 04 | 01 | 01 | 13 | 08 | 02 | 01 |
| N | 00004 | 1.88 | 8.03 | 2 | 0116116767 | 11/22/2016 | 15:39 | 3 | 0 | 0 | N | 01 | 01 | 01 | 13 | 11 | 02 | 01 |
| N | 00004 | 4.47 | 4.47 | 1 | 0116118272 | 11/26/2016 | 22:03 | 7 | 0 | 0 | N | 04 | 01 | 01 | 12 | 17 | 02 | 00 |
| N | 00004 | 2.29 | 8.44 | 2 | 0116119150 | 11/29/2016 | 17:19 | 3 | 0 | 0 | N | 04 | 04 | 02 | 13 | 06 | 02 | 04 |
| N | 00004 | 1.88 | 8.03 | 2 | 0116122684 | 12/09/2016 | 13:09 | 6 | 0 | 0 | N | 01 | 01 | 01 | 13 | 08 | 02 | 01 |
| N | 00004 | 4.54 | 4.54 | 1 | 0116122778 | 12/09/2016 | 17:28 | 6 | 0 | 0 | N | 04 | 01 | 01 | 12 | 17 | 02 | 00 |
| N | 00004 | 2.2 | 8.35 | 2 | 0116125006 | 12/15/2016 | 00:00 | 5 | 0 | 0 | N | 04 | 01 | 01 | 13 | 07 | 02 | 04 |
| N | 00004 | 3.76 | 3.76 | 1 | 0116125181 | 12/16/2016 | 12:47 | 6 | 0 | 0 | N | 01 | 01 | 01 | 13 | 08 | 02 | 01 |
| N | 00004 | 4.54 | 4.54 | 1 | 0116127968 | 12/23/2016 | 12:16 | 6 | 0 | 0 | N | 01 | 01 | 01 | 13 | 08 | 02 | 01 |
| N | 00004 | 1.86 | 8.01 | 2 | 0116128202 | 12/23/2016 | 21:09 | 6 | 0 | 0 | N | 04 | 01 | 01 | 12 | 17 | 02 | 00 |
| N | 00212 | 4.59 | 9.10 | 4 | 0116129256 | 12/27/2016 | 13:34 | 3 | 0 | 0 | N | 01 | 02 | 02 | 13 | 12 | 02 | 01 |
| N | 00004 | 3.51 | 3.51 | | 3216040171 | 04/30/2016 | 11:00 | 7 | 0 | 0 | N | 01 | 02 | 01 | 13 | 02 | 02 | 03 |

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| | | | | | | | | | | | | | | | | | | |
|---|-------|------|------|---|------------|------------|-------|---|---|---|---|----|----|----|----|----|----|----|
| N | 00004 | 3.64 | 3.64 | 1 | 0117001495 | 01/05/2017 | 14:14 | 5 | 0 | 0 | N | 01 | 02 | 01 | 13 | 12 | 02 | 01 |
| N | 00004 | 4.25 | 4.25 | 1 | 0117001800 | 01/06/2017 | 11:24 | 6 | 0 | 0 | N | 01 | 01 | 02 | 13 | 11 | 02 | 01 |
| N | 00004 | 1.56 | 7.71 | 2 | 0117010318 | 01/30/2017 | 14:41 | 2 | 0 | 0 | N | 01 | 02 | 01 | 13 | 08 | 02 | 01 |
| N | 00004 | 2.11 | 8.26 | 2 | 0117011671 | 02/03/2017 | 15:43 | 6 | 0 | 0 | N | 01 | 02 | 01 | 13 | 88 | 02 | 01 |
| N | 00004 | 3.57 | 3.57 | 1 | 0117013622 | 02/09/2017 | 06:24 | 5 | 0 | 0 | N | 01 | 06 | 03 | 13 | 12 | 02 | 00 |
| N | 00004 | 2.34 | 8.49 | 2 | 0117014245 | 02/11/2017 | 23:49 | 7 | 0 | 0 | N | 04 | 01 | 01 | 13 | 11 | 02 | 01 |
| N | 00004 | 2.34 | 8.49 | 2 | 0117016436 | 02/16/2017 | 21:14 | 5 | 0 | 0 | N | 04 | 01 | 01 | 13 | 11 | 02 | 03 |
| N | 00004 | 2.1 | 8.25 | 2 | 0117017026 | 02/18/2017 | 13:57 | 7 | 0 | 0 | N | 01 | 01 | 01 | 13 | 11 | 02 | 01 |
| N | 00004 | 3.64 | 3.64 | 1 | 0117019650 | 02/25/2017 | 13:47 | 7 | 0 | 0 | N | 01 | 01 | 01 | 13 | 11 | 02 | 01 |
| N | 00212 | 0 | 0.00 | 3 | 0117020301 | 02/27/2017 | 13:27 | 2 | 0 | 0 | N | 01 | 02 | 01 | 13 | 11 | 02 | 01 |
| N | 00004 | 4.46 | 4.46 | 1 | 0117020545 | 02/28/2017 | 08:21 | 3 | 0 | 0 | N | 01 | 01 | 01 | 42 | 11 | 02 | 01 |

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| CTY | RD | MP | C-MP | DIR | COMP/HQ# | Date | Time | Day | Fat | Inj | AL | LC | WC | SC | FHE | PC | Class | MOI | |
|------|--------|------|------|-----|------------|------------|-------|-----|-----|-----|----|----|----|----|-----|----|-------|-----|--|
| 2017 | | | | | | | | | | | | | | | | | | | |
| N | 00004 | 2.36 | 8.51 | 2 | 0117021002 | 03/01/2017 | 11:10 | 4 | 0 | 0 | N | 01 | 01 | 01 | 13 | 16 | 02 | 03 | |
| N | | | | | 0117022307 | 03/04/2017 | 15:05 | 7 | 0 | 0 | N | 01 | 01 | 01 | 13 | 99 | 02 | 99 | |
| N | 00004 | 1.89 | 8.04 | 2 | 0117024069 | 03/09/2017 | 12:43 | 5 | 0 | 0 | N | 01 | 02 | 01 | 13 | 08 | 02 | 01 | |
| N | 00004 | 3.89 | 3.89 | 1 | 0117026329 | 03/15/2017 | 10:54 | 4 | 0 | 0 | N | 01 | 01 | 01 | 13 | 11 | 02 | 04 | |
| N | 00004 | 3.5 | 3.50 | 1 | 0117026744 | 03/16/2017 | 12:35 | 5 | 0 | 0 | N | 01 | 01 | 01 | 13 | 08 | 02 | 01 | |
| N | 00004 | 2.65 | 8.80 | 2 | 0117030079 | 03/24/2017 | 22:31 | 6 | 0 | 0 | N | 04 | 01 | 01 | 13 | 99 | 02 | 01 | |
| N | 00004 | 3.63 | 3.63 | 1 | 0117032078 | 03/30/2017 | 14:09 | 5 | 0 | 0 | N | 01 | 01 | 01 | 13 | 07 | 02 | 04 | |
| N | 00004 | 3.93 | 3.93 | 1 | 0117032101 | 03/30/2017 | 15:15 | 5 | 0 | 1 | N | 01 | 01 | 01 | 13 | 08 | 03 | 01 | |
| N | 00004 | 4.39 | 4.39 | 1 | 0117032834 | 04/01/2017 | 14:26 | 7 | 0 | 1 | N | 01 | 01 | 01 | 13 | 11 | 03 | 01 | |
| N | 00004 | 4.4 | 4.40 | 1 | 0117032842 | 04/01/2017 | 14:48 | 7 | 0 | 3 | N | 01 | 01 | 01 | 13 | 11 | 03 | 01 | |
| N | 00004 | 1.61 | 7.76 | 2 | 0117033803 | 04/04/2017 | 07:38 | 3 | 0 | 0 | N | 01 | 02 | 02 | 12 | 17 | 02 | 00 | |
| N | 112350 | 0 | 0.00 | 2 | 0117034900 | 04/06/2017 | 18:01 | 5 | 0 | 1 | N | 01 | 01 | 02 | 13 | 02 | 03 | 03 | |
| N | 00004 | 3.81 | 3.81 | 1 | 0117036277 | 04/10/2017 | 13:08 | 2 | 0 | 0 | N | 01 | 01 | 01 | 13 | 11 | 02 | 01 | |
| N | 00004 | 2.35 | 8.50 | 2 | 0117037145 | 04/12/2017 | 11:49 | 4 | 0 | 0 | N | 01 | 01 | 01 | 13 | 99 | 02 | 01 | |
| N | 00004 | 3.87 | 3.87 | 1 | 0117038985 | 04/17/2017 | 12:05 | 2 | 0 | 0 | N | 01 | 02 | 02 | 13 | 07 | 02 | 03 | |
| N | 00004 | 3.64 | 3.64 | 1 | 0117041128 | 04/22/2017 | 20:10 | 7 | 0 | 0 | N | 04 | 01 | 02 | 13 | 99 | 02 | 01 | |
| N | 00004 | 2.23 | 8.38 | 2 | 0117043424 | 04/29/2017 | 12:46 | 7 | 0 | 0 | N | 01 | 01 | 01 | 13 | 11 | 02 | 01 | |
| N | 00004 | 2.13 | 8.28 | 2 | 0117043427 | 04/29/2017 | 12:44 | 7 | 0 | 0 | N | 01 | 01 | 01 | 13 | 12 | 02 | 04 | |
| N | 00004 | 2.21 | 8.36 | 2 | 0117045613 | 05/06/2017 | 10:43 | 7 | 0 | 0 | N | 01 | 02 | 01 | 13 | 04 | 02 | 01 | |
| N | 00004 | 4.25 | 4.25 | 1 | 0117047788 | 05/12/2017 | 10:47 | 6 | 0 | 0 | N | 01 | 02 | 01 | 13 | 09 | 02 | 03 | |
| N | 111000 | 0.01 | 0.01 | | 0117047886 | 05/12/2017 | 00:00 | 6 | 0 | 0 | N | 01 | 01 | 01 | 13 | 08 | 02 | 01 | |
| N | 00004 | 2.05 | 8.20 | 2 | 0117049643 | 05/17/2017 | 08:48 | 4 | 0 | 0 | N | 01 | 01 | 01 | 13 | 08 | 02 | 01 | |
| N | 00004 | 3.86 | 3.86 | 1 | 0117050143 | 05/18/2017 | 13:13 | 5 | 0 | 0 | N | 01 | 01 | 01 | 13 | 02 | 02 | 01 | |
| N | 00004 | 2.18 | 8.33 | 2 | 0117059299 | 06/10/2017 | 10:55 | 7 | 0 | 0 | N | 01 | 01 | 01 | 12 | 17 | 02 | 00 | |
| N | 00004 | 3.93 | 3.93 | 1 | 0117061730 | 06/16/2017 | 10:38 | 6 | 0 | 0 | N | 01 | 01 | 01 | 42 | 08 | 02 | 01 | |
| N | 00004 | 2.09 | 8.24 | 2 | 0117063382 | 06/20/2017 | 11:36 | 3 | 0 | 0 | N | 01 | 01 | 01 | 42 | 11 | 02 | 01 | |
| N | 00004 | 3.98 | 3.98 | 1 | 0117066909 | 06/29/2017 | 06:14 | 5 | 0 | 0 | N | 01 | 01 | 01 | 13 | 07 | 02 | 04 | |
| N | 00004 | 2.66 | 8.81 | 2 | 0117067289 | 06/30/2017 | 07:15 | 6 | 0 | 0 | N | 01 | 01 | 01 | 13 | 12 | 02 | 01 | |
| N | 00004 | 2.34 | 8.49 | 2 | 0117067373 | 06/30/2017 | 11:52 | 6 | 0 | 0 | N | 01 | 01 | 01 | 13 | 03 | 02 | 03 | |
| N | 00004 | 2.65 | 8.80 | 2 | 0117068409 | 07/02/2017 | 21:17 | 1 | 0 | 0 | N | 04 | 01 | 01 | 13 | 11 | 02 | 02 | |
| N | 00212 | 0 | 0.00 | 1 | 0117071470 | 07/10/2017 | 22:29 | 2 | 0 | 0 | N | 04 | 01 | 01 | 13 | 09 | 02 | 03 | |
| N | 00004 | 3.57 | 3.57 | 1 | 0117071544 | 07/11/2017 | 06:50 | 3 | 0 | 0 | N | 01 | 01 | 01 | 42 | 08 | 02 | 01 | |
| N | 00004 | 3.88 | 3.88 | 1 | 0117072234 | 07/12/2017 | 17:46 | 4 | 0 | 0 | N | 01 | 01 | 01 | 13 | 99 | 02 | 01 | |
| N | 00004 | 1.89 | 8.04 | 2 | 0117074909 | 07/19/2017 | 13:55 | 4 | 0 | 0 | N | 01 | 01 | 01 | 13 | 04 | 02 | 03 | |
| N | 00004 | 3.96 | 3.96 | 1 | 0117080510 | 08/02/2017 | 21:04 | 4 | 0 | 0 | N | 04 | 01 | 01 | 13 | 02 | 02 | 03 | |
| N | 00004 | 3.88 | 3.88 | 1 | 0117083397 | 08/10/2017 | 08:14 | 5 | 0 | 0 | N | 01 | 01 | 01 | 13 | 11 | 02 | 01 | |
| N | 00004 | 2.65 | 8.80 | 2 | 0117084873 | 08/14/2017 | 08:03 | 2 | 0 | 0 | N | 01 | 01 | 01 | 13 | 11 | 02 | 01 | |
| N | 00004 | 4.5 | 4.50 | 1 | 0117085318 | 08/15/2017 | 12:58 | 3 | 0 | 0 | N | 01 | 04 | 02 | 13 | 08 | 02 | 01 | |
| N | 00226 | 0.33 | 0.33 | 3 | 0117086257 | 08/17/2017 | 17:59 | 5 | 0 | 0 | N | 03 | 01 | 01 | 13 | 11 | 02 | 01 | |
| N | 00004 | 3.5 | 3.50 | 1 | 0117086849 | 08/19/2017 | 12:23 | 7 | 0 | 0 | N | 01 | 01 | 01 | 13 | 08 | 02 | 01 | |

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| CTY | RD | MP | C-MP | DIR | COMP/HQ# | Date | Time | Day | Fat | Inj | AL | LC | WC | SC | FHE | PC | Class | MOI | |
|------|-------|------|------|-----|------------|------------|-------|-----|-----|-----|----|----|----|----|-----|----|-------|-----|--|
| 2017 | | | | | | | | | | | | | | | | | | | |
| N | 00004 | 2.64 | 8.79 | 2 | 0117086870 | 08/19/2017 | 13:10 | 7 | 0 | 0 | N | 01 | 01 | 01 | 13 | 12 | 02 | 04 | |
| N | 00004 | 2.64 | 8.79 | 2 | 0117086915 | 08/19/2017 | 14:57 | 7 | 0 | 0 | N | 01 | 01 | 01 | 13 | 08 | 02 | 01 | |
| N | 00212 | 0 | 0.00 | 1 | 0117087648 | 08/21/2017 | 15:11 | 2 | 0 | 0 | N | 01 | 01 | 01 | 39 | 99 | 02 | 00 | |
| N | 00004 | 4.46 | 4.46 | 1 | 0117090635 | 08/29/2017 | 13:45 | 3 | 0 | 0 | N | 01 | 04 | 02 | 13 | 08 | 02 | 01 | |
| N | 00004 | 3.94 | 3.94 | 1 | 0117092264 | 09/02/2017 | 19:51 | 7 | 0 | 2 | N | 04 | 04 | 02 | 13 | 08 | 03 | 01 | |
| N | 00004 | 1.56 | 7.71 | 2 | 0117092540 | 09/03/2017 | 19:48 | 1 | 0 | 0 | N | 04 | 01 | 01 | 13 | 02 | 02 | 03 | |
| N | 00004 | 4.26 | 4.26 | 1 | 0117094598 | 09/09/2017 | 13:16 | 7 | 0 | 0 | N | 01 | 01 | 01 | 13 | 15 | 02 | 04 | |
| N | 00004 | 3.64 | 3.64 | 1 | 0117099515 | 09/22/2017 | 10:21 | 6 | 0 | 1 | N | 01 | 01 | 01 | 13 | 02 | 03 | 03 | |
| N | 00004 | 2.28 | 8.43 | 2 | 0117099727 | 09/22/2017 | 18:32 | 6 | 0 | 1 | N | 01 | 01 | 01 | 13 | 07 | 03 | 04 | |
| N | 00004 | 2.28 | 8.43 | 2 | 0117100850 | 09/25/2017 | 19:20 | 2 | 0 | 0 | N | 04 | 01 | 01 | 13 | 11 | 02 | 01 | |
| N | 00004 | 2.11 | 8.26 | 2 | 0117101058 | 09/26/2017 | 13:23 | 3 | 0 | 0 | N | 01 | 01 | 01 | 13 | 08 | 02 | 01 | |
| N | 00004 | 3.64 | 3.64 | 1 | 0117101858 | 09/28/2017 | 11:47 | 5 | 0 | 0 | N | 01 | 01 | 01 | 13 | 07 | 02 | 04 | |
| N | 00004 | 3.68 | 3.68 | 1 | 0117101896 | 09/28/2017 | 13:35 | 5 | 0 | 0 | N | 01 | 01 | 01 | 13 | 12 | 02 | 01 | |
| N | 00004 | 3.9 | 3.90 | 1 | 0117103087 | 10/01/2017 | 19:34 | 1 | 0 | 0 | Y | 05 | 01 | 01 | 14 | 11 | 02 | 01 | |
| N | 00004 | 2.22 | 8.37 | 2 | 0117104315 | 10/04/2017 | 20:23 | 4 | 0 | 0 | N | 04 | 01 | 01 | 13 | 07 | 02 | 04 | |
| N | 00004 | 4.52 | 4.52 | 1 | 0117107545 | 10/12/2017 | 11:01 | 5 | 0 | 0 | N | 01 | 02 | 06 | 13 | 12 | 02 | 01 | |
| N | 00004 | 2.25 | 8.40 | 2 | 0117109152 | 10/16/2017 | 14:56 | 2 | 0 | 0 | N | 01 | 01 | 01 | 13 | 07 | 02 | 03 | |
| N | | | | | 0117109898 | 10/18/2017 | 13:19 | 4 | 0 | 0 | N | 01 | 01 | 01 | 13 | 11 | 02 | 88 | |
| N | 00004 | 4.51 | 4.51 | 1 | 0117110220 | 10/19/2017 | 07:39 | 5 | 0 | 0 | N | 02 | 01 | 01 | 13 | 17 | 02 | 00 | |
| N | 00004 | 2.23 | 8.38 | 2 | 0117110517 | 10/19/2017 | 17:43 | 5 | 0 | 0 | N | 01 | 01 | 01 | 13 | 07 | 02 | 03 | |
| N | 00004 | 4.48 | 4.48 | 1 | 0117110804 | 10/20/2017 | 13:08 | 6 | 0 | 0 | N | 01 | 01 | 01 | 13 | 08 | 02 | 01 | |
| N | 00004 | 3.99 | 3.99 | 1 | 0117110965 | 10/20/2017 | 18:32 | 6 | 0 | 0 | N | 03 | 01 | 01 | 13 | 99 | 02 | 01 | |
| N | 00004 | 2.52 | 8.67 | 2 | 0117111340 | 10/21/2017 | 17:55 | 7 | 0 | 1 | N | 01 | 01 | 01 | 13 | 04 | 03 | 03 | |
| N | 00004 | 3.81 | 3.81 | 1 | 0117112461 | 10/24/2017 | 18:02 | 3 | 0 | 0 | N | 03 | 01 | 01 | 13 | 07 | 02 | 04 | |
| N | 00004 | 4.51 | 4.51 | 1 | 0117113457 | 10/27/2017 | 09:12 | 6 | 0 | 0 | N | 01 | 01 | 01 | 12 | 17 | 02 | 03 | |
| N | 00004 | 2.21 | 8.36 | 2 | 0117113685 | 10/27/2017 | 17:09 | 6 | 0 | 0 | N | 01 | 01 | 01 | 13 | 02 | 02 | 03 | |
| N | 00004 | 3.95 | 3.95 | 1 | 0117114997 | 10/31/2017 | 10:13 | 3 | 0 | 0 | N | 01 | 01 | 01 | 13 | 88 | 02 | 03 | |
| N | 00004 | 2.03 | 8.18 | 2 | 0117115531 | 11/01/2017 | 18:49 | 4 | 0 | 0 | N | 04 | 01 | 01 | 13 | 12 | 02 | 01 | |
| N | 00004 | 4.36 | 4.36 | 1 | 0117115866 | 11/02/2017 | 16:06 | 5 | 0 | 0 | N | 01 | 01 | 01 | 13 | 12 | 02 | 04 | |
| N | 00004 | 1.88 | 8.03 | 2 | 0117116137 | 11/03/2017 | 09:30 | 6 | 0 | 1 | N | 01 | 01 | 01 | 13 | 07 | 03 | 01 | |
| N | 00004 | 2.03 | 8.18 | 2 | 0117118323 | 11/08/2017 | 17:52 | 4 | 0 | 0 | N | 04 | 01 | 01 | 13 | 11 | 02 | 01 | |
| N | 00004 | 4.58 | 4.58 | 1 | 0117120794 | 11/14/2017 | 20:09 | 3 | 0 | 0 | N | 04 | 01 | 01 | 13 | 08 | 02 | 01 | |
| N | 00004 | 2.64 | 8.79 | 2 | 0117121305 | 11/16/2017 | 08:06 | 5 | 0 | 0 | N | 01 | 01 | 01 | 13 | 08 | 02 | 01 | |
| N | 00004 | 3.8 | 3.80 | 1 | 0117121510 | 11/16/2017 | 16:24 | 5 | 0 | 0 | N | 01 | 01 | 01 | 13 | 08 | 02 | 01 | |
| N | 00004 | 4.01 | 4.01 | 1 | 0117123629 | 11/22/2017 | 09:10 | 4 | 0 | 0 | N | 01 | 01 | 02 | 13 | 99 | 02 | 03 | |
| N | 00004 | 2.65 | 8.80 | 2 | 0117124452 | 11/24/2017 | 14:25 | 6 | 0 | 0 | N | 01 | 01 | 01 | 13 | 08 | 02 | 03 | |
| N | 00004 | 2.09 | 8.24 | 2 | 0117124920 | 11/25/2017 | 18:18 | 7 | 0 | 0 | N | 04 | 01 | 01 | 13 | 02 | 02 | 03 | |
| N | 00004 | 1.65 | 7.80 | 2 | 0117125067 | 11/26/2017 | 07:53 | 1 | 0 | 0 | N | 01 | 01 | 01 | 12 | 17 | 02 | 00 | |
| N | 00004 | 1.88 | 8.03 | 2 | 0117125246 | 11/26/2017 | 18:45 | 1 | 0 | 0 | N | 04 | 01 | 01 | 13 | 08 | 02 | 01 | |
| N | 00004 | 3.69 | 3.69 | 1 | 0117125951 | 11/28/2017 | 16:12 | 3 | 0 | 0 | N | 01 | 01 | 01 | 13 | 07 | 02 | 03 | |

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| CTY | RD | MP | C-MP | DIR | COMP/HQ# | Date | Time | Day | Fat | Inj | AL | LC | WC | SC | FHE | PC | Class | MOI |
|-----|----|----|------|-----|----------|------|------|-----|-----|-----|----|----|----|----|-----|----|-------|-----|
|-----|----|----|------|-----|----------|------|------|-----|-----|-----|----|----|----|----|-----|----|-------|-----|

2017

| | | | | | | | | | | | | | | | | | | |
|---|-------|------|------|---|------------|------------|-------|---|---|---|---|----|----|----|----|----|----|----|
| N | 00004 | 2.65 | 8.80 | 2 | 0117126078 | 11/28/2017 | 22:05 | 3 | 0 | 1 | N | 04 | 01 | 01 | 13 | 05 | 03 | 04 |
| N | 00004 | 1.89 | 8.04 | 2 | 0117132753 | 12/16/2017 | 12:25 | 7 | 0 | 2 | N | 01 | 01 | 02 | 13 | 12 | 03 | 01 |
| N | 00004 | 3.76 | 3.76 | 1 | 0117133549 | 12/18/2017 | 14:57 | 2 | 0 | 0 | N | 01 | 01 | 01 | 13 | 12 | 02 | 01 |
| N | 00227 | 1.41 | 1.41 | 3 | 3217001878 | 01/06/2017 | 18:00 | 6 | 0 | 2 | N | 03 | 01 | 01 | 13 | 11 | 03 | 01 |
| N | 00226 | 0.32 | 0.32 | 3 | 3217011395 | 02/04/2017 | 15:54 | 7 | 0 | 0 | N | 01 | 01 | 01 | 13 | 08 | 02 | 01 |
| N | 00226 | 0.33 | 0.33 | 3 | 3217026861 | 03/21/2017 | 08:24 | 3 | 0 | 0 | N | 01 | 02 | 02 | 13 | 11 | 02 | 01 |
| N | 00004 | 2.52 | 8.67 | 2 | 3217112664 | 11/30/2017 | 11:09 | 5 | 0 | 1 | N | 01 | 01 | 01 | 13 | 11 | 03 | 88 |

Report generated by TDTSKLG at 2018-01-09 13:58:00.583

Report Legend

Cty - County

Rd - Maintenance Road

MP - Milepoint

C-MP - Continuous Milepoint

Dir - Direction of Highway

COMP/HQ# - Complaint Number/Headquarters Number

DAY - Day Of Week Code

Fat - Fatality

Inj - Injury

AL - Alcohol Involved

LC - Lighting Condition

WC - Weather Condition

SC - Surface Condition

MHE - Most Harmful Event

PC - Primary Contributing Circumstance

Class - Report Classification

MOI - Manner of Impact

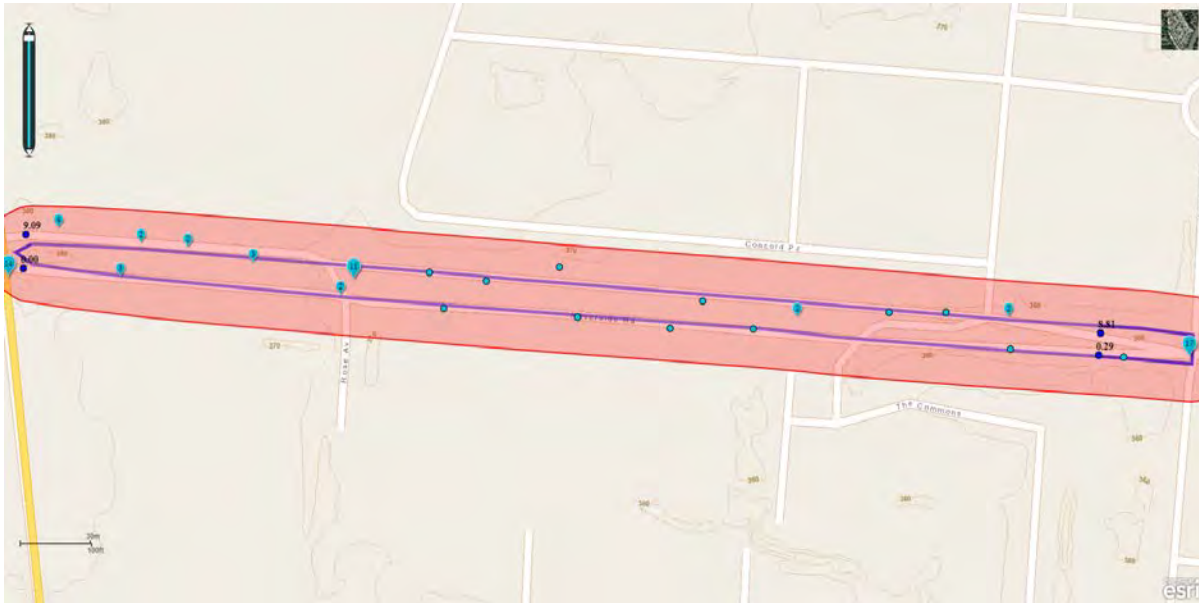
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SILVERSIDE RD, FROM US 202 TO BROOKFIELD LN

APPENDIX C – CRASH SUMMARIES

Delaware Crash Analysis Reporting System

Crash Study Time Period: 12/19/2012 - 12/19/2017
Query Type: CrashBufferTool
Description: Crash Summary Silverside Rdbetween US 202 and Brookfield Ave.
Study Requested By: John Federico- Urban Eng.
Study Generated By: TDTSKLG
Number of Crashes: 90
Includes Non-Reportable Crashes: N
Study Code:



State of Delaware Crash Study

| Summary | | Classification | | | Manner of Impact | | |
|--|--------------|-----------------|--------------|--------------------|--------------------------------------|--------------|--------------------|
| | # of Crashes | | # of Crashes | % of Total Crashes | | # of Crashes | % of Total Crashes |
| Total Crashes | 90 | Non-Reportable | 0 | 0.00% | Front to rear | 38 | 42.22% |
| Fatal Crashes | 0 | Reportable | 76 | 84.44% | Front to front | 1 | 1.11% |
| Total Alcohol-Related Crashes | 4 | Personal Injury | 14 | 15.56% | Angle | 33 | 36.67% |
| Total Non Alcohol-Related Crashes | 86 | Fatality | 0 | 0.00% | Sideswipe, same direction | 12 | 13.33% |
| Total Fatalities | 0 | Total | 90 | | Sideswipe, opposite direction | 0 | 0.00% |
| Total Pedestrian Fatalities | 0 | | | | Rear to side | 0 | 0.00% |
| Total Pedestrian Injuries | 0 | | | | Rear to rear | 0 | 0.00% |
| Total Pedestrian Crashes | 0 | | | | Other | 0 | 0.00% |
| Total Motorcycle Crashes | 0 | | | | Unknown | 1 | 1.11% |
| Total Pedalcyclist Crashes | 0 | | | | Not a collision between two vehicles | 5 | 5.56% |
| | | | | | Total | 90 | |

Alcohol Related Crashes By Classification

| | Non-reportable | Reportable | Personal Injury | Fatality | Total |
|---------------------|----------------|------------|-----------------|----------|-----------|
| Alcohol Related | 0 | 4 | 0 | 0 | 4 |
| Non-Alcohol Related | 0 | 72 | 14 | 0 | 86 |
| Total | 0 | 76 | 14 | 0 | 90 |

Manner of Impact By Classification

| | Non-Reportable | Reportable | Personal Injury | Fatality | Total |
|--------------------------------------|----------------|------------|-----------------|----------|-----------|
| Front to rear | 0 | 34 | 4 | 0 | 38 |
| Front to front | 0 | 1 | 0 | 0 | 1 |
| Angle | 0 | 23 | 10 | 0 | 33 |
| Sideswipe, same direction | 0 | 12 | 0 | 0 | 12 |
| Sideswipe, opposite direction | 0 | 0 | 0 | 0 | 0 |
| Rear to side | 0 | 0 | 0 | 0 | 0 |
| Rear to rear | 0 | 0 | 0 | 0 | 0 |
| Other | 0 | 0 | 0 | 0 | 0 |
| Unknown | 0 | 1 | 0 | 0 | 1 |
| Not a collision between two vehicles | 0 | 5 | 0 | 0 | 5 |
| Total | 0 | 76 | 14 | 0 | 90 |

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| Day Of Week | | |
|--------------|--------------|--------------------|
| | # of Crashes | % of Total Crashes |
| Sunday | 3 | 3.33% |
| Monday | 20 | 22.22% |
| Tuesday | 17 | 18.89% |
| Wednesday | 13 | 14.44% |
| Thursday | 14 | 15.56% |
| Friday | 16 | 17.78% |
| Saturday | 7 | 7.78% |
| Total | 90 | |

| Time Of Day (AM) | | |
|------------------|--------------|--------------------|
| | # of Crashes | % of Total Crashes |
| 00:00 - 00:59 | 0 | 0.00% |
| 01:00 - 01:59 | 0 | 0.00% |
| 02:00 - 02:59 | 0 | 0.00% |
| 03:00 - 03:59 | 0 | 0.00% |
| 04:00 - 04:59 | 0 | 0.00% |
| 05:00 - 05:59 | 0 | 0.00% |
| 06:00 - 06:59 | 1 | 1.11% |
| 07:00 - 07:59 | 0 | 0.00% |
| 08:00 - 08:59 | 4 | 4.44% |
| 09:00 - 09:59 | 3 | 3.33% |
| 10:00 - 10:59 | 10 | 11.11% |
| 11:00 - 11:59 | 6 | 6.67% |
| Total | 24 | |

| Time Of Day (PM) | | |
|------------------|--------------|--------------------|
| | # of Crashes | % of Total Crashes |
| 12:00 - 12:59 | 9 | 10.00% |
| 13:00 - 13:59 | 9 | 10.00% |
| 14:00 - 14:59 | 6 | 6.67% |
| 15:00 - 15:59 | 8 | 8.89% |
| 16:00 - 16:59 | 6 | 6.67% |
| 17:00 - 17:59 | 6 | 6.67% |
| 18:00 - 18:59 | 7 | 7.78% |
| 19:00 - 19:59 | 7 | 7.78% |
| 20:00 - 20:59 | 3 | 3.33% |
| 21:00 - 21:59 | 1 | 1.11% |
| 22:00 - 22:59 | 3 | 3.33% |
| 23:00 - 23:59 | 1 | 1.11% |
| Total | 66 | |
| Unknown Time | 0 | |

| Surface Conditions | | |
|--------------------------|--------------|--------------------|
| | # of Crashes | % of Total Crashes |
| Dry | 77 | 85.56% |
| Wet | 13 | 14.44% |
| Snow | 0 | 0.00% |
| Ice/Frost | 0 | 0.00% |
| Sand | 0 | 0.00% |
| Water (standing, moving) | 0 | 0.00% |
| Slush | 0 | 0.00% |
| Oil | 0 | 0.00% |
| Mud, Dirt, Gravel | 0 | 0.00% |
| Other | 0 | 0.00% |
| Unknown | 0 | 0.00% |
| Total | 90 | |

| Lighting Conditions | | |
|-----------------------|--------------|--------------------|
| | # of Crashes | % of Total Crashes |
| Daylight | 69 | 76.67% |
| Dawn | 0 | 0.00% |
| Dusk | 1 | 1.11% |
| Dark-Lighted | 17 | 18.89% |
| Dark-Not Lighted | 3 | 3.33% |
| Dark-Unknown Lighting | 0 | 0.00% |
| Other | 0 | 0.00% |
| Unknown | 0 | 0.00% |
| Total | 90 | |

| Weather Conditions | | |
|--|--------------|--------------------|
| | # of Crashes | % of Total Crashes |
| Clear | 69 | 76.67% |
| Cloudy | 15 | 16.67% |
| Fog, Smog, Smoke | 0 | 0.00% |
| Rain | 7 | 7.78% |
| Sleet, Hail (freezing rain or drizzle) | 0 | 0.00% |
| Snow | 0 | 0.00% |
| Blowing Snow | 0 | 0.00% |
| Severe Crosswinds | 0 | 0.00% |
| Blowing Sand, Soil, Dirt | 0 | 0.00% |
| Other | 0 | 0.00% |
| Unknown | 0 | 0.00% |
| Total | 91 | |

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| First Harmful Event | | |
|---|--------------|--------------------|
| | # of Crashes | % of Total Crashes |
| Overturn/Rollover, Non-Collision | 0 | 0.00% |
| Fire/Explosion, Non-Collision | 0 | 0.00% |
| Immersion, Non-Collision | 0 | 0.00% |
| Jackknife, Non-Collision | 0 | 0.00% |
| Cargo/Equipment Loss or Shift, Non-Collision | 0 | 0.00% |
| Fell/Jumped From Motor Vehicle, Non-Collision | 0 | 0.00% |
| Thrown or Falling Object, Non-Collision | 0 | 0.00% |
| Other Non-Collision, Non-Collision | 0 | 0.00% |
| Pedestrian, Collision With Person, Motor Vehicle, or Non-Fixed | 1 | 1.11% |
| Pedalcycle, Collision With Person, Motor Vehicle, or Non-Fixed | 0 | 0.00% |
| Railway Vehicle (train, engine), Collision With Person, Motor Vehicle, or Non-Fixed | 0 | 0.00% |
| Animal, Collision With Person, Motor Vehicle, or Non-Fixed | 0 | 0.00% |
| Motor Vehicle in Transport, Collision With Person, Motor Vehicle, or Non-Fixed | 78 | 86.67% |
| Legally Parked Motor Vehicle, Collision With Person, Motor Vehicle, or Non-Fixed | 1 | 1.11% |
| Struck by Anything Set in Motion by Vehicle, Collision With Person, Motor Vehicle, or Non-Fixed | 0 | 0.00% |
| Work Zone / Maintenance Equipment, Collision With Person, Motor Vehicle, or Non-Fixed | 0 | 0.00% |
| Other Non-Fixed Object, Collision With Person, Motor Vehicle, or Non-Fixed | 0 | 0.00% |
| Impact Attenuator/Crash Cushion, Collision With Fixed Object | 0 | 0.00% |
| Bridge Overhead Structure, Collision With Fixed Object | 0 | 0.00% |
| Bridge Pier or Support, Collision With Fixed Object | 0 | 0.00% |
| Bridge Rail, Collision With Fixed Object | 0 | 0.00% |
| Cable Barrier, Collision With Fixed Object | 0 | 0.00% |
| Culvert, Collision With Fixed Object | 0 | 0.00% |
| Curb, Collision With Fixed Object | 0 | 0.00% |
| Ditch, Collision With Fixed Object | 0 | 0.00% |
| Embankment, Collision With Fixed Object | 0 | 0.00% |
| Guardrail Face, Collision With Fixed Object | 0 | 0.00% |
| Guardrail End, Collision With Fixed Object | 0 | 0.00% |
| | 0 | 0.00% |

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| | | |
|---|-----------|-------|
| Concrete Traffic Barrier, Collision With Fixed Object | | |
| Other Traffic Barrier, Collision With Fixed Object | 0 | 0.00% |
| Tree (standing), Collision With Fixed Object | 0 | 0.00% |
| Utility Pole, Collision With Fixed Object | 0 | 0.00% |
| Light Support, Collision With Fixed Object | 0 | 0.00% |
| Traffic Sign Support, Collision With Fixed Object | 0 | 0.00% |
| Overhead Sign Support, Collision With Fixed Object | 0 | 0.00% |
| Traffic Signal Support, Collision With Fixed Object | 0 | 0.00% |
| Fence, Collision With Fixed Object | 0 | 0.00% |
| Mailbox, Collision With Fixed Object | 0 | 0.00% |
| Other Post, Pole or Support, Collision With Fixed Object | 4 | 4.44% |
| Other Fixed Object (wall, building, tunnel, etc.), Collision With Fixed Object | 0 | 0.00% |
| Illegally Parked Motor Vehicle, Collision With Person, Vehicle, Or Object Not Fixed | 0 | 0.00% |
| Stopped Motor Vehicle, Collision With Person, Vehicle, Or Object Not Fixed | 5 | 5.56% |
| Unknown, Collision With Fixed Object | 1 | 1.11% |
| Total | 90 | |

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| Primary Contributing Circumstance | | |
|--|--------------|--------------------|
| | # of Crashes | % of Total Crashes |
| Speeding | 0 | 0.00% |
| Failed to yield right of way | 12 | 13.33% |
| Passed Stop Sign | 0 | 0.00% |
| Disregard Traffic Signal | 12 | 13.33% |
| Wrong side or wrong way | 0 | 0.00% |
| Improper passing | 0 | 0.00% |
| Improper lane change | 8 | 8.89% |
| Following too close | 14 | 15.56% |
| Made improper turn | 5 | 5.56% |
| Driving under the influence | 3 | 3.33% |
| Driver inattention, distraction, or fatigue | 16 | 17.78% |
| Driving in a careless or reckless manner | 7 | 7.78% |
| Driving in an aggressive manner | 0 | 0.00% |
| Improper backing | 2 | 2.22% |
| Other improper driving | 0 | 0.00% |
| Mechanical defects | 0 | 0.00% |
| Animal in Roadway - Deer | 0 | 0.00% |
| Animal in Roadway - Other Animal | 0 | 0.00% |
| Other environmental circumstances - weather, glare | 1 | 1.11% |
| Roadway circumstances - debris, holes, work zone, | 0 | 0.00% |
| Other | 2 | 2.22% |
| Unknown | 7 | 7.78% |
| Total | 89 | |

Disclaimer for CARS: Crash data and associated police reports are intended for DelDOT use only and shall not be transmitted, copied, distributed or provided to any entity other than DelDOT unless written approval is received from the DelDOT Legal Section. Police reports are the property of the Delaware State Police.

| Driver Action | | |
|--|--------------|--------------------|
| | # of Drivers | % of Total Crashes |
| No Contributing Action | 89 | 50.57% |
| Failed to yield right of way | 12 | 6.82% |
| Ran Red Light | 13 | 7.39% |
| Ran Stop Sign | 0 | 0.00% |
| Disregard other traffic sign | 0 | 0.00% |
| Disregard other road markings | 0 | 0.00% |
| Exceeded authorized speed limit | 0 | 0.00% |
| Driving too fast for conditions | 1 | 0.57% |
| Made an improper turn | 5 | 2.84% |
| Improper backing | 2 | 1.14% |
| Wrong side or wrong way | 0 | 0.00% |
| Followed too closely | 15 | 8.52% |
| Failure to keep in proper lane | 11 | 6.25% |
| Ran off roadway | 0 | 0.00% |
| Operating vehicle in erratic, reckless, careless, negligent or aggressive manner | 17 | 9.66% |
| Swerving or avoiding due to wind, slippery surface, vehicle, object, non-motorist in roadway, etc. | 0 | 0.00% |
| Over-correcting/over-steering | 0 | 0.00% |
| Improper Passing | 0 | 0.00% |
| Other Contributing Action | 5 | 2.84% |
| Unknown | 6 | 3.41% |
| Total | 176 | |

Disclaimer for CARS: Crash data and associated police reports are intended for DeIDOT use only and shall not be transmitted, copied, distributed or provided to any entity other than DeIDOT unless written approval is received from the DeIDOT Legal Section. Police reports are the property of the Delaware State Police.

| CTY | RD | MP | C-MP | DIR | COMP/HQ# | Date | Time | Day | Fat | Inj | AL | LC | WC | SC | FHE | PC | Class | MOI |
|-----|----|----|------|-----|----------|------|------|-----|-----|-----|----|----|----|----|-----|----|-------|-----|
|-----|----|----|------|-----|----------|------|------|-----|-----|-----|----|----|----|----|-----|----|-------|-----|

2013

| | | | | | | | | | | | | | | | | | | |
|---|-------|------|------|---|------------|------------|-------|---|---|---|---|----|----|----|----|----|----|----|
| N | 00212 | 4.45 | 8.96 | 4 | 0113006391 | 01/23/2013 | 17:06 | 4 | 0 | 0 | N | 01 | 01 | 01 | 13 | 11 | 02 | 01 |
| N | 00212 | 4.51 | 9.02 | 4 | 0113010705 | 02/07/2013 | 18:26 | 5 | 0 | 0 | N | 05 | 01 | 01 | 13 | 11 | 02 | 01 |
| N | 00212 | 0.03 | 0.03 | 3 | 0113018730 | 03/08/2013 | 18:56 | 6 | 0 | 0 | N | 04 | 02 | 01 | 13 | 99 | 02 | 01 |
| N | 00004 | 3.97 | 3.97 | 1 | 0113020229 | 03/14/2013 | 08:17 | 5 | 0 | 0 | N | 01 | 01 | 01 | 13 | 07 | 02 | 03 |
| N | 00212 | 4.25 | 8.76 | 4 | 0113020530 | 03/15/2013 | 11:42 | 6 | 0 | 0 | N | 01 | 01 | 01 | 13 | 11 | 02 | 03 |
| N | 00004 | 3.96 | 3.96 | 1 | 0113020879 | 03/16/2013 | 12:26 | 7 | 0 | 0 | N | 01 | 02 | 01 | 13 | 11 | 02 | 01 |
| N | 00212 | 0.2 | 0.20 | 3 | 0113057071 | 07/17/2013 | 18:17 | 4 | 0 | 0 | N | 01 | 01 | 01 | 13 | 11 | 02 | 01 |
| N | | | | | 0113059297 | 07/25/2013 | 10:14 | 5 | 0 | 0 | N | 01 | 02 | 01 | 13 | 04 | 02 | 03 |
| N | 00212 | 0 | 0.00 | 3 | 0113060541 | 07/29/2013 | 10:18 | 2 | 0 | 0 | N | 01 | 01 | 01 | 13 | 08 | 02 | 01 |
| N | 00212 | 4.33 | 8.84 | 4 | 0113087913 | 11/01/2013 | 15:17 | 6 | 0 | 1 | N | 01 | 01 | 01 | 42 | 08 | 03 | 01 |
| N | 00212 | 4.38 | 8.89 | 4 | 0113102689 | 12/23/2013 | 13:14 | 2 | 0 | 0 | N | 01 | 04 | 02 | 13 | 19 | 02 | 01 |
| N | 00212 | 0.03 | 0.03 | 3 | 0113103414 | 12/26/2013 | 13:02 | 5 | 0 | 0 | N | 01 | 01 | 01 | 13 | 11 | 02 | 04 |

2014

| | | | | | | | | | | | | | | | | | | |
|---|-------|------|------|---|------------|------------|-------|---|---|---|---|----|----|----|----|----|----|----|
| N | 00212 | 0.32 | 0.32 | 3 | 0114005276 | 01/20/2014 | 11:15 | 2 | 0 | 0 | N | 01 | 01 | 01 | 13 | 09 | 02 | 04 |
| N | 00212 | 4.47 | 8.98 | 4 | 0114013603 | 02/20/2014 | 10:35 | 5 | 0 | 0 | N | 01 | 01 | 01 | 13 | 04 | 02 | 03 |
| N | 00004 | 3.97 | 3.97 | 1 | 0114015228 | 02/26/2014 | 12:11 | 4 | 0 | 0 | N | 01 | 01 | 02 | 13 | 04 | 02 | 03 |
| N | 00004 | 3.95 | 3.95 | 1 | 0114019000 | 03/12/2014 | 19:24 | 4 | 0 | 0 | N | 04 | 04 | 02 | 13 | 08 | 02 | 01 |
| N | 00004 | 3.96 | 3.96 | 1 | 0114019497 | 03/14/2014 | 14:56 | 6 | 0 | 0 | N | 01 | 01 | 01 | 13 | 04 | 02 | 03 |
| N | 00212 | 4.43 | 8.94 | 4 | 0114022894 | 03/27/2014 | 17:33 | 5 | 0 | 0 | N | 01 | 01 | 01 | 13 | 11 | 02 | 01 |
| N | 00004 | 3.97 | 3.97 | 1 | 0114031775 | 04/28/2014 | 19:08 | 2 | 0 | 1 | N | 01 | 04 | 02 | 13 | 04 | 03 | 03 |
| N | 00212 | 0.3 | 0.30 | 3 | 0114046261 | 06/16/2014 | 15:54 | 2 | 0 | 0 | N | 01 | 01 | 01 | 13 | 07 | 02 | 04 |
| N | 00212 | 0.32 | 0.32 | 3 | 0114058089 | 07/24/2014 | 17:34 | 5 | 0 | 0 | N | 01 | 01 | 01 | 13 | 12 | 02 | 01 |
| N | 00212 | 4.47 | 8.98 | 4 | 0114065890 | 08/19/2014 | 15:28 | 3 | 0 | 0 | N | 01 | 01 | 01 | 13 | 04 | 02 | 03 |
| N | 00212 | 0.09 | 0.09 | 3 | 0114072139 | 09/09/2014 | 17:04 | 3 | 0 | 1 | N | 01 | 01 | 01 | 13 | 11 | 03 | 01 |
| N | 00212 | 0.09 | 0.09 | 3 | 0114074998 | 09/19/2014 | 18:31 | 6 | 0 | 0 | Y | 01 | 01 | 01 | 13 | 10 | 02 | 01 |
| N | | | | | 0114075997 | 09/23/2014 | 11:29 | 3 | 0 | 0 | N | 01 | 01 | 01 | 14 | 99 | 02 | 99 |
| N | 00004 | 3.97 | 3.97 | 1 | 0114076703 | 09/25/2014 | 22:12 | 5 | 0 | 1 | N | 04 | 01 | 01 | 13 | 04 | 03 | 03 |
| N | 00212 | 0.01 | 0.01 | 3 | 0114079235 | 10/04/2014 | 11:17 | 7 | 0 | 0 | N | 01 | 01 | 01 | 13 | 02 | 02 | 03 |
| N | 00212 | 0.32 | 0.32 | 3 | 0114081975 | 10/14/2014 | 13:40 | 3 | 0 | 0 | N | 01 | 02 | 01 | 13 | 11 | 02 | 01 |
| N | 00212 | 0.09 | 0.09 | 3 | 0114084725 | 10/23/2014 | 12:30 | 5 | 0 | 0 | N | 01 | 02 | 01 | 13 | 04 | 02 | 03 |
| N | 00212 | 0.07 | 0.07 | 3 | 0114093291 | 11/21/2014 | 15:56 | 6 | 0 | 1 | N | 01 | 01 | 01 | 13 | 04 | 03 | 03 |
| N | 00212 | 4.25 | 8.76 | 4 | 0114098434 | 12/10/2014 | 09:49 | 4 | 0 | 0 | N | 01 | 02 | 01 | 13 | 02 | 02 | 03 |
| N | 00212 | 4.56 | 9.07 | 1 | 0114098471 | 12/10/2014 | 12:50 | 4 | 0 | 1 | N | 01 | 01 | 01 | 13 | 04 | 03 | 03 |
| N | 00212 | 0.12 | 0.12 | 3 | 0114101304 | 12/20/2014 | 09:28 | 7 | 0 | 0 | N | 01 | 02 | 01 | 13 | 08 | 02 | 01 |
| N | 00212 | 4.32 | 8.83 | 4 | 0114101326 | 12/20/2014 | 10:48 | 7 | 0 | 0 | N | 01 | 01 | 01 | 13 | 08 | 02 | 01 |

2015

| | | | | | | | | | | | | | | | | | | |
|---|-------|------|------|---|------------|------------|-------|---|---|---|---|----|----|----|----|----|----|----|
| N | 00212 | 4.25 | 8.76 | 4 | 0115017132 | 03/04/2015 | 12:18 | 4 | 0 | 2 | N | 01 | 02 | 02 | 13 | 02 | 03 | 03 |
| N | 00212 | 0.32 | 0.32 | 3 | 0115020723 | 03/17/2015 | 11:39 | 3 | 0 | 1 | N | 01 | 01 | 01 | 13 | 02 | 03 | 03 |
| N | 00212 | 4.47 | 8.98 | 4 | 0115028470 | 04/16/2015 | 11:37 | 5 | 0 | 1 | N | 01 | 01 | 01 | 13 | 02 | 03 | 03 |
| N | 00212 | 0.32 | 0.32 | 3 | 0115035454 | 05/11/2015 | 12:57 | 2 | 0 | 0 | N | 01 | 01 | 01 | 13 | 11 | 02 | 04 |
| N | 00212 | 4.51 | 9.02 | 4 | 0115038850 | 05/22/2015 | 19:28 | 6 | 0 | 0 | N | 01 | 01 | 01 | 13 | 12 | 02 | 03 |
| N | 00212 | 4.49 | 9.00 | 4 | 0115040291 | 05/27/2015 | 19:28 | 4 | 0 | 1 | N | 01 | 01 | 01 | 13 | 12 | 03 | 03 |

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| CTY | RD | MP | C-MP | DIR | COMP/HQ# | Date | Time | Day | Fat | Inj | AL | LC | WC | SC | FHE | PC | Class | MOI |
|-----|----|----|------|-----|----------|------|------|-----|-----|-----|----|----|----|----|-----|----|-------|-----|
|-----|----|----|------|-----|----------|------|------|-----|-----|-----|----|----|----|----|-----|----|-------|-----|

2015

| | | | | | | | | | | | | | | | | | | |
|---|-------|------|------|---|------------|------------|-------|---|---|---|---|----|----|----|----|----|----|----|
| N | 00004 | 3.96 | 3.96 | 1 | 0115043375 | 06/06/2015 | 10:49 | 7 | 0 | 0 | N | 01 | 01 | 01 | 13 | 99 | 02 | 04 |
| N | 00212 | 4.5 | 9.01 | 4 | 0115044953 | 06/11/2015 | 14:54 | 5 | 0 | 0 | N | 01 | 01 | 01 | 13 | 12 | 02 | 01 |
| N | 00212 | 0.27 | 0.27 | 3 | 0115052905 | 07/06/2015 | 16:24 | 2 | 0 | 0 | N | 01 | 01 | 01 | 13 | 11 | 02 | 01 |
| N | 00004 | 3.97 | 3.97 | 1 | 0115055747 | 07/15/2015 | 16:41 | 4 | 0 | 0 | N | 04 | 04 | 02 | 13 | 08 | 02 | 01 |
| N | 00004 | 3.95 | 3.95 | 1 | 0115067150 | 08/21/2015 | 10:24 | 6 | 0 | 0 | N | 01 | 01 | 01 | 13 | 07 | 02 | 03 |
| N | 00212 | 4.59 | 9.10 | 4 | 0115083789 | 10/13/2015 | 13:28 | 3 | 0 | 0 | N | 01 | 02 | 01 | 13 | 07 | 02 | 04 |
| N | 00212 | 0.32 | 0.32 | 3 | 0115087750 | 10/26/2015 | 12:39 | 2 | 0 | 0 | N | 01 | 01 | 01 | 13 | 11 | 02 | 04 |
| N | 00212 | 0.04 | 0.04 | 3 | 0115089172 | 10/30/2015 | 19:20 | 6 | 0 | 0 | N | 04 | 01 | 01 | 13 | 07 | 02 | 04 |
| N | 00212 | 4.59 | 9.10 | 4 | 0115092033 | 11/08/2015 | 18:01 | 1 | 0 | 0 | N | 04 | 01 | 01 | 13 | 04 | 02 | 03 |
| N | 00212 | 4.57 | 9.08 | 4 | 0115093584 | 11/13/2015 | 10:49 | 6 | 0 | 0 | N | 01 | 01 | 01 | 13 | 07 | 02 | 04 |
| N | 00212 | 4.57 | 9.08 | 4 | 0115094084 | 11/14/2015 | 18:30 | 7 | 0 | 0 | N | 04 | 01 | 01 | 13 | 14 | 02 | 01 |
| N | 00212 | 0.09 | 0.09 | 3 | 0115099836 | 12/03/2015 | 20:11 | 5 | 0 | 0 | Y | 04 | 01 | 01 | 39 | 10 | 02 | 00 |

2016

| | | | | | | | | | | | | | | | | | | |
|---|--------|------|------|---|------------|------------|-------|---|---|---|---|----|----|----|----|----|----|----|
| N | 00212 | 0.32 | 0.32 | 3 | 0116003452 | 01/12/2016 | 17:35 | 3 | 0 | 0 | N | 05 | 01 | 01 | 13 | 02 | 02 | 03 |
| N | 00212 | 4.59 | 9.10 | 4 | 0116014512 | 02/16/2016 | 18:19 | 3 | 0 | 0 | N | 04 | 01 | 01 | 13 | 08 | 02 | 01 |
| N | 00004 | 3.96 | 3.96 | 1 | 0116016745 | 02/23/2016 | 15:52 | 3 | 0 | 0 | N | 01 | 04 | 02 | 13 | 08 | 02 | 01 |
| N | 00212 | 0.15 | 0.15 | 3 | 0116018743 | 02/29/2016 | 15:00 | 2 | 0 | 0 | N | 01 | 01 | 01 | 42 | 08 | 02 | 01 |
| N | 00212 | 4.36 | 8.87 | 4 | 0116021599 | 03/09/2016 | 08:50 | 4 | 0 | 1 | N | 01 | 01 | 01 | 13 | 02 | 03 | 01 |
| N | 00212 | 4.59 | 9.10 | 4 | 0116029225 | 04/01/2016 | 23:42 | 6 | 0 | 0 | Y | 04 | 01 | 01 | 13 | 10 | 02 | 01 |
| N | 00004 | 3.97 | 3.97 | 1 | 0116030091 | 04/04/2016 | 13:06 | 2 | 0 | 0 | N | 01 | 02 | 01 | 42 | 99 | 02 | 01 |
| N | 00004 | 3.97 | 3.97 | 1 | 0116035366 | 04/21/2016 | 20:26 | 5 | 0 | 0 | N | 04 | 01 | 01 | 13 | 11 | 02 | 01 |
| N | 00212 | 4.3 | 8.81 | 4 | 0116051646 | 06/06/2016 | 13:57 | 2 | 0 | 0 | N | 01 | 01 | 01 | 13 | 08 | 02 | 01 |
| N | | | | | 0116066260 | 07/12/2016 | 14:45 | 3 | 0 | 0 | N | 01 | 01 | 01 | 99 | 99 | 02 | 03 |
| N | 00004 | 3.95 | 3.95 | 1 | 0116076276 | 08/08/2016 | 14:54 | 2 | 0 | 0 | N | 01 | 01 | 01 | 13 | 07 | 02 | 04 |
| N | 110500 | 0.15 | 0.15 | 3 | 0116084784 | 08/29/2016 | 12:28 | 2 | 0 | 0 | N | 01 | 01 | 01 | 13 | 02 | 02 | 03 |
| N | 00004 | 3.95 | 3.95 | 1 | 0116094582 | 09/23/2016 | 13:01 | 6 | 0 | 0 | N | 01 | 01 | 01 | 13 | 08 | 02 | 01 |
| N | 00212 | 4.49 | 9.00 | 4 | 0116094669 | 09/23/2016 | 16:25 | 6 | 0 | 1 | N | 01 | 01 | 01 | 13 | 04 | 03 | 03 |
| N | 00004 | 3.96 | 3.96 | 1 | 0116101189 | 10/11/2016 | 14:12 | 3 | 0 | 0 | N | 01 | 01 | 01 | 13 | 09 | 02 | 04 |
| N | 00212 | 0.21 | 0.21 | 3 | 0116107438 | 10/28/2016 | 09:20 | 6 | 0 | 0 | N | 01 | 01 | 01 | 13 | 08 | 02 | 01 |
| N | | | | | 0116108645 | 10/31/2016 | 15:21 | 2 | 0 | 0 | N | 01 | 01 | 01 | 13 | 99 | 02 | 01 |
| N | 00212 | 0.18 | 0.18 | 3 | 0116109008 | 11/01/2016 | 16:16 | 3 | 0 | 0 | N | 01 | 02 | 01 | 42 | 08 | 02 | 01 |
| N | 00212 | 0.32 | 0.32 | 3 | 0116117462 | 11/24/2016 | 19:27 | 5 | 0 | 0 | N | 04 | 01 | 01 | 09 | 21 | 02 | 00 |
| N | 00212 | 0.09 | 0.09 | 3 | 0116121205 | 12/05/2016 | 08:45 | 2 | 0 | 0 | N | 01 | 02 | 02 | 13 | 02 | 02 | 03 |
| N | 00212 | 0.09 | 0.09 | 3 | 0116121289 | 12/05/2016 | 14:44 | 2 | 0 | 0 | N | 01 | 01 | 01 | 13 | 12 | 02 | 01 |
| N | 110500 | 0.15 | 0.15 | 3 | 0116124017 | 12/13/2016 | 10:36 | 3 | 0 | 0 | N | 01 | 01 | 01 | 13 | 11 | 02 | 03 |
| N | 00212 | 4.59 | 9.10 | 4 | 0116129256 | 12/27/2016 | 13:34 | 3 | 0 | 0 | N | 01 | 02 | 02 | 13 | 12 | 02 | 01 |

2017

| | | | | | | | | | | | | | | | | | | |
|---|-------|------|------|---|------------|------------|-------|---|---|---|---|----|----|----|----|----|----|----|
| N | | | | | 0117001758 | 01/06/2017 | 08:30 | 6 | 0 | 0 | N | 01 | 01 | 02 | 13 | 02 | 02 | 02 |
| N | 00212 | 4.49 | 9.00 | 4 | 0117008248 | 01/24/2017 | 17:05 | 3 | 0 | 0 | N | 03 | 01 | 01 | 39 | 09 | 02 | 00 |
| N | 00212 | 4.58 | 9.09 | 4 | 0117014683 | 02/12/2017 | 10:21 | 1 | 0 | 0 | N | 01 | 04 | 02 | 13 | 14 | 02 | 03 |
| N | 00212 | 0 | 0.00 | 3 | 0117020301 | 02/27/2017 | 13:27 | 2 | 0 | 0 | N | 01 | 02 | 01 | 13 | 11 | 02 | 01 |
| N | 00212 | 0.32 | 0.32 | 3 | 0117027316 | 03/17/2017 | 16:41 | 6 | 0 | 0 | N | 01 | 01 | 02 | 13 | 11 | 02 | 01 |

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| CTY | RD | MP | C-MP | DIR | COMP/HQ# | Date | Time | Day | Fat | Inj | AL | LC | WC | SC | FHE | PC | Class | MOI |
|-----|----|----|------|-----|----------|------|------|-----|-----|-----|----|----|----|----|-----|----|-------|-----|
|-----|----|----|------|-----|----------|------|------|-----|-----|-----|----|----|----|----|-----|----|-------|-----|

2017

| | | | | | | | | | | | | | | | | | | |
|---|-------|------|------|---|------------|------------|-------|---|---|---|---|----|------------|----|----|----|----|----|
| N | 00212 | 4.55 | 9.06 | 4 | 0117057060 | 06/04/2017 | 22:18 | 1 | 0 | 0 | Y | 04 | 01 | 01 | 42 | 08 | 02 | 01 |
| N | 00212 | 4.48 | 8.99 | 4 | 0117058316 | 06/07/2017 | 20:58 | 4 | 0 | 0 | N | 04 | 01 | 01 | 13 | 07 | 02 | 04 |
| N | 00212 | 0 | 0.00 | 1 | 0117071470 | 07/10/2017 | 22:29 | 2 | 0 | 0 | N | 04 | 01 | 01 | 13 | 09 | 02 | 03 |
| N | 00212 | 0.32 | 0.32 | 3 | 0117075000 | 07/19/2017 | 16:43 | 4 | 0 | 0 | N | 01 | 01 | 01 | 13 | 09 | 02 | 03 |
| N | 00004 | 3.96 | 3.96 | 1 | 0117080510 | 08/02/2017 | 21:04 | 4 | 0 | 0 | N | 04 | 01 | 01 | 13 | 02 | 02 | 03 |
| N | 00212 | 4.49 | 9.00 | 4 | 0117082246 | 08/07/2017 | 12:51 | 2 | 0 | 1 | N | 01 | 04 - 02 | 02 | 13 | 02 | 03 | 03 |
| N | 00212 | 0 | 0.00 | 1 | 0117087648 | 08/21/2017 | 15:11 | 2 | 0 | 0 | N | 01 | 01 | 01 | 39 | 99 | 02 | 00 |
| N | 00212 | 4.55 | 9.06 | 4 | 0117114927 | 10/31/2017 | 06:30 | 3 | 0 | 0 | N | 04 | 01 | 01 | 39 | 88 | 02 | 00 |
| N | 00004 | 3.95 | 3.95 | 1 | 0117114997 | 10/31/2017 | 10:13 | 3 | 0 | 0 | N | 01 | 01 | 01 | 13 | 88 | 02 | 03 |
| N | 00212 | 0.31 | 0.31 | 3 | 0117127642 | 12/02/2017 | 19:49 | 7 | 0 | 1 | N | 05 | 01 | 01 | 13 | 12 | 03 | 01 |

Report generated by TDTSKLG at 2017-12-19 09:44:45.304

Report Legend

Cty - County
Rd - Maintenance Road
MP - Milepoint
C-MP - Continuous Milepoint
Dir - Direction of Highway
COMP/HQ# - Complaint Number/Headquarters Number
DAY - Day Of Week Code
Fat - Fatality
Inj - Injury
AL - Alcohol Involved
LC - Lighting Condition
WC - Weather Condition
SC - Surface Condition
MHE - Most Harmful Event
PC - Primary Contributing Circumstance
Class - Report Classification
MOI - Manner of Impact

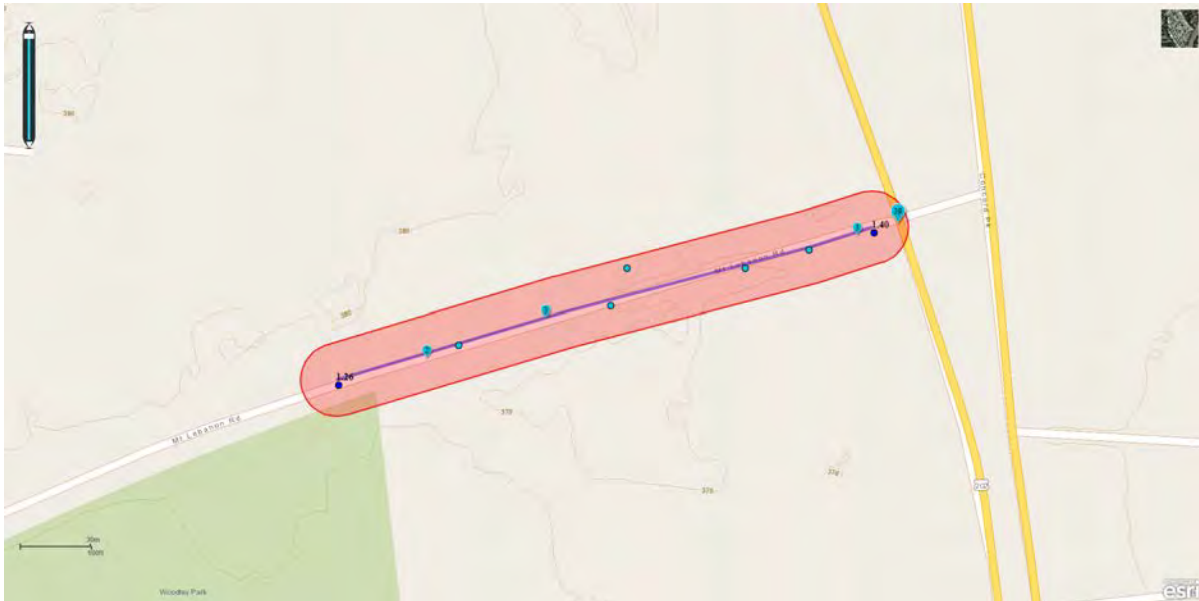
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MOUNT LEBANON RD, WEST OF US 202

APPENDIX C – CRASH SUMMARIES

Delaware Crash Analysis Reporting System

Crash Study Time Period: 12/19/2012 - 12/17/2017
Query Type: CrashBufferTool
Description: Crash Summary Mt. Lebanon Rd
Study Requested By: John Federico- Urban Eng.
Study Generated By: TDTSKLG
Number of Crashes: 21
Includes Non-Reportable Crashes: N
Study Code:



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State of Delaware Crash Study

| Summary | | Classification | | | Manner of Impact | | |
|--|--------------|-----------------|--------------|--------------------|--------------------------------------|--------------|--------------------|
| | # of Crashes | | # of Crashes | % of Total Crashes | | # of Crashes | % of Total Crashes |
| Total Crashes | 21 | | | | Front to rear | 11 | 52.38% |
| Fatal Crashes | 0 | Non-Reportable | 0 | 0.00% | Front to front | 0 | 0.00% |
| Total Alcohol-Related Crashes | 0 | Reportable | 18 | 85.71% | Angle | 5 | 23.81% |
| Total Non Alcohol-Related Crashes | 21 | Personal Injury | 3 | 14.29% | Sideswipe, same direction | 1 | 4.76% |
| Total Fatalities | 0 | Fatality | 0 | 0.00% | Sideswipe, opposite direction | 0 | 0.00% |
| Total Pedestrian Fatalities | 0 | Total | 21 | | Rear to side | 0 | 0.00% |
| Total Pedestrian Injuries | 0 | | | | Rear to rear | 0 | 0.00% |
| Total Pedestrian Crashes | 0 | | | | Other | 1 | 4.76% |
| Total Motorcycle Crashes | 0 | | | | Unknown | 0 | 0.00% |
| Total Pedalcyclist Crashes | 0 | | | | Not a collision between two vehicles | 3 | 14.29% |
| | | | | | Total | 21 | |

Alcohol Related Crashes By Classification

| | Non-reportable | Reportable | Personal Injury | Fatality | Total |
|---------------------|----------------|------------|-----------------|----------|-----------|
| Alcohol Related | 0 | 0 | 0 | 0 | 0 |
| Non-Alcohol Related | 0 | 18 | 3 | 0 | 21 |
| Total | 0 | 18 | 3 | 0 | 21 |

Manner of Impact By Classification

| | Non-Reportable | Reportable | Personal Injury | Fatality | Total |
|--------------------------------------|----------------|------------|-----------------|----------|-----------|
| Front to rear | 0 | 9 | 2 | 0 | 11 |
| Front to front | 0 | 0 | 0 | 0 | 0 |
| Angle | 0 | 4 | 1 | 0 | 5 |
| Sideswipe, same direction | 0 | 1 | 0 | 0 | 1 |
| Sideswipe, opposite direction | 0 | 0 | 0 | 0 | 0 |
| Rear to side | 0 | 0 | 0 | 0 | 0 |
| Rear to rear | 0 | 0 | 0 | 0 | 0 |
| Other | 0 | 1 | 0 | 0 | 1 |
| Unknown | 0 | 0 | 0 | 0 | 0 |
| Not a collision between two vehicles | 0 | 3 | 0 | 0 | 3 |
| Total | 0 | 18 | 3 | 0 | 21 |

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| Day Of Week | | |
|--------------|--------------|--------------------|
| | # of Crashes | % of Total Crashes |
| Sunday | 4 | 19.05% |
| Monday | 3 | 14.29% |
| Tuesday | 3 | 14.29% |
| Wednesday | 4 | 19.05% |
| Thursday | 3 | 14.29% |
| Friday | 3 | 14.29% |
| Saturday | 1 | 4.76% |
| Total | 21 | |

| Time Of Day (AM) | | |
|------------------|--------------|--------------------|
| | # of Crashes | % of Total Crashes |
| 00:00 - 00:59 | 0 | 0.00% |
| 01:00 - 01:59 | 0 | 0.00% |
| 02:00 - 02:59 | 0 | 0.00% |
| 03:00 - 03:59 | 0 | 0.00% |
| 04:00 - 04:59 | 0 | 0.00% |
| 05:00 - 05:59 | 0 | 0.00% |
| 06:00 - 06:59 | 1 | 4.76% |
| 07:00 - 07:59 | 0 | 0.00% |
| 08:00 - 08:59 | 1 | 4.76% |
| 09:00 - 09:59 | 1 | 4.76% |
| 10:00 - 10:59 | 0 | 0.00% |
| 11:00 - 11:59 | 2 | 9.52% |
| Total | 5 | |

| Time Of Day (PM) | | |
|------------------|--------------|--------------------|
| | # of Crashes | % of Total Crashes |
| 12:00 - 12:59 | 2 | 9.52% |
| 13:00 - 13:59 | 2 | 9.52% |
| 14:00 - 14:59 | 1 | 4.76% |
| 15:00 - 15:59 | 2 | 9.52% |
| 16:00 - 16:59 | 3 | 14.29% |
| 17:00 - 17:59 | 1 | 4.76% |
| 18:00 - 18:59 | 2 | 9.52% |
| 19:00 - 19:59 | 0 | 0.00% |
| 20:00 - 20:59 | 1 | 4.76% |
| 21:00 - 21:59 | 1 | 4.76% |
| 22:00 - 22:59 | 0 | 0.00% |
| 23:00 - 23:59 | 1 | 4.76% |
| Total | 16 | |
| Unknown Time | 0 | |

| Surface Conditions | | |
|--------------------------|--------------|--------------------|
| | # of Crashes | % of Total Crashes |
| Dry | 14 | 66.67% |
| Wet | 6 | 28.57% |
| Snow | 1 | 4.76% |
| Ice/Frost | 0 | 0.00% |
| Sand | 0 | 0.00% |
| Water (standing, moving) | 0 | 0.00% |
| Slush | 0 | 0.00% |
| Oil | 0 | 0.00% |
| Mud, Dirt, Gravel | 0 | 0.00% |
| Other | 0 | 0.00% |
| Unknown | 0 | 0.00% |
| Total | 21 | |

| Lighting Conditions | | |
|-----------------------|--------------|--------------------|
| | # of Crashes | % of Total Crashes |
| Daylight | 15 | 71.43% |
| Dawn | 1 | 4.76% |
| Dusk | 1 | 4.76% |
| Dark-Lighted | 2 | 9.52% |
| Dark-Not Lighted | 2 | 9.52% |
| Dark-Unknown Lighting | 0 | 0.00% |
| Other | 0 | 0.00% |
| Unknown | 0 | 0.00% |
| Total | 21 | |

| Weather Conditions | | |
|--|--------------|--------------------|
| | # of Crashes | % of Total Crashes |
| Clear | 14 | 66.67% |
| Cloudy | 1 | 4.76% |
| Fog, Smog, Smoke | 0 | 0.00% |
| Rain | 5 | 23.81% |
| Sleet, Hail (freezing rain or drizzle) | 0 | 0.00% |
| Snow | 1 | 4.76% |
| Blowing Snow | 1 | 4.76% |
| Severe Crosswinds | 0 | 0.00% |
| Blowing Sand, Soil, Dirt | 0 | 0.00% |
| Other | 0 | 0.00% |
| Unknown | 0 | 0.00% |
| Total | 22 | |

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| First Harmful Event | | |
|---|--------------|--------------------|
| | # of Crashes | % of Total Crashes |
| Overturn/Rollover, Non-Collision | 0 | 0.00% |
| Fire/Explosion, Non-Collision | 0 | 0.00% |
| Immersion, Non-Collision | 0 | 0.00% |
| Jackknife, Non-Collision | 0 | 0.00% |
| Cargo/Equipment Loss or Shift, Non-Collision | 0 | 0.00% |
| Fell/Jumped From Motor Vehicle, Non-Collision | 0 | 0.00% |
| Thrown or Falling Object, Non-Collision | 0 | 0.00% |
| Other Non-Collision, Non-Collision | 0 | 0.00% |
| Pedestrian, Collision With Person, Motor Vehicle, or Non-Fixed | 0 | 0.00% |
| Pedalcycle, Collision With Person, Motor Vehicle, or Non-Fixed | 0 | 0.00% |
| Railway Vehicle (train, engine), Collision With Person, Motor Vehicle, or Non-Fixed | 0 | 0.00% |
| Animal, Collision With Person, Motor Vehicle, or Non-Fixed | 1 | 4.76% |
| Motor Vehicle in Transport, Collision With Person, Motor Vehicle, or Non-Fixed | 17 | 80.95% |
| Legally Parked Motor Vehicle, Collision With Person, Motor Vehicle, or Non-Fixed | 1 | 4.76% |
| Struck by Anything Set in Motion by Vehicle, Collision With Person, Motor Vehicle, or Non-Fixed | 0 | 0.00% |
| Work Zone / Maintenance Equipment, Collision With Person, Motor Vehicle, or Non-Fixed | 0 | 0.00% |
| Other Non-Fixed Object, Collision With Person, Motor Vehicle, or Non-Fixed | 0 | 0.00% |
| Impact Attenuator/Crash Cushion, Collision With Fixed Object | 0 | 0.00% |
| Bridge Overhead Structure, Collision With Fixed Object | 0 | 0.00% |
| Bridge Pier or Support, Collision With Fixed Object | 0 | 0.00% |
| Bridge Rail, Collision With Fixed Object | 0 | 0.00% |
| Cable Barrier, Collision With Fixed Object | 0 | 0.00% |
| Culvert, Collision With Fixed Object | 0 | 0.00% |
| Curb, Collision With Fixed Object | 0 | 0.00% |
| Ditch, Collision With Fixed Object | 0 | 0.00% |
| Embankment, Collision With Fixed Object | 0 | 0.00% |
| Guardrail Face, Collision With Fixed Object | 0 | 0.00% |
| Guardrail End, Collision With Fixed Object | 0 | 0.00% |
| | 0 | 0.00% |

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| | | |
|---|-----------|-------|
| Concrete Traffic Barrier, Collision With Fixed Object | | |
| Other Traffic Barrier, Collision With Fixed Object | 0 | 0.00% |
| Tree (standing), Collision With Fixed Object | 0 | 0.00% |
| Utility Pole, Collision With Fixed Object | 0 | 0.00% |
| Light Support, Collision With Fixed Object | 0 | 0.00% |
| Traffic Sign Support, Collision With Fixed Object | 0 | 0.00% |
| Overhead Sign Support, Collision With Fixed Object | 0 | 0.00% |
| Traffic Signal Support, Collision With Fixed Object | 0 | 0.00% |
| Fence, Collision With Fixed Object | 0 | 0.00% |
| Mailbox, Collision With Fixed Object | 0 | 0.00% |
| Other Post, Pole or Support, Collision With Fixed Object | 0 | 0.00% |
| Other Fixed Object (wall, building, tunnel, etc.), Collision With Fixed Object | 1 | 4.76% |
| Illegally Parked Motor Vehicle, Collision With Person, Vehicle, Or Object Not Fixed | 0 | 0.00% |
| Stopped Motor Vehicle, Collision With Person, Vehicle, Or Object Not Fixed | 1 | 4.76% |
| Unknown, Collision With Fixed Object | 0 | 0.00% |
| Total | 20 | |

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| Primary Contributing Circumstance | | |
|--|--------------|--------------------|
| | # of Crashes | % of Total Crashes |
| Speeding | 0 | 0.00% |
| Failed to yield right of way | 1 | 4.76% |
| Passed Stop Sign | 0 | 0.00% |
| Disregard Traffic Signal | 1 | 4.76% |
| Wrong side or wrong way | 0 | 0.00% |
| Improper passing | 0 | 0.00% |
| Improper lane change | 1 | 4.76% |
| Following too close | 4 | 19.05% |
| Made improper turn | 0 | 0.00% |
| Driving under the influence | 0 | 0.00% |
| Driver inattention, distraction, or fatigue | 3 | 14.29% |
| Driving in a careless or reckless manner | 3 | 14.29% |
| Driving in an aggressive manner | 0 | 0.00% |
| Improper backing | 0 | 0.00% |
| Other improper driving | 0 | 0.00% |
| Mechanical defects | 1 | 4.76% |
| Animal in Roadway - Deer | 1 | 4.76% |
| Animal in Roadway - Other Animal | 0 | 0.00% |
| Other environmental circumstances - weather, glare | 1 | 4.76% |
| Roadway circumstances - debris, holes, work zone, | 0 | 0.00% |
| Other | 1 | 4.76% |
| Unknown | 4 | 19.05% |
| Total | 21 | |

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| Driver Action | | |
|--|--------------|--------------------|
| | # of Drivers | % of Total Crashes |
| No Contributing Action | 19 | 51.35% |
| Failed to yield right of way | 1 | 2.70% |
| Ran Red Light | 2 | 5.41% |
| Ran Stop Sign | 0 | 0.00% |
| Disregard other traffic sign | 0 | 0.00% |
| Disregard other road markings | 0 | 0.00% |
| Exceeded authorized speed limit | 0 | 0.00% |
| Driving too fast for conditions | 0 | 0.00% |
| Made an improper turn | 0 | 0.00% |
| Improper backing | 0 | 0.00% |
| Wrong side or wrong way | 0 | 0.00% |
| Followed too closely | 4 | 10.81% |
| Failure to keep in proper lane | 2 | 5.41% |
| Ran off roadway | 0 | 0.00% |
| Operating vehicle in erratic, reckless, careless, negligent or aggressive manner | 3 | 8.11% |
| Swerving or avoiding due to wind, slippery surface, vehicle, object, non-motorist in roadway, etc. | 0 | 0.00% |
| Over-correcting/over-steering | 0 | 0.00% |
| Improper Passing | 0 | 0.00% |
| Other Contributing Action | 2 | 5.41% |
| Unknown | 4 | 10.81% |
| Total | 37 | |

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| CTY | RD | MP | C-MP | DIR | COMP/HQ# | Date | Time | Day | Fat | Inj | AL | LC | WC | SC | FHE | PC | Class | MOI |
|-----|----|----|------|-----|----------|------|------|-----|-----|-----|----|----|----|----|-----|----|-------|-----|
|-----|----|----|------|-----|----------|------|------|-----|-----|-----|----|----|----|----|-----|----|-------|-----|

2013

| | | | | | | | | | | | | | | | | | | |
|---|-------|------|------|---|------------|------------|-------|---|---|---|---|----|------------|----|----|----|----|----|
| N | 00004 | 2.36 | 8.51 | 2 | 0113044742 | 06/07/2013 | 09:21 | 6 | 0 | 0 | N | 01 | 04 | 02 | 13 | 12 | 02 | 01 |
| N | 00004 | 2.35 | 8.50 | 2 | 0113046177 | 06/11/2013 | 17:02 | 3 | 0 | 0 | N | 01 | 01 | 01 | 42 | 99 | 02 | 01 |
| N | 00004 | 2.35 | 8.50 | 2 | 0113065523 | 08/14/2013 | 23:31 | 4 | 0 | 0 | N | 04 | 01 | 01 | 13 | 08 | 02 | 01 |
| N | 00004 | 2.35 | 8.50 | 2 | 0113090428 | 11/10/2013 | 18:27 | 1 | 0 | 0 | N | 04 | 01 | 01 | 13 | 07 | 02 | 04 |
| N | 00227 | 1.41 | 1.41 | 3 | 0113098339 | 12/08/2013 | 12:43 | 1 | 0 | 0 | N | 01 | 02 - 07 | 03 | 13 | 19 | 02 | 03 |

2014

| | | | | | | | | | | | | | | | | | | |
|---|-------|------|------|---|------------|------------|-------|---|---|---|---|----|----|----|----|----|----|----|
| N | 00004 | 2.35 | 8.50 | 2 | 0114064669 | 08/15/2014 | 16:59 | 6 | 0 | 0 | N | 01 | 01 | 01 | 13 | 12 | 02 | 01 |
| N | 00004 | 2.35 | 8.50 | 2 | 0114077839 | 09/29/2014 | 16:42 | 2 | 0 | 0 | N | 01 | 04 | 02 | 13 | 08 | 02 | 01 |
| N | 00227 | 1.33 | 1.33 | 3 | 0114080515 | 10/09/2014 | 06:20 | 5 | 0 | 0 | N | 02 | 01 | 01 | 12 | 17 | 02 | 00 |
| N | 00227 | 1.34 | 1.34 | 3 | 3214059653 | 06/08/2014 | 21:40 | 1 | 0 | 0 | N | 05 | 01 | 01 | 40 | 99 | 02 | 00 |

2015

| | | | | | | | | | | | | | | | | | | |
|---|-------|------|------|---|------------|------------|-------|---|---|---|---|----|----|----|----|----|----|----|
| N | 00004 | 2.36 | 8.51 | 2 | 0115014665 | 02/23/2015 | 13:08 | 2 | 0 | 0 | N | 01 | 01 | 01 | 13 | 02 | 02 | 03 |
| N | 00004 | 2.35 | 8.50 | 2 | 0115047866 | 06/20/2015 | 12:46 | 7 | 0 | 0 | N | 01 | 01 | 01 | 13 | 08 | 02 | 01 |
| N | 00004 | 2.36 | 8.51 | 2 | 0115048970 | 06/23/2015 | 20:21 | 3 | 0 | 0 | N | 05 | 04 | 02 | 13 | 88 | 02 | 03 |
| N | 00004 | 2.35 | 8.50 | 2 | 0115073623 | 09/10/2015 | 13:50 | 5 | 0 | 2 | N | 01 | 04 | 02 | 13 | 11 | 03 | 01 |

2016

| | | | | | | | | | | | | | | | | | | |
|---|-------|------|------|---|------------|------------|-------|---|---|---|---|----|----|----|----|----|----|----|
| N | 00004 | 2.36 | 8.51 | 2 | 0116004932 | 01/17/2016 | 15:14 | 1 | 0 | 0 | N | 01 | 06 | 02 | 13 | 12 | 02 | 01 |
| N | | | | | 0116053319 | 06/09/2016 | 16:30 | 5 | 0 | 0 | N | 01 | 01 | 01 | 14 | 99 | 02 | 88 |
| N | 00227 | 1.41 | 1.41 | 3 | 0116066382 | 07/13/2016 | 15:32 | 4 | 0 | 1 | N | 01 | 04 | 02 | 13 | 04 | 03 | 03 |

2017

| | | | | | | | | | | | | | | | | | | |
|---|-------|------|------|---|------------|------------|-------|---|---|---|---|----|----|----|----|----|----|----|
| N | 00004 | 2.36 | 8.51 | 2 | 0117021002 | 03/01/2017 | 11:10 | 4 | 0 | 0 | N | 01 | 01 | 01 | 13 | 16 | 02 | 03 |
| N | 00004 | 2.35 | 8.50 | 2 | 0117037145 | 04/12/2017 | 11:49 | 4 | 0 | 0 | N | 01 | 01 | 01 | 13 | 99 | 02 | 01 |
| N | 00227 | 1.41 | 1.41 | 3 | 3217001878 | 01/06/2017 | 18:00 | 6 | 0 | 2 | N | 03 | 01 | 01 | 13 | 11 | 03 | 01 |
| N | 00227 | 1.3 | 1.30 | 3 | 3217014482 | 02/14/2017 | 14:45 | 3 | 0 | 0 | N | 01 | 01 | 01 | 13 | 11 | 02 | 00 |
| N | 00227 | 1.39 | 1.39 | 3 | 3217052657 | 06/05/2017 | 08:55 | 2 | 0 | 0 | N | 01 | 01 | 01 | 13 | 08 | 02 | 01 |

Report generated by TDTSKLG at 2017-12-27 07:37:15.821

Report Legend

Cty - County
Rd - Maintenance Road
MP - Milepoint
C-MP - Continuous Milepoint
Dir - Direction of Highway
COMP/HQ# - Complaint Number/Headquarters Number
DAY - Day Of Week Code
Fat - Fatality
Inj - Injury
AL - Alcohol Involved
LC - Lighting Condition
WC - Weather Condition
SC - Surface Condition
MHE - Most Harmful Event
PC - Primary Contributing Circumstance
Class - Report Classification
MOI - Manner of Impact

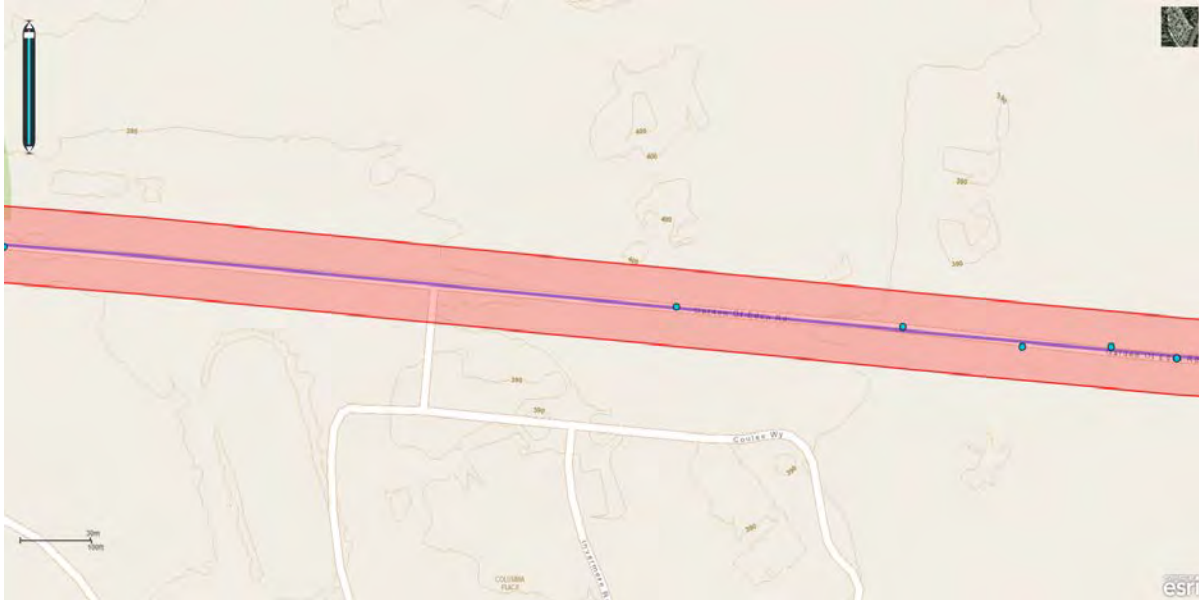
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GARDEN OF EDEN RD, FROM TRAILHEAD TO US 202

APPENDIX C – CRASH SUMMARIES

Delaware Crash Analysis Reporting System

Crash Study Time Period: 12/19/2012 - 12/19/2017
Query Type: CrashBufferTool
Description: Crash Summary Garden of Eden Road to the Trailhead of Brandywine Park
Study Requested By: John Federico- Urban Eng.
Study Generated By: TDTSKLG
Number of Crashes: 15
Includes Non-Reportable Crashes: N
Study Code:



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State of Delaware Crash Study

| Summary | | Classification | | | Manner of Impact | | |
|--|--------------|-----------------|--------------|--------------------|--------------------------------------|--------------|--------------------|
| | # of Crashes | | # of Crashes | % of Total Crashes | | # of Crashes | % of Total Crashes |
| Total Crashes | 15 | Non-Reportable | 0 | 0.00% | Front to rear | 9 | 60.00% |
| Fatal Crashes | 0 | Reportable | 14 | 93.33% | Front to front | 0 | 0.00% |
| Total Alcohol-Related Crashes | 1 | Personal Injury | 1 | 6.67% | Angle | 4 | 26.67% |
| Total Non Alcohol-Related Crashes | 14 | Fatality | 0 | 0.00% | Sideswipe, same direction | 0 | 0.00% |
| Total Fatalities | 0 | Total | 15 | | Sideswipe, opposite direction | 0 | 0.00% |
| Total Pedestrian Fatalities | 0 | | | | Rear to side | 0 | 0.00% |
| Total Pedestrian Injuries | 0 | | | | Rear to rear | 0 | 0.00% |
| Total Pedestrian Crashes | 0 | | | | Other | 0 | 0.00% |
| Total Motorcycle Crashes | 0 | | | | Unknown | 0 | 0.00% |
| Total Pedalcyclist Crashes | 0 | | | | Not a collision between two vehicles | 2 | 13.33% |
| | | | | | Total | 15 | |

Alcohol Related Crashes By Classification

| | Non-reportable | Reportable | Personal Injury | Fatality | Total |
|---------------------|----------------|------------|-----------------|----------|-----------|
| Alcohol Related | 0 | 0 | 1 | 0 | 1 |
| Non-Alcohol Related | 0 | 14 | 0 | 0 | 14 |
| Total | 0 | 14 | 1 | 0 | 15 |

Manner of Impact By Classification

| | Non-Reportable | Reportable | Personal Injury | Fatality | Total |
|--------------------------------------|----------------|------------|-----------------|----------|-----------|
| Front to rear | 0 | 9 | 0 | 0 | 9 |
| Front to front | 0 | 0 | 0 | 0 | 0 |
| Angle | 0 | 4 | 0 | 0 | 4 |
| Sideswipe, same direction | 0 | 0 | 0 | 0 | 0 |
| Sideswipe, opposite direction | 0 | 0 | 0 | 0 | 0 |
| Rear to side | 0 | 0 | 0 | 0 | 0 |
| Rear to rear | 0 | 0 | 0 | 0 | 0 |
| Other | 0 | 0 | 0 | 0 | 0 |
| Unknown | 0 | 0 | 0 | 0 | 0 |
| Not a collision between two vehicles | 0 | 1 | 1 | 0 | 2 |
| Total | 0 | 14 | 1 | 0 | 15 |

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| Day Of Week | | |
|--------------|--------------|--------------------|
| | # of Crashes | % of Total Crashes |
| Sunday | 2 | 13.33% |
| Monday | 1 | 6.67% |
| Tuesday | 3 | 20.00% |
| Wednesday | 2 | 13.33% |
| Thursday | 2 | 13.33% |
| Friday | 3 | 20.00% |
| Saturday | 2 | 13.33% |
| Total | 15 | |

| Time Of Day (AM) | | |
|------------------|--------------|--------------------|
| | # of Crashes | % of Total Crashes |
| 00:00 - 00:59 | 0 | 0.00% |
| 01:00 - 01:59 | 0 | 0.00% |
| 02:00 - 02:59 | 0 | 0.00% |
| 03:00 - 03:59 | 1 | 6.67% |
| 04:00 - 04:59 | 0 | 0.00% |
| 05:00 - 05:59 | 0 | 0.00% |
| 06:00 - 06:59 | 0 | 0.00% |
| 07:00 - 07:59 | 0 | 0.00% |
| 08:00 - 08:59 | 1 | 6.67% |
| 09:00 - 09:59 | 1 | 6.67% |
| 10:00 - 10:59 | 2 | 13.33% |
| 11:00 - 11:59 | 1 | 6.67% |
| Total | 6 | |

| Time Of Day (PM) | | |
|------------------|--------------|--------------------|
| | # of Crashes | % of Total Crashes |
| 12:00 - 12:59 | 0 | 0.00% |
| 13:00 - 13:59 | 0 | 0.00% |
| 14:00 - 14:59 | 2 | 13.33% |
| 15:00 - 15:59 | 2 | 13.33% |
| 16:00 - 16:59 | 2 | 13.33% |
| 17:00 - 17:59 | 1 | 6.67% |
| 18:00 - 18:59 | 2 | 13.33% |
| 19:00 - 19:59 | 0 | 0.00% |
| 20:00 - 20:59 | 0 | 0.00% |
| 21:00 - 21:59 | 0 | 0.00% |
| 22:00 - 22:59 | 0 | 0.00% |
| 23:00 - 23:59 | 0 | 0.00% |
| Total | 9 | |
| Unknown Time | 0 | |

| Surface Conditions | | |
|--------------------------|--------------|--------------------|
| | # of Crashes | % of Total Crashes |
| Dry | 10 | 66.67% |
| Wet | 3 | 20.00% |
| Snow | 1 | 6.67% |
| Ice/Frost | 0 | 0.00% |
| Sand | 0 | 0.00% |
| Water (standing, moving) | 1 | 6.67% |
| Slush | 0 | 0.00% |
| Oil | 0 | 0.00% |
| Mud, Dirt, Gravel | 0 | 0.00% |
| Other | 0 | 0.00% |
| Unknown | 0 | 0.00% |
| Total | 15 | |

| Lighting Conditions | | |
|-----------------------|--------------|--------------------|
| | # of Crashes | % of Total Crashes |
| Daylight | 12 | 80.00% |
| Dawn | 0 | 0.00% |
| Dusk | 1 | 6.67% |
| Dark-Lighted | 1 | 6.67% |
| Dark-Not Lighted | 1 | 6.67% |
| Dark-Unknown Lighting | 0 | 0.00% |
| Other | 0 | 0.00% |
| Unknown | 0 | 0.00% |
| Total | 15 | |

| Weather Conditions | | |
|--|--------------|--------------------|
| | # of Crashes | % of Total Crashes |
| Clear | 11 | 73.33% |
| Cloudy | 1 | 6.67% |
| Fog, Smog, Smoke | 0 | 0.00% |
| Rain | 2 | 13.33% |
| Sleet, Hail (freezing rain or drizzle) | 0 | 0.00% |
| Snow | 1 | 6.67% |
| Blowing Snow | 0 | 0.00% |
| Severe Crosswinds | 0 | 0.00% |
| Blowing Sand, Soil, Dirt | 0 | 0.00% |
| Other | 0 | 0.00% |
| Unknown | 0 | 0.00% |
| Total | 15 | |

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| First Harmful Event | | |
|---|--------------|--------------------|
| | # of Crashes | % of Total Crashes |
| Overturn/Rollover, Non-Collision | 0 | 0.00% |
| Fire/Explosion, Non-Collision | 0 | 0.00% |
| Immersion, Non-Collision | 0 | 0.00% |
| Jackknife, Non-Collision | 0 | 0.00% |
| Cargo/Equipment Loss or Shift, Non-Collision | 0 | 0.00% |
| Fell/Jumped From Motor Vehicle, Non-Collision | 0 | 0.00% |
| Thrown or Falling Object, Non-Collision | 0 | 0.00% |
| Other Non-Collision, Non-Collision | 0 | 0.00% |
| Pedestrian, Collision With Person, Motor Vehicle, or Non-Fixed | 0 | 0.00% |
| Pedalcycle, Collision With Person, Motor Vehicle, or Non-Fixed | 0 | 0.00% |
| Railway Vehicle (train, engine), Collision With Person, Motor Vehicle, or Non-Fixed | 0 | 0.00% |
| Animal, Collision With Person, Motor Vehicle, or Non-Fixed | 1 | 6.67% |
| Motor Vehicle in Transport, Collision With Person, Motor Vehicle, or Non-Fixed | 13 | 86.67% |
| Legally Parked Motor Vehicle, Collision With Person, Motor Vehicle, or Non-Fixed | 0 | 0.00% |
| Struck by Anything Set in Motion by Vehicle, Collision With Person, Motor Vehicle, or Non-Fixed | 0 | 0.00% |
| Work Zone / Maintenance Equipment, Collision With Person, Motor Vehicle, or Non-Fixed | 0 | 0.00% |
| Other Non-Fixed Object, Collision With Person, Motor Vehicle, or Non-Fixed | 0 | 0.00% |
| Impact Attenuator/Crash Cushion, Collision With Fixed Object | 0 | 0.00% |
| Bridge Overhead Structure, Collision With Fixed Object | 0 | 0.00% |
| Bridge Pier or Support, Collision With Fixed Object | 0 | 0.00% |
| Bridge Rail, Collision With Fixed Object | 0 | 0.00% |
| Cable Barrier, Collision With Fixed Object | 0 | 0.00% |
| Culvert, Collision With Fixed Object | 0 | 0.00% |
| Curb, Collision With Fixed Object | 1 | 6.67% |
| Ditch, Collision With Fixed Object | 0 | 0.00% |
| Embankment, Collision With Fixed Object | 0 | 0.00% |
| Guardrail Face, Collision With Fixed Object | 0 | 0.00% |
| Guardrail End, Collision With Fixed Object | 0 | 0.00% |
| | 0 | 0.00% |

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| | | |
|---|-----------|-------|
| Concrete Traffic Barrier, Collision With Fixed Object | | |
| Other Traffic Barrier, Collision With Fixed Object | 0 | 0.00% |
| Tree (standing), Collision With Fixed Object | 0 | 0.00% |
| Utility Pole, Collision With Fixed Object | 0 | 0.00% |
| Light Support, Collision With Fixed Object | 0 | 0.00% |
| Traffic Sign Support, Collision With Fixed Object | 0 | 0.00% |
| Overhead Sign Support, Collision With Fixed Object | 0 | 0.00% |
| Traffic Signal Support, Collision With Fixed Object | 0 | 0.00% |
| Fence, Collision With Fixed Object | 0 | 0.00% |
| Mailbox, Collision With Fixed Object | 0 | 0.00% |
| Other Post, Pole or Support, Collision With Fixed Object | 0 | 0.00% |
| Other Fixed Object (wall, building, tunnel, etc.), Collision With Fixed Object | 0 | 0.00% |
| Illegally Parked Motor Vehicle, Collision With Person, Vehicle, Or Object Not Fixed | 0 | 0.00% |
| Stopped Motor Vehicle, Collision With Person, Vehicle, Or Object Not Fixed | 0 | 0.00% |
| Unknown, Collision With Fixed Object | 0 | 0.00% |
| Total | 14 | |

Disclaimer for CARS: Crash data and associated police reports are intended for DelDOT use only and shall not be transmitted, copied, distributed or provided to any entity other than DelDOT unless written approval is received from the DelDOT Legal Section. Police reports are the property of the Delaware State Police.

| Primary Contributing Circumstance | | |
|--|--------------|--------------------|
| | # of Crashes | % of Total Crashes |
| Speeding | 0 | 0.00% |
| Failed to yield right of way | 4 | 26.67% |
| Passed Stop Sign | 0 | 0.00% |
| Disregard Traffic Signal | 0 | 0.00% |
| Wrong side or wrong way | 0 | 0.00% |
| Improper passing | 0 | 0.00% |
| Improper lane change | 0 | 0.00% |
| Following too close | 4 | 26.67% |
| Made improper turn | 0 | 0.00% |
| Driving under the influence | 1 | 6.67% |
| Driver inattention, distraction, or fatigue | 2 | 13.33% |
| Driving in a careless or reckless manner | 1 | 6.67% |
| Driving in an aggressive manner | 0 | 0.00% |
| Improper backing | 2 | 13.33% |
| Other improper driving | 0 | 0.00% |
| Mechanical defects | 0 | 0.00% |
| Animal in Roadway - Deer | 1 | 6.67% |
| Animal in Roadway - Other Animal | 0 | 0.00% |
| Other environmental circumstances - weather, glare | 0 | 0.00% |
| Roadway circumstances - debris, holes, work zone, | 0 | 0.00% |
| Other | 0 | 0.00% |
| Unknown | 0 | 0.00% |
| Total | 15 | |

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| Driver Action | | |
|--|--------------|--------------------|
| | # of Drivers | % of Total Crashes |
| No Contributing Action | 13 | 48.15% |
| Failed to yield right of way | 3 | 11.11% |
| Ran Red Light | 0 | 0.00% |
| Ran Stop Sign | 0 | 0.00% |
| Disregard other traffic sign | 0 | 0.00% |
| Disregard other road markings | 0 | 0.00% |
| Exceeded authorized speed limit | 0 | 0.00% |
| Driving too fast for conditions | 0 | 0.00% |
| Made an improper turn | 0 | 0.00% |
| Improper backing | 2 | 7.41% |
| Wrong side or wrong way | 0 | 0.00% |
| Followed too closely | 4 | 14.81% |
| Failure to keep in proper lane | 0 | 0.00% |
| Ran off roadway | 0 | 0.00% |
| Operating vehicle in erratic, reckless, careless, negligent or aggressive manner | 4 | 14.81% |
| Swerving or avoiding due to wind, slippery surface, vehicle, object, non-motorist in roadway, etc. | 0 | 0.00% |
| Over-correcting/over-steering | 0 | 0.00% |
| Improper Passing | 0 | 0.00% |
| Other Contributing Action | 1 | 3.70% |
| Unknown | 0 | 0.00% |
| Total | 27 | |

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| CTY | Rd | MP | C-MP | DIR | COMP/HQ# | Date | Time | Day | Fat | Inj | AL | LC | WC | SC | FHE | PC | Class | MOI |
|-----|----|----|------|-----|----------|------|------|-----|-----|-----|----|----|----|----|-----|----|-------|-----|
|-----|----|----|------|-----|----------|------|------|-----|-----|-----|----|----|----|----|-----|----|-------|-----|

2013

| | | | | | | | | | | | | | | | | | | |
|---|-------|------|------|---|------------|------------|-------|---|---|---|---|----|----|----|----|----|----|----|
| N | 00226 | 0.29 | 0.29 | 3 | 3213095896 | 08/26/2013 | 10:54 | 2 | 0 | 0 | N | 01 | 01 | 01 | 13 | 02 | 02 | 03 |
| N | 00226 | 0.24 | 0.24 | 3 | 3213136590 | 12/10/2013 | 09:53 | 3 | 0 | 0 | N | 01 | 06 | 03 | 13 | 02 | 02 | 03 |

2015

| | | | | | | | | | | | | | | | | | | |
|---|--------|------|------|---|------------|------------|-------|---|---|---|---|----|----|----|----|----|----|----|
| N | 00004 | 2.16 | 8.31 | 2 | 0115017728 | 03/06/2015 | 11:46 | 6 | 0 | 0 | N | 01 | 01 | 02 | 13 | 12 | 02 | 01 |
| N | 00004 | 2.17 | 8.32 | 2 | 0115059112 | 07/26/2015 | 14:09 | 1 | 0 | 0 | N | 01 | 01 | 01 | 13 | 02 | 02 | 03 |
| N | 00226 | 0.33 | 0.33 | 3 | 0115080368 | 10/02/2015 | 14:31 | 6 | 0 | 0 | N | 01 | 04 | 02 | 13 | 08 | 02 | 01 |
| N | 00004 | 2.17 | 8.32 | 2 | 0115088443 | 10/28/2015 | 18:20 | 4 | 0 | 0 | N | 04 | 04 | 06 | 13 | 08 | 02 | 01 |
| N | 111940 | 0.45 | 0.45 | 3 | 3215072421 | 07/22/2015 | 16:55 | 4 | 0 | 0 | N | 01 | 01 | 01 | 13 | 02 | 02 | 03 |

2016

| | | | | | | | | | | | | | | | | | | |
|---|--------|------|------|---|------------|------------|-------|---|---|---|---|----|----|----|----|----|----|----|
| N | 00226 | 0.33 | 0.33 | 3 | 0116112312 | 11/10/2016 | 15:12 | 5 | 0 | 0 | N | 01 | 01 | 01 | 13 | 08 | 02 | 01 |
| N | 00226 | 0.03 | 0.03 | 3 | 3216070240 | 07/24/2016 | 03:46 | 1 | 0 | 1 | Y | 05 | 01 | 01 | 24 | 10 | 03 | 00 |
| N | 111940 | 0.45 | 0.45 | | 3216076151 | 08/09/2016 | 18:00 | 3 | 0 | 0 | N | 01 | 01 | 01 | 13 | 14 | 02 | 01 |

2017

| | | | | | | | | | | | | | | | | | | |
|---|-------|------|------|---|------------|------------|-------|---|---|---|---|----|----|----|----|----|----|----|
| N | 00004 | 2.18 | 8.33 | 2 | 0117059299 | 06/10/2017 | 10:55 | 7 | 0 | 0 | N | 01 | 01 | 01 | 12 | 17 | 02 | 00 |
| N | 00226 | 0.33 | 0.33 | 3 | 0117086257 | 08/17/2017 | 17:59 | 5 | 0 | 0 | N | 03 | 01 | 01 | 13 | 11 | 02 | 01 |
| N | 00226 | 0.32 | 0.32 | 3 | 3217011395 | 02/04/2017 | 15:54 | 7 | 0 | 0 | N | 01 | 01 | 01 | 13 | 08 | 02 | 01 |
| N | 00226 | 0.33 | 0.33 | 3 | 3217026861 | 03/21/2017 | 08:24 | 3 | 0 | 0 | N | 01 | 02 | 02 | 13 | 11 | 02 | 01 |
| N | 00226 | 0.27 | 0.27 | 3 | 3217080998 | 08/25/2017 | 16:57 | 6 | 0 | 0 | N | 01 | 01 | 01 | 13 | 14 | 02 | 01 |

Report generated by TDTSKLG at 2017-12-27 08:28:11.455

Report Legend







Cty - County
Rd - Maintenance Road
MP - Milepoint
C-MP - Continuous Milepoint
Dir - Direction of Highway
COMP/HQ# - Complaint Number/Headquarters Number
DAY - Day Of Week Code
Fat - Fatality
Inj - Injury
AL - Alcohol Involved
LC - Lighting Condition
WC - Weather Condition
SC - Surface Condition
MHE - Most Harmful Event
PC - Primary Contributing Circumstance
Class - Report Classification
MOI - Manner of Impact

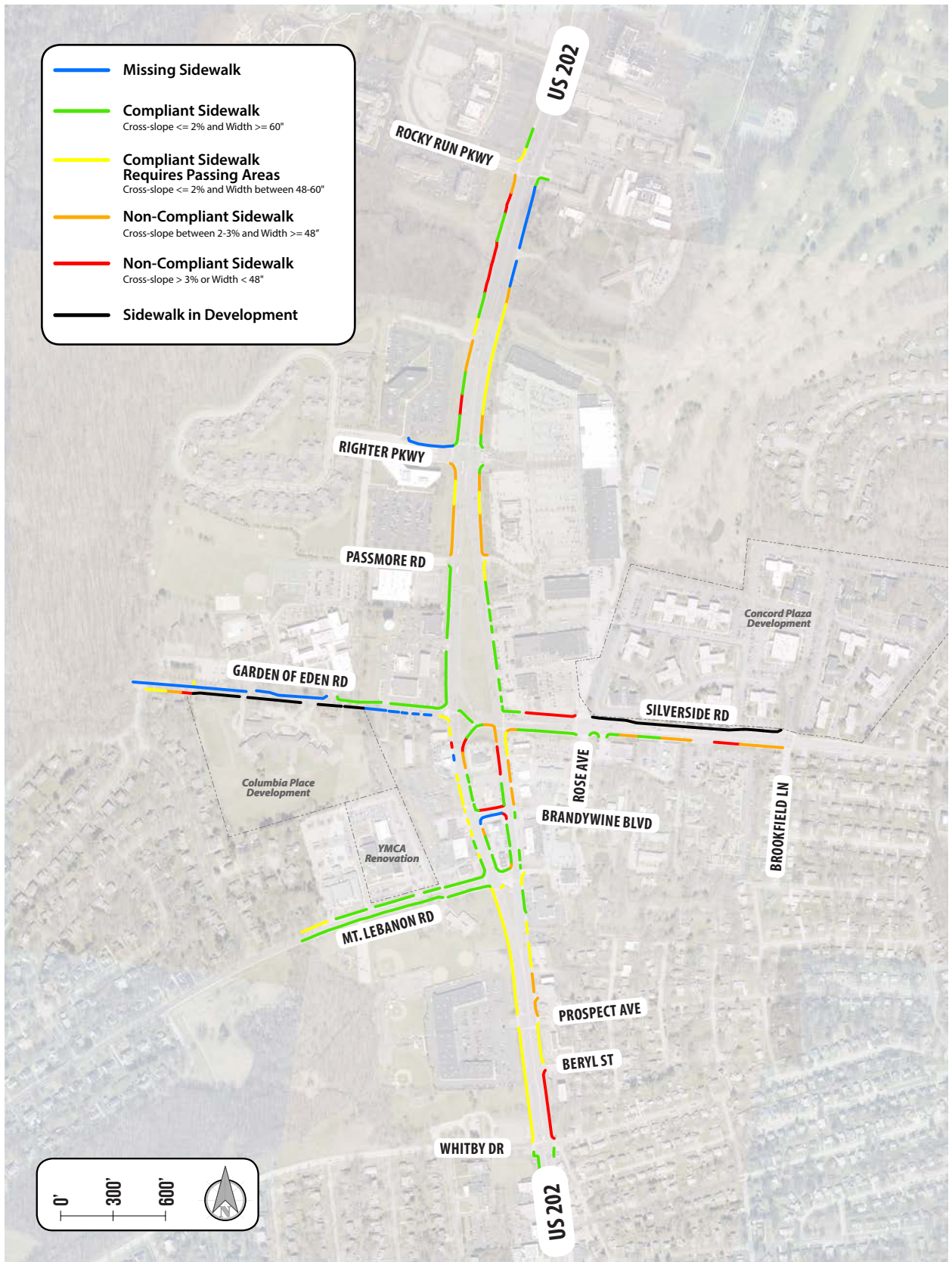
Disclaimer for CARS: Crash data and associated police reports are intended for DelDOT use only and shall not be transmitted, copied, distributed or provided to any entity other than DelDOT unless written approval is received from the DelDOT Legal Section. Police reports are the property of the Delaware State Police.

APPENDIX D

SIDEWALK INVENTORY & ANALYSIS

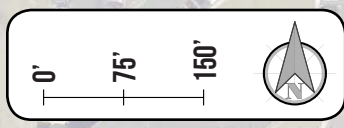
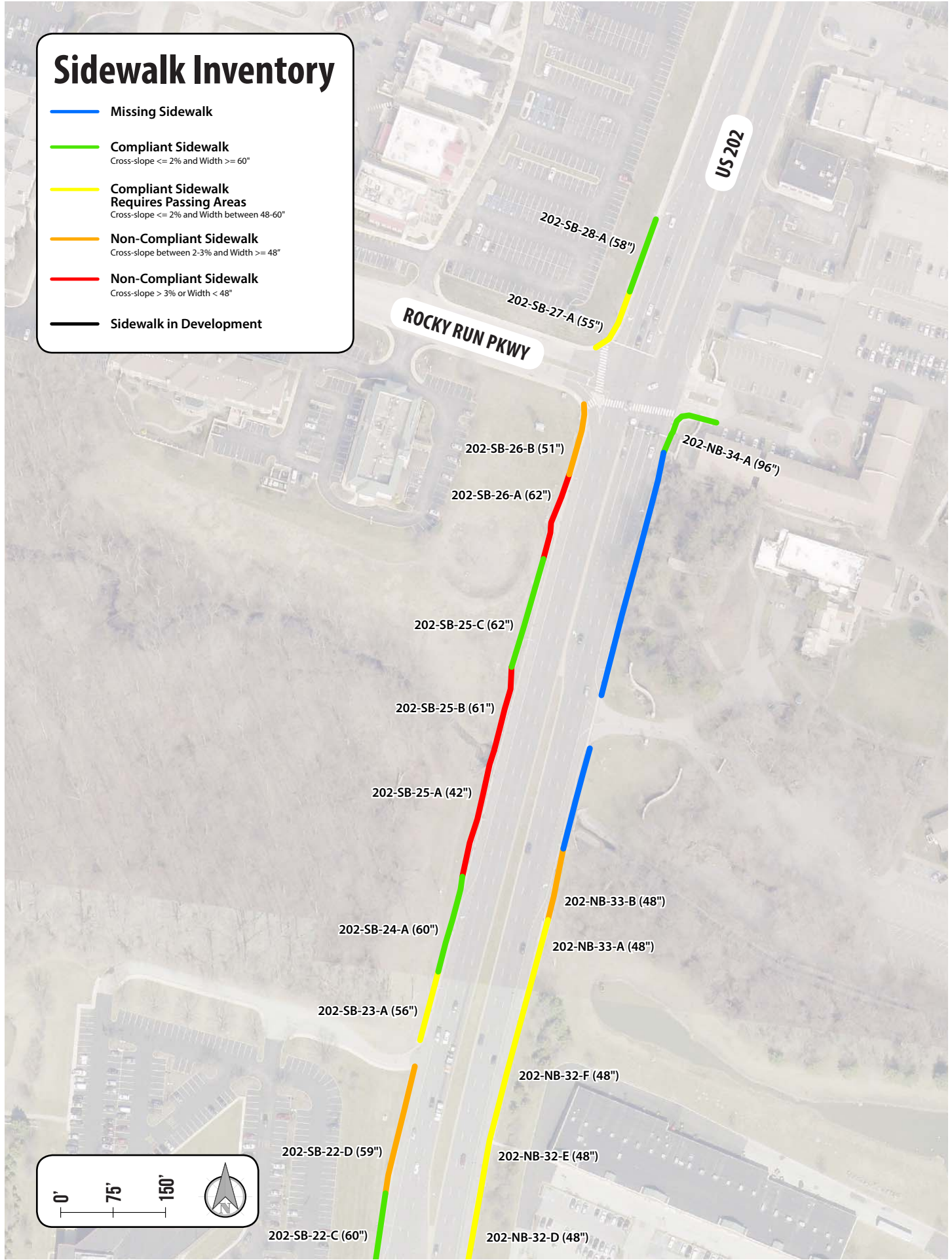
Sidewalk Inventory

-  Missing Sidewalk
-  Compliant Sidewalk
Cross-slope $\leq 2\%$ and Width $\geq 60"$
-  Compliant Sidewalk
Requires Passing Areas
Cross-slope $\leq 2\%$ and Width between 48-60"
-  Non-Compliant Sidewalk
Cross-slope between 2-3% and Width $\geq 48"$
-  Non-Compliant Sidewalk
Cross-slope $> 3\%$ or Width $< 48"$
-  Sidewalk in Development



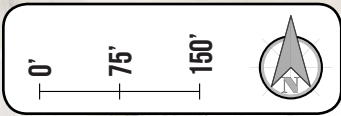
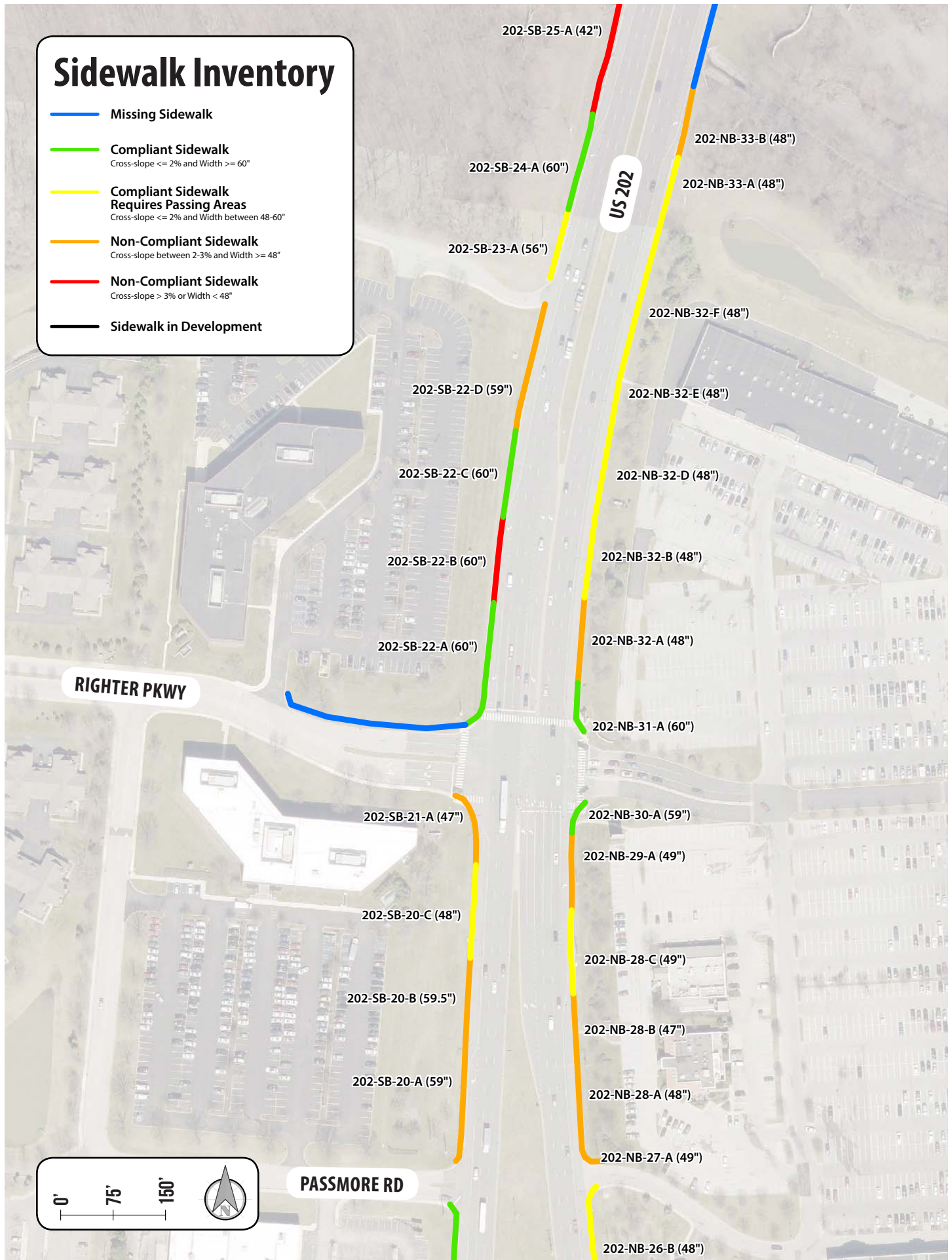
Sidewalk Inventory

- Missing Sidewalk
- Compliant Sidewalk
Cross-slope \leq 2% and Width \geq 60"
- Compliant Sidewalk
Requires Passing Areas
Cross-slope \leq 2% and Width between 48-60"
- Non-Compliant Sidewalk
Cross-slope between 2-3% and Width \geq 48"
- Non-Compliant Sidewalk
Cross-slope $>$ 3% or Width $<$ 48"
- Sidewalk in Development



Sidewalk Inventory

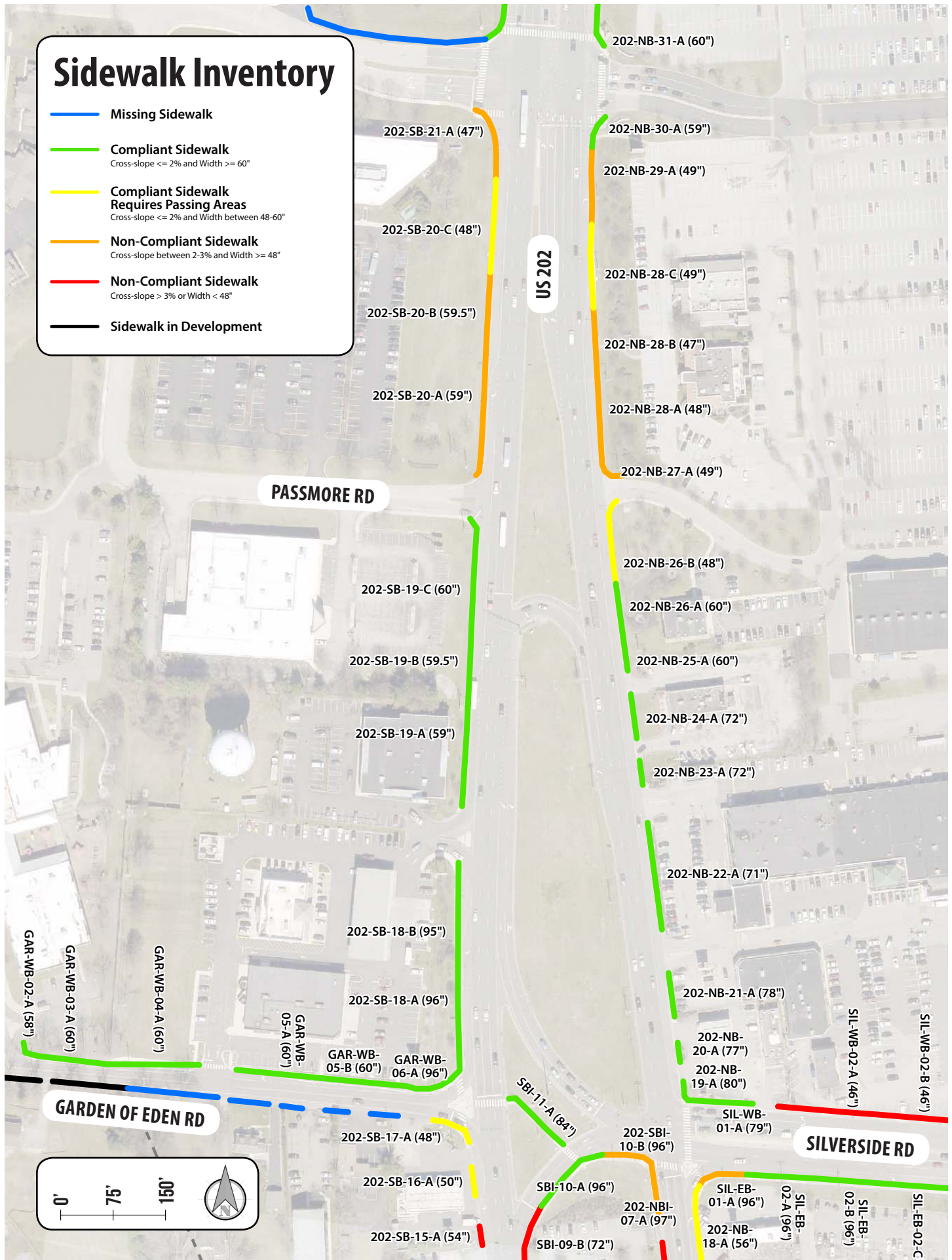
- **Missing Sidewalk**
- **Compliant Sidewalk**
Cross-slope $\leq 2\%$ and Width $\geq 60"$
- **Compliant Sidewalk Requires Passing Areas**
Cross-slope $\leq 2\%$ and Width between 48-60"
- **Non-Compliant Sidewalk**
Cross-slope between 2-3% and Width $\geq 48"$
- **Non-Compliant Sidewalk**
Cross-slope $> 3\%$ or Width $< 48"$
- **Sidewalk in Development**



PASSMORE RD

Sidewalk Inventory

- Missing Sidewalk
- Compliant Sidewalk
Cross-slope \leq 2% and Width \geq 60"
- Compliant Sidewalk
Requires Passing Areas
Cross-slope \leq 2% and Width between 48-60"
- Non-Compliant Sidewalk
Cross-slope between 2-3% and Width \geq 48"
- Non-Compliant Sidewalk
Cross-slope $>$ 3% or Width $<$ 48"
- Sidewalk in Development

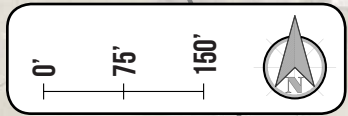


PASSMORE RD

US 202

GARDEN OF EDEN RD

SILVERSIDE RD



202-SB-21-A (47")
202-SB-20-C (48")
202-SB-20-B (59.5")
202-SB-20-A (59")

202-SB-19-C (60")
202-SB-19-B (59.5")
202-SB-19-A (59")

202-SB-18-B (95")
202-SB-18-A (96")
GAR-WB-05-A (60")
GAR-WB-05-B (60")
GAR-WB-06-A (96")

202-SB-17-A (48")
202-SB-16-A (50")
202-SB-15-A (54")

202-NB-31-A (60")
202-NB-30-A (59")
202-NB-29-A (49")
202-NB-28-C (49")
202-NB-28-B (47")
202-NB-28-A (48")
202-NB-27-A (49")

202-NB-26-B (48")
202-NB-26-A (60")
202-NB-25-A (60")
202-NB-24-A (72")
202-NB-23-A (72")

202-NB-22-A (71")
202-NB-21-A (78")
202-NB-20-A (77")
202-NB-19-A (80")

SBI-11-A (84")
SBI-10-A (96")
202-SBI-10-B (96")
SBI-09-B (72")
202-NBI-07-A (97")

SIL-WB-02-A (46")
SIL-WB-02-B (46")
SIL-WB-01-A (79")
SIL-EB-01-A (96")
202-NB-18-A (56")
SIL-EB-02-A (96")
SIL-EB-02-B (96")
SIL-EB-02-C

GAR-WB-02-A (58")
GAR-WB-03-A (60")
GAR-WB-04-A (60")

Sidewalk Inventory

- Missing Sidewalk
- Compliant Sidewalk
Cross-slope \leq 2% and Width \geq 60"
- Compliant Sidewalk
Requires Passing Areas
Cross-slope \leq 2% and Width between 48-60"
- Non-Compliant Sidewalk
Cross-slope between 2-3% and Width \geq 48"
- Non-Compliant Sidewalk
Cross-slope $>$ 3% or Width $<$ 48"
- Sidewalk in Development

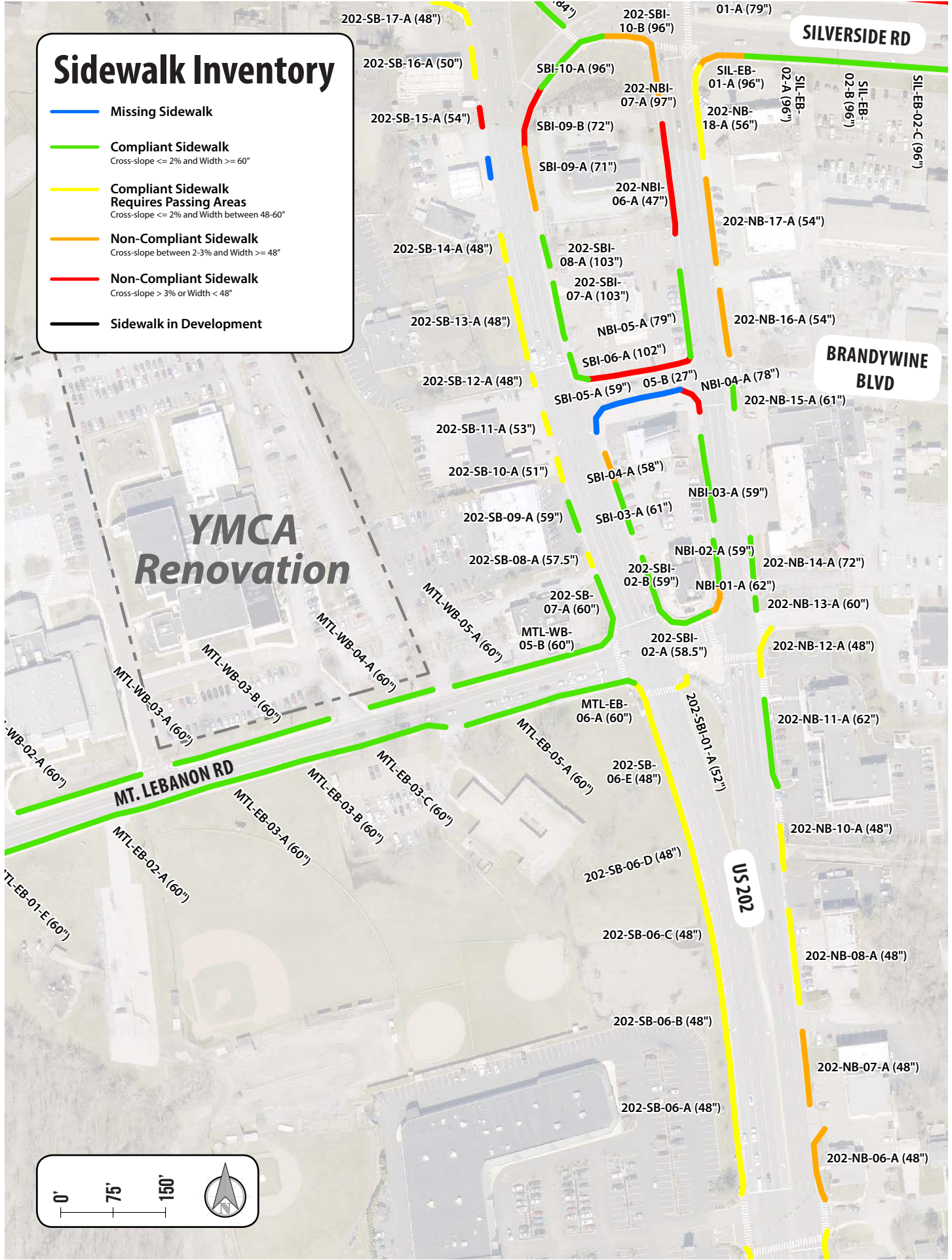
**YMCA
Renovation**

SILVERSIDE RD

BRANDYWINE BLVD

MT. LEBANON RD

US 202



202-SB-17-A (48")

202-SB-16-A (50")

202-SB-15-A (54")

202-SB-14-A (48")

202-SB-13-A (48")

202-SB-12-A (48")

202-SB-11-A (53")

202-SB-10-A (51")

202-SB-09-A (59")

202-SB-08-A (57.5")

202-SB-07-A (60")

202-SB-06-E (48")

202-SB-06-C (48")

202-SB-06-B (48")

202-SB-06-A (48")

202-SB-05-A (59")

202-SB-04-A (58")

202-SB-03-A (61")

202-SB-02-B (59")

202-SB-02-A (58.5")

202-SB-01-A (52")

202-NB-18-A (56")

202-NB-17-A (54")

202-NB-16-A (54")

202-NB-15-A (61")

202-NB-14-A (72")

202-NB-13-A (60")

202-NB-12-A (48")

202-NB-11-A (62")

202-NB-10-A (48")

202-NB-08-A (48")

202-NB-07-A (48")

202-NB-06-A (48")

SBI-10-A (96")

SBI-09-B (72")

SBI-09-A (71")

SBI-08-A (103")

SBI-07-A (103")

SBI-06-A (102")

SBI-05-A (59")

SBI-04-A (58")

SBI-03-A (61")

SBI-02-B (59")

SBI-02-A (58.5")

SBI-01-A (96")

01-A (79")

02-A (96")

02-B (96")

02-C (96")

05-B (27")

NBI-05-A (79")

NBI-04-A (78")

NBI-03-A (59")

NBI-02-A (59")

NBI-01-A (62")

MTL-WB-05-A (60")

MTL-WB-05-B (60")

MTL-WB-04-A (60")

MTL-WB-03-B (60")

MTL-WB-03-A (60")

MTL-WB-02-A (60")

MTL-WB-01-E (60")

MTL-EB-06-A (60")

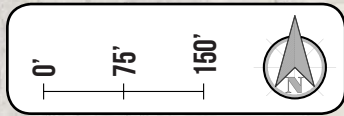
MTL-EB-05-A (60")

MTL-EB-03-C (60")

MTL-EB-03-B (60")

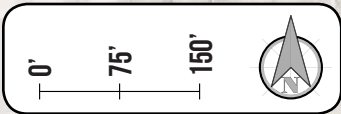
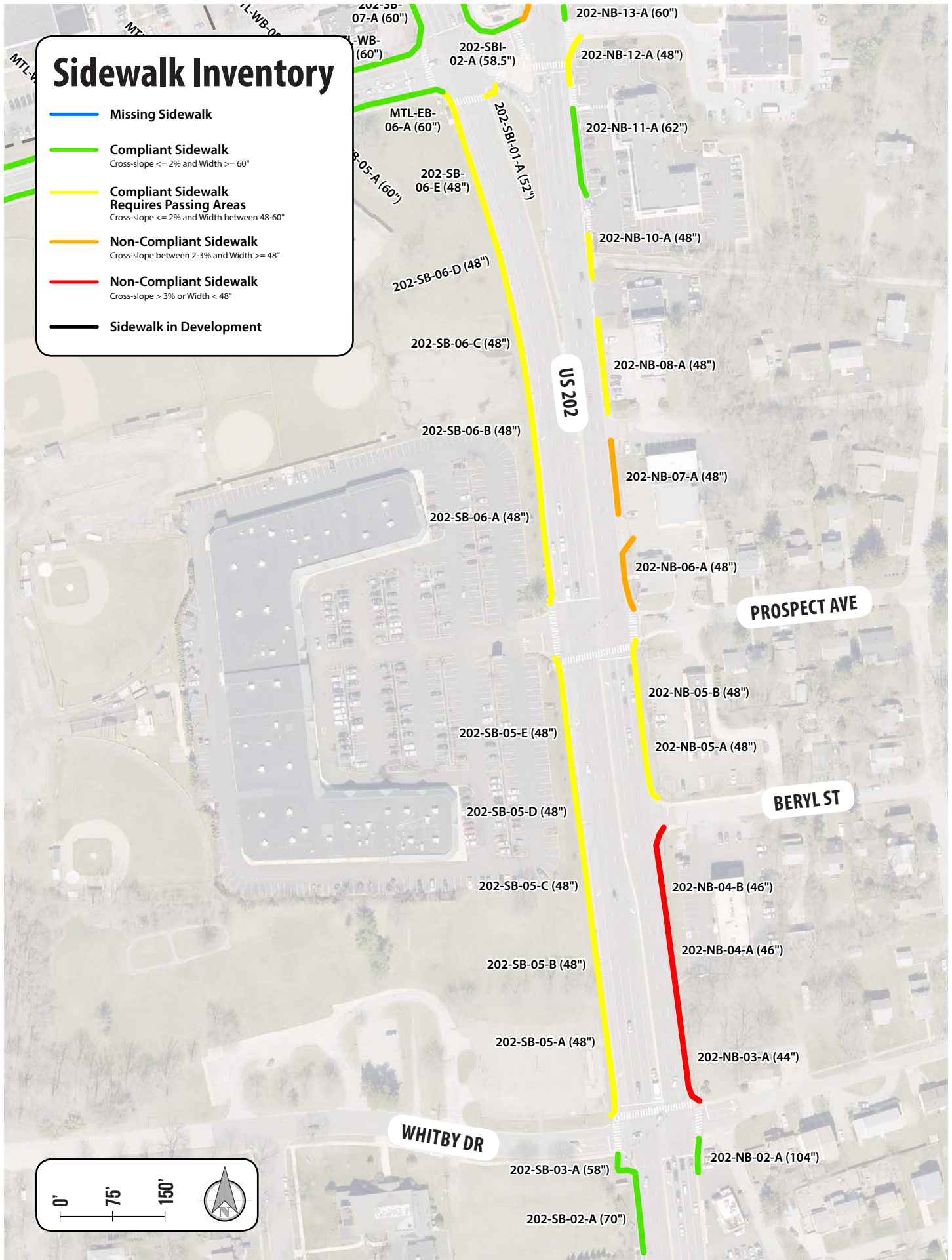
MTL-EB-03-A (60")

MTL-EB-02-A (60")



Sidewalk Inventory

- Missing Sidewalk
- Compliant Sidewalk
Cross-slope \leq 2% and Width \geq 60"
- Compliant Sidewalk
Requires Passing Areas
Cross-slope \leq 2% and Width between 48-60"
- Non-Compliant Sidewalk
Cross-slope between 2-3% and Width \geq 48"
- Non-Compliant Sidewalk
Cross-slope $>$ 3% or Width $<$ 48"
- Sidewalk in Development



Sidewalk Inventory

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- Compliant Sidewalk
Cross-slope $\leq 2\%$ and Width $\geq 60"$
- Compliant Sidewalk
Requires Passing Areas
Cross-slope $\leq 2\%$ and Width between 48-60"
- Non-Compliant Sidewalk
Cross-slope between 2-3% and Width $\geq 48"$
- Non-Compliant Sidewalk
Cross-slope $> 3\%$ or Width $< 48"$
- Sidewalk in Development

US 202

202-SB-18-B (95")

202-SB-18-A (96")

GAR-WB-05-B (60")

GAR-WF-06-A (96")

GAR-WB-05-A (60")

202-SB-17-A (48")

202-SB-16-A (50")

202-SB-15-A (54")

202-SB-14-A (41")

202-SB-13-A

GAR-WB-04-A (60")

GAR-WB-03-A (60")

GAR-WB-02-A (58")

GARDEN OF EDEN RD

Columbia Place
Development

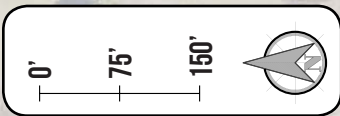
GAR-WB-01-A (48")

GAR-EB-02-A (41")

GAR-EB-01-B (48")

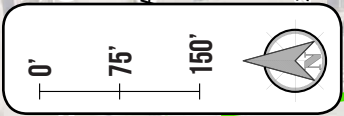
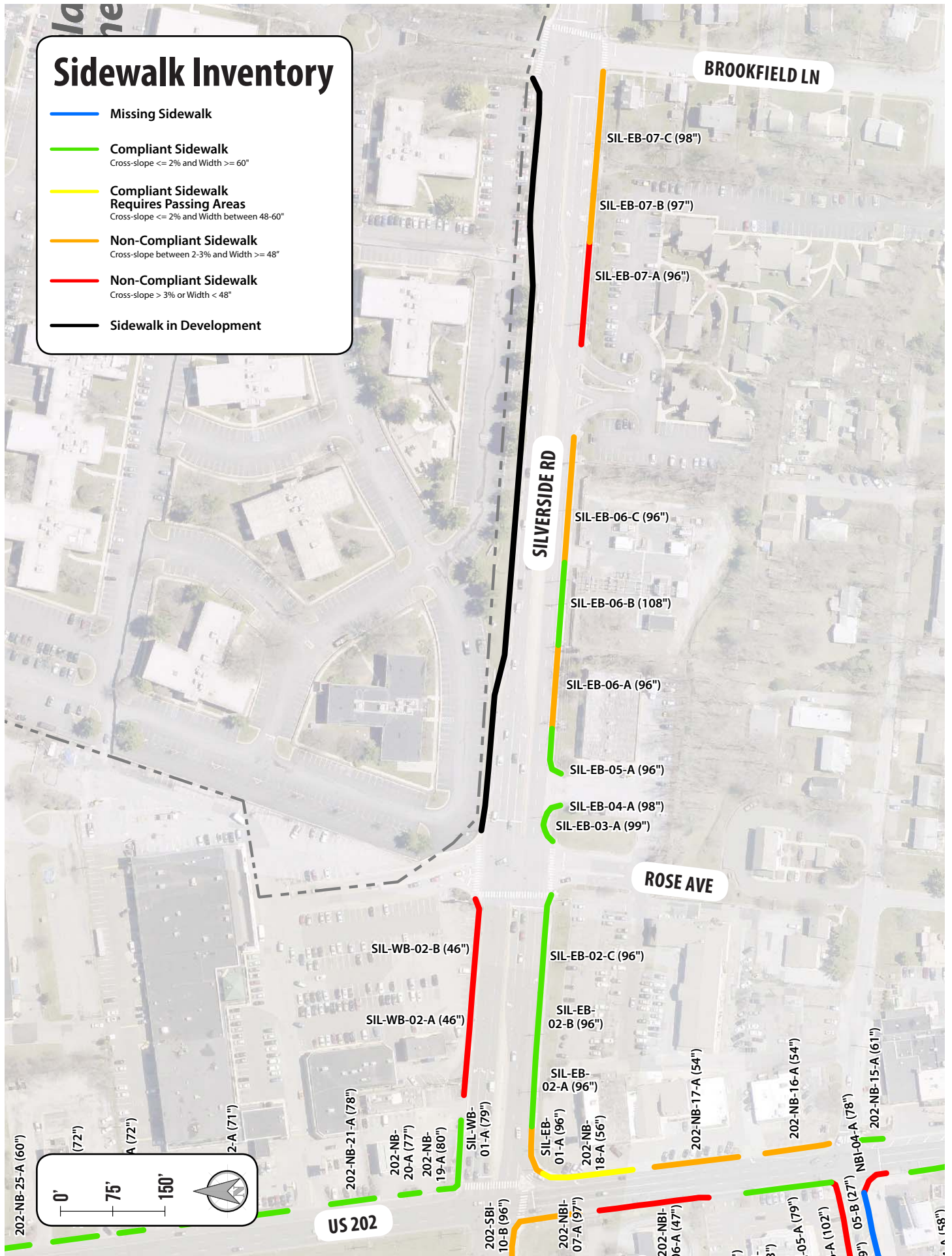
GAR-EB-01-A (48")

TRAILHEAD



Sidewalk Inventory

- Missing Sidewalk
- Compliant Sidewalk
Cross-slope \leq 2% and Width \geq 60"
- Compliant Sidewalk
Requires Passing Areas
Cross-slope \leq 2% and Width between 48-60"
- Non-Compliant Sidewalk
Cross-slope between 2-3% and Width \geq 48"
- Non-Compliant Sidewalk
Cross-slope $>$ 3% or Width $<$ 48"
- Sidewalk in Development



SIDEWALK INVENTORY

DeIDOT PAR Agreement 1793, Task Order #3
US Route 202 Pedestrian Access Route Assessment

Sidewalk measurements taken at approximate 100' intervals

Observed Compliance Legend

| | |
|----|---|
| Y | Compliant (cross slope <= 2% and width >= 60") |
| Y* | Compliant (cross slope <= 2% and width between 48-60") |
| N* | Non-Compliant (cross-slope between 2-3% and width >= 48") |
| N | Non-Compliant (cross-slope > 3% or width < 48") |

| SR 202 North - from Lyndhurst Ave (south of Whitby Drive) to Half Acre Dr | | | | | | | | | | |
|---|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|--|
| Location ID | SW-202-NB-02-A | SW-202-NB-03-A | SW-202-NB-04-A | SW-202-NB-04-B | SW-202-NB-05-A | SW-202-NB-05-B | SW-202-NB-06-A | SW-202-NB-07-A | SW-202-NB-08-A | |
| Running Slope (%) | 1.0% | 0.6% | 0.3% | 0.5% | 0.5% | 0.5% | 1.0% | 0.2% | 0.1% | |
| Cross Slope (%) | 1.5% | 4.5% | 1.7% | 2.9% | 1.5% | 0.6% | 2.3% | 2.3% | 1.8% | |
| Width (in) | 104 | 44 | 46 | 46 | 48 | 48 | 48 | 48 | 48 | |
| Observed Compliance | Y | N | N | N | Y* | Y* | N* | N* | Y* | |
| Gateway (GIS) Compliance | N | N | N | | N | | N | N | N | |

| SR 202 North - from Half Acre Dr to Silverside Rd | | | | | | | | | |
|---|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|
| Location ID | SW-202-NB-10-A | SW-202-NB-11-A | SW-202-NB-12-A | SW-202-NB-13-A | SW-202-NB-14-A | SW-202-NB-15-A | SW-202-NB-16-A | SW-202-NB-17-A | SW-202-NB-18-A |
| Running Slope (%) | 3.0% | 0.2% | 2.4% | 1.9% | 0.6% | 0.3% | 1.4% | 0.0% | 0.3% |
| Cross Slope (%) | 1.9% | 1.0% | 0.4% | 0.6% | 2.0% | 1.2% | 2.1% | 2.4% | 1.9% |
| Width (in) | 48 | 62 | 48 | 60 | 72 | 61 | 54 | 54 | 56 |
| Observed Compliance | Y* | Y | Y* | Y | Y | Y | N* | N* | Y* |
| Gateway (GIS) Compliance | Y | N | Y | | N | | N | N | N |

| SR 202 North - from Silverside Rd to Righter Pkwy | | | | | | | | | | | | | | | |
|---|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|
| Location ID | SW-202-NB-19-A | SW-202-NB-20-A | SW-202-NB-21-A | SW-202-NB-22-A | SW-202-NB-23-A | SW-202-NB-24-A | SW-202-NB-25-A | SW-202-NB-26-A | SW-202-NB-26-B | SW-202-NB-27-A | SW-202-NB-28-A | SW-202-NB-28-B | SW-202-NB-28-C | SW-202-NB-29-A | SW-202-NB-30-A |
| Running Slope (%) | 0.7% | 0.5% | 0.6% | 0.2% | 0.2% | 1.0% | 0.5% | 1.3% | 2.0% | 1.6% | 1.4% | 0.9% | 0.5% | 0.1% | 3.1% |
| Cross Slope (%) | 0.9% | 0.3% | 0.7% | 2.0% | 0.3% | 1.7% | 0.0% | 1.5% | 1.3% | 3.0% | 2.1% | 2.5% | 1.4% | 2.8% | 0.5% |
| Width (in) | 80 | 77 | 78 | 71 | 72 | 72 | 60 | 60 | 48 | 49 | 48 | 47 | 49 | 49 | 59 |
| Observed Compliance | Y | Y | Y | Y | Y | Y | Y | Y | Y* | N* | N* | N* | Y* | N* | Y |
| Gateway (GIS) Compliance | N | N | N | | N | | N | Y | | | Y | | | N | Y |

| SR 202 North - from Righter Pkwy to Widener University | | | | | | | | |
|--|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|
| Location ID | SW-202-NB-31-A | SW-202-NB-32-A | SW-202-NB-32-B | SW-202-NB-32-D | SW-202-NB-32-E | SW-202-NB-32-F | SW-202-NB-33-A | SW-202-NB-33-B |
| Running Slope (%) | 1.0% | 1.4% | 0.7% | 2.4% | 2.0% | 2.3% | 1.9% | 1.2% |
| Cross Slope (%) | 0.2% | 2.8% | 1.7% | 0.5% | 1.4% | 1.8% | 0.3% | 2.8% |
| Width (in) | 60 | 48 | 48 | 48 | 48 | 48 | 48 | 48 |
| Observed Compliance | Y | N* | Y* | Y* | Y* | Y* | Y* | N* |
| Gateway (GIS) Compliance | Y | N | | | | | Y | |

| SR 202 North - from Widener University to Rocky Run Pkwy/Gracie Church Ln | | | | |
|---|----------------|----------------|----------------|----------------|
| Location ID | SW-202-NB-34-A | SW-202-NB-35-A | SW-202-NB-36-A | SW-202-NB-37-A |
| Running Slope (%) | 0.7% | 0.4% | 1.3% | 0.3% |
| Cross Slope (%) | 0.7% | 4.3% | 2.8% | 1.2% |
| Width (in) | 96 | 47 | 45 | 47 |
| Observed Compliance | Y | N | N | N |
| Gateway (GIS) Compliance | N | N | N | |

SIDEWALK INVENTORY

DeIDOT PAR Agreement 1793, Task Order #3
US Route 202 Pedestrian Access Route Assessment

Sidewalk measurements taken at approximate 100' intervals

Observed Compliance Legend

| | |
|----|---|
| Y | Compliant (cross slope <= 2% and width >= 60") |
| Y* | Compliant (cross slope <= 2% and width between 48-60") |
| N* | Non-Compliant (cross-slope between 2-3% and width >= 48") |
| N | Non-Compliant (cross-slope > 3% or width < 48") |

| SR 202 South - from Lyndhurst Ave (south of Whitby Drive) to Prospect Ave | | | | | | | | | | | | | |
|---|----------------|----------------|----------------|----------------|----------------|----------------|----------------|--|--|--|--|--|--|
| Location ID | SW-202-SB-02-A | SW-202-SB-03-A | SW-202-SB-05-A | SW-202-SB-05-B | SW-202-SB-05-C | SW-202-SB-05-D | SW-202-SB-05-E | | | | | | |
| Running Slope (%) | 1.5% | 2.4% | 0.5% | 1.3% | 1.3% | 0.1% | 0.6% | | | | | | |
| Cross Slope (%) | 1.5% | 0.3% | 1.1% | 1.9% | 1.2% | 1.9% | 1.3% | | | | | | |
| Width (in) | 70 | 58 | 48 | 48 | 48 | 48 | 48 | | | | | | |
| Observed Compliance | Y | Y | Y* | Y* | Y* | Y* | Y* | | | | | | |
| Gateway (GIS) Compliance | Y | Y | Y | | | | | | | | | | |

| SR 202 South - from Prospect Ave to Mt Lebanon Rd | | | | | | | | | | | | | |
|---|----------------|----------------|----------------|----------------|----------------|--|--|--|--|--|--|--|--|
| Location ID | SW-202-SB-06-A | SW-202-SB-06-B | SW-202-SB-06-C | SW-202-SB-06-D | SW-202-SB-06-E | | | | | | | | |
| Running Slope (%) | 0.3% | 0.3% | 0.0% | 0.1% | 0.4% | | | | | | | | |
| Cross Slope (%) | 0.1% | 0.1% | 0.6% | 0.0% | 0.2% | | | | | | | | |
| Width (in) | 48 | 48 | 48 | 48 | 48 | | | | | | | | |
| Observed Compliance | Y* | Y* | Y* | Y* | Y* | | | | | | | | |
| Gateway (GIS) Compliance | Y | | | | | | | | | | | | |

| SR 202 South - from Mt Lebanon Rd to Garden of Eden Rd | | | | | | | | | | | | | |
|--|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|--|
| Location ID | SW-202-SB-07-A | SW-202-SB-08-A | SW-202-SB-09-A | SW-202-SB-10-A | SW-202-SB-11-A | SW-202-SB-12-A | SW-202-SB-13-A | SW-202-SB-14-A | SW-202-SB-14-B | SW-202-SB-15-A | SW-202-SB-16-A | SW-202-SB-17-A | |
| Running Slope (%) | 0.4% | 1.1% | 1.3% | 0.5% | 3.8% | 0.3% | 0.6% | 1.4% | | 2.9% | 0.3% | 2.1% | |
| Cross Slope (%) | 1.2% | 0.4% | 0.6% | 0.3% | 1.0% | 0.7% | 0.4% | 1.1% | | 3.6% | 1.0% | 1.0% | |
| Width (in) | 60 | 57.5 | 59 | 51 | 53 | 48 | 48 | 48 | | 54 | 50 | 48 | |
| Observed Compliance | Y | Y* | Y | Y* | Y* | Y* | Y* | Y* | | N | Y* | Y* | |
| Gateway (GIS) Compliance | N | N | Y | | Y | | N | Y | | N | N | N | |

| SR 202 South - from Garden of Eden Rd to Righter Pkwy | | | | | | | | | | | | | |
|---|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|--|--|--|--|
| Location ID | SW-202-SB-18-A | SW-202-SB-18-B | SW-202-SB-19-A | SW-202-SB-19-B | SW-202-SB-19-C | SW-202-SB-20-A | SW-202-SB-20-B | SW-202-SB-20-C | SW-202-SB-21-A | | | | |
| Running Slope (%) | 1.5% | 0.2% | 1.0% | 1.1% | 1.3% | 0.5% | 0.7% | 0.1% | 0.6% | | | | |
| Cross Slope (%) | 0.3% | 1.2% | 0.7% | 0.6% | 0.9% | 2.4% | 2.1% | 0.5% | 2.2% | | | | |
| Width (in) | 96 | 95 | 59 | 59.5 | 60 | 59 | 59.5 | 48 | 47 | | | | |
| Observed Compliance | Y | Y | Y | Y | Y | N* | N* | Y* | N* | | | | |
| Gateway (GIS) Compliance | Y | | Y | | | | | | N | | | | |

| SR 202 South - from Righter Pkwy to Rocky Run Pkwy | | | | | | | | | | | | | |
|--|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|
| Location ID | SW-202-SB-22-A | SW-202-SB-22-B | SW-202-SB-22-C | SW-202-SB-22-D | SW-202-SB-23-A | SW-202-SB-24-A | SW-202-SB-25-A | SW-202-SB-25-B | SW-202-SB-25-C | SW-202-SB-26-A | SW-202-SB-26-B | SW-202-SB-27-A | SW-202-SB-28-A |
| Running Slope (%) | 0.8% | 2.8% | 1.5% | | 1.6% | 1.4% | 0.7% | 0.2% | 2.4% | 3.0% | 2.3% | 0.1% | 0.4% |
| Cross Slope (%) | 2.0% | 3.7% | 1.7% | 2.5% | 1.9% | 0.9% | 0.0% | 4.0% | 2.0% | 3.6% | 2.3% | 0.4% | 1.3% |
| Width (in) | 60 | 60 | 60 | 59 | 56 | 60 | 42 | 61 | 62 | 62 | 51 | 55 | 58 |
| Observed Compliance | Y | N | Y | N* | Y* | Y | N | N | Y | N | N* | Y* | Y |
| Gateway (GIS) Compliance | Y | | | | N | N | | | | N | | Y | Y |

SIDEWALK INVENTORY

DeIDOT PAR Agreement 1793, Task Order #3
US Route 202 Pedestrian Access Route Assessment

Sidewalk measurements taken at approximate 100' intervals

Observed Compliance Legend

| | |
|----|---|
| Y | Compliant (cross slope <= 2% and width >= 60") |
| Y* | Compliant (cross slope <= 2% and width between 48-60") |
| N* | Non-Compliant (cross-slope between 2-3% and width >= 48") |
| N | Non-Compliant (cross-slope > 3% or width < 48") |

| SR 202 North - Median Area - from Mt Lebanon Rd to Silverside Rd | | | | | | | | | | | | | | |
|--|-----------------|-----------------|-----------------|-----------------|-----------------|-----------------|-----------------|-----------------|--|--|--|--|--|--|
| Location ID | SW-202-NBI-01-A | SW-202-NBI-02-A | SW-202-NBI-03-A | SW-202-NBI-04-A | SW-202-NBI-05-A | SW-202-NBI-06-A | SW-202-NBI-07-A | SW-202-SBI-10-B | | | | | | |
| Running Slope (%) | 1.9% | 1.3% | 0.8% | 0.3% | 1.3% | 0.8% | 0.2% | 0.5% | | | | | | |
| Cross Slope (%) | 2.4% | 1.4% | 1.8% | 4.2% | 0.0% | 1.7% | 2.3% | 2.3% | | | | | | |
| Width (in) | 62 | 59 | 59 | 78 | 79 | 47 | 97 | 96 | | | | | | |
| Observed Compliance | N* | Y | Y | N | Y | N | N* | N* | | | | | | |
| Gateway (GIS) Compliance | N | Y | Y | Y | Y | N | Y | | | | | | | |

| SR 202 South - Median Area - from Mt Lebanon Rd to Silverside Rd | | | | | | | | | | | | | | | |
|--|-----------------|-----------------|-----------------|-----------------|-----------------|-----------------|-----------------|-----------------|-----------------|-----------------|-----------------|-----------------|-----------------|-----------------|--|
| Location ID | SW-202-SBI-01-A | SW-202-SBI-02-A | SW-202-SBI-02-B | SW-202-SBI-03-A | SW-202-SBI-04-A | SW-202-SBI-05-A | SW-202-SBI-05-B | SW-202-SBI-06-A | SW-202-SBI-07-A | SW-202-SBI-08-A | SW-202-SBI-09-A | SW-202-SBI-09-B | SW-202-SBI-10-A | SW-202-SBI-11-A | |
| Running Slope (%) | 0.9% | 0.4% | 0.1% | 1.5% | 1.3% | 0.9% | 0.8% | 0.8% | 0.7% | 0.1% | 0.2% | 0.4% | 1.1% | 0.0% | |
| Cross Slope (%) | 0.0% | 2.0% | 1.3% | 1.4% | 2.4% | 3.8% | 5.0% | 0.7% | 1.1% | 0.0% | 2.6% | 11.3% | 1.6% | 1.2% | |
| Width (in) | 52 | 58.5 | 59 | 61 | 58 | 59 | 27 | 102 | 103 | 103 | 71 | 72 | 96 | 84 | |
| Observed Compliance | Y* | Y | Y | Y | N* | N | N | Y | Y | Y | N* | N | Y | Y | |
| Gateway (GIS) Compliance | Y | Y | | | N | | | Y | Y | Y | Y | | Y | Y | |

| Mt Lebanon Rd East | | | | | | | | | | | | | | |
|--------------------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|--|--|--|--|--|
| Location ID | SW-MTL-EB-01-D | SW-MTL-EB-01-E | SW-MTL-EB-02-A | SW-MTL-EB-03-A | SW-MTL-EB-03-B | SW-MTL-EB-03-C | SW-MTL-EB-04-A | SW-MTL-EB-05-A | SW-MTL-EB-06-A | | | | | |
| Running Slope (%) | 1.0% | 0.5% | 0.5% | 0.2% | 1.0% | 0.0% | 0.2% | 0.5% | 0.7% | | | | | |
| Cross Slope (%) | 1.5% | 1.2% | 0.8% | 0.3% | 1.8% | 1.7% | 1.9% | 0.8% | 1.0% | | | | | |
| Width (in) | 60 | 60 | 60 | 60 | 60 | 60 | 60 | 60 | 60 | | | | | |
| Observed Compliance | Y | Y | Y | Y | Y | Y | Y | Y | Y | | | | | |
| Gateway (GIS) Compliance | Y | | N | | | | Y | Y | N | | | | | |

| Mt Lebanon Rd West | | | | | | | | | | | | | | |
|--------------------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|--|--|--|--|--|--|--|
| Location ID | SW-MTL-WB-01-C | SW-MTL-WB-02-A | SW-MTL-WB-03-A | SW-MTL-WB-03-B | SW-MTL-WB-04-A | SW-MTL-WB-05-A | SW-MTL-WB-05-B | | | | | | | |
| Running Slope (%) | 1.9% | 0.5% | 0.3% | 0.4% | 0.6% | 0.5% | 0.8% | | | | | | | |
| Cross Slope (%) | 1.0% | 1.9% | 0.9% | 1.1% | 1.2% | 0.6% | 0.8% | | | | | | | |
| Width (in) | 48 | 60 | 60 | 60 | 60 | 60 | 60 | | | | | | | |
| Observed Compliance | Y* | Y | Y | Y | Y | Y | Y | | | | | | | |
| Gateway (GIS) Compliance | N | N | Y | | Y | | | | | | | | | |

SIDEWALK INVENTORY

DeIDOT PAR Agreement 1793, Task Order #3
US Route 202 Pedestrian Access Route Assessment

Sidewalk measurements taken at approximate 100' intervals

Observed Compliance Legend

| | |
|----|---|
| Y | Compliant (cross slope <= 2% and width >= 60") |
| Y* | Compliant (cross slope <= 2% and width between 48-60") |
| N* | Non-Compliant (cross-slope between 2-3% and width >= 48") |
| N | Non-Compliant (cross-slope > 3% or width < 48") |

| Garden of Eden Road East | | | | | | | | | | | | | |
|----------------------------|----------------|----------------|----------------|--|--|--|--|--|--|--|--|--|--|
| Location ID | SW-GAR-EB-01-A | SW-GAR-EB-01-B | SW-GAR-EB-02-A | | | | | | | | | | |
| Running Slope (%) | 1.5% | 0.6% | 1.8% | | | | | | | | | | |
| Cross Slope (%) | 1.2% | 2.1% | 1.0% | | | | | | | | | | |
| Width (in) | 48 | 48 | 41 | | | | | | | | | | |
| Observed Compliance | Y* | N* | N | | | | | | | | | | |
| Gateway (GIS) Compliance | N | | Y | | | | | | | | | | |

| Garden of Eden Road West | | | | | | | | | | | | | |
|----------------------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|--|--|--|--|--|--|
| Location ID | SW-GAR-WB-01-A | SW-GAR-WB-02-A | SW-GAR-WB-03-A | SW-GAR-WB-04-A | SW-GAR-WB-05-A | SW-GAR-WB-05-B | SW-GAR-WB-06-A | | | | | | |
| Running Slope (%) | 4.4% | 2.1% | 2.1% | 1.5% | 0.4% | 0.4% | 0.2% | | | | | | |
| Cross Slope (%) | 0.3% | 0.6% | 0.1% | 0.8% | 0.4% | 0.6% | 1.0% | | | | | | |
| Width (in) | 48 | 58 | 60 | 60 | 60 | 60 | 96 | | | | | | |
| Observed Compliance | Y* | Y | Y | Y | Y | Y | Y | | | | | | |
| Gateway (GIS) Compliance | | | Y | | Y | | Y | | | | | | |

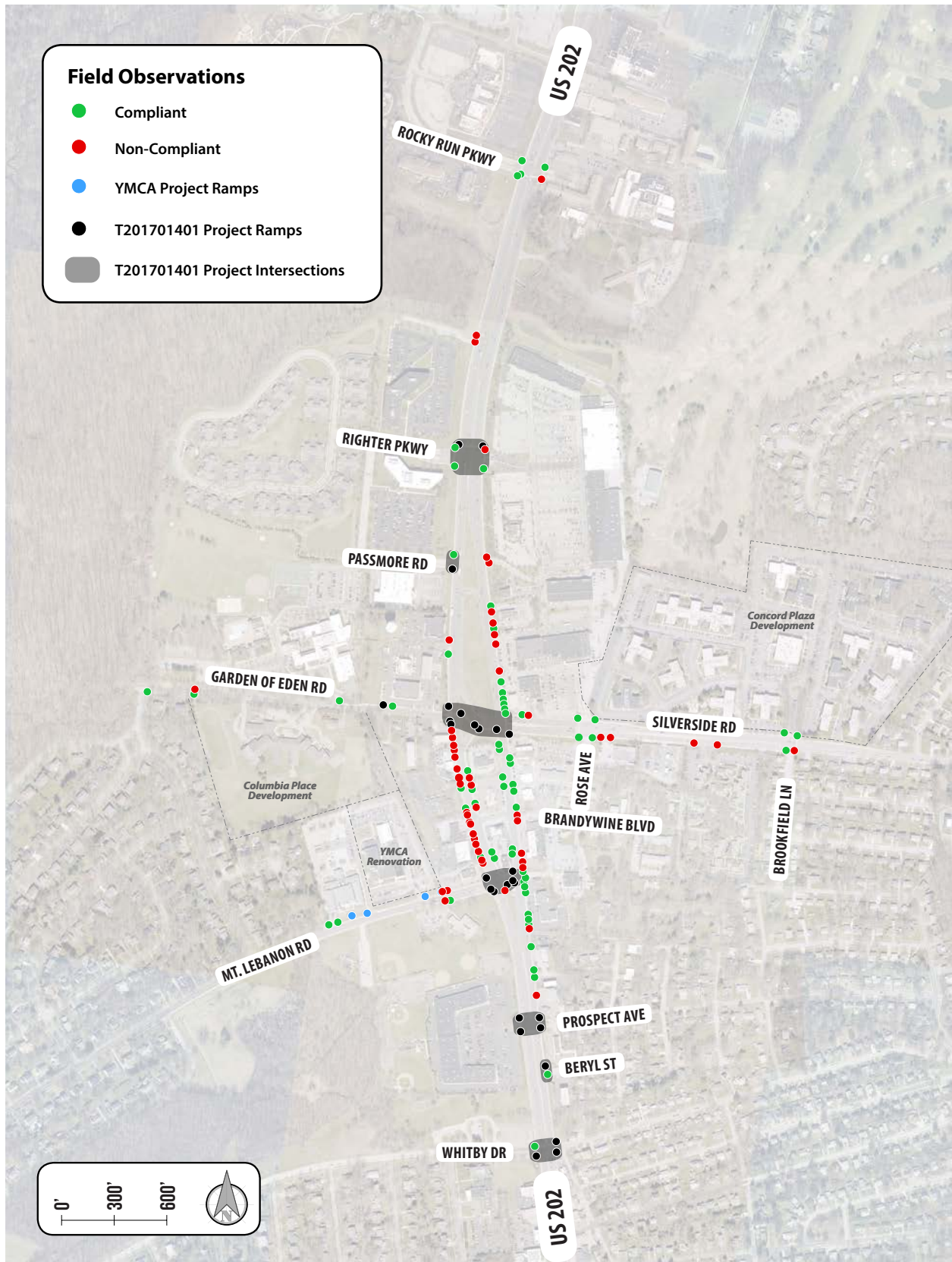
| Silverside Road East | | | | | | | | | | | | | | | | |
|----------------------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|--|
| Location ID | SW-SIL-EB-01-A | SW-SIL-EB-02-A | SW-SIL-EB-02-B | SW-SIL-EB-02-C | SW-SIL-EB-03-A | SW-SIL-EB-04-A | SW-SIL-EB-05-A | SW-SIL-EB-06-A | SW-SIL-EB-06-B | SW-SIL-EB-06-C | SW-SIL-EB-07-A | SW-SIL-EB-07-B | SW-SIL-EB-07-C | SW-SIL-EB-08-A | SW-SIL-EB-09-A | |
| Running Slope (%) | 1.7% | 1.8% | 0.9% | 1.5% | 2.0% | 0.9% | 0.8% | - | 1.0% | 0.4% | 0.4% | 0.0% | 0.2% | 0.6% | 0.4% | |
| Cross Slope (%) | 2.7% | 1.1% | 1.7% | 1.6% | 0.6% | 1.0% | 0.7% | 2.9% | 1.2% | 2.4% | 3.2% | 2.9% | 2.9% | 1.3% | 0.7% | |
| Width (in) | 96 | 96 | 96 | 96 | 99 | 98 | 96 | 96 | 108 | 96 | 96 | 97 | 98 | 48 | 48 | |
| Observed Compliance | N* | Y | Y | Y | Y | Y | Y | N* | Y | N* | N | N* | N* | Y* | Y* | |
| Gateway (GIS) Compliance | N | N | | | N | | | N | | | N | | | N | Y | |

| Silverside Road West | | | | | | | | | | | | | |
|----------------------------|----------------|----------------|----------------|----------------|----------------|----------------|----------------|--|--|--|--|--|--|
| Location ID | SW-SIL-WB-01-A | SW-SIL-WB-02-A | SW-SIL-WB-02-B | SW-SIL-WB-04-A | SW-SIL-WB-04-B | SW-SIL-WB-05-A | SW-SIL-WB-05-B | | | | | | |
| Running Slope (%) | 1.6% | 1.8% | 0.8% | 0.3% | 0.1% | 0.2% | 0.7% | | | | | | |
| Cross Slope (%) | 1.1% | 2.4% | 2.4% | 1.2% | 1.3% | 2.2% | 2.5% | | | | | | |
| Width (in) | 79 | 46 | 46 | 58 | 60 | 50 | 48 | | | | | | |
| Observed Compliance | Y | N | N | Y | Y | N* | N* | | | | | | |
| Gateway (GIS) Compliance | Y | N | | | | | | | | | | | |

APPENDIX E

CURB RAMP INVENTORY & ANALYSIS

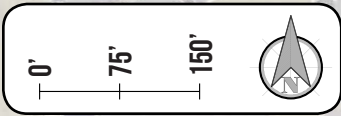
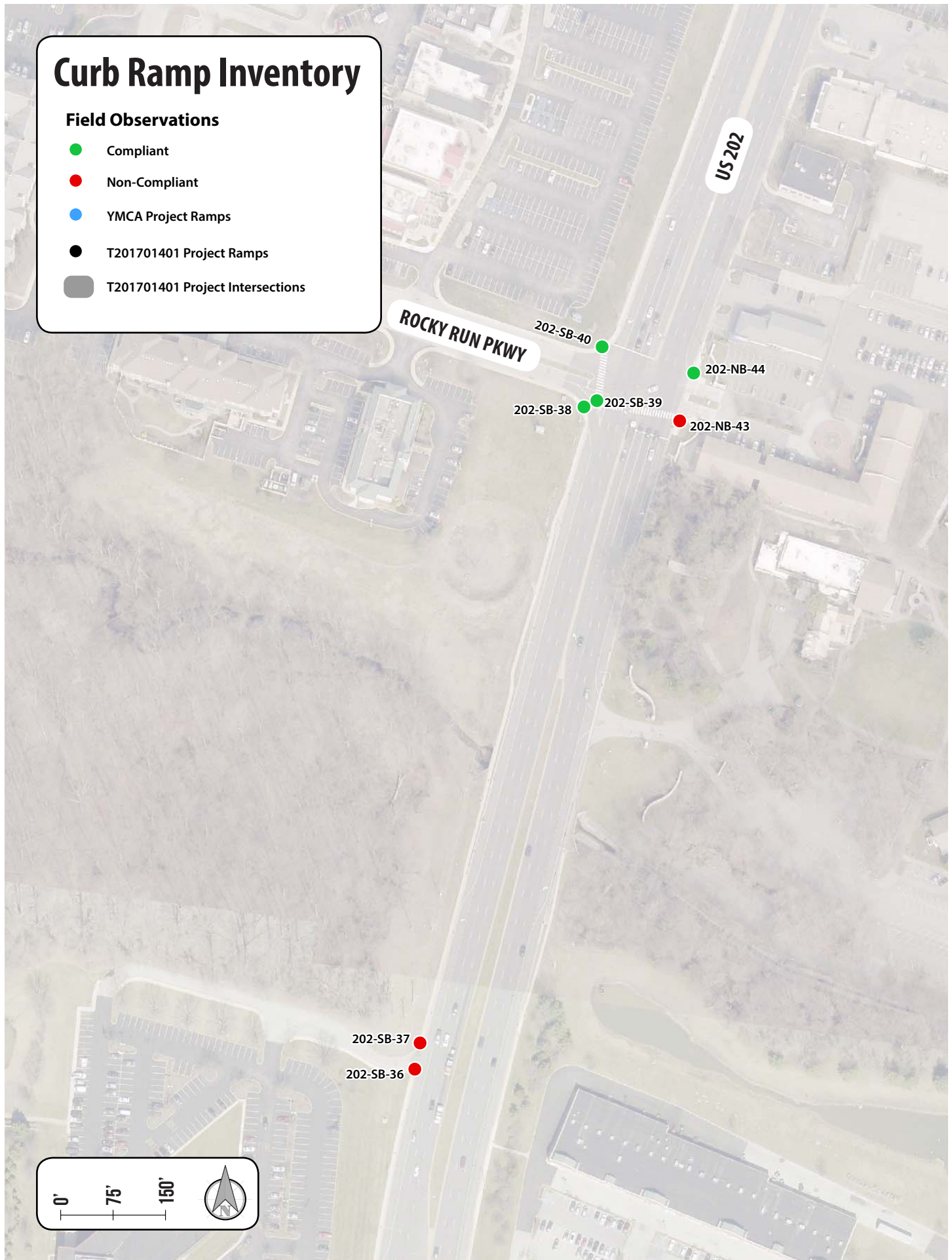
Curb Ramp Inventory



Curb Ramp Inventory

Field Observations

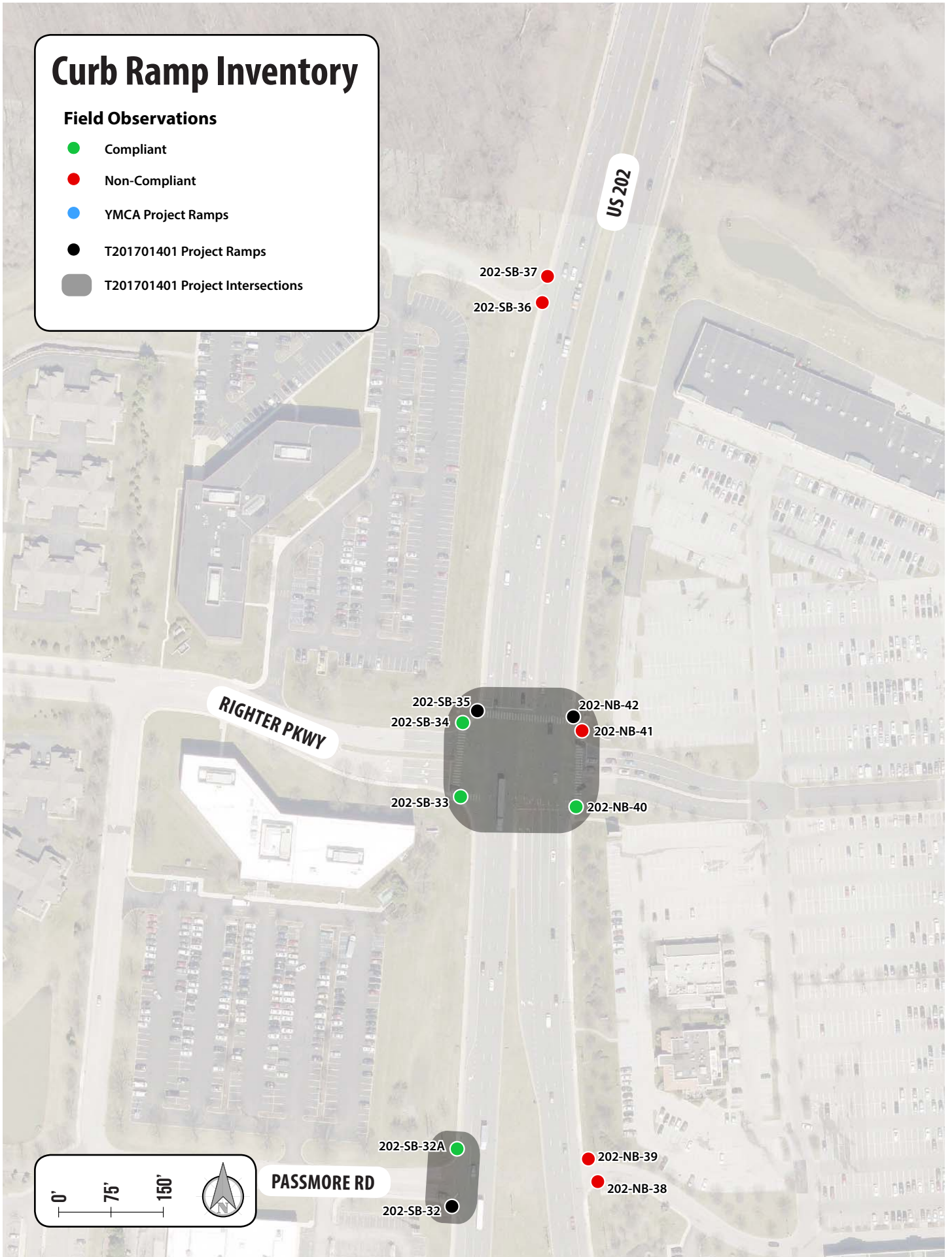
- Compliant
- Non-Compliant
- YMCA Project Ramps
- T201701401 Project Ramps
- T201701401 Project Intersections



Curb Ramp Inventory

Field Observations

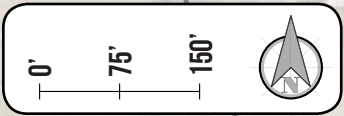
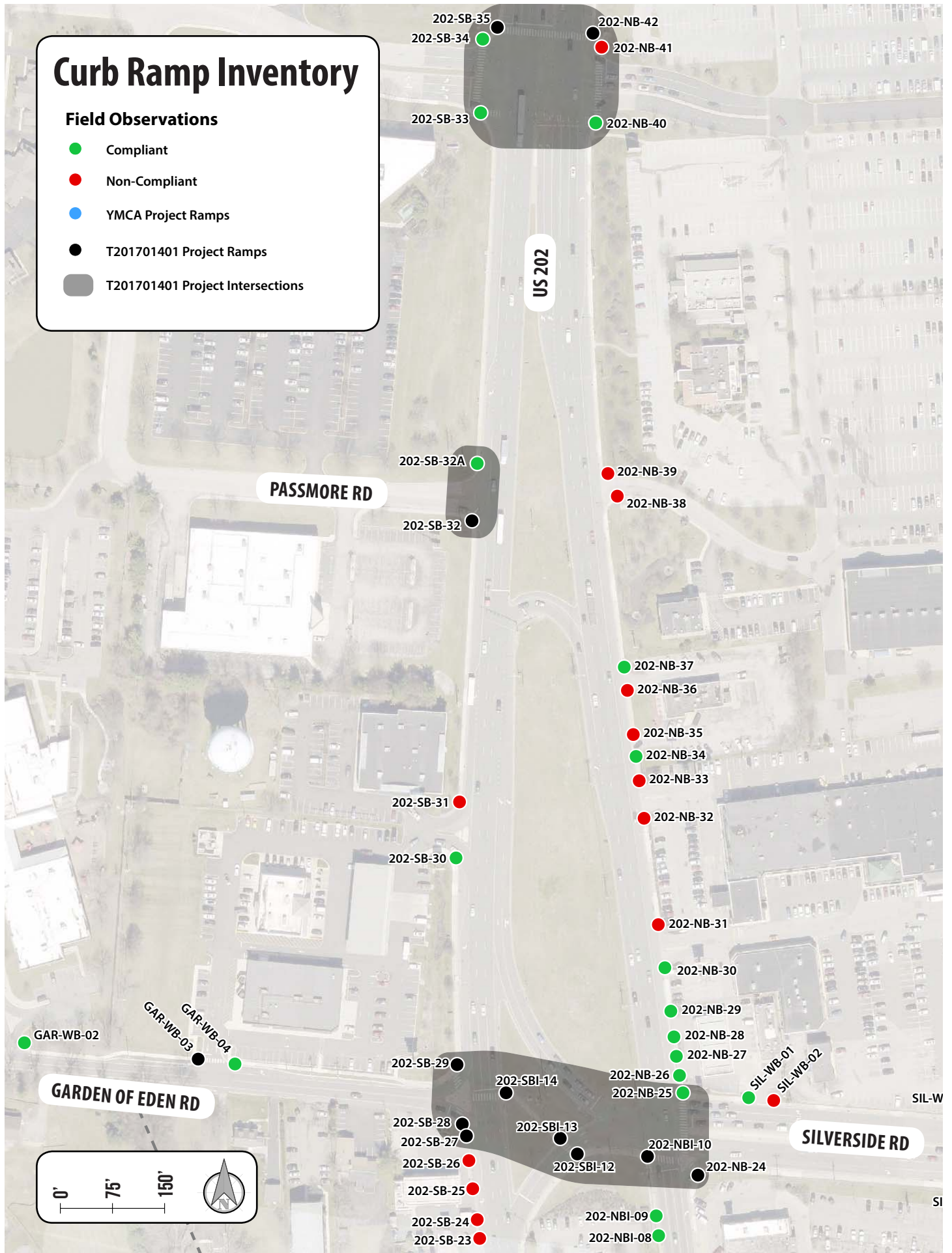
- Compliant
- Non-Compliant
- YMCA Project Ramps
- T201701401 Project Ramps
- T201701401 Project Intersections



Curb Ramp Inventory

Field Observations

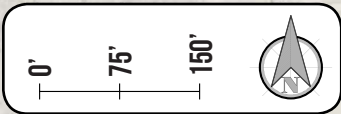
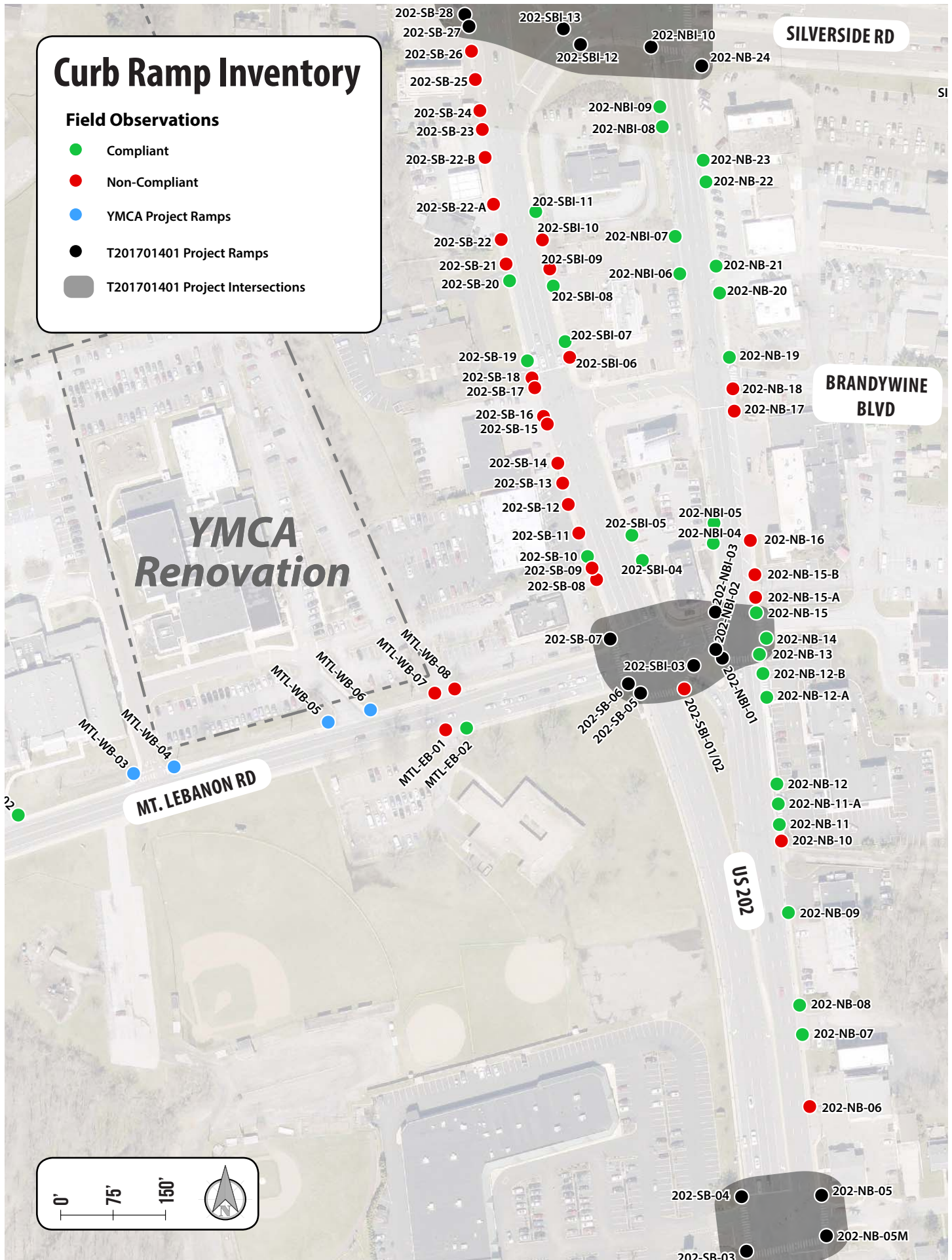
- Compliant
- Non-Compliant
- YMCA Project Ramps
- T201701401 Project Ramps
- T201701401 Project Intersections



Curb Ramp Inventory

Field Observations

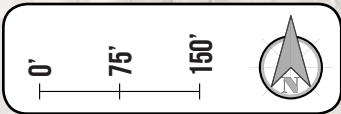
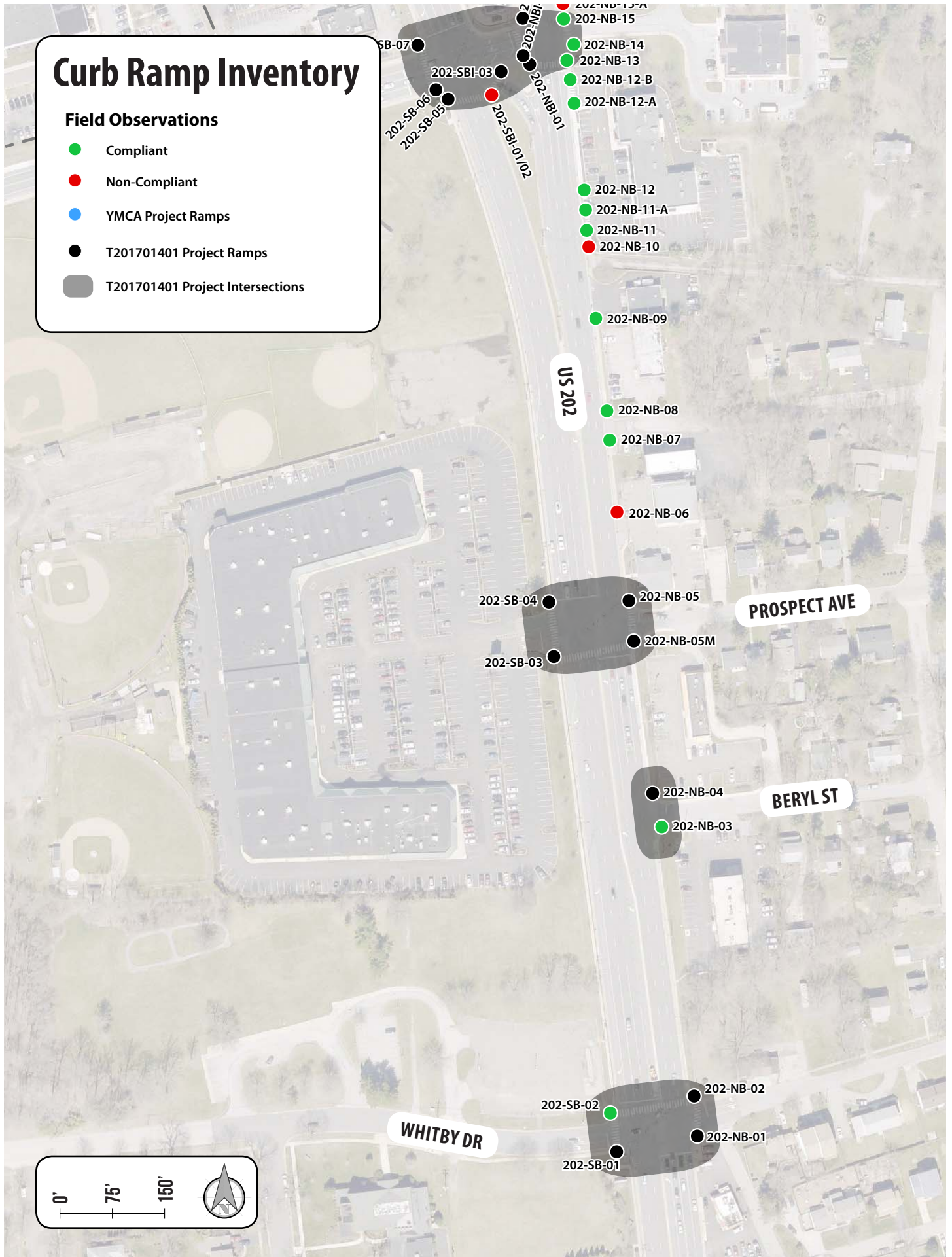
- Compliant
- Non-Compliant
- YMCA Project Ramps
- T201701401 Project Ramps
- T201701401 Project Intersections



Curb Ramp Inventory

Field Observations

- Compliant
- Non-Compliant
- YMCA Project Ramps
- T201701401 Project Ramps
- T201701401 Project Intersections



Curb Ramp Inventory

Field Observations

- Compliant
- Non-Compliant
- YMCA Project Ramps
- T201701401 Project Ramps
- T201701401 Project Intersections

US 202

- 202-SB-28
- 202-SB-27
- 202-SB-26
- 202-SB-25
- 202-SB-24
- 202-SB-23
- 202-SB-22-B
- 202-SB-22
- 202-SB-
- 202-SB
- 202-SB

GARDEN OF EDEN RD

Columbia Place
Development

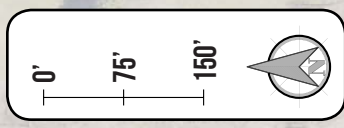
GAR-WB-04
GAR-WB-03

GAR-WB-02

GAR-WB-01
GAR-EB-02

GAR-EB-01

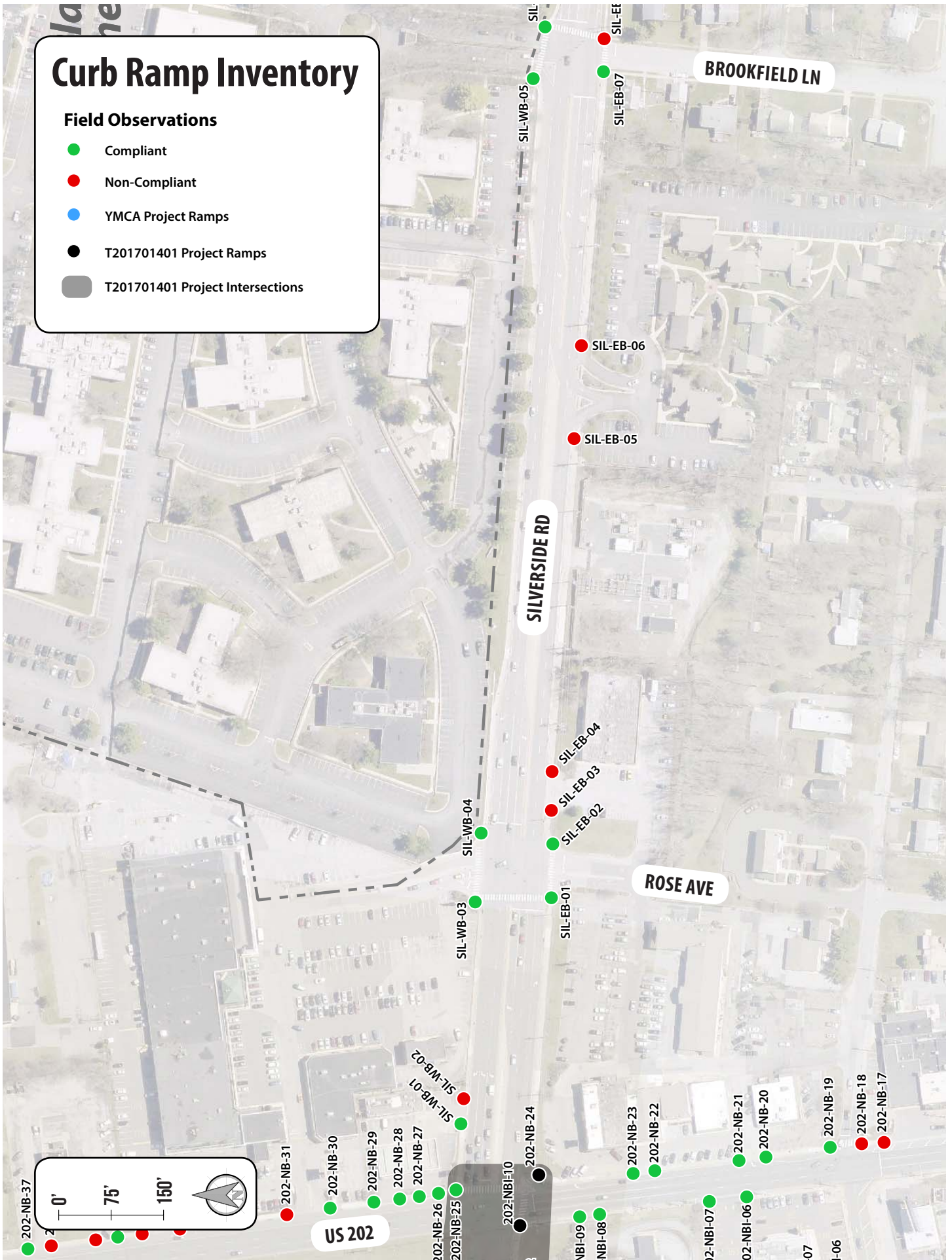
TRAILHEAD



Curb Ramp Inventory

Field Observations

- Compliant
- Non-Compliant
- YMCA Project Ramps
- T201701401 Project Ramps
- T201701401 Project Intersections



CURB RAMP INVENTORY

DeIDOT PAR Agreement 1793, Task Order #3

US Route 202 Pedestrian Access Route Assessment

| Curb Ramp Inventory | | | | |
|---------------------|--------------------|--------------|----------|---|
| Ramp ID | Type | Compliance | | Notes |
| | | 2012 Gateway | Observed | |
| 202-NB-01 | Type 1 (BY OTHERS) | N | | Included with T201701401 |
| 202-NB-02 | Type 2 (BY OTHERS) | N | | Included with T201701401 |
| 202-NB-03 | Type 2 | Y | Y | |
| 202-NB-04 | Type 2 (BY OTHERS) | N | | Included with T201701401 |
| 202-NB-05 | Type 2 (BY OTHERS) | N | | Included with T201701401 |
| 202-NB-05M | Type 2 (BY OTHERS) | N | | Included with T201701401 |
| 202-NB-06 | Type 1 | N | N | Stop-controlled w/o DWS |
| 202-NB-07 | Type 1 | N | Y | |
| 202-NB-08 | Type 1 | N | Y | |
| 202-NB-09 | Type 1 | N | Y | |
| 202-NB-10 | Type 1 | N | N | |
| 202-NB-11 | Type 1 | N | Y | Reconstructed w/ Seasons |
| 202-NB-11-A | Type 5 | | Y | Reconstructed w/ Seasons |
| 202-NB-12 | Type 1 | N | Y | Reconstructed w/ Seasons |
| 202-NB-12-A | Type 3 | | Y | Reconstructed w/ Seasons |
| 202-NB-12-B | Type 1 | | Y | |
| 202-NB-13 | Type 2 | Y | Y | |
| 202-NB-14 | Type 2 | N | Y | |
| 202-NB-15 | Type 1 | N | Y | |
| 202-NB-15-A | Type 1 | | N | |
| 202-NB-15-B | Type 1 | | N | |
| 202-NB-16 | Type 1 | N | N | |
| 202-NB-17 | Type 1 | N | N | |
| 202-NB-18 | Type 1 | N | N | |
| 202-NB-19 | Type 1 | N | Y | |
| 202-NB-20 | Type 1 | N | Y | |
| 202-NB-21 | Type 1 | N | Y | |
| 202-NB-22 | Type 1 | N | Y | |
| 202-NB-23 | Type 1 | N | Y | |
| 202-NB-24 | Type 2 (BY OTHERS) | N | | Included with T201701401 |
| 202-NB-25 | Type 2 | N | Y | @ Silverside |
| 202-NB-26 | Type 1 | N | Y | |
| 202-NB-27 | Type 1 | N | Y | |
| 202-NB-28 | Type 1 | N | Y | |
| 202-NB-29 | Type 1 | N | Y | |
| 202-NB-30 | Type 1 | N | Y | |
| 202-NB-31 | Type 1 | N | N | |
| 202-NB-32 | Type 1 | N | N | Candidate for DWS |
| 202-NB-33 | Type 1 | N | N | Candidate for DWS |
| 202-NB-34 | Type 1 | N | Y | |
| 202-NB-35 | Type 1 | N | N | |
| 202-NB-36 | Type 1 | N | N | |
| 202-NB-37 | Type 1 | N | Y | |
| 202-NB-38 | Type 2 | N | N | Major driveway w/o DWS |
| 202-NB-39 | Type 2 | N | N | Major driveway w/o DWS |
| 202-NB-40 | Type 1 | N | Y | @ Righter Parkway |
| 202-NB-41 | Type 2 | N | N | @ Righter - DWS is settling |
| 202-NB-42 | Type 1 (BY OTHERS) | Y | | Included with T201701401 |
| 202-NB-43 | Type 2 | N | N | @ Rocky Run - Further Evaluation Needed |
| 202-NB-44 | Type 2 | N | Y | @ Rocky Run |
| 202-NBI-01 | Type 5 (BY OTHERS) | N | | Included with T201701401 |
| 202-NBI-02 | Type 5 (BY OTHERS) | N | | Included with T201701401 |
| 202-NBI-03 | Type 2 (BY OTHERS) | Y | | Included with T201701401 |
| 202-NBI-04 | Type 3 | Y | Y | Reconstructed w/ Taco Bell |

CURB RAMP INVENTORY

DeIDOT PAR Agreement 1793, Task Order #3

US Route 202 Pedestrian Access Route Assessment

| Curb Ramp Inventory | | | | |
|----------------------------|--------------------|---------------------|-----------------|---------------------------|
| Ramp ID | Type | Compliance | | Notes |
| | | 2012 Gateway | Observed | |
| 202-NBI-05 | Type 1 | Y | Y | |
| 202-NBI-06 | Type 1 | N | Y | |
| 202-NBI-07 | Type 1 | N | Y | |
| 202-NBI-08 | Type 1 | N | Y | |
| 202-NBI-09 | Type 1 | N | Y | |
| 202-NBI-10 | Type 2 (BY OTHERS) | N | | Included with T201701401 |
| 202-SB-01 | Type 2 (BY OTHERS) | N | | Included with T201701401 |
| 202-SB-02 | Type 1 | N | Y | @ Whitby Drive |
| 202-SB-03 | Type 1 (BY OTHERS) | N | | |
| 202-SB-04 | Type 1 (BY OTHERS) | Y | | Included with T201701401 |
| 202-SB-05 | Type 2 (BY OTHERS) | Y | | Included with T201701401 |
| 202-SB-06 | Type 2 (BY OTHERS) | Y | | Included with T201701401 |
| 202-SB-07 | Type 2 (BY OTHERS) | N | | Included with T201701401 |
| 202-SB-08 | Type 1 | N | N | |
| 202-SB-09 | Type 1 | N | N | |
| 202-SB-10 | Type 1 | N | Y | |
| 202-SB-11 | Type 1 | N | N | |
| 202-SB-12 | Type 1 | N | N | |
| 202-SB-13 | Type 1 | N | N | |
| 202-SB-14 | Type 1 | N | N | |
| 202-SB-15 | Type 1 | N | N | |
| 202-SB-16 | Type 1 | N | N | |
| 202-SB-17 | Type 1 | N | N | |
| 202-SB-18 | Type 1 | N | N | |
| 202-SB-19 | Type 1 | N | Y | |
| 202-SB-20 | Type 1 | N | Y | |
| 202-SB-21 | Type 1 | N | N | |
| 202-SB-22 | Type 1 | N | N | |
| 202-SB-22-A | Type 1 | | N | |
| 202-SB-22-B | Type 1 | | N | |
| 202-SB-23 | Type 1 | N | N | |
| 202-SB-24 | Type 1 | N | N | |
| 202-SB-25 | Type 1 | N | N | |
| 202-SB-26 | Type 1 | N | N | |
| 202-SB-27 | Type 2 (BY OTHERS) | N | | Included with T201701401 |
| 202-SB-28 | Type 2 (BY OTHERS) | N | | Included with T201701401 |
| 202-SB-29 | Type 2 (BY OTHERS) | Y | | Included with T201701401 |
| 202-SB-30 | Type 2 | N | Y | Reconstructed w/ Wawa |
| 202-SB-31 | Type 1 | N | N | Stop-controlled w/o DWS |
| 202-SB-32 | Type 2(BY OTHERS) | Y | Y | Reconstructed w/ Passmore |
| 202-SB-32A | Type 1 | | Y | |
| 202-SB-33 | Type 2 | N | Y | @ Righter |
| 202-SB-34 | Type 2 | N | Y | @ Righter |
| 202-SB-35 | Type 2 (BY OTHERS) | N | | Included with T201701401 |
| 202-SB-36 | Type 3 | N | N | Major driveway w/o DWS |
| 202-SB-37 | Type 1 | N | N | |
| 202-SB-38 | Type 2 | N | Y | @ Rocky Run |
| 202-SB-39 | Type 5 | Y | Y | @ Rocky Run |
| 202-SB-40 | Type 2 | | Y | @ Rocky Run |
| 202-SBI-01 | Type 5 | N | N | |
| 202-SBI-02 | Type 5 | N | N | |
| 202-SBI-03 | Type 5 (BY OTHERS) | Y | | Included with T201701401 |
| 202-SBI-04 | Type 1 | Y | Y | |
| 202-SBI-05 | Type 1 | N | Y | |

CURB RAMP INVENTORY

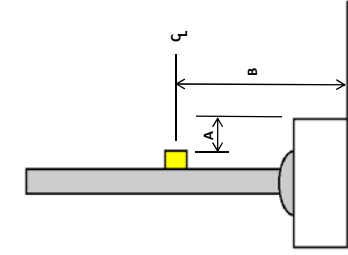
DeIDOT PAR Agreement 1793, Task Order #3

US Route 202 Pedestrian Access Route Assessment

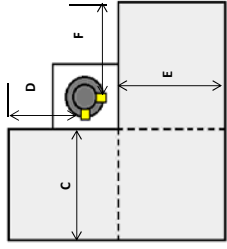
| Curb Ramp Inventory | | | | |
|----------------------------|--------------------|---------------------|-----------------|-------------------------------|
| Ramp ID | Type | Compliance | | Notes |
| | | 2012 Gateway | Observed | |
| 202-SBI-06 | Type 1 | N | N | |
| 202-SBI-07 | Type 1 | N | Y | |
| 202-SBI-08 | Type 1 | N | Y | Candidate for DWS |
| 202-SBI-09 | Type 1 | N | N | Candidate for DWS |
| 202-SBI-10 | Type 1 | N | N | Candidate for DWS |
| 202-SBI-11 | Type 1 | N | Y | Candidate for DWS |
| 202-SBI-12 | Type 2 (BY OTHERS) | N | | Included with T201701401 |
| 202-SBI-13 | Type 1 (BY OTHERS) | N | | Included with T201701401 |
| 202-SBI-14 | Type 1 (BY OTHERS) | Y | | Included with T201701401 |
| GAR-EB-01 | Type 1 | Y | Y | |
| GAR-EB-02 | Type 1 | Y | Y | Need to clear brush |
| GAR-WB-01 | Type 1 | Y | N | Drop-off on both sides |
| GAR-WB-02 | Type 1 | Y | Y | |
| GAR-WB-03 | Type 3 (BY OTHERS) | Y | | Included with T201701401 |
| GAR-WB-04 | Type 3 | N | Y | Reconstructed w/ Wawa |
| MTL-EB-01 | Type 1 | N | N | Candidate for DWS |
| MTL-EB-02 | Type 1 | N | Y | Candidate for DWS |
| MTL-WB-01 | Type 1 | Y | Y | |
| MTL-WB-02 | Type 2 | Y | Y | |
| MTL-WB-03 | Type 1 (BY OTHERS) | Y | | |
| MTL-WB-04 | Type 1 (BY OTHERS) | Y | | |
| MTL-WB-05 | Type 1 (BY OTHERS) | Y | | |
| MTL-WB-06 | Type 1 (BY OTHERS) | Y | | |
| MTL-WB-07 | Type 1 | N | N | Stop-controlled w/o DWS |
| MTL-WB-08 | Type 1 | N | N | Stop-controlled w/o DWS |
| SIL-EB-01 | Type 2 | Y | Y | |
| SIL-EB-02 | Type 2 | Y | Y | |
| SIL-EB-03 | Type 2 | N | N | Candidate for DWS |
| SIL-EB-04 | Type 2 | N | N | Candidate for DWS |
| SIL-EB-05 | Type 2 | N | N | Stop-controlled w/o DWS |
| SIL-EB-05-A | Type 5 | | Y | |
| SIL-EB-05-B | Type 5 | | Y | |
| SIL-EB-05-C | Type 5 | | Y | |
| SIL-EB-05-D | Type 5 | | Y | |
| SIL-EB-06 | Type 2 | N | N | Stop-controlled w/o DWS |
| SIL-EB-07 | Type 2 | Y | Y | |
| SIL-EB-08 | Type 2 | N | N | Deterioration in front of DWS |
| SIL-WB-01 | Type 1 | N | Y | |
| SIL-WB-02 | Type 1 | N | N | |
| SIL-WB-03 | Type 2 | Y | Y | |
| SIL-WB-04 | Type 2 | Y | Y | |
| SIL-WB-05 | Type 2 | N | Y | Landing < 48" |
| SIL-WB-06 | Type 2 | Y | Y | |

APPENDIX F

TRAFFIC SIGNAL INVENTORY & ANALYSIS

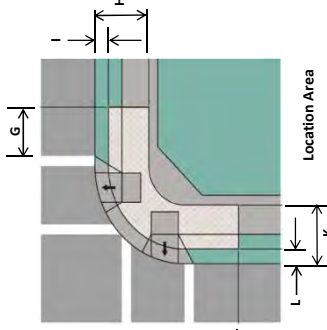


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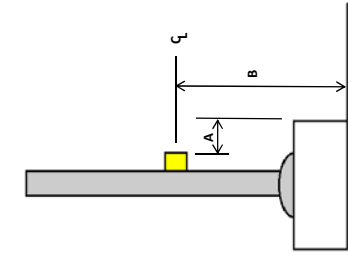
Reach & Height

Landing Area

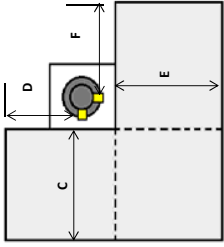


Location Area

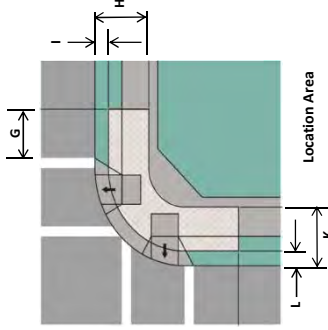
| Date | Street/Corner & Block | Loc. | Dir. | No. of Buttons | Buttons | Reach & Height | | | | Landing Area | | | | Location Area | | | | Comments | | | |
|-----------|----------------------------------|--------|------|----------------|---------|----------------|-------------|--------|--------|--------------|---|--------|--------|---------------|--------|--------|------|----------|------|------|--|
| | | | | | | East-West | North-South | A | B | C | D | E | F | G | H | I | J | | K | L | |
| | | | | | | Inches | Inches | Inches | Inches | Inches | % | Inches | Inches | % | Inches | Inches | Feet | Feet | Feet | Feet | |
| 1/22/2018 | 202/Florence-Whitby 202-NB-01 | Corner | S.E. | Yes | 1 | <10 | 42.00 | >10 | 42.00 | | | | | | | | | | | | 12' from Face of curb, no arrow indications |
| 1/22/2018 | 202/Florence-Whitby 202-NB-02 | Corner | N.E. | Yes | 2 | <10 | 42.00 | <10 | 42.00 | | | | | | | | | | | | No arrow indications on push buttons or sign |
| 1/22/2018 | 202/Florence-Whitby 202-SB-01 | Corner | S.W. | Yes | 2 | <10 | 42.00 | <10 | 42.00 | | | | | | | | | | | | No arrow indications on push buttons or sign |
| 1/22/2018 | 202/Florence-Whitby 202-SB-02 | Corner | N.W. | Yes | 2 | <10 | 42.00 | <10 | 42.00 | | | | | | | | | | | | No arrow indications on push buttons or sign |
| 1/22/2018 | 202/Prospect 202-NB-05M | Corner | S.E. | Yes | 2 | >10 | 42.00 | <10 | 42.00 | | | | | | | | | | | | |
| 1/22/2018 | 202/Prospect 202-NB-05 | Corner | N.E. | Yes | 1 | <10 | 42.00 | <10 | 42.00 | | | | | | | | | | | | |
| 1/22/2018 | 202/Prospect 202-SB-03 | Corner | S.W. | Yes | 2 | <10 | 42.00 | <10 | 42.00 | | | | | | | | | | | | |
| 1/22/2018 | 202/Prospect 202-SB-04 | Corner | N.W. | Yes | 1 | <10 | 42.00 | <10 | 42.00 | | | | | | | | | | | | |
| 1/22/2018 | 202/Mount Lebanon 202-NB-13 | Corner | S.E. | Yes | 1 | <10 | 42.00 | <10 | 42.00 | | | | | | | | | | | | |
| 1/22/2018 | 202/Mount Lebanon 202-NB1-01 | Corner | S.E. | Yes | 1 | <10 | 42.00 | <10 | 42.00 | | | | | | | | | | | | |
| 1/22/2018 | 202/Mount Lebanon 202-NB1-02 | Corner | S.E. | Yes | 1 | <10 | 42.00 | <10 | 42.00 | | | | | | | | | | | | |
| 1/22/2018 | 202/Mount Lebanon 202-NB1-03 | Corner | S.E. | Yes | 1 | <10 | 42.00 | <10 | 42.00 | | | | | | | | | | | | |
| 1/22/2018 | 202/Mount Lebanon 202-SB1-01 | Corner | S.W. | Yes | 2 | <10 | 38.00 | <10 | 38.00 | | | | | | | | | | | | |
| 1/22/2018 | 202/Mount Lebanon 202-SB1-02 | Corner | S.W. | Yes | 1 | <10 | 42.00 | <10 | 42.00 | | | | | | | | | | | | |
| 1/22/2018 | 202/Mount Lebanon 202-SB-05 | Corner | S.W. | Yes | 1 | <10 | 42.00 | <10 | 42.00 | | | | | | | | | | | | |
| 1/22/2018 | 202/Mount Lebanon 202-SB-06 | Corner | S.W. | Yes | 1 | <10 | 44.00 | <10 | 44.00 | | | | | | | | | | | | |
| 1/22/2018 | 202/Mount Lebanon 202-SB-07 | Corner | N.W. | Yes | 1 | <10 | 42.00 | <10 | 42.00 | | | | | | | | | | | | |
| 1/22/2018 | 202/Siverside 202-NB-xx (Island) | Corner | S.E. | Yes | 2 | <10 | 40.00 | <10 | 40.00 | | | | | | | | | | | | Not Adjacent to Landing Area |
| 1/22/2018 | 202/Siverside 202-NB-25 | Corner | N.E. | Yes | 1 | <10 | 42.00 | <10 | 42.00 | | | | | | | | | | | | |
| 1/22/2018 | 202/Siverside 202-NB1-10 | Corner | S.W. | Yes | 1 | <10 | 42.00 | <10 | 42.00 | | | | | | | | | | | | |
| 1/22/2018 | 202/Siverside 202-SB1-12 | Corner | S.W. | Yes | 1 | <10 | 48.00 | <10 | 48.00 | | | | | | | | | | | | |
| 1/22/2018 | 202/Siverside 202-SB1-13 | Corner | S.W. | Yes | 1 | <10 | 45.00 | <10 | 45.00 | | | | | | | | | | | | |
| 1/22/2018 | 202/Siverside 202-SB1-14 | Corner | S.W. | Yes | 1 | <10 | 43.00 | <10 | 43.00 | | | | | | | | | | | | |
| 1/22/2018 | 202/Siverside 202-SB-xx (Island) | Corner | S.E. | Yes | 1 | <10 | 40.00 | <10 | 40.00 | | | | | | | | | | | | |
| 1/22/2018 | 202/Righter 202-NB-40 | Corner | S.E. | Yes | 2 | <10 | 42.00 | <10 | 42.00 | | | | | | | | | | | | |
| 1/22/2018 | 202/Righter 202-NB-41 | Corner | N.E. | Yes | 1 | <10 | 42.00 | <10 | 42.00 | | | | | | | | | | | | |
| 1/22/2018 | 202/Righter 202-NB-42 | Corner | N.E. | Yes | 1 | <10 | 42.00 | <10 | 42.00 | | | | | | | | | | | | |
| 1/22/2018 | 202/Righter 202-SB-33 | Corner | S.W. | Yes | 2 | <10 | 42.00 | <10 | 42.00 | | | | | | | | | | | | |
| 1/22/2018 | 202/Righter 202-SB-34 | Corner | N.W. | Yes | 1 | <10 | 42.00 | <10 | 42.00 | | | | | | | | | | | | |



#####



Landing Area



Location Area

| Date | Street/Corner & Block | Loc. | Dir. | No. of Buttons | Aligned with direction of travel | Reach & Height | | | | Landing Area | | | | Location Area | | | | Comments | |
|-----------|---------------------------------|--------|------|----------------|----------------------------------|----------------|--------|--------|--------|--------------|---|--------|--------|---------------|------|------|------|----------|--|
| | | | | | | A | B | C | D | E | F | G | H | I | J | K | L | | |
| | | | | | | Inches | Inches | Inches | Inches | Inches | % | Inches | Inches | Feet | Feet | Feet | Feet | Feet | Feet |
| 1/22/2018 | 202/Richter 202-SB-35 | Corner | N.W. | Yes | Yes | <10 | 42.00 | | | | | | | | | | | | No landing area |
| 1/22/2018 | 202/Rocky Run 202-NB-43 | Corner | S.E. | Yes | Yes | <10 | 45.00 | | | | | | | | | | | | |
| 1/22/2018 | 202/Rocky Run 202-NB-44 | Corner | N.E. | Yes | Yes | <10 | 45.00 | | | | | | | | | | | | |
| 1/22/2018 | 202/Rocky Run 202-SB-39a | Corner | S.W. | Yes | Yes | <10 | 42.00 | | | | | | | | | | | | |
| 1/22/2018 | 202/Rocky Run 202-SB-39b | Corner | S.W. | Yes | Yes | <10 | 37.00 | | | | | | | | | | | | |
| 1/22/2018 | 202/Rocky Run 202-SB-40 | Corner | N.W. | Yes | Yes | <10 | 42.00 | | | | | | | | | | | | |
| 1/22/2018 | Silverside/Rose SIL-EB-01 | Corner | S.W. | Yes | Yes | <10 | 42.00 | | | | | | | | | | | | Signal under Construction |
| 1/22/2018 | Silverside/Rose SIL-EB-02 | Corner | S.E. | Yes | Yes | <10 | 42.00 | | | | | | | | | | | | Signal under Construction |
| 1/22/2018 | Silverside/Rose SIL-WB-03 | Corner | N.W. | Yes | Yes | <10 | 42.00 | | | | | | | | | | | | Signal under Construction |
| 1/22/2018 | Silverside/Rose SIL-WB-04 | Corner | N.E. | Yes | Yes | <10 | 42.00 | | | | | | | | | | | | Signal under Construction |
| 1/22/2018 | Silverside/Brookfield SIL-EB-07 | Corner | S.W. | Yes | Yes | <10 | 47.00 | | | | | | | | | | | | Signal under Construction |
| 1/22/2018 | Silverside/Brookfield SIL-EB-08 | Corner | S.E. | Yes | Yes | <10 | 42.00 | | | | | | | | | | | | Signal under Construction |
| 1/22/2018 | Silverside/Brookfield SIL-WB-05 | Corner | N.W. | Yes | No | <10 | 42.00 | | | | | | | | | | | | Button not adjacent to landing area - Construction |
| 1/22/2018 | Silverside/Brookfield SIL-WB-06 | Corner | N.E. | Yes | No | <10 | 36.00 | | | | | | | | | | | | No landing area - Construction |

Contract No. _____ ADA Reviewer _____ ADA Coordinator _____ Field Inspector _____

F.A.P. No. _____ ADA Coordinator _____ Project Supervisor _____

US Route 202 (Concord Pike) Rocky Run Parkway to Whitby Drive Pedestrian Access Route Assessment

MAINTENANCE ISSUES AT SIGNALIZED INTERSECTIONS

US 202 & Rocky Run Parkway (N479)

- One of the pushbuttons in the island on the SW corner is missing a directional arrow

US 202 & Righter Parkway (N481)

- Pedestrian clearance times for crossing the northern leg of US 202 should be re-evaluated
- Crosswalk markings for the southern leg are faded

US 202 & Garden of Eden Road/Silverside Road (N102)

- No pedestrian pushbutton sign on pole in larger island that actuates crossing US 202 SB

US 202 & Brandywine Boulevard (N104)

- The eastern leg of the intersection of Brandywine Boulevard and US 202 NB (side street crossing) is not in recall for pedestrians

US 202 & Mount Lebanon Road (N349)

- Most of the pushbuttons are missing directional arrows
- Multiple pushbuttons are not perpendicular to the crosswalk they control
- Different styles of pushbuttons throughout intersection

US 202 & Prospect Ave (N468)

- Pedestrian clearance times for crossing US 202 and the western crosswalk in front of shopping center should be re-evaluated
- Pedestrian signal heads on the SW corner are misaligned (twisted) and not visible from the corresponding crosswalks
- No pedestrian recall for the side street crossings

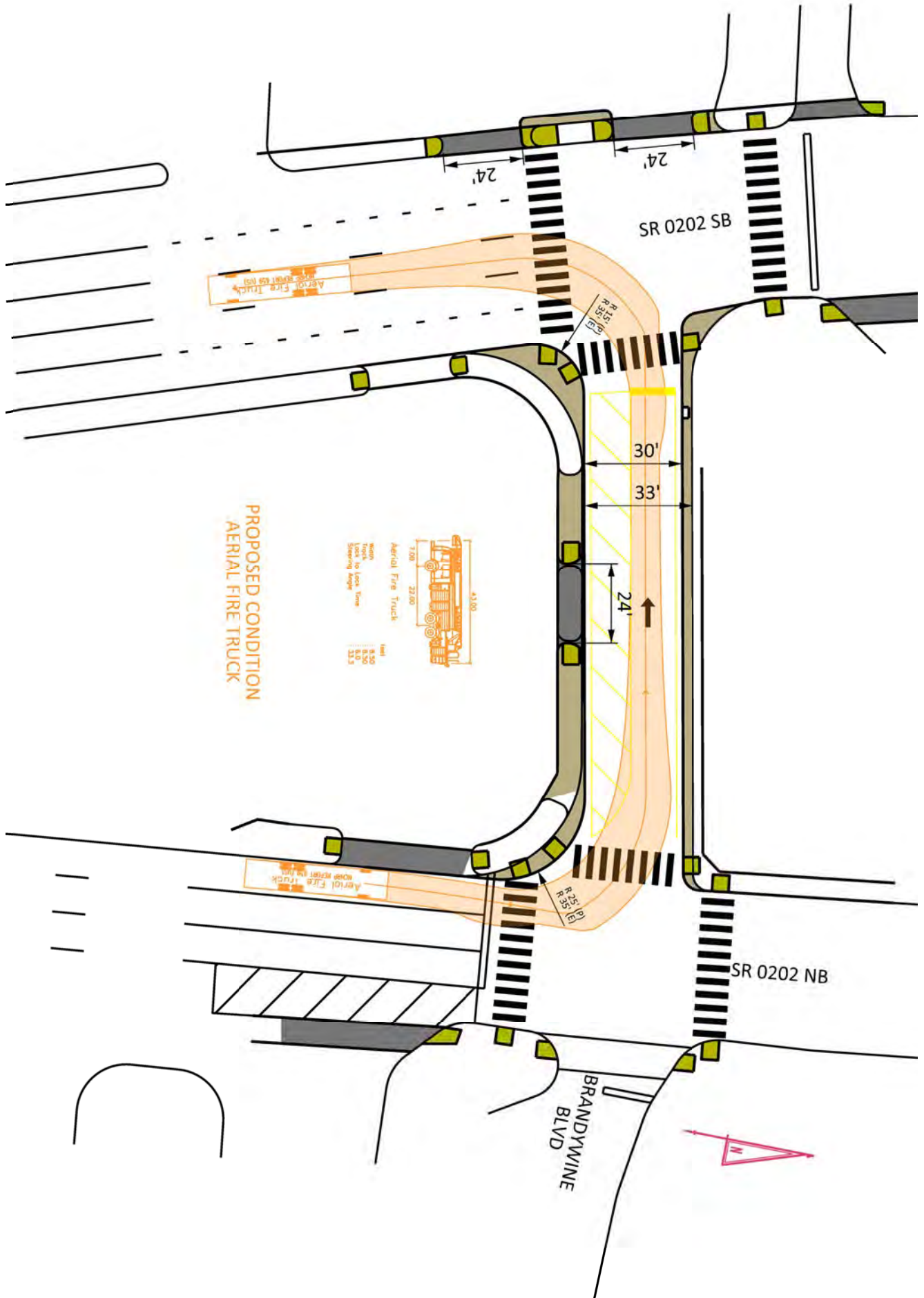
US 202 & Whitby Drive (N327)

- No arrow indications on pushbuttons on NE and NW corners to indicate which crossing the pushbuttons actuate

Silverside Road & Rose Ave (N504)

- No pedestrian recall for the side street crossings

US 202 & Brandywine Blvd Intersection - Autoturn Output



Signal Timing Analysis for Recommended Crosswalks

| Intersection | Leg | Crossing Distance | Required Time | | | Existing Time ** | | | Satisfied |
|-----------------------------|-----|-------------------|---------------|-----|-------|-----------------------|-----|----|-------------|
| | | | W | FDW | Total | AM | MID | PM | |
| US 202/Rocky Run Blvd | N | 126' | 7 | 36 | 43 | 24 | 21 | 22 | No * |
| US 202 (NB)/Brandywine Blvd | All | N - 50' | 7 | 15 | 22 | 24 | 24 | 24 | Yes |
| | | S - 50' | 7 | 15 | 22 | | | | |
| US 202 (SB)/Brandywine Blvd | All | N - 52' | 7 | 15 | 22 | 24 | 24 | 24 | Yes - North |
| | | S - 63' | 7 | 18 | 25 | | | | No - South |
| US 202 (NB)/Mt Lebanon Rd | N | 50' | 7 | 15 | 22 | Separate phase needed | | | No * |
| US 202 (SB)/Mt Lebanon Rd | N | 70' | 7 | 20 | 27 | 33 | 33 | 31 | Yes |
| US 202/Prospect Ave | N | 110' | 7 | 32 | 39 | 18 | 30 | 18 | No * |
| US 202/Whitby Dr | S | 89' | 7 | 26 | 33 | 40 | 40 | 40 | Yes |
| Silverside Rd/Rose Ave | E | 94' | 7 | 27 | 34 | 31 | 31 | 31 | No |
| Silverside Rd/Brookfield Ln | W | 96' | 7 | 28 | 35 | 27 | 27 | 27 | No * |
| | | | | | | <i>Total Split</i> | | | |

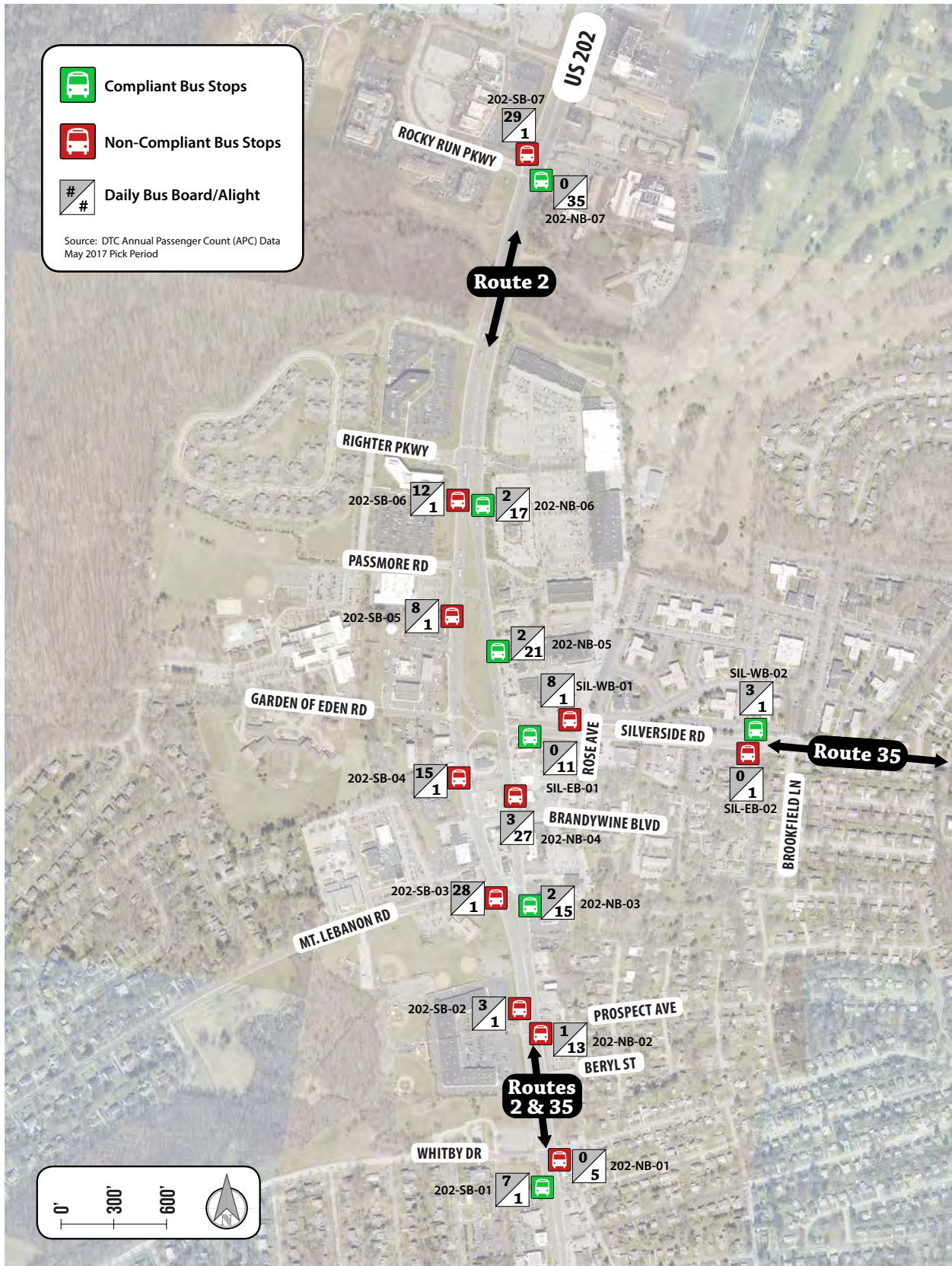
* Further analysis is recommended at these locations

** Existing times shown are for the AM, Midday, and PM peaks based on the timings provided by DeIDOT. This would be the times when modifications to the times would have the greatest impact. Off peak or overnight timings would be impacted the most but at those times the traffic volumes are assumed to be low enough as to not cause significant issues for the occasional pedestrian override.

APPENDIX G

TRANSIT INVENTORY & ANALYSIS

Transit Inventory



Transit Amenities & Ridership

| Location | Bus Stop IDs | Routes | Type | Amenities | Lighting | On | Off |
|-------------------------------|--------------|--------|----------|-----------|----------|----|-----|
| US 202 @ Florence Ave | 202-NB-01 | 2 & 35 | Nearside | None | Yes | 0 | 5 |
| US 202 @ Prospect Ave | 202-NB-02 | 2 & 35 | Nearside | None | Yes | 1 | 13 |
| US 202 @ Seasons Pizza | 202-NB-03 | 2 & 35 | Nearside | Pad | Yes | 2 | 15 |
| US 202 @ Brandywine Blvd | 202-NB-04 | 2 & 35 | Farside | None | Yes | 3 | 27 |
| US 202 @ Silverside Rd | 202-NB-05 | 2 | Midblock | Pad | Yes* | 2 | 21 |
| US 202 @ Concord Sq | 202-NB-06 | 2 | Nearside | Pad | No | 2 | 17 |
| US 202 @ Rocky Run Pkwy | 202-NB-07 | 2 | Nearside | Wide SW | Yes | 0 | 35 |
| US 202 @ Rocky Run Pkwy | 202-SB-07 | 2 | Nearside | None | Yes | 29 | 1 |
| US 202 @ Righter Pkwy | 202-SB-06 | 2 | Farside | Pad | Yes | 12 | 1 |
| US 202 @ Garden of Eden Rd | 202-SB-05 | 2 | Midblock | None | Yes | 8 | 1 |
| US 202 @ Silverside Rd | 202-SB-04 | 2 & 35 | Midblock | None | Yes | 15 | 1 |
| US 202 @ Mt. Lebanon Rd | 202-SB-03 | 2 & 35 | Farside | Shelter | Yes | 28 | 1 |
| US 202 @ Prospect Ave | 202-SB-02 | 2 & 35 | Nearside | None | Yes | 3 | 1 |
| US 202 @ Whitby Rd | 202-SB-01 | 2 & 35 | Farside | Shelter | Yes | 7 | 1 |
| Silverside Rd @ US 202 | SIL-EB-01 | 35 | Farside | Wide SW | No | 0 | 11 |
| Silverside Rd @ the Commons | SIL-EB-02 | 35 | Nearside | Wide SW | No | 0 | 1 |
| Silverside Rd @ Concord Plaza | SIL-WB-02 | 35 | Farside | Shelter | No | 3 | 1 |
| Silverside Rd @ Rose Ave | SIL-WB-01 | 35 | Farside | None | No | 8 | 1 |

* Adequate lighting at stop but not between stop and nearest crosswalk

TRANSIT INVENTORY

DeIDOT PAR Agreement 1793, Task Order #3

US Route 202 Pedestrian Access Route Assessment

| Bus Stop Compliance Analysis | | | | | | | |
|------------------------------|--------------|-------------|-------------|---------------|---|--------------------------|---------------------|
| Bus Stop IDs | Clear Length | Clear Width | Cross Slope | Running Slope | Other Notes | Gateway (GIS) Compliance | Observed Compliance |
| | 96" (MIN) | 60" (MIN) | 2.0% | 5.0% | | | |
| Bus-202-NB-01 | 104.0 | 60.0 | 6.5% | 1.0% | Sidewalk uneven | N | N |
| Bus-202-NB-02 | 48.0 | 60.0 | 0.6% | 0.5% | | N | N |
| Bus-202-NB-03 | 107.0 | 94.0 | 1.0% | 0.2% | | N | Y |
| Bus-202-NB-04 | 54.0 | 60.0 | 2.0% | 1.4% | | N | N |
| Bus-202-NB-05 | 97.0 | 58.0 | 1.3% | 0.2% | Nominally Compliant | N | Y* |
| Bus-202-NB-06 | 96.0 | 61.0 | 1.1% | 0.5% | | Y | Y |
| Bus-202-NB-07 | 96.0 | 60.0 | 0.2% | 0.7% | | N | Y |
| Bus-202-SB-01 | 130.0 | 60.0 | 1.2% | 1.5% | Transit shelter | Y | Y |
| Bus-202-SB-02 | 48.0 | 60.0 | 0.7% | 0.3% | | N | N |
| Bus-202-SB-03 | 96.0 | 60.0 | 3.1% | 0.4% | 1.0" horizontal gap at curb | Y | N |
| Bus-202-SB-04 | 42.0 | 60.0 | 1.3% | 1.4% | Rocks and vegetation in the way. Consider relocating bus stop closer to Brandywine Blvd | N | N |
| Bus-202-SB-05 | 59.0 | 60.0 | 0.6% | 1.0% | | N | N |
| Bus-202-SB-06 | 96.0 | 61.0 | 1.2% | 0.1% | 1" vertical elevation difference was noted between the bus pad and the sidewalk | Y | N |
| Bus-202-SB-07 | 58.0 | 60.0 | 0.6% | 0.1% | | N | N |
| Bus-SIL-EB-01 | 96.0 | 60.0 | 1.3% | 1.8% | | N | Y |
| Bus-SIL-EB-02 | 95.0 | 60.0 | 2.7% | 0.0% | | N | N |
| Bus-SIL-WB-01 | 46.0 | 60.0 | 2.1% | 0.8% | | N | N |
| Bus-SIL-WB-02 | 106.0 | 108.0 | 1.1% | 0.3% | Transit shelter | Y | Y |

DTC Bus Stop Facilities Matrix (Draft, April 2018)

Appendix G

Bus Stop Facilities Matrix

| Transit Density | Average Daily Boarding's | Facilities |
|-----------------|--------------------------|--|
| High | 40 | Shelter, trash receptacle, phone |
| | 20 | Bench (18 inches from PAR), trash receptacle |
| Moderate | 20 | Shelter, trash receptacle |
| | 10 | Bench (18 inches from PAR) |
| Low | 10 | Shelter |
| | 5 | Bench (18 inches from PAR) |

All DTC facilities must provide a pedestrian accessible route (PAR) 60 inches wide for the full length of the bus stop that connects one of more pedestrian access routes to the closest intersection or pedestrian destination. All facilities are to be located beyond the PAR and provide space for a bench that is 18 inches beyond the PAR. A DelDOT Full Compliance Waiver is required for a PAR less than 60 inches wide and locating a bench less than 18 inches from the PAR.

The size of pad or shelter to be installed is based upon peak hour boarding's as an indicator of the number of passengers who may be waiting at the stop at any one time:

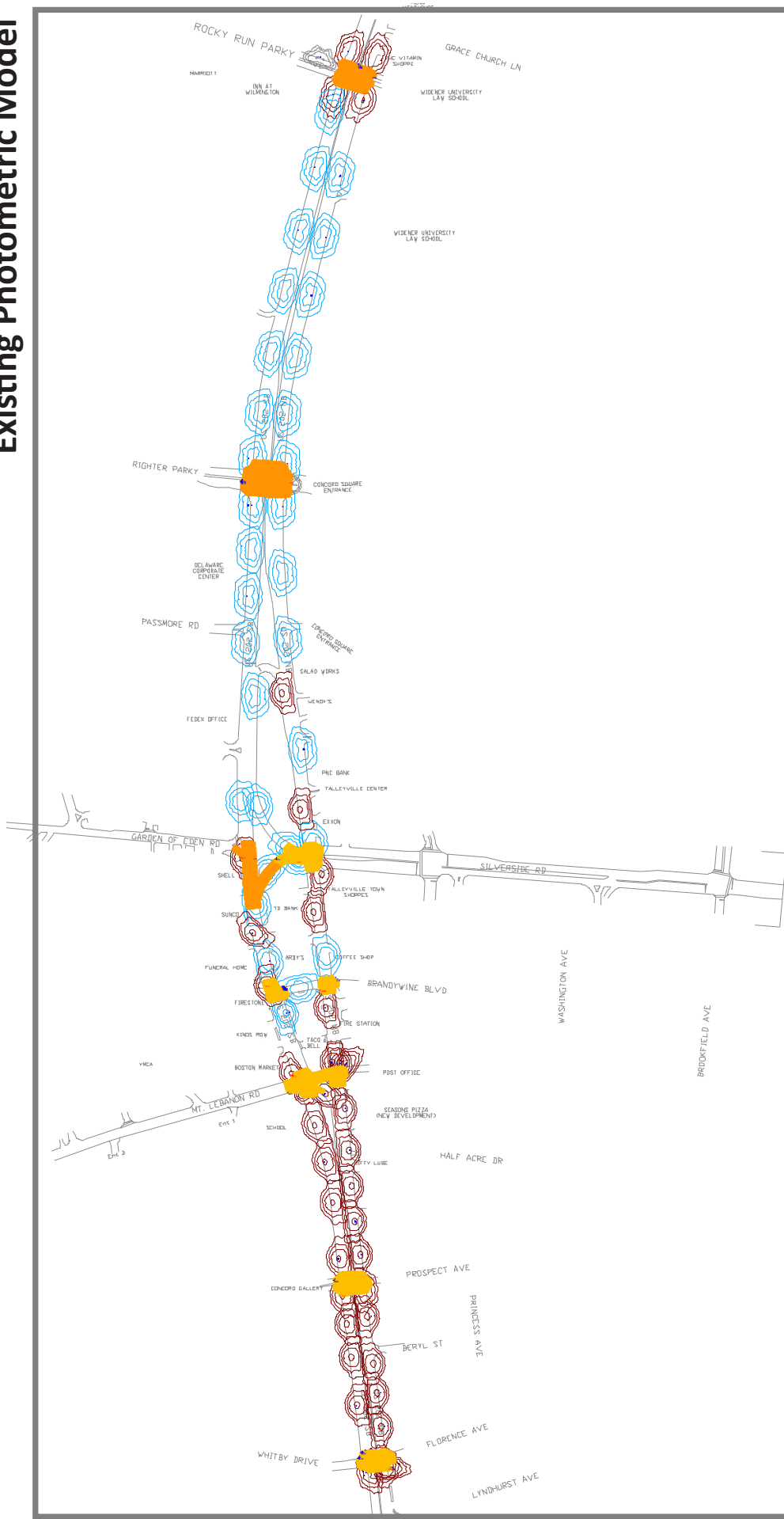
| Peak Hour Boarding's | Typical Pad Size | Typical Shelter Size |
|----------------------|------------------|----------------------|
| 20 | 12' x 8' | 5' x 15' |
| 10 | 8' x 8' | 5' x 10' |
| 5 | 5' x 8' | 5' x 10' |

Shelters are modular and can be enlarged if required due to ridership growth.

APPENDIX H

LIGHTING INVENTORY & ANALYSIS

Existing Photometric Model



US 202 Safety Audit Existing Roadway Lighting

Modeler
RLM
Date
2/28/2018
Scale
Not to Scale

Existing Photometric Model

| Luminaire Schedule | | | | | | |
|---|-----|-------------------------|--------------------------|---|-----------------|------|
| Label | QTY | Catalog Number | Description | Lamp | Lumens per Lamp | LLF |
| 400W HPS 12' arm on UP | 6 | M_RL40M____RMS3_ | M-400 | 400W High Pressure Sodium | 33100 | 0.75 |
| 250W HPS 12' arm on UP | 30 | M2AC25S***GMC3* | M-250A2 POWR DOOR CUTOFF | 250W High Pressure Sodium | 28000 | 0.75 |
| 400W HPS 12' arm on State Pole | 22 | M_RL40M____RMS3_ | M-400 | 400W High Pressure Sodium | 33100 | 0.75 |
| 250W HPS 12' arm on State Pole | 11 | M2AC25S***GMC3* | M-250A2 POWR DOOR CUTOFF | 250W High Pressure Sodium | 28000 | 0.75 |
| 100W HPS 12' arm on UP | 1 | M2AC15S***GMC3* | M-250A2 POWR DOOR CUTOFF | 150W High Pressure Sodium | 9500 | 0.75 |
| 400W HPS 8' arm on UP | 2 | M_RL40M____RMS3_ | M-400 | 400W High Pressure Sodium | 33100 | 0.75 |
| Parking Lot Luminaire @ Rocky Run | 1 | KVR2 250M SR2FL (PROBE) | Round Area Light | 250W (assumed for modeling purposes) | 20000 | 0.75 |
| Decorative Ped Scale Light @ Righter - Shopping Center | 1 | AR15AHP00XA5C | ARLINGTON | 150W High Pressure Sodium (assumed for modeling purposes) | 16000 | 0.75 |

| Statistics | | | |
|-----------------------------------|--------|---------|---------|
| Description | Symbol | Avg | Avg/Min |
| Brandywine Blvd Intersection East | + | 0.44 fc | 2.4:1 |
| Brandywine Blvd Intersection West | + | 1.90 fc | 4.0:1 |
| Mid-Block Crossing @ JCC | + | 0.00 fc | N/A |
| Mount Lebanon Intersection | + | 1.19 fc | 13.2:1 |
| Prospect Ave Intersection | + | 1.24 fc | 5.6:1 |
| Righter Parkway | + | 0.67 fc | 4.2:1 |
| Rocky Run Intersection | + | 0.56 fc | 1.9:1 |
| Silverside Rd Intersection East | + | 1.85 fc | 2.7:1 |
| Silverside Rd Intersection West | + | 1.22 fc | 4.7:1 |
| Whitby Dr Intersection | + | 1.70 fc | 8.9:1 |

Lighting Conditions at Signalized Intersections

| Signalized Intersections | DeIDOT Guidelines | | Existing | |
|--------------------------------|--------------------------------|--------------------------|--------------------------------|--------------------------|
| | Average Maintained Illuminance | Avg:Min Uniformity Ratio | Average Maintained Illuminance | Avg:Min Uniformity Ratio |
| US 202 @ Rocky Run Pkwy | 1.2 | 3:1 | 0.6 | 2:1 |
| US 202 @ Righter Pkwy | 1.2 | 3:1 | 0.7 | 4:1 |
| US 202 @ Garden of Eden Rd | 1.2 | 3:1 | 1.2 | 5:1 |
| US 202 @ Silverside Rd | 1.2 | 3:1 | 1.9 | 3:1 |
| US 202 @ Brandywine Blvd East | 1.2 | 3:1 | 0.4 | 2:1 |
| US 202 @ Brandywine Blvd West | 1.2 | 3:1 | 1.9 | 4:1 |
| US 202 @ Mt Lebanon Rd | 1.2 | 3:1 | 1.2 | 13:1 |
| US 202 @ Prospect Ave | 1.2 | 3:1 | 1.2 | 6:1 |
| US 202 @ Whitby Dr | 1.2 | 3:1 | 1.7 | 9:1 |
| Silverside Rd @ Rose Ave | 1.0 | 4:1 | 0.0 | NA |
| Silverside Rd @ Brookefield Ln | 1.0 | 4:1 | 0.0 | NA |

Notes:

DeIDOT-recommended light levels for all US 202 intersections, including the crosswalks, are as follows:

- *Average maintained illuminance of 1.2 foot-candles or greater*
- *Average-to-Minimum illuminance uniformity ratio of 3:1 or less*

DeIDOT-recommended light levels for Silverside Rd intersections, including the crosswalks, are as follows:

- *Average maintained illuminance of 1.0 foot-candles or greater*
- *Average-to-Minimum illuminance uniformity ratio of 4:1 or less*

Proposed Photometric Model



US 202 Safety Audit Existing Roadway Lighting with Proposed Recommendations

Modeler
 RLM
Date
 4/27/2018
Scale
 Not to Scale

Proposed Photometric Model

| Luminaire Schedule | | | | | | |
|--|-----|-----------------------------------|--|--------------------------------------|-----------------|------|
| Label | QTY | Catalog Number | Description | Lamp | Lumens per Lamp | LLF |
| 400W HPS 12' arm on UP | 6 | M_RL40M____RMS3 - | M-400 | 400W High Pressure Sodium | 33100 | 0.75 |
| 250W HPS 12' arm on UP | 30 | M2AC25S***GMC3* | M-250A2 POWR DOOR CUTOFF | 250W High Pressure Sodium | 28000 | 0.75 |
| 400W HPS 12' arm on State Pole | 22 | M_RL40M____RMS3 - | M-400 | 400W High Pressure Sodium | 33100 | 0.75 |
| 250W HPS 12' arm on State Pole | 11 | M2AC25S***GMC3* | M-250A2 POWR DOOR CUTOFF | 250W High Pressure Sodium | 28000 | 0.75 |
| 100W HPS 12' arm on UP | 1 | M2AC15S***GMC3* | M-250A2 POWR DOOR CUTOFF | 150W High Pressure Sodium | 9500 | 0.75 |
| 400W HPS 8' arm on UP | 2 | M_RL40M____RMS3 - | M-400 | 400W High Pressure Sodium | 33100 | 0.75 |
| Parking Lot Luminaire @ Rocky Run | 1 | KVR2 250M SR2FL (PROBE) | Round Area Light | 250W (assumed for modeling purposes) | 20000 | 0.75 |
| Decorative Ped Scale Light @ Righter - Shopping Center | 1 | AR15AHP00XA5C | ARLINGTON | 150W High Pressure Sodium | 16000 | 0.75 |
| Proposed Mongoose | 1 | MGLED 5 4/5K AX F L X X | Mongoose LED with 5 COBs, 4000K & 5000K Color Temperature, 1050mA Drive Current, Type F Lens (Forward), Low Tilt | LED | 25231 | 0.75 |
| Proposed 250W-Equivalent LED Cobrahead | 4 | ATB2 60BLEDE70 XXXXX R3 4K/5K | ATB2 SERIES LED 700MA TYPE 3 4000K/5000K CCT | LED Array | 17713 | 0.75 |
| Proposed 150W-Equivalent LED Cobrahead | 3 | ATB2 40BLEDE70 XXXXX R3 4K/5K HSS | ATB2 SERIES LED 700MA TYPE 3 4000K/5000K CCT HSS | LED Array | 9785 | 0.75 |
| Proposed 49W LED Post Top | 2 | AWDE2 P20 40K AH 5 N | Acrylic Washington Postlite II LED, LED Package 20, 49W, 4000K, 347-480V, Type 5 distribution, No trim | LED | 5301 | 0.75 |
| Proposed 400W-Equivalent LED Cobrahead | 2 | ATB2 80BLEDE85 XXXXX R3 4K/5K | ATB2 SERIES LED 850MA TYPE 3 4000K/5000K CCT | LED Array | 26878 | 0.75 |

| Statistics | | | |
|---------------------------------------|--------|---------|---------|
| Description | Symbol | Avg | Avg/Min |
| Brandywine Blvd Intersection East | + | 1.30 fc | 3.6:1 |
| Brandywine Blvd Intersection West | + | 1.90 fc | 4.0:1 |
| Mid-Block Crossing @ JCC | + | 1.02 fc | 1.1:1 |
| Mount Lebanon Intersection | + | 1.32 fc | 4.7:1 |
| Prospect Ave Intersection | + | 1.24 fc | 5.6:1 |
| Righter Parkway | + | 0.67 fc | 4.2:1 |
| Rocky Run Intersection | + | 1.19 fc | 3.7:1 |
| Silverside Rd @ Brookefield Ln | + | 1.40 fc | 2.5:1 |
| Silverside Rd @ Rose Ave Intersection | + | 1.05 fc | 4.0:1 |
| Silverside Rd Intersection East | + | 1.85 fc | 2.7:1 |
| Silverside Rd Intersection West | + | 1.22 fc | 4.7:1 |
| Whitby Dr Intersection | + | 2.05 fc | 4.8:1 |