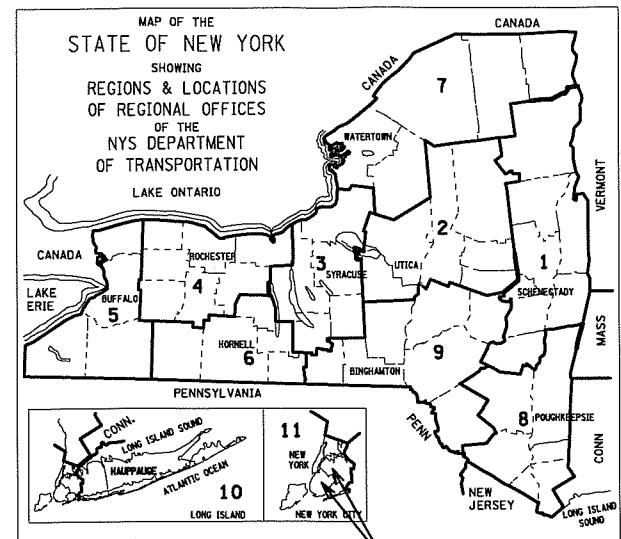


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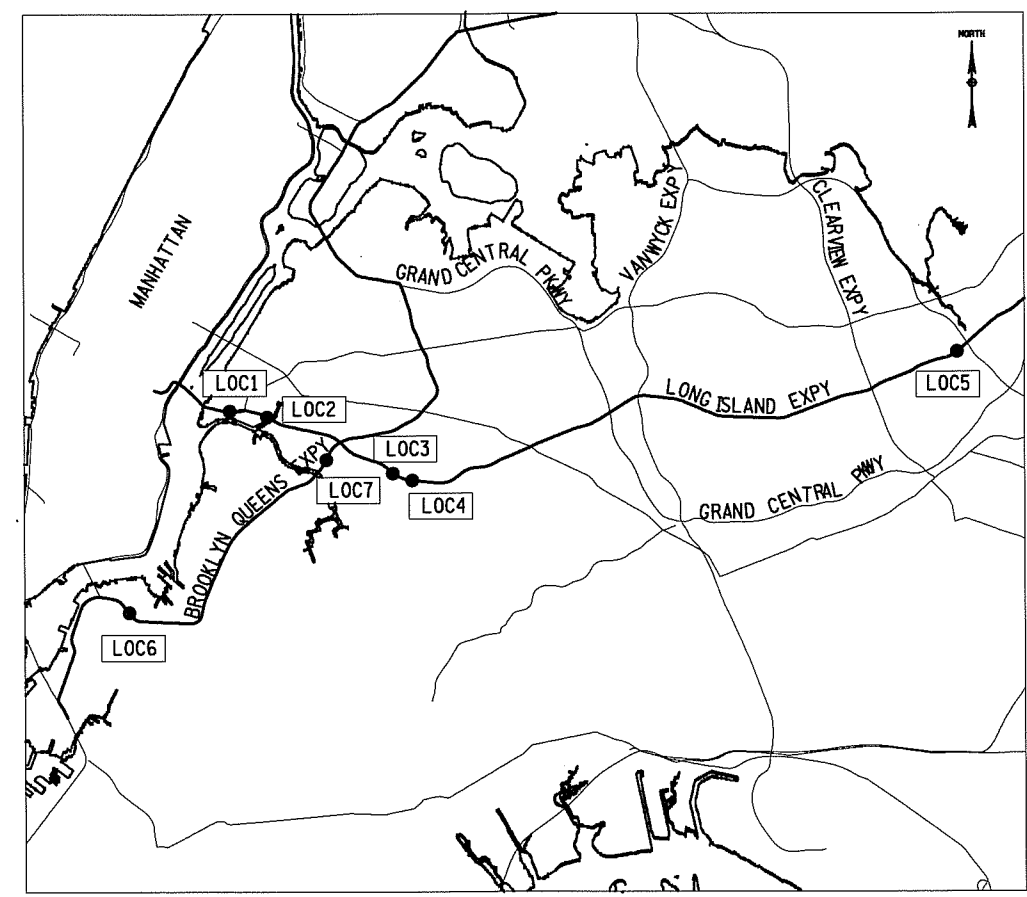
DESIGN SUPERVISOR RANDOLPH HUNT JOB MANAGER MAREK CWIOK DESIGNED BY YELENA GRIG CHECKED BY MAREK CWIOK DRAFTED BY YELENA GRIG CHECKED BY MAREK CWIOK



STATE OF NEW YORK
DEPARTMENT OF TRANSPORTATION
ITS TRAVEL TIME I-278 AND I-495
KINGS, QUEENS COUNTIES
CONTRACT D262095

NON RESTRICTED HIGHWAY
PROJECT LOCATION :
KINGS AND QUEENS COUNTIES VARIOUS LOCATIONS
SCOPE OF WORK :
INSTALLATION OF TRANSMIT READERS AND TRAVEL TIME SIGNS
TYPE OF CONSTRUCTION :
INSTALLATION OF ITS EQUIPMENT

CONTRACTOR'S NAME _____	
AWARD DATE _____	
COMPLETION DATE _____	
FINAL ACCEPTANCE DATE _____	
REGIONAL DIRECTOR _____	
ENGINEER IN CHARGE _____	
FINAL COST TOTAL _____	
FISCAL SHARE _____	COST(S) _____



PROJECT LOCATIONS

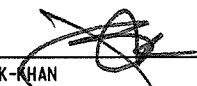
ALL WORK CONTEMPLATED UNDER THIS CONTRACT IS TO BE COVERED BY AND IN CONFORMITY WITH THE STANDARD SPECIFICATIONS (METRIC UNITS) OF MAY 4, 2006, EXCEPT AS MODIFIED ON THESE PLANS AND IN THE ITEMIZED PROPOSAL.

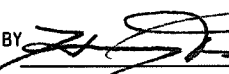
MAINTENANCE NOTE:


ALL EXISTING FACILITIES CURRENTLY MAINTAINED BY THE CITY OF NEW YORK WITHIN THE CONTRACT LIMITS SHALL REMAIN THE MAINTENANCE RESPONSIBILITY OF THE CITY OF NEW YORK FOR THE DURATION OF THIS CONTRACT, AND AFTER COMPLETION OF THE CONTRACT, NYSDOT WILL BE RESPONSIBLE FOR THE MAINTENANCE OF ALL THE SYSTEM EQUIPMENT INSTALLED UNDER THIS CONTRACT.

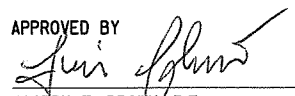
ALL EXISTING SANITARY AND OTHER SEWERS NOT DEEMED TO BE PART OF THE PROJECT BY THE COMMISSIONER, WATER MAINS, HYDRANTS AND OTHER MUNICIPALLY OR PRIVATELY OWNED FACILITIES WITHIN THE LIMITS OF THE R.O.W. WHICH REMAIN IN SERVICE UNCHANGED AND ALL SUCH FACILITIES RELOCATED OR PROTECTED AS PART OF THE WORK PERFORMED UNDER THE PROJECT WHETHER CROSSING, LOCATED WITHIN OR ADJACENT TO THE R.O.W. SHALL BE MAINTAINED AS THE CASE MAY BE BY THE MUNICIPALITY OR BY THE AGENCY OR UNIT HAVING CONTROL OR JURISDICTION THEREOF AT NO COST OR EXPENSE TO THE STATE.

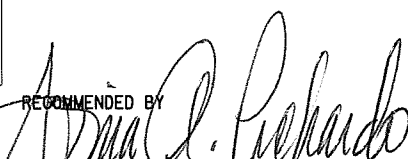
APPROVED PURSUANT TO THE PROVISIONS OF ARTICLE 12-B, SECTION 349-C, OF THE HIGHWAY LAW, BY THE MAYOR OF THE CITY OF NEW YORK.


APPROVED BY  8.1.12
JANETTE SADIK-KHAN
COMMISSIONER
DEPARTMENT OF TRANSPORTATION, CITY OF NEW YORK

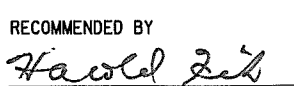
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HENRY D. PERAHIA, P.E.
DEPUTY COMMISSIONER / CHIEF BRIDGE OFFICER
DEPARTMENT OF TRANSPORTATION, CITY OF NEW YORK

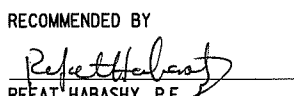
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JAY JABER, P.E.
ASSISTANT COMMISSIONER
OFFICE OF CONSTRUCTION COORDINATION AND MITIGATION
DEPARTMENT OF TRANSPORTATION, CITY OF NEW YORK

APPROVED BY  7/23/12
JOSEPH T. BROWN, P.E.
ACTING REGIONAL DIRECTOR
N.Y.S.D.O.T. REGION 11

RECOMMENDED BY  7/18/12
SONIA A. PICHARDO, P.E.
DIRECTOR OF DESIGN
N.Y.S.D.O.T. REGION 11

RECOMMENDED BY  7/20/12
BRUCE OGUREK, P.E.
ACTING DIRECTOR OF CONSTRUCTION
N.Y.S.D.O.T. REGION 11

RECOMMENDED BY  7/18/12
HAROLD FINK, P.E.
ACTING DIRECTOR OF STRUCTURES
N.Y.S.D.O.T. REGION 11

RECOMMENDED BY  7/20/12
REFAT HABASHY, P.E.
DIRECTOR OF OPERATIONS
N.Y.S.D.O.T. REGION 11

TRAVEL TIME INTELLENT TRANSPORT SYSTEMS		
KINGS, QUEENS COUNTIES		
FED. ROAD REG. NO.	STATE	SHEET NO.
1	N.Y.	01
FEDERAL AID PROJECT NO.		
CAPITAL PROJECT IDENTIFICATION NO. X806.49		
INDEX ON SHEET NO. 3		

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DESIGN SUPERVISOR RANDOLPH HUNT

JOB MANAGER MAREK CWIOK

DESIGN YELENA GRIG

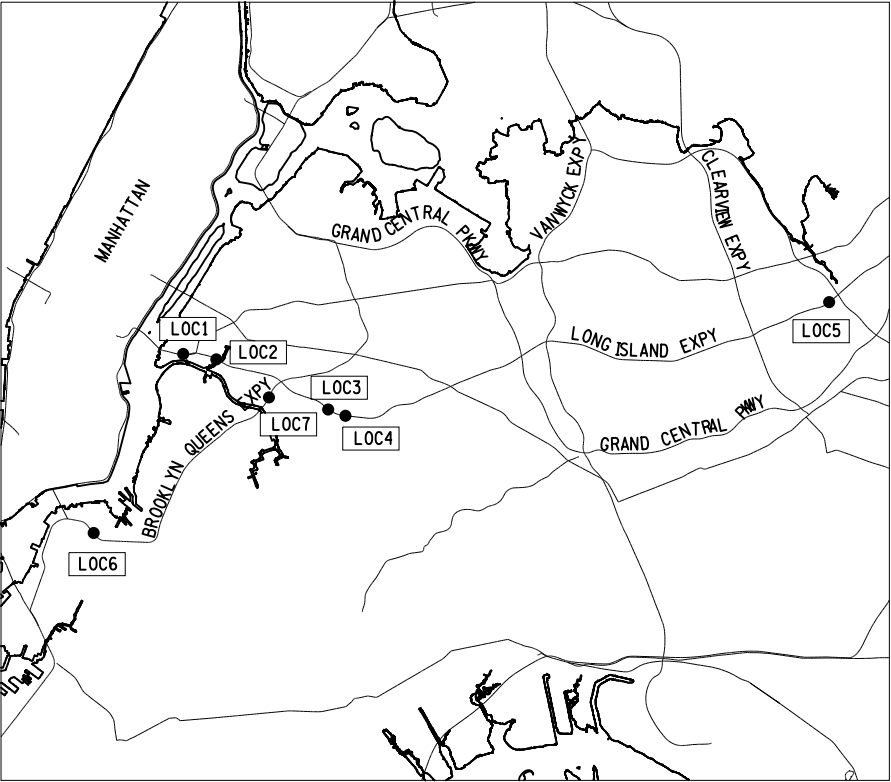
CHECK MAREK CWIOK

DRAFTING YELENA GRIG

CHECK MAREK CWIOK

PROJECT MANAGER RAJ AMIN

LOCATION PLAN




LOC NO	DEVICE TYPE	LOCATION	STRUCTURE ID
LOC1	TRAVEL TIME SIGN	LIE EB AT 23RD STREET	SSN7278
LOC2	TRANSMIT DETECTORS	LIE AT GREENPOINT AVENUE	SSN0807
LOC3	TRANSMIT DETECTORS	LIE AT 48TH STREET (LOWER LEVEL)	SSN2314
LOC4	TRANSMIT DETECTORS	LIE AT 63RD STREET	SSN0795
LOC5	TRANSMIT DETECTORS	LIE E HAMPTON BLVD	BIN1055340
LOC6	TRANSMIT DETECTORS	BQE AT TILLARY STREET	SSN2305
LOC7	TRANSMIT DETECTORS	BQE AT LIE 54TH AVENUE	SSN0531

AFFIX SEAL: ON:	ALTERED BY: ON:












AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	ITS TRAVEL TIME I-278 AND I-495	PIN X806.49	BRIDGES	CULVERTS	ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED	CONTRACT NUMBER	
	PS&E DATE: 06/21/2012					D262095	
						DRAWING NO. CVR-02	
	COUNTY: KINGS AND QUEENS					SHEET NO. 02	
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.						NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11 DOCUMENT NAME: X80649_cph_cvr_02.dgn	

































































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SHEET NUMBER	DRAWING NAME	DRAWING NUMBER
56	TAG READER ANTENNA DETAILS	DTL-03
57	CABINET MOUNTING DETAILS	DTL-04
58	MOUNTING DETAILS	DTL-05
59	CONDUIT MOUNTING DETAILS	DTL-06
60	PULLBOX MOUNTING DETAILS	DTL-07
61	MISCELLANEOUS CONSTRUCTION DETAILS	DTL-08
62	REINFORCED CONCRETE PULLBOXES - 1	DTL-09
63	REINFORCED CONCRETE PULLBOXES - 2	DTL-10
64	TYPICAL TRENCH RESTORATION DETAILS	DTL-11
65	PULL BOX ON STEEP GRADE	DTL-12
66	CONTROL CABINET WIRING DIAGRAM	CCD-01
67	TRANSMIT DETECTOR AND VMS BLOCK DIAGRAM	DIA-01
68	FIBER CIRCUIT DIAGRAMS - 1	FCD-01
69	FIBER CIRCUIT DIAGRAMS - 2	FCD-02
70	FIBER CIRCUIT DIAGRAMS - 3	FCD-03
71	FIBER SPLICE DIAGRAM - LIE MH-6 PROP	FSD-01
72	FIBER SPLICE DIAGRAM - LIE MH-7 PROP	FSD-02
73	FIBER SPLICE DIAGRAM - LIE MH-6 EXIST	FSD-03
74	FIBER SPLICE DIAGRAM - LIE MH-7 EXIST	FSD-04
75	FIBER OPTIC SPLICING TABLE - 1	FST-01
76	FIBER OPTIC SPLICING TABLE - 2	FST-02
77	FIBER OPTIC SPLICING TABLE - 3	FST-03
78	FIBER OPTIC SPLICING TABLE - 4	FST-04
79	FIBER TERMINATION TABLE - 1	FTT-01
80	FIBER TERMINATION TABLE - 2	FTT-02
81	RISER DIAGRAM - 1	MCD-01
82	RISER DIAGRAM - 2	MCD-02
83	RISER DIAGRAM - 3	MCD-03
84	RISER DIAGRAM - 4	MCD-04
85	RISER DIAGRAM - 5	MCD-05
86	METER CABINET DETAILS - 1	MCD-06
87	METER CABINET DETAILS - 2	MCD-07
88	METER CABINET DETAILS - 3	MCD-08
89	ITS EQUIPMENT TABLES - 1	ITT-01
90	ITS EQUIPMENT TABLES - 2	ITT-02
91	ITS EQUIPMENT TABLES - 3	ITT-03
92	ITS EQUIPMENT TABLES - 4	ITT-04
93	ITS EQUIPMENT TABLES - 5	ITT-05
94	CONTROL EQUIPMENT CABINET DETAILS	ITT-06
95	LANDSCAPE DETAILS - 1	LAD-01
96	LANDSCAPE DETAILS - 2	LAD-02
97	LANDSCAPE DETAILS - 3	LAD-03
98	LANDSCAPE DETAILS - 4	LAD-04
99	LANDSCAPE DETAILS - 5	LAD-05
100	LANDSCAPE NOTES - 1	LAN-01
101	LANDSCAPE NOTES - 2	LAN-02
102	LANDSCAPE - 63RD ST	LAP-01
103	LANDSCAPE - EAST HAMPTON - 1	LAP-02
104	LANDSCAPE - EAST HAMPTON - 2	LAP-03
105	LANDSCAPE - EAST HAMPTON - 3	LAP-04
106	LANDSCAPE - EAST HAMPTON - 4	LAP-05
107	LANDSCAPE - EAST HAMPTON - 5	LAP-06
108	LANDSCAPE - TILLARY ST	LAP-07
109	LANDSCAPE - 54TH AVE	LAP-08
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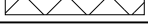



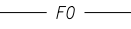

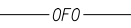
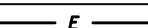
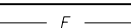
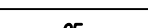
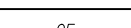
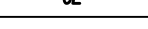

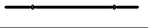
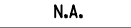

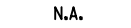

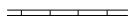
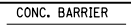
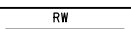
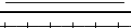
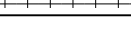
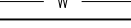
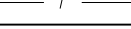

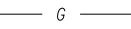
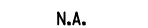
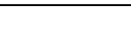
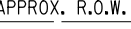



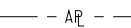
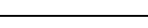
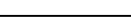




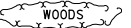
NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11
DOCUMENT NAME: X80649_cph.ind.01.dgn

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	ITS TRAVEL TIME I-278 AND I-495	PIN X806.49	BRIDGES	CULVERTS	ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED	CONTRACT NUMBER	
	PS&E DATE: 06/21/2012				INDEX OF DRAWINGS	D262095	
						DRAWING NO. IND-01	
						SHEET NO. 03	
	COUNTY: KINGS AND QUEENS						


NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11
 DOCUMENT NAME: X80649_cph_ind_01.dgn

PULLBOX AND JUNCTION BOX DESIGNATIONS			
PLAN SYMBOL	ITEM NO.	COVER LEGEND	DIMENSIONS
 F1	11680.514002	NYSDOT FIBER	1219mm X 914mm X 1219mm
 F2	11680.515101	NYSDOT FIBER	610mm X 457mm X 914mm
 F3	11680.5161	-	660mm X 508mm X 203mm
 F4	11680.5160	-	1067mm X 762mm X 305mm
 E1	680.515001	NYSDOT ELECTRIC	610mm X 457mm X 914mm
 E2	670.40	-	406mm X 305mm X 203mm
 E3	670.40	-	457mm X 457mm X 457mm
 E4	670.40	-	762mm X 610mm X 305mm
 E5	11680.515002	NYSDOT ELECTRIC	914mm X 610mm X 914mm
 E6	670.40	-	610mm X 610mm X 610mm
 S	11680.956448	ELECTRICAL SERVICE	610mm X 457mm X 914mm


LEGEND		
FEATURE	SYMBOL	
	PROPOSED	EXISTING
COMMUNICATIONS HUB		
COMMUNICATIONS SHELTER		
METERING EQUIPMENT CABINET	CM 	
TELCO DEMARC CABINET	CD 	
EQUIPMENT CABINET		
TRANSFORMATION EQUIPMENT CABINET	CT 	
POWER DISTRIBUTION PANEL	PP 	
ELECTRICAL PULLBOX, OVERHEAD		
ELECTRICAL PULLBOX, UNDERGROUND		
CIRCULAR PULLBOX		
FIBER OPTIC PULLBOX, UNDERGROUND		
FIBER OPTIC PULLBOX, OVERHEAD		
ITS PULLBOX		
FIBER SPLICE		
CCTV		
CAMERA ASSAMBLY		
RANGING RADAR DETECTOR (NON-INTRUSIVE)		
VIDEO CLASSIFICATION DETECTOR		
VIDEO IMAGE DETECTION SYSTEM		
TRANSMIT DETECTOR		
VEHICLE CLASSIFICATION DETECTOR		
SPEED DETECTOR		
HIGHWAY ADVISORY RADIO		
HAR BEACON		
VARIABLE MESSAGE SIGN (VMS)		
BLANK-OUT SIGN		
CAMERA ASSEMBLY		
OVERHEAD SPAN SIGN STRUCTURE		
CANTILEVER SIGN STRUCTURE		
LAMPPOST		
OPTICAL FIBER CABLE		
COAXIAL CABLE		
WIRELESS MODEM ANTENNA		
SURGE ARRESTOR		
CIRCUIT BREAKER		
CONVENIENCE OUTLET		

LEGEND		
FEATURE	SYMBOL	
	PROPOSED	EXISTING
LANDSCAPE WORK ZONE		N.A.
REMEDIATION DISTURBED SOIL		N.A.
VEGETATION PROTECTION BARRIER		N.A.
FIBER OPTIC, UNDERGROUND		
FIBER OPTIC, OVERHEAD		
ELECTRIC LINE, UNDERGROUND		
ELECTRIC LINE, OVERHEAD		
GUIDERAIL		
SILT FENCE		N.A.
PNEUMATIC EXCAVATED TRENCH		N.A.
CENTER MEDIAN MALL BARRIER		
CONCRETE BARRIER		
RETAINING WALL		
RAILROAD TRACKS	N.A.	
WATER	N.A.	
TELEPHONE	N.A.	
GAS	N.A.	
SANITARY SEWER	N.A.	
APPROXIMATE RIGHT OF WAY		
APPROXIMATE PROPERTY LINE		
EROSION	N.A.	
FLOW ARROW		
TREE LINE	N.A.	
BRUSH	N.A.	
TREE DECIDUOUS		
TREES CONIFEROUS		
SHRUB DECIDUOUS		
SHRUB CONIFEROUS		
REFERENCE MARKER		EACH MILE
		1/10 MILE INCREMENT
NORTH ARROW		

AFFIX SEAL:
ON:



ALTERED BY:
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	PS&E DATE: 06/21/2012					D262095	
						DRAWING NO. LEG-01	
	COUNTY: KINGS AND QUEENS					SHEET NO. 05	
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.					NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11 DOCUMENT NAME: X80641_cph_leg_01.dgn		

GENERAL NOTES

1. OTHER CONTRACTS IN THE VICINITY OF THIS CONTRACT WORK AREA MAY BE UNDER CONSTRUCTION DURING THE TIME OF THIS PROJECT. THE CONTRACTOR SHALL CONSULT WITH THE VARIOUS PARTIES AND IS TO BECOME FAMILIAR WITH THE SCHEDULING OF SUCH PROJECTS AND SCHEDULE ACTIVITIES ACCORDINGLY TO FACILITATE THE FLOW OF TRAFFIC AND THE LEAST AMOUNT OF CONFLICT BETWEEN PROJECTS. THE SCHEDULING OF ALL ACTIVITIES SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL IN ADVANCE. THE REGIONAL CONSTRUCTION ENGINEER WILL RESOLVE ANY DISPUTES OF ACCESSIBILITY TO THE WORK SITE.
OTHER KNOWN ONGOING CONSTRUCTION CONTRACTS ARE:
D261663 (TTS)
D260743 (LIE ITS)

2. THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE FACT THAT, DUE TO THE NATURE OF THE WORK ON THIS PROJECT, THE EXACT EXTENT OF THE WORK CANNOT ALWAYS BE ACCURATELY DETERMINED PRIOR TO THE COMMENCEMENT OF WORK. CONTRACT DOCUMENTS AND/OR WORK ORDER HAVE BEEN PREPARED BASED ON FIELD INSPECTION AND OTHER AVAILABLE INFORMATION. ACTUAL FIELD CONDITIONS MAY REQUIRE MODIFICATIONS TO CONSTRUCTION DETAILS AND WORK QUANTITIES. THE CONTRACTOR SHALL PERFORM THE WORK, AS APPROPRIATE, ACKNOWLEDGING FIELD CONDITIONS AND AS ORDERED BY THE ENGINEER.

3. ALL BIDDERS SHOULD INSPECT THE PROJECT SITE PRIOR TO SUBMITTING BIDS TO VERIFY THE FIELD CONDITIONS WHICH MAY BE ENCOUNTERED AND THE NATURE OF THE WORK TO BE DONE UNDER THIS CONTRACT. NO COMPENSATION WILL BE ALLOWED TO THE BIDDER FOR FAILURE TO INCLUDE ALL ITEMS, LABOR, MATERIALS AND EQUIPMENT COST NECESSARY TO COMPLETE THE WORK.

4. NO PAYMENT WILL BE MADE FOR WORK CALLED FOR BY NOTES IN THE PLANS OR UNDER THE HEADING GENERAL NOTES AND/OR SPECIAL NOTES UNLESS PAYMENT IS SPECIFICALLY INDICATED BY AN ITEM NUMBER. THE COST OF WORK NOT SPECIFICALLY CALLED FOR BY AN ITEM NUMBER SHALL BE INCLUDED IN THE UNIT PRICES BID FOR THE VARIOUS RELATED ITEMS IN THE CONTRACT.

5. THE CONTRACTOR SHALL PERFORM ALL WORK WITH CARE SO THAT ANY MATERIAL WHICH IS TO REMAIN IN PLACE OR WHICH IS TO REMAIN THE PROPERTY OF THE STATE OR CITY WILL NOT BE DAMAGED. IF THE CONTRACTOR DAMAGES ANY MATERIALS WHICH ARE TO REMAIN IN PLACE OR WHICH ARE TO REMAIN THE PROPERTY OF THE STATE OR CITY, THE DAMAGED MATERIALS SHALL BE REPAIRED OR REPLACED IN A MANNER SATISFACTORY TO THE ENGINEER AT NO EXPENSE TO THE STATE OF NEW YORK OR NEW YORK CITY.

6. IN THE EVENT THAT A STAGING AREA, THAT IS NOT IDENTIFIED IN THE CONTRACT PLANS, IS REQUESTED BY THE CONTRACTOR FOR THIS CONTRACT WITHIN THE STATE'S RIGHT OF WAY, THE FINAL LOCATION OF THE STAGING AREA SHALL BE DETERMINED AFTER THE AWARD OF THE CONTRACT, SUBJECT TO THE APPROVAL OF THE NEW YORK STATE DEPARTMENT OF TRANSPORTATION. REFER TO SUBSECTION 107-08 OF THE STANDARD SPECIFICATIONS FOR MINIMUM RESTORATION REQUIREMENTS. SETUP AND RESTORATION OF AN AREA NOT IDENTIFIED IN THE CONTRACT PLANS WILL BE PERFORMED AT THE CONTRACTOR'S EXPENSE. IT IS THE CONTRACTOR'S RESPONSIBILITY TO SUPPLY/SECURE STORAGE AREA FOR MATERIALS AND EQUIPMENT.

7. A STAGING AREA RESTORATION PLAN SHALL BE SUBMITTED TO THE ENGINEER FOR APPROVAL BEFORE COMMENCING ACTUAL SETUP.

8. ALL AREAS DAMAGED BY THE CONTRACTOR SHAL BE RESTORED AS ORDERED BY THE ENGINEER, AT NO EXPENSE TO THE STATE.

9. WHEN PROPOSED WORK SHOWN IN THE PLANS AND PROPOSAL DIFFERS FROM THE STANDARD SHEETS AND THE STANDARD SPECIFICATIONS, THE INFORMATION AS DETAILED ON THE PLANS AND THEN THE PROPOSAL SHALL GOVERN.

10. THE CONTRACTOR SHALL PROVIDE THE ENGINEER, AT LEAST TWO WEEKS IN ADVANCE, WITH ITS PROPOSED SCHEDULE FOR EACH WEEK PERTAINING TO LANE CLOSINGS AND RAMP CLOSINGS. THE PROPOSAL SHALL INCLUDE LAYOUT AND SIGNING FOR ANY METHODS AVAILABLE TO IMPROVE TRAFFIC FLOW NOT SHOWN ON THE TYPICAL WORK ZONE TRAFFIC CONTROL DETAILS. MODIFICATIONS ORDERED BY THE ENGINEER SHALL BE MADE BEFORE IMPLEMENTATION OF THESE CLOSURES.

11. THE CONTRACTOR MUST NOTIFY THE ENGINEER, THE NEW YORK CITY DEPARTMENT OF TRANSPORTATION OFFICE OF CONSTRUCTION MITIGATION AND COORDINATION, THE NEW YORK CITY POLICE DEPARTMENT, THE NEW YORK CITY FIRE DEPARTMENT OPERATIONS DIVISION, ALL LOCAL HOSPITALS, AMBULANCE AND OTHER EMERGENCY SERVICES OF ALL LANE CLOSURES, DETOURS, OR ANY WORK THAT MIGHT AFFECT THE MOBILITY OR ACCESS OF THE FIRE OR POLICE DEPARTMENT TWO WEEKS IN ADVANCE OF THEIR IMPLEMENTATION. IN ADDITION, THE CONTRACTOR SHALL ENSURE THAT HYDRANTS AND ALARM BOXES ARE KEPT CLEAR AND AVAILABLE.
THE NEW YORK CITY POLICE DEPARTMENT REQUESTS THAT CONTRACTORS MAKE NOTIFICATION DIRECTLY TO THE NYPD TRAFFIC MANAGEMENT CENTER, NO LESS THAN ONE HOUR PRIOR TO ACTUALLY CLOSING TRAVEL LANES. THE PREFERRED METHOD OF NOTIFICATION IS E-MAIL AND SHOULD INCLUDE THE START AND END TIMES OF THE CLOSURE AS WELL AS HOW MANY LANES WILL BE IMPACTED.
NYC POLICE DEPARTMENT E-MAIL ADDRESS IS:
TMCWHEEL@NYPD.ORG & TMCSGT@NYPD.ORG

12. HOMELESS PEOPLE AND THEIR POSSESSIONS MAY BE ENCOUNTERED WITHIN THE CONTRACT LIMITS. PRIOR TO ESTABLISHING A WORK SITE WHICH HAS HOMELESS PEOPLE AT THAT SITE, THE CONTRACTOR SHALL CONTACT, IN WRITING, THE ENGINEER APPROXIMATELY ONE MONTH BEFORE ESTABLISHING THE WORK SITE. THE LETTER SHALL INFORM THE DEPARTMENT OF THE CONTRACTOR'S WORK WHICH MAY AFFECT THE HOMELESS PEOPLE. THE CONTRACTOR SHALL PROVIDE THE LOCATION OF THE HOMELESS PEOPLE AND DATE AND TIME WHICH THE CONTRACTOR PLANS TO ESTABLISH THE WORK SITE. THE ENGINEER SHALL THEN CONTACT THE REGIONAL OFFICE (OPERATIONS/MAINTENANCE). THE CONTRACTOR SHALL IMMEDIATELY MOVE INTO THE WORK SITE AFTER THE HOMELESS PEOPLE HAVE BEEN RELOCATED (BY THE APPROPRIATE AGENCIES) AND THE LOCATION HAS BEEN CLEARED. IN THE CASE WHERE HOMELESS PEOPLE ARE ENCOUNTERED DURING THE COURSE OF THE CONTRACTOR'S ACTIVITIES, THE CONTRACTOR SHALL IMMEDIATELY REMOVE ITS EMPLOYEES FROM THE AFFECTED SITE AND CONTACT THE ENGINEER.

13. THE CONTRACTOR SHALL SCHEDULE A FIELD VISIT, BEFORE START OF ANY WORK, TO PERFORM A PRE-CONDITION SURVEY OF THE ENTIRE PROJECT AREA, IN THE PRESENCE OF THE ENGINEER OR AN AUTHORIZED REPRESENTATIVE OF THE ENGINEER. THE PURPOSE OF THIS SURVEY IS TO PREPARE AN INVENTORY OF ALL THE EXISTING STATE AND CITY OWNED ASSETS AND FACILITIES. THE CONTRACTOR IS RESPONSIBLE FOR DOCUMENTING IF THE SAID ASSETS AND FACILITIES ARE NOT IN WORKING ORDER. THE CONTRACTOR SHALL REPLACE OR REPAIR (AS DETERMINED BY THE ENGINEER), AT NO EXTRA COST TO THE STATE, ALL DAMAGED OR IRRECOVERABLE ASSETS AND FACILITIES, WHICH WERE IN WORKING ORDER PRIOR TO THE COMMENCEMENT OF WORK.
THE PRE-CONSTRUCTION SURVEY SHALL BE PERFORMED USING DIGITAL CAMERAS AND VIDEO CAMERAS/CAMCORDERS, AS APPROPRIATE, TO DOCUMENT THE EXISTING CONDITIONS.

14. THE CONTRACTOR SHALL TAKE THE NECESSARY PRECAUTIONS TO AVOID FILLING CATCH BASINS AND/OR SCUPPERS WITHIN THE CONTRACT LIMITS WITH DEBRIS RESULTING FROM CONTRACT OPERATIONS. IN THE EVENT THE CONTRACTOR'S OPERATION DAMAGES OR BLOCKS THE DRAINAGE SYSTEM, THE CONTRACTOR SHALL, AT ITS OWN EXPENSE, IMMEDIATELY REPAIR OR RESTORE THE DRAINAGE SYSTEM AS DIRECTED BY THE ENGINEER.

15. TO ACCOMMODATE SPECIAL EVENT REQUIREMENTS, ON OR THROUGH THE JOB SITE, THE CONTRACTOR SHALL REMOVE ALL LANE CLOSURES AND WORK ZONE TRAFFIC CONTROL DEVICES AS ORDERED BY THE ENGINEER.

16. THE CONTRACTOR IS REQUIRED TO CLEAN UP AND RESTORE THE LANDSCAPE (REMOVE CONSTRUCTION DEBRIS, REGRADE THE AREA, RE-ESTABLISH TURF, ETC.) AT EACH WORK SITE UPON COMPLETION OF THE WORK AT THAT SITE.

17. EXISTING SPUN ALUMINUM BRACKET ARMS AND LAMPPOSTS (EXCEPT TYPE 10) WHICH ARE REMOVED ITEMS AND NOT RE-USED IN THE FINISHED WORK, SHALL BE DELIVERED TO NYCDOT STREET LIGHTING WAREHOUSE AT 45-03 37TH AVENUE IN LONG ISLAND CITY. A MINIMUM TWO WEEK ADVANCE NOTICE MUST BE GIVEN TO THE FACILITY PRIOR TO THE DELIVERY OF ANY MATERIAL. ALL COSTS ASSOCIATED WITH THIS WORK SHALL BE INCLUDED IN THE RESPECTIVE REMOVAL ITEMS. STEEL ROADWAY PLATES WHICH ARE REMOVED AND NOT RE-USED IN THE FINISHED WORK, SHALL BE NEATLY STORED AT LOCATIONS ON THE JOB SITE DESIGNATED BY THE ENGINEER FOR PICKUP BY NEW YORK CITY FORCES WITHIN 30 DAYS OF NOTIFICATION. IF NOT REMOVED AFTER 30 DAYS FROM NOTIFICATION, THE MATERIAL BECOMES THE PROPERTY OF THE CONTRACTOR.

18. NO CONSTRUCTION SIGNS ARE TO BE MOUNTED ON NYPD OR FDNY FACILITIES.

19. THE USE OF CONTRACTOR VEHICLES ON PARKWAYS ARE SUBJECT TO THE NYCDOT PERMIT. THE CONTRACTOR IS ADVISED THAT PARKWAY BRIDGES HAVE LESS THAN STANDARD CLEARANCE. THE CONTRACTOR MUST AVOID THESE AREAS OR SELECT VEHICLES CONFORMING TO THE REDUCED CLEARANCE. THE USE OF COMMERCIAL VEHICLES ALONG PARKWAYS FOR GENERAL SUPERVISION IS RESTRICTED TO WITHIN ONE ENTRANCE BEFORE AND ONE EXIT FOLLOWING THE CONTRACT LIMITS. COMMERCIAL VEHICLES DELIVERING HEAVY EQUIPMENT AND MATERIAL MAY ONLY ENTER THE PARKWAY ONE ENTRANCE BEFORE THE PARTICULAR CONSTRUCTION INVOLVED AND MUST LEAVE THE PARKWAY AT THE EXIT IMMEDIATELY FOLLOWING THE POINT OF DELIVERY. CONTRACTOR'S EQUIPMENT AND VEHICLES ARE SUBJECT TO ALL TRAFFIC LAWS AND REGULATIONS AT ALL TIMES.

20. ALL VEHICLES USED FOR CONSTRUCTION OPERATIONS SHALL BE EQUIPPED WITH BACKUP ALARMS. VEHICLES WITH DEFECTIVE OR NON-OPERATING BACKUP ALARMS SHALL NOT BE PERMITTED TO BE USED. NO SEPARATE PAYMENT SHALL BE MADE FOR THIS SAFETY EQUIPMENT.

21. WORKERS SHALL NOT BE ALLOWED TO CROSS ACTIVE TRAFFIC LANES WITHIN THE PROJECT LIMITS.

22. THE CONTRACTOR WILL BE REQUIRED TO PROVIDE PAVEMENT MARKINGS IN ACCORDANCE WITH THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) BEFORE OPENING TO TRAFFIC, IN THE AREA WHERE THE OLD PAVEMENT MARKINGS HAVE BEEN ERASED, DUE TO CONTRACT WORK OPERATIONS, AREAS NEWLY PAVED, OR AS ORDERED BY THE ENGINEER.

23. PRIOR TO THE START OF ANY WORK OPERATIONS, THE CONTRACTOR IS RESPONSIBLE FOR THE DOCUMENTATION OF THE EXISTING PAVEMENT MARKINGS AND THEIR LOCATIONS. THIS INVENTORY IS TO BE USED FOR THE RE-ESTABLISHMENT OF THESE MARKINGS.

24. IN THE EVENT THAT GROUND WATER IS ENCOUNTERED DURING THE COURSE OF CONSTRUCTION, THE COST OF DEWATERING, PUMPING, BAILING OR DRAINING NECESSARY DURING CONSTRUCTION TO KEEP THE EXCAVATION FREE OF WATER SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE APPROPRIATE EXCAVATION ITEMS.

25. THE CONTRACTOR IS RESPONSIBLE FOR SECURING ALL OPENINGS IN FENCING RESULTING FROM CONSTRUCTION ACTIVITIES. TO PREVENT UNAUTHORIZED PERSONNEL FROM ENTERING STATE PROPERTY, THE OPENINGS SHALL BE SECURED TO THE SATISFACTION OF THE ENGINEER. ALL OPENINGS IN FENCING SHALL BE SECURED WHEN NOT IN USE. FAILURE BY THE CONTRACTOR TO SECURE THESE OPENINGS SHALL RESULT IN NON-PAYMENT IN ACCORDANCE WITH ITEM 619.01 OR ITEM 619.0101.

26. ANY PROPOSED ITS WORK RELATED TO THE INSTALLATION, ATTACHMENT, DRILLING, CUTTING AND WELDING ETC., THAT MAY IMPACT CITY - OWNED BRIDGES, SHALL BE PRE-APPROVED BY THE DIVISION. DETAILS SHALL ALSO BE SUBMITTED TO THE DIVISION FOR APPROVALS, PRIOR TO THE COMMENCEMENT OF ANY WORK.

GUIDE RAIL NOTES

1. FIXED HAZARDS CURRENTLY PROTECTED SHALL REMAIN PROTECTED AT ALL TIMES. AT LOCATIONS WHERE EXISTING GUIDE RAIL OR CONCRETE BARRIER IS TO BE REMOVED, THE WORK SHALL BE PERFORMED IN A TIMELY MANNER TO MINIMIZE THE TIME, THE AREA IS WITHOUT GUIDE RAIL OR CONCRETE BARRIER (PERMANENT OR TEMPORARY). THE WORK SHALL BE FURTHER RESTRICTED AS FOLLOWS:

A) DRUMS SHALL BE INSTALLED AT 6 m INTERVALS THROUGHOUT THE WORK AREA WHEN GUIDE RAIL OR CONCRETE BARRIER IS REMOVED.

B) THE CONTRACTOR SHALL SCHEDULE ITS WORK SO THAT NO AREA IS LEFT WITHOUT GUIDE RAIL OR CONCRETE BARRIER (PERMANENT OR TEMPORARY) AT THE END OF THE WORK SHIFT.

NO SEPARATE PAYMENT SHALL BE MADE FOR TEMPORARY GUIDE RAIL OR TEMPORARY CONCRETE BARRIER OR A AND B ABOVE EXCEPT WHERE INDICATED ON THE PLANS. WHERE NOT INDICATED ON THE PLANS, COSTS SHALL BE INCLUDED IN THE VARIOUS CONTRACT ITEMS.

2. THE CONTRACTOR IS REQUIRED TO MAKE UP AND COMPLETE EACH SPLICE OF GUIDE RAIL THAT IS INSTALLED IN ANY GIVEN DAY; UNDER NO CIRCUMSTANCES WILL THE CONTRACTOR BE PERMITTED TO LEAVE RAIL SPLICES INCOMPLETE AT THE END OF THE WORK SHIFT.

3. THE CONTRACTOR SHALL PLACE ASPHALT MOWING STRIPS AROUND GUIDE RAIL AND BARRIER INSTALLATIONS TO AVOID GAPS LESS THAN 1.5 m WIDE BETWEEN THE CURB AND THE MOWING STRIP. MOW STRIPS MUST BE INSTALLED IN FORMS. ASPHALT MOWING STRIPS SHALL ALSO BE PLACED IN GORE AREAS AND MALLS LESS THAN 3.6 m WIDE TO LIMIT VEGETATION AND FACILITATE MOWER ACCESS. MOWING STRIPS SHOULD BE PLACED AND COMPACTED BY ROLLING, PRIOR TO THE SETTING OF THE GUIDE RAIL. THE ABOVE PLACEMENT METHODOLOGY MAY BE MODIFIED, AS ORDERED BY THE ENGINEER, PROVIDED IT ACHIEVES AN ACCEPTABLE APPEARANCE AND COMPACTION. THE JOINTS AT THE POSTS, BETWEEN THE CURB AND MOWING STRIP, AND BETWEEN THE ADJACENT EDGE OF ROADWAY/SHOULDER AND MOWING STRIP, SHALL BE SEALED WITH HOT LIQUID ASPHALT CEMENT MEETING THE REQUIREMENTS OF THE STANDARD SPECIFICATIONS, SECTION 702, MATERIAL DESIGNATION 702-0700, MISCELLANEOUS ASPHALT CEMENT. THE COST OF HOT LIQUID ASPHALT CEMENT SHALL BE INCLUDED IN THE PRICE BID FOR THE VARIOUS ITEMS IN THE CONTRACT. MOWING STRIPS SHALL BE INSTALLED AS SHOWN ON STANDARD SHEET M606-BR3 (HEAVY POST BLOCKED-OUT CORRUGATED BEAM GUIDE RAILING WITH PLASTIC, SYNTHETIC OR TIMBER BLOCK-OUTS), UNLESS THERE IS ANOTHER DETAIL SHOWN IN THE CONTRACT DOCUMENTS, OR THE SPECIFIC REPAIR ORDER.

4. ALL GUIDE RAIL SHALL BE INSTALLED SO THAT NO BLUNT/UNANCHORED ENDS ARE EXPOSED TO ON-COMING TRAFFIC. THE LEADING END ANCHORAGE UNITS SHALL BE INSTALLED AS THE FIRST ITEM OF WORK. GUIDE RAIL SHALL THEN BE INSTALLED BEGINNING AT THE LEADING END ANCHORAGE UNIT AND CONTINUING WITHOUT GAPS IN THE DIRECTION OF TRAFFIC.

5. WHENEVER THE OFFSET OF EXISTING GUIDE RAIL TO BE REMOVED IS LESS THAN THE OFFSET OF PROPOSED GUIDE RAIL, THE EXISTING GUIDE RAIL SHALL REMAIN IN PLACE UNTIL THE NEW GUIDE RAIL IS PROPERLY AND COMPLETELY INSTALLED.

6. THE CONTRACTOR SHALL INSTALL ALL GUIDE RAIL SO AS NOT TO INTERFERE WITH EXISTING DRAINAGE SYSTEM OR EXISTING UNDERGROUND UTILITIES.

SIGNING NOTES

1. EXISTING SIGNS DESIGNATED FOR REMOVAL ARE NOT TO BE REMOVED BEFORE BEING REPLACED BY NEW SIGNS AT THE LOCATIONS CALLED FOR IN THE PLANS. IF EXISTING SIGNS ARE REMOVED BEFORE THE NEW ONES ARE INSTALLED, TEMPORARY SIGNS SHALL BE PROVIDED AT THESE LOCATIONS. THE REMOVAL OF EXISTING SIGNS THAT ARE NOT BEING REPLACED SHALL BE REMOVED AOBE. FURNISHING, MOUNTING AND REMOVAL OF TEMPORARY SIGNS WILL BE PAID FOR UNDER ITEM 619.01 OR ITEM 619.0101.


2. THE CONTRACTOR SHALL NOT REMOVE EXISTING GROUND-MOUNTED GUIDE SIGNS UNTIL PROPOSED GUIDE SIGNS ARE INSTALLED TO THE SATISFACTION OF THE ENGINEER.


3. NYCDOT STANDARD SIGNS MAY BE PURCHASED BY THE CONTRACTOR FROM NEW YORK CITY SIGN SHOP. PAYMENT FOR THESE SIGNS TO BE INCLUDED IN VARIOUS CONTRACT ITEMS FOR SIGNS. THE CONTRACTOR SHALL NOTIFY NYCDOT OCMC TEN (10) WORKING DAYS PRIOR TO INSTALLATION OF PARKING RESTRICTION SIGNS.

4. THE SIGN LOCATIONS SHOWN ON THE PLANS ARE APPROXIMATE. THE EXACT LOCATIONS SHALL BE APPROVED BY THE ENGINEER IN THE FIELD.

5. THE CONTRACTOR SHALL STAMP THE BACK OF ALL PERMANENT SIGN PANELS WITH THE CONTRACT NUMBER AND DATE OF INSTALLATION.

6. THE CONTRACTOR SHALL NOT DAMAGE OR ALTER NYCDOT ADOPT-A-HIGHWAY (AAH) SIGNS. AT LEAST TWO WEEKS PRIOR TO COMMENCING WORK IN AN AREA CONTAINING A AAH SIGN, THE CONTRACTOR SHALL NOTIFY THE ENGINEER OF ITS LOCATION. THE CONTRACTOR SHALL COVER ALL AAH SIGNS IN THE PROJECT LIMITS THAT WILL NOT INTERFERE WITH THE WORK. IF IT IS DECIDED THAT ANY AAH SIGNS WILL INTERFERE WITH THE WORK, THEY SHALL BE REMOVED, STORED, AND THEN RE-INSTALLED BY NYCDOT. ROY PENTANGELO, EXECUTIVE DIRECTOR, NYCDOT OFFICE OF ROADWAY REPAIR AND MAINTENANCE, ADOPT A HIGHWAY UNIT, MUST BE CONTACTED BY THE ENGINEER AT 718-712-7563 TO HAVE ANY AAH SIGN REMOVED. REMOVED AAH SIGNS WILL BE STORED BY NYCDOT DURING THE WORK AND THEN RE-INSTALLED BY NYCDOT AFTER THE ENGINEER NOTIFIES THE CITY THAT THIS CONTRACT IS COMPLETED. THOSE AAH SIGNS INTERFERING WITH THE WORK AND NOT REMOVED BY THE CITY IN A TIMELY MANNER SHALL BE, AFTER RECEIVING PERMISSION FROM THE ENGINEER, REMOVED BY THE CONTRACTOR AND GIVEN TO THE ENGINEER. ALL COSTS FOR THIS WORK SHALL BE INCLUDED IN THE VARIOUS CONTRACT ITEMS.

AFFIX SEAL: ON:	ALTERED BY: ON:
	

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	PS&E DATE: 06/21/2012					D262095		
					GENERAL NOTES - 1			DRAWING NO. GNN-01
	COUNTY: KINGS AND QUEENS							SHEET NO. 06
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.						NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11 DOCUMENT NAME: X80649_cph_gnn_01.dgn		

GENERAL NOTES (CONT.)

UTILITIES NOTES

1. THE CONTRACTOR IS ALERTED TO THE FACT THAT THE PROVISIONS OF NEW YORK STATE INDUSTRIAL CODE 53 (16 NYCRR PART 753), PROTECTION OF UNDERGROUND FACILITIES, APPLY TO THIS PROJECT. NO DIRECT PAYMENT FOR ANY WORK REQUIRED BY THE CODE WILL BE MADE. WHERE EXISTING UTILITIES ARE LOCATED WITHIN THE CONTRACT LIMITS, THE CONTRACTOR SHALL BE REQUIRED TO GIVE THE PROPER BUREAUS AND THE VARIOUS OWNING COMPANIES (SHOWN BELOW) AT LEAST 72 HOURS NOTICE BEFORE PERFORMING ANY WORK AT OR NEAR UNDERGROUND FACILITIES. THE CONTRACTOR WILL BE RESPONSIBLE FOR ASCERTAINING THE ACTUAL LOCATION OF ALL UTILITIES NOT MARKED OUT THROUGH THE ONE CALL CENTER. THE CONTRACTOR SHALL CONTACT NYC ONE CALL CENTER AT (800) 272-4480.

VERIZON, NYNEX, CON EDISON, NYCDEP, NYTC DIVISION OF STREET LIGHTING, CABLE PROVIDER, KEY SPAN.

2. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO TAKE NECESSARY PRECAUTIONS TO PREVENT INTERFERENCE WITH OR DAMAGE TO UTILITIES OR OTHER FACILITIES DURING THE COURSE OF CONSTRUCTION. THE COST OF ALL WORK CONNECTED WITH MAINTAINING AND PROTECTING UTILITIES AFFECTED BY THE WORK OF THE CONTRACT SHALL BE BORNE BY THE CONTRACTOR AND THE COST INCLUDED IN THE PRICE BID FOR THE VARIOUS ITEMS IN THE CONTRACT.

3. IN THE EVENT THE CONTRACTOR DAMAGES AN EXISTING UTILITY OR INTERRUPTS UTILITY SERVICE, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY ITS OWNER AND THE ENGINEER, AND SHALL COMMENCE REPAIR/REPLACEMENT WORK AS INSTRUCTED BY THE ENGINEER. IN THE EVENT THE CONTRACTOR CAUSES AN INTERRUPTION IN UTILITY SERVICE, THE CONTRACTOR SHALL IMMEDIATELY ARRANGE FOR SERVICE TO BE RESTORED AND MAY NOT CEASE THE REPAIR WORK UNTIL SERVICE IS RESTORED. THE CONTRACTOR MAY NOT CONTINUE OPERATIONS UNTIL SERVICE IS RESTORED, UNLESS OTHERWISE DIRECTED BY THE ENGINEER. ALL CORRECTIVE UTILITY WORK SHALL BE ACCEPTABLE TO THE ENGINEER AND THE SUBJECT UTILITY OWNER. IF ANY UTILITIES ARE DAMAGED AND THE NECESSARY REPAIRS ARE NOT SATISFACTORY TO THE ENGINEER, THE WORK MAY BE PERFORMED BY THE RESPECTIVE UTILITY COMPANY AND THE COST THEREOF CHARGED AGAINST THE CONTRACTOR. THE COST ASSOCIATED WITH THE REPAIR OF THE UTILITY OR THE RESTORATION OF SERVICE SHALL BE AT CONTRACTOR'S EXPENSE.

4. ALL ELECTRICAL INSTALLATION SHALL CONFORM TO ALL PROVISIONS OF THE NATIONAL ELECTRICAL CODE (NEC), THE NATIONAL ELECTRICAL SAFETY CODE (NESC), LOCAL CODES, AND OSHA; WHERE CODES CONFLICT, THE MORE STRINGENT REQUIREMENTS SHALL APPLY (A.O.B.E.).

5. UTILITY QUALITY LEVEL DEFINITIONS:

QUALITY LEVEL A - THE HIGHEST DEGREE OF ACCURACY; THE UTILITY INFORMATION ON THE CONTRACT PLANS HAS BEEN FIELD LOCATED AND VERIFIED BY EXCAVATION, WHEN APPROPRIATE. (SHOWN AS QLA)
QUALITY LEVEL B - SUBSURFACE GEOPHYSICAL LOCATING TECHNIQUES (I.E., UNDERGROUND CAMERAS, RADAR, SONAR, TONE OUTS, ETC.) AND EXISTING RECORD PLANS HAVE BEEN USED TO LOCATE UTILITIES. NO EXCAVATIONS WERE PERFORMED. (SHOWN AS QLB)
QUALITY LEVEL C - RECORD INFORMATION PROVIDED BY UTILITY OWNERS WAS PLOTTED ON THE CONTRACT PLANS. DEPTHS WERE NOT FIELD VERIFIED. PHYSICAL SURFACE FEATURES LIKE MANHOLES, VALVE BOXES AND HYDRANTS HAVE BEEN FIELD LOCATED. (SHOWN AS QLC)
QUALITY LEVEL D - EXISTING NYSOT AND UTILITY COMPANY RECORDS WERE USED TO LOCATE SUBSURFACE UTILITIES. (SHOWN AS QLD)

6. THE QUALITY LEVEL INFORMATION SHOWN ON THE PLANS DOES NOT RELIEVE THE CONTRACTOR OF OBLIGATIONS UNDER SECTIONS 659 THROUGH 680 OF THE STANDARD SPECIFICATIONS, NOR DOES IT RELIEVE THE UTILITY OWNERS OF THEIR OBLIGATION TO ACCURATELY LOCATE THEIR FACILITIES.

7. EXISTING NYSOT AND UTILITY COMPANY RECORDS WERE USED TO LOCATE SUBSURFACE UTILITIES SHOWN ON THESE PLANS (QLD).

EROSION AND SEDIMENT CONTROL NOTES

1. THE CONTRACTOR SHALL BE REQUIRED TO PERFORM ALL CONSTRUCTION OPERATIONS IN SUCH A MANNER AS TO MINIMIZE SOIL EROSION AND TO ENSURE SEDIMENT CONTROL. THE CONTRACTOR SHALL COMPLY WITH THE EROSION AND SEDIMENT CONTROL PLANS (EROSION AND SEDIMENT CONTROL MEASURES SHOWN AND/OR DESCRIBED IN THE CONTRACT DOCUMENTS) AND/OR SUBSECTION 107-12 OF THE STANDARD SPECIFICATIONS. IN ADDITION, THE CONTRACTOR SHALL COMPLY WITH ALL MEASURES DEEMED NECESSARY BY THE ENGINEER DURING THE COURSE OF CONSTRUCTION.

2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING ALL EROSION AND SEDIMENT CONTROL MEASURES PER DETAILS, AND SPECIFICATIONS, AND A.O.B.E. THE COST OF INSTALLING, CLEANING, AND REMOVING TEMPORARY EROSION AND SEDIMENT CONTROL MEASURES SHALL BE PAID FOR UNDER THE APPROPRIATE ITEM NUMBERS.

SPECIAL NOTE:

NOISE CONTROL

THE CONTRACTOR SHOULD BE AWARE THAT TITLE 15 OF THE RULES OF THE CITY OF NEW YORK WAS AMENDED 07/01/07 BY ADDING A NEW CHAPTER 28, CITYWIDE CONSTRUCTION NOISE MITIGATION. THE DETAILS AND EXACT TEXT OF THIS NEW RULE CAN BE FOUND ONLINE AT HTTP://WWW.NYC.GOV/HTML/DEP/HTML/AIR_AND_NOISE/INDEX.SHTML.

IN ADDITION TO COMPLIANCE WITH PROCEDURES FOR ABATEMENT OF HIGHWAY TRAFFIC NOISE AND CONSTRUCTION NOISE (23CFR PART 772), THE CONTRACTOR SHALL USE AND IMPLEMENT ALL REASONABLE EFFORTS TO ACCOMMODATE THE SPIRIT AND INTENT OF CHAPTER 28 OF THE CITY RULES BY PLANNING AND CARRYING OUT THE WORK ON THIS PROJECT TO ENSURE THAT THE NOISE FROM CONSTRUCTION EQUIPMENT AND ACTIVITIES IS KEPT TO A MINIMUM. THESE EFFORTS TO ACCOMMODATE THE SPIRIT AND INTENT OF CHAPTER 28 SHALL BE SUBMITTED BY THE CONTRACTOR, IN WRITING, IN THE FORM OF A CONSTRUCTION NOISE MITIGATION PLAN, TO THE ENGINEER FOR APPROVAL. NO CITY PERMIT PURSUANT TO CHAPTER 28 IS REQUIRED.

THE CONTRACTOR SHALL PROVIDE THE ENGINEER WITH TWO NOISE METERS MEETING THE REQUIREMENTS OF AN INTEGRATING SOUND LEVEL METER THAT IS IN COMPLIANCE WITH THE CRITERIA FOR A TYPE 1 (PRECISION) OR TYPE 2 (GENERAL PURPOSE) SOUND LEVEL METER AS DEFINED IN THE CURRENT REVISION OF ANSI STANDARD S1.4. TWO ACOUSTIC CALIBRATORS OF THE TYPE RECOMMENDED BY THE METER MANUFACTURER SHALL ALSO BE PROVIDED.

IF ADDITIONAL NOISE MITIGATION IS REQUIRED DUE TO COMPLAINTS; OR IF ADHERENCE TO THE CONSTRUCTION NOISE MITIGATION PLAN CAN NOT BE SUBSTANTIALLY COMPLIED WITH, AN ALTERNATIVE CONSTRUCTION NOISE MITIGATION PLAN SHALL BE PREPARED BY THE CONTRACTOR, IN WRITING, AND SUBMITTED TO THE ENGINEER FOR APPROVAL. AN APPROVED ALTERNATIVE CONSTRUCTION NOISE MITIGATION PLAN MUST BE FULLY IMPLEMENTED BY THE CONTRACTOR WITHIN FIVE (5) BUSINESS DAYS AFTER THE ENGINEER'S APPROVAL.


SHOULD THE CONTRACTOR FAIL TO IMPLEMENT THE NOISE ABATEMENT OPERATIONS AND CONDITIONS OF THE APPROVED CONSTRUCTION NOISE MITIGATION PLAN, OR ANY APPROVED ALTERNATIVE CONSTRUCTION NOISE MITIGATION PLAN WITHIN FIVE (5) BUSINESS DAYS OF APPROVAL THEREOF, THE ENGINEER SHALL HAVE THE AUTHORITY TO SUSPEND ALL WORK UNTIL SUCH TIME AS THE ENGINEER DEEMS THAT THE CONTRACTOR HAS COMPLIED WITH THE REQUIREMENTS.


THE COST OF COMPLYING WITH THIS MITIGATION PLAN SHALL BE INCORPORATED INTO THE PRICES BID FOR THE APPROPRIATE PAY ITEMS.

SURVEY NOTE:

ALL WORK IS TO BE PERFORMED WITHIN THE EXISTING RIGHT-OF-WAY, (INCLUDING EASEMENTS) OF THE VARIOUS ROADWAYS.

UNDER ITEM 625.01 (SURVEY OPERATIONS) CONTRACTOR SHALL STAKE OUT ALL ITS DEVICES INCLUDING PULL BOXES, CAMERA POLES, CAMERAS, SIGN STRUCTURES, CONTROL CABINETS, CONDUITS ETC. AND ASK FOR APPROVAL OF THE EIC PRIOR TO COMMENCING ANY WORK. AFTER THE STAKE OUT, THE CONTRACTOR WOULD NOTIFY THE EIC, TE&S AND MAINTENANCE FOR A FIELD VISIT TO REVIEW THE PROPOSED LOCATION. A TWO WEEK WINDOW FROM NOTIFICATION WOULD BE MAXIMUM TIME FOR APPROVAL OR REQUEST TO RELOCATE, SO AS NOT HOLD UP THE CONTRACTOR. AT THIS TIME, THE APPROVALS OR THE REQUEST TO LOCATE WOULD BE ISSUED. WHEN FINAL STAKE OUT IS APPROVED, THE CONTRACTOR WOULD ALSO STAKEOUT VERTICAL CONTROL SO THAT FOUNDATION FRAME AND GRATE COVERS ETC CAN BE PLACED. THE CONTRACTOR IS RESPONSIBLE FOR CONFORMANCE; ANY DEVIATION OR ERRORS WILL BE REDONE CORRECTLY AT HIS EXPENSE, OR INCLUDED IN THE ORIGINAL BID PRICE FOR THE WORK.

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IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.							
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ADDITIONAL NOTES

ELECTRICAL AND COMMUNICATIONS REQUIREMENTS:

1. ELECTRICAL WIRES ENCOUNTERED IN THE REMOVAL OF SIGNS ARE TO BE REMOVED TO THE NEAREST ELECTRICAL SERVICE BOX AND PROPERLY TERMINATED AND IDENTIFIED AT THAT POINT. THE CONDUIT IS TO BE TERMINATED IN THE VICINITY OF THE SIGN BEING REMOVED. THIS WORK IS TO BE INCLUDED IN THE BID PRICE FOR THE SIGN REMOVAL ITEM.
2. WHENEVER METALLIC CONDUIT MEETING THE REQUIREMENTS OF SUB-SECTION 723-20, METAL STEEL CONDUIT, ZINC COATED, IS SPECIFIED, IT SHALL BE CLASS 1-RIGID METAL STEEL CONDUIT.
3. ALL GALVANIZED STEEL CONDUIT SHALL BE HOT DIPPED. ELECTRO-COATED CONDUIT SHALL NOT BE USED.
4. UNLESS OTHERWISE NOTED ON THE PLANS, ALL CONDUITS SHALL EXIT FROM NARROW SIDE OF BOXES.
5. CONDUIT ENTRY ENCLOSURES SHALL HAVE THREADED BUSHINGS AND DOUBLE LOCK-NUTS OR A SINGLE LOCK-NUT WHERE ENTRY IS THREADED.
6. COST TO CUT AND RETHREAD EXISTING CONDUIT, REMOVE EXISTING CONDUCTORS AND CONNECT TO PROPOSED CONDUIT SHALL BE INCLUDED IN THE PRICE BID FOR NEW CONDUIT ITEM.
7. CONDUIT ENTRIES INTO METAL JUNCTION BOXES SHALL BE MADE BY DRILLING OR PUNCHING. PAYMENT FOR THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR THE APPROPRIATE CONDUIT ITEM.
8. ALL CONDUITS TERMINATED UNDERGROUND SHALL BE THREADED AND CAPPED AND CABLE SHALL BE REMOVED. WORK IS TO BE PERFORMED UNDER VARIOUS CONTRACT ITEMS.
9. NEUTRAL CONDUCTOR SHALL BE GROUNDED AT SERVICE POINT ONLY.
10. ELECTRICAL INSTALLATION SHALL CONFORM TO PROVISIONS OF THE NATIONAL ELECTRICAL CODE (NEC), NATIONAL ELECTRICAL SAFETY CODE (NESC), NEW YORK CITY ELECTRIC CODE AND OSHA. WHERE CODES CONFLICT, THE MOST STRINGENT REQUIREMENTS SHALL APPLY AOB.
11. THE COST OF UNCOVERING ELECTRICAL BOXES AND UNSPLICING, RESPLICING, ABANDONING, ETC. OF ELECTRICAL CONDUCTORS AND CONNECTORS AND RESTORING THE SURFACE SHALL BE INCLUDED IN THE PRICE BID FOR THE VARIOUS ELECTRICAL ITEMS IN THIS CONTRACT.
12. WHEN WIRING IS TO BE PLACED IN EXISTING CONDUIT THE COST OF REMOVING THE OLD CONDUCTOR SHALL BE INCLUDED IN THE PRICE BID FOR THE NEW CONDUCTOR.
13. THE LOCATION OF CONDUITS AND EQUIPMENT SHOWN (UNLESS DIMENSIONED) ARE DIAGRAMMATIC. EXACT LOCATIONS SHALL BE DETERMINED IN THE FIELD AND APPROVED BY THE EIC.

ADDITIONAL NOTES (CONTINUED):

ELECTRICAL AND COMMUNICATIONS REQUIREMENTS:

14. WHEREVER CONDUITS ARE INSTALLED FOR FIBER OPTIC (OR ELECTRICAL POWER) A NYLON PULL WIRE SHALL BE INSTALLED WITH THE CABLES. THE PULL WIRE SHALL BE CONTINUOUS BETWEEN PULL POINTS. PULL WIRES SHALL BE INSTALLED AND LEFT IN ANY PROPOSED EMPTY CONDUITS OR INNERDUCTS. ALL COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR CONDUIT OR INNERDUCTS.
15. THE CONTRACTOR SHALL FURNISH SHOP DRAWINGS, (NINE COPIES) OF ALL ELECTRICAL EQUIPMENT TO BE INSTALLED. THE SHOP DRAWINGS MUST BE APPROVED BY THE EIC BEFORE THESE MATERIALS ARE ORDERED.
16. POWER UTILITY CONNECTION IS SHOWN ON THE PLANS FOR CONTRACTOR REFERENCE ONLY. THE MATERIALS AND WORK ASSOCIATED WITH THIS ITEM WILL BE SUPPLIED AND PERFORMED BY CON EDISON. THE CONTRACTOR SHALL BE REQUIRED TO PAY ANY UTILITY FEES FOR ELECTRICAL SERVICE INCLUDING SET UP AND DELIVERY COSTS NECESSARY TO EXTEND THE UTILITY CABLES TO THE ITS EQUIPMENT LOCATIONS. THE COST FOR THIS SHALL BE INCLUDED IN THE ITEM 11680.956060- UTILITY CONNECTION COST.
17. THE CONTRACTOR SHALL CONTACT THE CON EDISON COMMERCIAL SERVICE REPRESENTATIVE LISTED IN THE CON EDISON BLUE BOOK (AVAILABLE ONLINE AT HTTP://MO20-W5.CONED.COM/ES/CONTACTS.PDF) TO DETERMINE THE EXACT LOCATION OF THE ELECTRIC SERVICE EQUIPMENT. THE CONTRACTOR IS RESPONSIBLE TO STUB CONDUIT 305MM PAST CURB OR EDGE OF PAVEMENT FROM THE PROPERTY LINE BOX AS DIRECTED BY CON EDISON. THIS CONDUIT SHALL BE PAID FOR UNDER ITEM 670.2004 GALVANIZED STEEL CONDUIT 3NPS.
18. SUPPORT ALL EXPOSED CONDUITS ON STRUCTURES AT 3M O.C. MIN. AS SHOWN ON THE CONTRACT DRAWINGS OR AS APPROVED BY THE EIC. UNLESS OTHERWISE NOTED, CONTRACTOR SHALL SUBMIT THE MOUNTING DETAILS TO EIC FOR APPROVAL.

CONDUITS SHALL CROSS EXPANSION JOINTS IN STRUCTURES IN A DIRECTION PARALLEL TO THE STRUCTURE MOVEMENT. EXPANSION FITTINGS SHALL BE PROVIDED AT ALL EXPANSION JOINTS. PAYMENT SHALL BE INCLUDED IN THE PRICE BID FOR RESPECTIVE CONDUIT ITEMS.
19. THE CONTRACTOR SHALL LOCATE AND INSTALL PULLBOXES AND JUNCTION BOXES AS REQUIRED, TO PREVENT DAMAGE TO CABLE DURING INSTALLATION AND TO ACCOMMODATE SPLICES. ADDITIONAL PULLBOXES AND JUNCTION BOXES THAT MAY BE REQUIRED SHALL BE APPROVED BY THE EIC.
20. THE COVERS OF SURFACE MOUNTED PULLBOXES INSTALLED LESS THAN 3M HIGH SHALL BE SECURED EITHER WITH A PADLOCK KEYPED THE SAME AS THE EQUIPMENT CABINET PADLOCKS OR WITH A MINIMUM OF FOUR (4) PENTA BOLTS. ALL OTHER REQUIREMENTS FOR THIS ITEM REMAIN UNCHANGED. THE COST OF MOUNTING PULLBOXES FOR FIBER OPTIC BACKBONE ON STRUCTURE, INCLUDING ANY REQUIRED BRACKETS, DRILLING, BOLTS AND OTHER FASTENERS SHALL BE INCLUDED IN THE BID PRICE FOR PULLBOX.
21. ALL ELECTRICAL EQUIPMENT FURNISHED UNDER THIS CONTRACT SHALL BE NEW UNLESS OTHERWISE APPROVED IN WRITING BY THE EIC.
22. BARE STRANDED GROUNDING CONDUCTORS SHALL BE SIZED BASED ON THE LARGEST ENTERING SERVICE CONDUCTORS AS FOLLOWS:
23.

SIZE OF SERVICE CONDUCTOR	SIZE OF GROUNDING CONDUCTOR
*2	*6
*1/0	*6
*2/0	*4
*3/0	*4
*4/0	*2
24. THE CONTRACTOR SHALL LEAVE 2 METERS OF COILED CABLE IN ALL PROPERTY LINE BOXES AND PULLBOXES WHERE FINAL CON EDISON SERVICE CONNECTIONS ARE TO BE MADE. THE CABLE ENDS SHALL BE WATERPROOFED USING THERMOPLASTIC HEAT SHRINKABLE END CAPS AND SCOTCHKOTE SEALING COMPOUND OR APPROVED EQUAL. THE CONTRACTOR SHALL COORDINATE MANHOLE ENTRY LOCATIONS, SERVICE BOX LOCATIONS AND METERING CABINET LOCATION WITH CON EDISON BEFORE STARTING WORK AT A SITE.
25. THE CONTRACTOR SHALL FURNISH AND INSTALL GROUND RODS AND *6 AWG GROUND CABLES AT ALL METER CABINETS, ELECTRICAL PULLBOXES, PROPERTY LINE BOXES, POLES, TELEPHONE DEMARCATION CABINETS, GROUND MOUNTED EQUIPMENT CONTROL CABINETS AND SIGN STRUCTURES. EQUIPMENT CONTROL CABINETS SHALL BE GROUNDED VIA THE ELECTRICAL SYSTEM AND ASSOCIATED GROUND RODS ADJACENT TO POLE, SIGN STRUCTURE OR CABINET. THE CONTRACTOR SHALL PROVIDE ALL TERMINATION LUGS, GROUND BARS, CONDUITS, FITTINGS, ETC. NECESSARY TO COMPLETE SCOPE OF WORK. PAYMENT SHALL BE INCLUDED IN THE RESPECTIVE ITEM. WORK SHALL BE IN ACCORDANCE WITH STANDARD SPECIFICATIONS SECTION 680-3.12 AND NYSDOT STANDARD DRAWING NO. M680-12.

ADDITIONAL NOTES (CONTINUED):

ELECTRICAL AND COMMUNICATIONS REQUIREMENTS:


26. THROUGH SPLICES TO THE BACKBONE FIBER CABLE SHOWN ON THE PLANS ARE FOR REFERENCE ONLY. WITH THE PERMISSION OF THE EIC, THE CONTRACTOR MAY ELECT TO MOVE THE SPLICES TO OTHER SUITABLE LOCATIONS. THE LOCATION OF ALL SPLICES SHALL BE SUBMITTED TO THE EIC AS PART OF THE CABLE PLAN DESIGN SUBMISSION. ADDITIONAL SPLICES MAY BE REQUIRED AS ORDERED BY THE EIC.
27. THE CONTRACTOR SHALL LABEL THE TERMINALS OF ALL ELECTRICAL CABLES.
28. COLOR CODE ALL ELECTRICAL WIRES AT TERMINATIONS IN ACCORDANCE WITH THE CONTRACTOR CODE REQUIREMENTS AND STANDARD PROCEDURES.
29. A MAINTENANCE CARD SHALL BE PLACED IN EACH CABINET INDICATING A RECORD OF ALL MAINTENANCE TO THE EQUIPMENT AT THAT LOCATION.
30. THE CONTRACTOR SHALL NOTIFY NYSDOT TRAFFIC AND SAFETY GROUP OF ANY WORK INVOLVING REMOVAL OR MODIFICATION OF ITS EQUIPMENT, PRIOR TO THE COMMENCEMENT OF SUCH WORK.
31. ALL FIBER OPTIC CABLES IN A PULLBOX SHALL BE IDENTIFIED PERMANENTLY AS TO FUNCTION (I.E. DISTRIBUTION, BACKONE OR DROP CABLE NUMBER) PER THE FIBER OPTIC DIAGRAMS.
32. TO PREVENT DELAYS IN PROVIDING UTILITY SERVICE DUE TO BACKLOG, THE CONTRACTOR IS ADVISED TO START COORDINATION WITH THE NYCDOT BUREAU OF STREETLIGHTING EARLY IN THE PROJECT.
33. THE CONTRACTOR SHALL CONTACT MR. GEORGE BERMUDEZ AT (646) 879-6191 OF THE NYCDOT BUREAU OF STREETLIGHTING FOR POWER CONNECTION TO STREETLIGHTS AND POWER DISTRIBUTION CABINETS.


ADDITIONAL UTILITY NOTES:

1. THE CONTRACTOR SHALL CONTACT NYC ONE CALL CENTER AT (800) 272-4480 72 HOURS BEFORE COMMENCING ANY UNDERGROUND CONSTRUCTION WORK IN ACCORDANCE WITH NEW YORK STATE INDUSTRIAL CODE RULE * 53. THE CONTRACTOR WILL BE RESPONSIBLE FOR ASCERTAINING THE ACTUAL LOCATION OF ALL UTILITIES NOT MARKED OUT THROUGH THE ONE CALL CENTER.
2. THE CONTRACTOR SHALL CONTACT RICHARD HOLLAND OF VERIZON AT 212-567-2455 TO ARRANGE FOR THE INSTALLATION OF ADDITIONAL TELEPHONE LINES.
3. THE CONTRACTOR SHALL BE REQUIRED TO PAY ANY UTILITY FEES FOR TELEPHONE SERVICE INCLUDING SET UP AND DELIVERY COSTS NECESSARY TO EXTEND THE UTILITY CABLES TO THE ITS EQUIPMENT LOCATIONS. THE COST FOR THIS SHALL BE INCLUDED IN THE ITEM 11680.956060- UTILITY CONNECTION COSTS.
4. IN PLACES WHERE THE PROPOSED CONDUIT CROSSES OVER EXISTING WATER MAINS AND SEWERS, THE MINIMUM VERTICAL CLEARANCE BETWEEN EITHER OF THESE UTILITIES AND THE CONDUIT SHALL BE 300 mm.
5. THE CONTRACTOR SHALL DIG TEST PITS TO LOCATE THE EXACT LOCATION OF CONDUIT RUNS IN ORDER TO MINIMIZE CONFLICTS WITH EXISTING SUBSURFACE UTILITIES. PAYMENT FOR TEST PITS RELATED TO INSTALLING CONDUIT AND PULLBOXES WILL BE PAID FOR UNDER ITEM 206.05 - TEST PIT EXCAVATION. TEST PITS ARE INCLUDED IN THE PRICE BID FOR EQUIPMENT POLE FOUNDATION ITEMS.

ADDITIONAL ASPHALT MOW STRIP REQUIREMENTS:

1. REFER TO GENERAL GUIDE RAIL NOTE 3 (DWG. NO. GNN-01). IN ADDITION TO THE REQUIREMENTS OF NOTE 3, ALL ASPHALT CONCRETE MOW STRIPS (NOT LIMITED TO THOSE PLACED AT GUIDE RAIL AND BARRIERS, SHALL BE PLACED USING TIMBER FORMWORK TO ENSURE THAT THE RESULTANT MOW STRIP HAS A SMOOTH, UNIFORM AND FULL DEPTH EDGE. THERE WILL BE NO SEPARATE PAYMENT FOR THE TIMBER FORMWORK.

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AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	ITS TRAVEL TIME I-278 AND I-495	PIN X806.49	BRIDGES	CULVERTS	ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED	CONTRACT NUMBER D262095	
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ITS GENERAL NOTES:

1. THE CONTRACTOR SHALL FURNISH SHOP DRAWINGS, (NINE COPIES) OF ALL ELECTRICAL EQUIPMENT TO BE INSTALLED. THE SHOP DRAWINGS MUST BE APPROVED BY THE EIC BEFORE THESE MATERIALS ARE ORDERED. SHOP DRAWINGS SHALL INCLUDE CONSTRUCTION METHODOLOGIES AND PRODUCT MANUFACTURER SPECIFICATIONS.

2. ALL CONDUIT CROSSING ROADS SHALL BE AT RIGHT ANGLES TO ROADWAY EXCEPT AS OTHERWISE INDICATED.

3. CONDUIT IN GROUND SHALL HAVE A MINIMUM COVER OF 760mm EXCEPT WHERE PRECLUDED BY EXISTING SUBSURFACE CONDITIONS SUCH AS DUCT LINES, SEWER PIPES, WATER MAINS, SUBWAY TUNNELS OR OTHER OBSTRUCTIONS PREVENTING REACHING THE REQUIRED DEPTH. WHERE THE AVAILABLE DEPTH BETWEEN THE TOP OF THE CONDUIT AND THE SURFACE OF THE ROADWAY OR SHOULDER IS LESS THAN 350mm, THE CONTRACTOR SHALL FURNISH AND INSTALL GALVANIZED STEEL PLATES NOT LESS THAN 300mm WIDE AND 6mm THICK TO COVER THE TOP OF CONDUIT. THE COST OF STEEL PLATES SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE RESPECTIVE CONDUIT EXCAVATION ITEMS.

4. WHERE NEW CONDUIT ENTERS AN EXISTING PULLBOX, PAYMENT FOR ACCESSING THE PULLBOX AND SEALING OF THE ENTRY SHALL BE INCLUDED IN THE COST BID FOR THE CONDUIT.

5. ALL CABLES SHALL BE SUPPLIED WITH TAGS SHOWING CIRCUIT DESIGNATIONS. PAYMENT FOR THE WORK SHALL BE INCLUDED IN THE PRICE BID FOR THE APPROPRIATE CABLE ITEMS.

6. THE FIBER OPTIC INNERDUCT SHALL BE PROVIDED IN THE FOLLOWING COLORS: BLACK, GREEN, BLUE, AND ORANGE. THE BACKBONE FIBER SHALL BE INSTALLED IN THE GREEN INNERDUCT AND THE DISTRIBUTION FIBER SHALL BE INSTALLED IN THE BLUE INNERDUCT, UNLESS OTHERWISE NOTED.

7. ALL ELECTRICAL WIRES SHALL BE COLOR CODED IN ACCORDANCE WITH CODE REQUIREMENTS AND STANDARD PROCEDURES.

8. THE CONTRACTOR SHALL FURNISH AND INSTALL GROUND RODS AND *6 AWG GROUND CABLES AT ALL METER CABINETS, ELECTRICAL PULLBOXES, PROPERTY LINE BOXES, POLES, TELEPHONE DEMARCATION CABINETS, GROUND MOUNTED EQUIPMENT CONTROL CABINETS AND SIGN STRUCTURES. EQUIPMENT CONTROL CABINETS SHALL BE GROUNDED VIA THE ELECTRICAL SYSTEM AND ASSOCIATED GROUND RODS ADJACENT TO POLE, SIGN STRUCTURE OR CABINET. THE CONTRACTOR SHALL PROVIDE ALL TERMINATION LUGS, GROUND BARS, CONDUITS, FITTINGS, ETC. NECESSARY TO COMPLETE SCOPE OF WORK. PAYMENT SHALL BE INCLUDED IN THE RESPECTIVE ITEM. WORK SHALL BE IN ACCORDANCE WITH STANDARD SPECIFICATIONS SECTION 680-3.12 AND NYSDOT STANDARD SHEET M680-12.

9. GROUND RODS IN ACCORDANCE WITH THE NYSDOT STANDARD SHEET M680-12 SHALL BE INSTALLED AT EACH PULLBOX, METER ASSEMBLY, EQUIPMENT CABINET AND POLE. THE PULLBOX FRAME, RIGID GALVANIZED STEEL CONDUIT(S) ENTERING THE PULLBOX, METER ASSEMBLY, EQUIPMENT CABINET AND POLE SHALL BE CONNECTED TO THE GROUND ROD IN ACCORDANCE WITH THE NYSDOT STANDARD SHEET M680-12.

10. THE CONTRACTOR SHALL COORDINATE MANHOLE ENTRY LOCATIONS, SERVICE BOX LOCATIONS AND METERING CABINET LOCATION WITH CON EDISON BEFORE STARTING WORK AT A SITE. THE CONTRACTOR IS RESPONSIBLE TO STUB CONDUIT 305mm PAST CURB OR EDGE OF PAVEMENT FROM THE PROPERTY LINE BOX AS DIRECTED BY CON EDISON. THE CONTRACTOR SHALL LEAVE 2 METERS OF COILED CABLE IN ALL PROPERTY LINE BOXES AND PULLBOXES WHERE FINAL CON EDISON SERVICE CONNECTIONS ARE TO BE MADE. THE CABLE ENDS SHALL BE WATERPROOFED USING THERMOPLASTIC HEAT SHRINKABLE END CAPS AND SCOTCHKOTE SEALING COMPOUND OR APPROVED EQUAL. THE CONTRACTOR SHALL CONTACT PETER FARRELL OF CON EDISON AT 718-275-3992.

11. THE CONTRACTOR SHALL CONTACT VERIZON TO DETERMINE THE EXACT LOCATION OF THE SERVICE UTILITY POLE OR MANHOLE AND TELEPHONE DEMARCATION CABINET. THE CONTRACTOR SHALL CONTACT THE OWNER OF THE MANHOLE, EITHER VERIZON OR EMPIRE CITY SUBWAY (ECS) TO WHICH A MANHOLE ENTRY MUST BE MADE TO DETERMINE THE LOCATION OF THE ENTRY.
12. CONDUITS SHALL CROSS EXPANSION JOINTS IN STRUCTURES IN A DIRECTION PARALLEL TO THE STRUCTURE MOVEMENT. EXPANSION FITTINGS SHALL BE PROVIDED AT ALL EXPANSION JOINTS. DEFLECTION FITTINGS SHALL BE PROVIDED TO FOLLOW THE NATURAL CURVATURE OF THE STRUCTURE OR AT LOCATIONS WHEREVER ANGULAR MOVEMENT MAY BE ENCOUNTERED. PAYMENT SHALL BE INCLUDED IN THE PRICE BID FOR RESPECTIVE CONDUIT ITEMS.

13. NOMINAL DIMENSIONS FOR CAST IRON JUNCTION BOXES AND SURFACE MOUNTED PULL BOXES ARE AS FOLLOWS:
ITEM 680.51500111 610mm X 457mm X 914mm
ITEM 670.40 AS REQUIRED

14. THE CONTRACTOR SHALL BE REQUIRED TO RE-PROGRAM OR REDIRECT/REPOSITION THE EXISTING RADAR DETECTOR SYSTEM WHENEVER A NEW TRAFFIC PATTERN IS INSTITUTED OR NEW CONSTRUCTION STAGE IS ADVANCED, IN ORDER TO BE ABLE TO CAPTURE ALL VEHICULAR TRAFFIC WITHIN THE SYSTEM THROUGHOUT THE DURATION OF THE CONTRACT. THE CONTRACTOR SHALL, AT THE COMPLETION OF THE CONSTRUCTION WORK, REPROGRAM THE DETECTORS TO MONITOR NORMAL TRAFFIC PATTERNS. NO ADDITIONAL COST WILL BE PAID FOR THIS WORK.

15. FINAL CONNECTIONS TO VMS ASSEMBLIES SHALL UTILIZE A 2 NPS FLEXIBLE LIQUID TIGHT CONDUIT WITH A MINIMUM LENGTH OF 152mm. PAYMENT SHALL BE MADE UNDER THE 2 NPS GALVANIZED STEEL CONDUIT ITEM.

16. ALL GROUND MOUNTED TRAFFIC SIGNAL POLES SHALL HAVE A BREAK-AWAY TRANSFORMER BASE. TRAFFIC SIGNAL POLES MOUNTED ON A VIADUCT OR BRIDGE STRUCTURE SHALL INCLUDE AN INTERNAL DAMPENER. THE TRAFFIC SIGNAL POLES SHALL BE OF GALVANIZED STEEL CONSTRUCTION.

17. CORE DRILLING OF WALLS OR SAWCUTTING OF BARRIER FOR TRANSITION OF CONDUITS FROM UNDERGROUND TO ABOVE GRADE STRUCTURES SHALL BE PAID FOR UNDER ITEM 11206.0312M. CONTRACTOR SHALL PATCH AND WATERPROOF ALL PENETRATIONS UPON INSTALLATION OF CONDUITS.

18. THE CONTRACTOR SHALL BE MADE AWARE THAT THERE ARE AREAS WHERE THE TOP OF GRADE IS LEVEL OR APPROXIMATELY 1 METER ABOVE THE ROADWAY AT LOCATIONS SHOWN ON THE PLANS. THE CONTRACTOR SHALL BE RESPONSIBLE TO TRANSITION CONDUIT FROM THE ROADWAY TO EQUIPMENT LOCATED AT GRADE. ALL NECESSARY SUPPLEMENTAL WORK TO ACCOMPLISH THIS, SUCH AS REMOVAL/REINSTALLATION OF BARRIER, CORE DRILLING BARRIER FOUNDATION, SUBSTANTIAL GRADE EXCAVATION, SAWCUTTING AND PATCHING BARRIER, ETC. SHALL BE INCLUDED AND PAID FOR UNDER THE VARIOUS CONTRACT PAYMENT ITEMS.

19. ALL NECESSARY SUPPLEMENTAL WORK TO ACCOMPLISH THE INSTALLATION OF CONDUITS AND EQUIPMENT AT BRIDGES AND VIADUCTS, SUCH AS REMOVAL AND REINSTALLATION OF FENCES AND RAILINGS, INCLUSIVE OF RESETTNG OF POSTS, SHALL BE INCLUDED AND PAID FOR UNDER THE VARIOUS CONTRACT PAYMENT ITEMS. THE CONTRACTOR SHALL SCHEDULE HIS WORK SO THAT NO AREA IS LEFT UNPROTECTED AT THE END OF THE WORK SHIFT.

20. UNLESS SHOWN OTHERWISE IN THE ITS CONSTRUCTION PLANS, THE CONTRACTOR SHALL COIL ADDITIONAL SLACK INTO PULLBOXES AS FOLLOWS:
ELECTRICAL PULL BOXES 2 METERS

COMMUNICATION PULL BOXES

ITEM 680.51400211 20 METER
ITEM 680.5196 11 20 METER

21. ITEM 680.51400211 AND ITEM 680.5196 11 PULL BOXES SHALL BE LOCATED SUCH THAT A SPLICE CAN BE EXTENDED FROM THE PULLBOX TO A VEHICLE WHERE SPLICING WORK CAN BE PERFORMED.

22. THE CONTRACTOR SHALL BE MADE AWARE THAT ITEM 10680.5301M - CLEAN EXISTING CONDUIT, AS SHOWN ON THE PLANS, IS ACTUALLY USED FOR AN EMPTY CONDUIT SYSTEM. THE BID PRICE SHALL REFLECT THAT REMOVAL OF CABLES PRIOR TO CLEANING, AS OUTLINED IN THE SPECIFICATION, IS NOT REQUIRED. MULTI-CELL CONDUIT IS REPRESENTED BY THE QUANTITY (4) TO INDICATE THE CLEANING OF EACH INNERDUCT. ALL UNUSED INNER DUCTS SHALL BE PLUGGED AFTER CLEANING.

23. PULL BOXES SHOWN ON THE PLANS IN CLOSE PROXIMITY TO TREES OR SHRUBS SHALL BE LOCATED, WITH PRIOR EIC APPROVAL. REFER TO THE LANDSCAPE PLANS INCLUDED IN THIS CONTRACT.

24. THE CONTRACTOR SHALL NOTIFY CON-EDISON AT LEAST 2-WEEKS IN ADVANCE OF REMOVAL OF ITS ELECTRICAL SERVICES FOR DE-ENERGIZATION OF SERVICE ENTRANCE CONDUCTORS. THE CONTRACTOR SHALL NOTIFY VERIZON AT LEAST 2-WEEKS IN ADVANCE OF REMOVAL OF ITS TELEPHONE SERVICES FOR DE-ENERGIZATION OF SERVICE ENTRANCE CONDUCTORS.

25. WHENEVER METALLIC CONDUIT MEETING THE REQUIREMENTS OF SUB-SECTION 723.20, METAL STEEL CONDUIT, ZINC COATED IS SPECIFIED, IT SHALL BE CLASS 1 - RIGID METAL STEEL CONDUIT. ALL GALVANIZED STEEL CONDUITS SHALL BE HOT DIPPED. ELECTRO - COATED SHALL NOT BE USED. CONDUITS ENTERING ENCLOSURES SHALL HAVE THREADED BUSHINGS AND DOUBLE LOCK - NUTS OR A SINGLE LOCK - NUT WHERE ENTRY IS THREADED. CONDUIT INSULATED GROUNDING BUSHING SHALL BE MALLEABLE IRON WITH BRONZE LUG.

26. ALL SURFACE MOUNTED FIBER OPTIC PULL BOXES SHALL BE KEY AND PAD LOCKABLE. CONTRACTOR SHALL PROVIDE A 4-DIGIT RESETTABLE BRASS COMBINATION PADLOCK FOR EACH PULL BOX.

27. THE CONTRACTOR SHALL DRILL OPENINGS IN EXISTING POLES, AS REQUIRED, TO ALLOW FOR THE INSTALLATION OF AN EQUIPMENT CABLE TERMINATION FITTING OR A CONDUIT ENTRY. WORK SHALL BE PAID FOR UNDER RESPECTIVE EQUIPMENT ITEM.

28. THE CONTRACTOR SHALL CONTACT "ORTI-COM MANUFACTURING NETWORK, INC." AT 504-736-0131 FOR PROVIDING PARTS AND ACCESSORIES FOR EXISTING MULTI-CELL CONDUIT MODIFICATIONS OR REPAIRS.

29. ALL UNDERGROUND CONDUITS SHALL BE PROPERLY PITCHED TO ALLOW POTENTIAL WATER INFILTRATION TO DRAIN TOWARDS THE PULL BOXES.

30. THE CONTRACTOR SHALL BE MADE AWARE THAT SEPARATE CONDUIT SYSTEMS ARE SHOWN FOR EACH OF THE FOLLOWING: FIBER OPTIC CABLES, EQUIPMENT COMPOSITE CABLES TO EXTERNAL DEVICES AND ELECTRICAL CABLES. FIBER OPTIC CABLES ARE ONLY TO BE INSTALLED IN THE INNERDUCTS OF THE 4 NPS OR MULTI-CELL CONDUIT AND IN THE 2 NPS OR 3 NPS CONDUIT ORIGINATING FROM THE RESPECTIVE FIBER OPTIC PULL BOX, CONTAINING THE TERMINATION CABLE SPLICE, TO THE CONTROL EQUIPMENT CABINET. COMPOSITE CABLES ARE ONLY TO BE INSTALLED IN THE 2 NPS CONDUIT ORIGINATING FROM THE RESPECTIVE CONTROL EQUIPMENT CABINET TO THE REMOTE RADAR DETECTORS VIA FIBER OPTIC PULL BOXES, WHERE SHOWN ON THE PLANS.

31. THE CONTRACTOR SHALL EXAMINE AND VERIFY ALL EXISTING AND GIVEN CONDITIONS AND DIMENSIONS WITH THOSE SHOWN IN THE CONTRACT DOCUMENTS. IF THE FIELD CONDITIONS AND DIMENSIONS DIFFER FROM THOSE SHOWN ON THE CONTRACT DOCUMENTS, THE CONTRACTOR SHALL NOTIFY THE ENGINEER. ALL FIELD CONDITIONS AND DIMENSIONS SHALL BE SO NOTED ON THE DRAWINGS AND SUBMITTED FOR APPROVAL.

32. A SURVEY INSPECTION OF THE EXISTING STREET LIGHTING SYSTEM SHALL BE PERFORMED BY THE CONTRACTOR, THE EIC, AND NYCDOT STREET LIGHTING DIVISION REPRESENTATIVE PRIOR TO THE START OF ANY WORK TO BE PERFORMED. ALL LIGHTS SHOULD BE LISTED AS FUNCTIONAL OR NON-FUNCTIONAL DURING THIS SURVEY INSPECTION AND AGREED TO BY ALL PARTIES.

33. ALL CONDUIT WORK SHOWN AT THE EDGE OF THE ROADWAY SHALL BE LOCATED IN THE SHOULDER OR RAISED ASPHALT AREA ADJACENT TO ROADWAY, UNLESS OTHERWISE NOTED.

34. WALL MOUNTED CONDUITS, SURFACE MOUNTED PULL BOXES AND CAST IRON JUNCTION BOXES SHALL BE LOCATED NEAR THE TOP OF THE WALL OR COLUMN, UNLESS OTHERWISE NOTED.

35. ANY TYPE OF CONDUITS ENCOUNTERED WHILE EXCAVATING FOR A SIGN STRUCTURE, POLE, CONDUIT, PULLBOX, CABINET FOOTING, OR SIMILAR FACILITY ARE TO BE TEMPORARILY SUPPORTED AND OFFSET TO THE SIDE OF THE PROPOSED FACILITY. PAYMENT FOR THIS WORK SHALL BE INCLUDED IN THE BID PRICES OF VARIOUS WORK ITEMS IN THIS CONTRACT.

36. AGENCIES WITH WHICH THE CONTRACTOR MAY BE DIRECTLY OR INDIRECTLY INVOLVED IN NOTIFICATIONS AND COORDINATION INCLUDE BUT NOT LIMITED TO THE FOLLOWING AGENCIES:

NEW YORK CITY DEPARTMENT OF TRANSPORTATION
BUREAU OF ELECTRICAL CONTROL (BUREAU OF GAS AND ELECTRICITY)
NEW YORK CITY POLICE DEPARTMENT
NEW YORK CITY FIRE DEPARTMENT
DEPARTMENT OF ENVIRONMENTAL PROTECTION
MTA BRIDGES AND TUNNELS
PORT AUTHORITY OF NEW YORK AND NEW JERSEY
NEW YORK CITY DEPARTMENT OF PARKS AND RECREATION
JOINT TRAFFIC MANAGEMENT CENTER
TRAFFIC ENGINEERING AND SAFETY OF NYSDOT

37. THE LOCATION OF CONDUITS AND EQUIPMENT ARE DIAGRAMMATIC (UNLESS DIMENSIONED). THE CONTRACTOR SHALL SURVEY AND STAKEOUT ALL ITS EQUIPMENT LOCATIONS (SUCH AS TRANSMITS, VMS STRUCTURES, HUBS, CAMERA POLES, CABINETS, ETC.) AND THE GENERAL LAYOUT OF THE TRENCHING AND EXCAVATION TO BE CONSTRUCTED FOR APPROVAL BY THE EIC. SURVEY AND STAKEOUT SHALL BE PERFORMED WITH INPUT FROM THE EIC BY A WALK THROUGH PRIOR TO THE ACTUAL STAKEOUT. PAYMENT SHALL BE MADE UNDER ITEM 625.01M.

38. WITHIN TEN (10) DAYS AFTER AWARD OF CONTRACT, THE CONTRACTOR SHALL PROVIDE TEN (10) COPIES OF A PROPOSED SEQUENCE OF CONSTRUCTION OF THE ENTIRE PROJECT AND A PROJECT SCHEDULE COVERING ALL ITEMS OF WORK. THIS SCHEDULE SHALL CLEARLY INDICATE THE INTERRELATIONSHIP OF INDIVIDUAL TASKS. THE SCHEDULE SHALL BE DIVIDED INTO LOGICAL STAGES INCLUDING SCHEDULING OF EQUIPMENT, APPROVAL AND FACTORY DEMONSTRATION TESTS, DELIVERY OF EQUIPMENT, AND CLEARLY INDICATED INSTALLATION AND COMPLETION DATES.

39. PRESSURE SENSITIVE REFLECTIVE SHEETING EQUIPMENT IDENTIFICATION TAGS SHALL BE FURNISHED AND INSTALLED ON ALL CONTROL EQUIPMENT CABINETS, TRAFFIC SIGNAL POLES (EXCEPT INTERSECTION SIGNAL POLES), EXISTING OR PROPOSED STRUCTURES FOR VMS SIGNS, NON-METALLIC PULL BOXES, AND RISERS. THE TAGS FURNISHED SHALL DISPLAY THE APPROPRIATE IDENTIFICATION TEXT, AS DIRECTED BY THE ENGINEER, IN A HORIZONTAL FORMAT AND SHALL BE INSTALLED ON THE SIDE OF THE EQUIPMENT FACING ONCOMING TRAFFIC. THE IDENTIFICATIONS TAGS SHALL BE TYPICALLY PLACED 2.1 METERS ABOVE GRADE OR AS ORDERED BY THE ENGINEER. THE TEXT SHALL BE 76mm HIGH, SERIES C, AND SHALL CONSIST OF WHITE COLORING ON A DARK BLUE BACKGROUND. SHEETING SHALL BE SPECIFIED AS IN SUBSECTION 730-05.01. PAYMENT FOR FURNISHING AND INSTALLING IDENTIFICATION TAGS SHALL BE INCLUDED IN THE VARIOUS CONTRACT ITEMS. NO SEPARATE PAYMENT SHALL BE MADE FOR THIS WORK.

40. THE EIC WILL PROVIDE DETAILS OF ALL CONNECTIONS WITH EXISTING EQUIPMENT LOCATED AT THE JOINT TRANSPORTATION MANAGEMENT CENTER (JTM). ALL WORK PERFORMED AT THE JTM SHALL BE COORDINATED WITH THE EIC. A MINIMUM OF TEN (10) WORKING DAYS PRIOR TO COMMENCING ANY WORK AT THE JTM THE CONTRACTOR SHALL SUBMIT IN WRITING TO THE EIC A DETAILED WORK PLAN, THAT SHALL INCLUDE A SCHEDULE OF THE WORK TO BE PERFORMED AND ASSOCIATED DOWN TIME FOR THE EXISTING EQUIPMENT AT THE JTM.

41. CONDUITS SHALL ENTER SURFACE MOUNTED PULL BOXES AND JUNCTION BOXES ADJACENT TO CORNERS SO AS TO PROVIDE FOR THE MAXIMUM ALLOWABLE CABLE BENDING RADIUS AND ADDITIONAL FUTURE CONDUIT ENTRIES.

42. WHEREVER CONDUITS ARE INSTALLED FOR FIBER OPTIC (OR ELECTRICAL POWER) A NYLON PULL WIRE SHALL BE INSTALLED WITH THE CABLES. THE PULL WIRE SHALL BE CONTINUOUS BETWEEN PULL POINTS. PULL WIRES SHALL BE INSTALLED AND LEFT IN ANY PROPOSED EMPTY CONDUITS OR INNERDUCTS. ALL COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR CONDUIT OR INNERDUCTS.

43. POWER UTILITY CONNECTION IS SHOWN ON THE PLANS FOR CONTRACTOR REFERENCE ONLY. THE MATERIALS AND WORK ASSOCIATED WITH THIS ITEM WILL BE SUPPLIED AND PERFORMED BY CON EDISON. THE CONTRACTOR SHALL BE REQUIRED TO PAY ANY UTILITY FEES FOR ELECTRICAL SERVICE INCLUDING SET UP AND DELIVERY COSTS NECESSARY TO EXTEND THE UTILITY CABLES TO THE ITS EQUIPMENT LOCATIONS. THE COST FOR THIS SHALL BE INCLUDED IN THE ITEM 11662.7510M - ELECTRICAL UTILITY MAKE READY.

44. TO PREVENT DELAYS IN PROVIDING UTILITY SERVICE DUE TO BACKLOG, THE CONTRACTOR IS ADVISED TO START COORDINATION WITH THE UTILITY COMPANIES EARLY IN THE PROJECT.

45. THE CONTRACTOR SHALL DIG TEST PITS, WITH PRIOR EIC APPROVAL, TO DETERMINE THE EXACT LOCATION OF PROPOSED CONDUITS AND PULL BOXES AND IN ORDER TO MINIMIZE CONFLICTS WITH EXISTING SUBSURFACE UTILITIES. PAYMENT FOR TEST PITS RELATED TO THIS WORK SHALL BE PAID FOR UNDER ITEM 206.05M - TEST PIT EXCAVATION. TEST PITS ARE INCLUDED IN THE PRICE BID FOR EQUIPMENT POLE AND SIGN STRUCTURE FOUNDATION ITEMS.



AFFIX SEAL:
ON:

ALTERED BY:
ON:

AS-BUILT REVISIONS
DESCRIPTION OF ALTERATIONS:

ITS TRAVEL TIME I-278 AND I-495

PS&E DATE: 06/21/2012

COUNTY: KINGS AND QUEENS

PIN X806.49

BRIDGES

CULVERTS

ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED

CONTRACT NUMBER

D262095

DRAWING NO. NOT-01

SHEET NO. 09



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ITS GENERAL NOTES: (CONTINUED)


46. IF ANY UTILITIES ARE DAMAGED AND THE NECESSARY REPAIRS ARE NOT SATISFACTORY TO THE EIC, THE WORK MAY BE PERFORMED BY THE RESPECTIVE UTILITY COMPANY AND THE COST THEREOF CHARGED AGAINST THE CONTRACTOR. THE COST ASSOCIATED WITH THE RESTORATION OF SERVICE SHALL BE AT THE CONTRACTOR'S EXPENSE. THE COST OF ALL WORK CONNECTED WITH MAINTAINING AND PROTECTING UTILITIES AFFECTED BY THE WORK OF THE CONTRACT SHALL BE BORNE BY THE CONTRACTOR AND THE COST INCLUDED IN THE PRICE BID FOR THE VARIOUS PAYMENT ITEMS.
47. CONDUITS CROSSING A BRIDGE OR VIADUCT IN A DIRECTION PARALLEL TO THE ROADWAY SHALL BE MOUNTED TO STEEL BEAM NEAREST TO CONCRETE FASCIA, UNLESS OTHERWISE NOTED.
48. THE CONTRACTOR'S ATTENTION IS CALLED TO THE PRESENCE OF SOLAR POWERED CELLULAR CALLBOXES WHICH WILL BE IN PLACE AT THE TIME CONSTRUCTION IS SCHEDULED TO BEGIN. THE CONTRACTOR SHALL TAKE THE NECESSARY PRECAUTIONS TO PREVENT ANY DAMAGE TO THESE CALLBOXES DURING THE COURSE OF CONSTRUCTION. IF ANY OF THESE CALLBOXES INTERFERE WITH THE WORK, THEY MAY BE TEMPORARILY RELOCATED AS ORDERED BY THE EIC, AND THEN PUT BACK AT THE CONTRACTOR'S EXPENSE. ANY BOXES DAMAGED BY THE CONTRACTOR SHALL BE REPLACED IN A MANNER SATISFACTORY TO THE EIC AND THE NYPD CALLBOX UNIT AT NO EXPENSE TO THE CITY AND STATE. THE CONTRACTOR IS NOTIFIED THAT EACH CALLBOX HAS AN INTERNAL ALARM THAT MUST BE SHUT OFF PRIOR TO REMOVAL. THE COST OF THE WORK IS TO BE INCLUDED UNDER ITEM 619.01M. THEREFORE THE NEW YORK CITY POLICE CALLBOX UNIT MUST BE NOTIFIED 48 HOURS PRIOR TO THE REMOVAL AT (212)-678-1851.
49. WITHIN 30 DAYS OF THE COMPLETION OF THE CABLE, CONDUIT AND EQUIPMENT INSTALLATIONS, THE CONTRACTOR SHALL SUBMIT TO THE NYSDOT AS-BUILT 1:500 SCALE PLAN VIEW SURVEY SHEETS SHOWING THE EXACT LOCATION OF THE CABLE AND EQUIPMENT WITH OFFSETS AND ELEVATION OF CONDUITS INSTALLED IN SHALLOW TRENCHES (LESS THAN 350mm) TO THE RIGHT EDGE OF THE TRAVEL LANE AND TO OTHER FIXED REFERENCE POINTS AS NECESSARY IN A MANNER APPROVED BY THE EIC. THESE PLAN SHEETS SHALL BE REPRODUCIBLE AND THE SAME SIZE AS THE CONTRACT PLANS. PAYMENT SHALL BE MADE UNDER ITEM 625.01M - SURVEY OPERATIONS.
50. CONTRACTOR SHALL BE MADE AWARE THAT THERE IS LIMITED ACCESS TO GRADED AREAS ADJACENT TO THE ROADWAY DUE TO LANDSCAPE PROTECTION SCHEMES. THESE AREAS MAY BE LOCATED BEHIND A BARRIER OR RETAINING WALL. THE CONTRACTOR SHALL PROVIDE A CRANE STATIONED ON THE ROADWAY, AS REQUIRED, TO LIFT SMALL MACHINERY OVER SUCH OBSTRUCTIONS IN ORDER TO PERFORM THE NECESSARY WORK AT THESE LOCATIONS. ALL COSTS ASSOCIATED WITH THE UTILIZATION OF A CRANE SHALL BE PAID FOR UNDER VARIOUS CONTRACT PAY ITEMS.
51. ALL EXCAVATED MATERIAL SHALL BE REMOVED FROM THE SITE AND SHALL NOT BE SPREAD OVER EXISTING PROFILES.
52. THE CONTRACTOR SHALL STOCKPILE MATERIALS ONLY IN THE STORAGE AREA. MATERIAL SHALL NOT BE STOCKED PILED IN ANY WORK ZONE. ANY MATERIAL DELIVERED TO THE WORK ZONE SHALL BE INSTALLED AND NOT STOCKPILED.
53. NO EQUIPMENTS SHALL BE CONNECTED TO THE GFI OUTLETS OF THE CABINET.
54. WHENEVER PROPRIETARY EQUIPMENT IS SPECIFIED AND THE CONTRACTOR PROPOSES "EQUAL" EQUIPMENT, THE CONTRACTOR SHALL DEMONSTRATE TO THE SATISFACTION OF THE ENGINEER THAT PROPOSED EQUIPMENT IS DIRECTLY INTERCHANGEABLE WITH THE EXISTING SPECIFIED PROPRIETARY EQUIPMENT.
55. THE TOPS OF ALL NEW GROUND-INSTALLED ITS DEVICES (CABINETS, BOXES, MANHOLES, ETC) SHALL MATCH THE SLOPES OF THE EXISTING GRADES AT THESE LOCATIONS.


ITS REMOVAL NOTES:

- (ALL WORK NOTED BELOW SHALL BE PAID UNDER ITEM 11680.9991 - ITS REMOVALS)
1. CONTRACTOR IS RESPONSIBLE TO PATCH ALL FINISHED SURFACES TO MATCH EXISTING AFTER CONDUITS, CABLES, PULL BOXES, JUNCTION BOXES AND EQUIPMENT ARE REMOVED AT LOCATIONS SHOWN ON THE PLANS.
2. ALL OPENINGS REMAINING IN EXISTING JUNCTION BOXES, PULL BOXES, POLES, LIGHTING CONTROL CABINETS, ETC. AFTER REMOVAL OF CONDUITS, CABLES AND EQUIPMENT SHALL BE SEALED AND MADE WATERPROOF.
3. THE WORK ASSOCIATED WITH THE REMOVAL OF UNUSED CABINET FOUNDATIONS SHALL BE PERFORMED PRIOR TO THE INSTALLATION OF PROPOSED PULL BOXES THAT ARE LOCATED IN THE SAME AREA AND ARE INTENDED TO INTERCEPT ONE OR MORE CONDUITS. UNDERGROUND CONDUITS SHALL BE CAREFULLY CUT AT 305MM (MIN.) FROM EDGE OF CONCRETE AND ENTIRE FOUNDATION IS TO BE REMOVED.
4. CONTRACTOR SHALL LOCATE AND DISCONNECT THE POWER SUPPLY PRIOR TO REMOVAL OF ELECTRICAL CABLES FOR ITS EQUIPMENT.
5. CONTRACTOR SHALL CUT AND CAP CONDUIT 305MM (MIN.) BELOW GROUND. SEVERED ENDS OF REMAINING CABLES SHALL BE SEALED AND MADE WATERPROOF. BACKFILL AND RESTORE AREA, AS REQUIRED, TO MATCH EXISTING CONDITIONS.
6. ALL REMOVALS SHALL BE PROPERLY DISPOSED OF BY THE CONTRACTOR, WHEN REQUIRED OR AS DIRECTED BY THE ENGINEER.
7. MATERIALS TO BE SALVAGED SHALL BE SHIPPED TO A STATE STORAGE SITE AS DIRECTED BY THE ENGINEER.
8. IT IS THE INTENT OF THIS CONTRACT TO INSTALL AND MAKE THE PROPOSED ITS SYSTEM FULLY OPERATIONAL BEFORE THE VARIOUS COMPONENTS OF THE EXISTING ITS SYSTEM ARE REMOVED. REMOVALS CAN BE PERFORMED ONLY AFTER THE ENTIRE SYSTEM PROVES THAT ALL NEW EQUIPMENT IS COMPLETELY DEPENDABLE, AS VERIFIED BY THE ENGINEER.
9. UPON REMOVAL OF ITEMS IN GRADED AREAS THE CONTRACTOR SHALL BACKFILL WITH SUITABLE EXCAVATED MATERIAL TO WITHIN 300MM OF THE EXISTING TOP OF GRADE. THE CONTRACTOR SHALL BE REQUIRED TO RESTORE THE EXCAVATED AREA TO MATCH EXISTING GRADE WITH 300MM (MIN. DEPTH) OF NEW TOPSOIL (TYPE A) AS SPECIFIED IN THE LANDSCAPE RESTORATION DETAILS PROVIDED IN THESE CONTRACT PLANS.
10. EXCAVATED MATERIAL GENERATED BY PULL BOX, CONDUIT AND FOUNDATION INSTALLATIONS, INCLUDING ANY OTHER EXCAVATIONS NEEDED TO PERFORM THE WORK, SHALL BE REMOVED FROM THE SITE AND SHALL NOT BE SPREAD OVER THE EXISTING TERRAIN.
11. CONTRACTOR NEEDS TO CONTACT JTMC 5 DAYS PRIOR TO THE START OF WORK, PROVIDE LIST OF ITS DEVICES AFFECTED, AND EXPECTED TIME & DURATION OF WORK. JTMC WILL PROVIDE OPERATIONAL STATUS OF THE DEVICES.
12. CONTRACTOR NEED TO SCHEDULE THE WORK A SUCH WAY AS TO MINIMIZE DOWNTIME OF EXISTING DEVICES.
13. ALL ITS DEVICES MUST BE TURNED ON AT THE END OF EACH WORK DAY.
14. ALL DEVICES DAMAGED OR NOT BEING ABLE TO COMMUNICATE WITH JTMC, AFTER WORK COMPLETION MUST BE REPAIRED BY CONTRACTOR AT NO COST TO STATE.

SURVEY OPERATIONS NOTE:

EQUIPMENT LOCATIONS SHOWN IN THE PLAN SHEETS ARE APPROXIMATE. THE CONTRACTOR SHALL SURVEY AND STAKEOUT (INCLUDING VERTICAL CONTROLS) ALL ITS EQUIPMENT LOCATIONS (SUCH AS VMS STRUCTURES, HUBS, CAMERA POLES, CABINETS, ETC.) AND THE GENERAL LAYOUT OF THE TRENCHING AND EXCAVATION TO BE CONSTRUCTED, PRIOR TO COMMENCING WORK, FOR APPROVAL BY THE EIC, NYSDOT TE&S AND MAINTENANCE. SURVEY AND STAKEOUT SHALL BE PERFORMED WITH INPUT FROM THE EIC BY A WALK THROUGH PRIOR TO THE ACTUAL STAKEOUT. PAYMENT SHALL BE MADE UNDER ITEM 625.01M - SURVEY OPERATIONS. UPON COMPLETION OF THE ACTUAL STAKEOUT, THE CONTRACTOR SHALL NOTIFY THE EIC AND REQUEST THAT THE STAKEOUT LOCATIONS BE REVIEWED BY THE EIC, TE&S AND MAINTENANCE. A LOCATION APPROVAL OR A REQUEST TO RELOCATE WILL BE ISSUED BY THE EIC, NYSDOT TE&S AND MAINTENANCE WITHIN 2 WEEKS OF THE CONTRACTOR'S REQUEST FOR REVIEW.

AFFIX SEAL: ON:	ALTERED BY: ON:
	

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	ITS TRAVEL TIME I-278 AND I-495	PIN X806.49	BRIDGES	CULVERTS	ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED	CONTRACT NUMBER		
	PS&E DATE: 06/21/2012					D262095		
							ITS NOTES - 2	DRAWING NO. NOT-02
	COUNTY: KINGS AND QUEENS							SHEET NO. 10


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NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11
DOCUMENT NAME: X80649_cph.its_not_02.dgn

SUGGESTED SEQUENCE OF CONSTRUCTION

TYPE OF WORK	MONTH 1	MONTH 2	MONTH 3	MONTH 4	MONTH 5	MONTH 6	MONTH 7	MONTH 8	MONTH 9	MONTH 10	MONTH 11	MONTH 12	MONTH 13	MONTH 14	MONTH 15	MONTH 16	MONTH 17	MONTH 18	MONTH 19	MONTH 20	MONTH 21	MONTH 22	MONTH 23	MONTH 24
MOBILIZATION																								
EQUIPMENT REMOVAL																								
CONDUIT INSTALLATION/FOUNDATION/POLES																								
WIRE AND CABLE INSTALLATIONS																								
EQUIPMENT INSTALLATIONS																								
TESTING																								
SYSTEM INTEGRATION																								
TRAINING																								
SYSTEM ACCEPTANCE																								
LANDSCAPE RESTORATION																								
PROJECT CLOSING/PUNCHLIST																								

AFFIX SEAL:
ON:



ALTERED BY:
ON:

AS-BUILT REVISIONS
DESCRIPTION OF ALTERATIONS:

ITS TRAVEL TIME I-278 AND I-495

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COUNTY: KINGS AND QUEENS

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BRIDGES

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CONSTRUCTION SCHEDULE

CONTRACT NUMBER
D262095

DRAWING NO. SCH-01
SHEET NO. 11

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NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11
DOCUMENT NAME: X80649_cph_con_sch.dgn

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	ITS TRAVEL TIME I-278 AND I-495	PIN X80649	BRIDGES	CULVERTS	ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED	CONTRACT NUMBER	
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						DRAWING NO. MJP-01	
						SHEET NO. 12	
	COUNTY: KINGS AND QUEEN						

NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11

DOCUMENT NAME: X80649_cph_mjt.dgn

FILE NAME = X80649_cph_wzn_01.dgn
DATE/TIME = 03-AUG-2012 16:02
USER = kdesai

DESIGN SUPERVISOR RANDOLPH HUNT

JOB MANAGER MAREK CMIK

DESIGN YELENA GRIG

CHECK MAREK CMIK

DRAFTING YELENA GRIG

CHECK MAREK CMIK

PROJECT MANAGER RAJ AMIN

GENERAL

1. THE TYPICAL DETAILS DEPICTED ON THE STANDARD SHEETS AND IN THE NATIONAL MUTCD, AND NYS SUPPLIMENT REFLECT THE MINIMUM REQUIREMENTS.
2. THE CONTRACTOR MUST SUBMIT TO THE ENGINEER, IN WRITING, PROPOSED REVISIONS TO THE TRAFFIC CONTROL PLAN FOR REVIEW AND APPROVAL BY THE REGIONAL DIRECTOR OR HIS/HER DESIGNEE FIVE (5) WORK DAYS PRIOR TO THE PLANNED IMPLEMENTATION OF SUCH PROPOSED REVISIONS, EXCEPT FOR CHANGES THAT ALTER THE SCOPE OF THE TRAFFIC CONTROL PLAN. SUCH CHANGES IN SCOPE MUST BE SUBMITTED TO THE ENGINEER FOR APPROVAL BY THE REGIONAL DIRECTOR OR HIS/HER DESIGNEE THIRTY (30) WORKING DAYS PRIOR TO IMPLEMENTATION OF SUCH REVISIONS.
3. THE CONTRACTOR SHALL PROVIDE THE ENGINEER, IN WRITING, WITH THE NAMES, ADDRESSES, AND TELEPHONE NUMBERS OF STAFF WHO ARE AUTHORIZED TO SECURE LABOR, MATERIALS, AND EQUIPMENT FOR EMERGENCY REPAIRS OUTSIDE NORMAL WORKING HOURS. THE ENGINEER WILL PROVIDE THE SUBMITTED INFORMATION TO REGIONAL MANAGEMENT, THE NEW YORK STATE POLICE, THE RESIDENT ENGINEER, AND THE LOCAL POLICE.

ACTIVITY AREA

1. VACANT.
2. WHEN TWO OR MORE ACTIVITY AREAS ARE ADJACENT, OVERLAP, OR ARE IN CLOSE PROXIMITY, THE CONTRACTOR SHALL ENSURE THERE ARE NO CONFLICTING SIGNS AND THAT LANE CONTINUITY IS MAINTAINED THROUGHOUT ALL WORK AREAS.

SIGNS

1. THE LOCATIONS OF THE SIGNS SHOWN ON THE WORK ZONE TRAFFIC CONTROL PLANS AND DETAILS MAY BE ADJUSTED BASED ON SIGHT DISTANCE AND OTHER CONSIDERATIONS. THE FINAL LOCATIONS OF SIGNS ARE SUBJECT TO APPROVAL OF THE ENGINEER.
2. ANY EXISTING SIGNS, INCLUDING OVERHEAD SIGNS, WHICH CONFLICT WITH THE TEMPORARY TRAFFIC CONTROL SIGN LAYOUT SHALL BE COVERED, REMOVED, STORED OR RESET, AS APPROVED BY THE ENGINEER. ALL APPROPRIATE EXISTING SIGNS SHALL BE RESTORED TO THEIR ORIGINAL CONDITION AND/OR LOCATION UNLESS OTHERWISE REPLACED IN THIS CONTRACT.
3. SIGNS AT OR NEAR INTERSECTIONS SHALL BE PLACED SO THAT THEY DO NOT OBSTRUCT A MOTORIST’S LINE OF SIGHT.
4. ALL WARNING AND REGULATORY SIGNS SHALL BE POSTED ON BOTH SIDES OF MULTI-LANE DIVIDED HIGHWAYS, MULTI-LANE RAMPS, AND ONE-WAY STREETS. IN CASES WHERE LANE RESTRICTIONS REDUCE THE TRAVEL LANE TO ONE LANE, SIGNS SHALL BE POSTED ON THE RIGHT SIDE OF THE ACTIVE TRAVEL LANE, UNLESS OTHERWISE AUTHORIZED BY THE ENGINEER.
5. SIGNS MOUNTED ON THE MEDIAN OF DIVIDED HIGHWAYS WHERE MEDIAN BARRIER IS IN PLACE MAY BE MOUNTED ON THE BARRIER WITH A SADDLE TYPE BRACKET. LAYING THE SIGN DOWN IN A HORIZONTAL POSITION IS NOT PERMITTED.
6. THE DIMENSIONS OF WORK ZONE TRAFFIC CONTROL SIGNS ARE DESCRIBED IN THE NATIONAL MUTCD. ANY CHANGES TO THE DIMENSIONS SHALL BE APPROVED BY THE REGIONAL DIRECTOR OR BY HIS/HER DESIGNEE.
7. NYR9-12 MAY BE USED IN PLACE OF NYR9-11.

CHANELIZATION DEVICES

1. WHERE POSSIBLE ALL CHANNELIZING AND GUIDING DEVICES ARE TO BE PLACED SO AS TO PROVIDE A MINIMUM 0.6m LATERAL CLEARANCE TO THE TRAVELED WAY.

PUBLIC ACCESS

1. PROPERTY OWNERS WHOSE DRIVEWAYS WILL BE MADE INACCESSIBLE SHALL BE NOTIFIED BY THE CONTRACTOR AT LEAST 24 HOURS PRIOR TO RESTRICTING USE OF THE DRIVEWAY. FOR MULTIPLE ACCESS PROPERTIES, AT LEAST ONE DRIVEWAY SHALL BE OPEN AT ALL TIMES. ACCESS SHALL BE RESTORED TO ALL DRIVEWAYS AS SOON AS POSSIBLE.
2. SUITABLE RAMPS SHALL BE INSTALLED TO MAINTAIN SMOOTH TRANSITIONS FROM RESIDENTIAL AND COMMERCIAL DRIVEWAYS TO AND FROM THE WORK AREA.

LANE CLOSURES

1. THE CONTRACTOR SHALL LOCATE LANE CLOSURES TO PROVIDE OPTIMUM VISIBILITY, I.E. BEFORE CURVES AND CRESTS, TO THE EXTENT CONDITIONS PERMIT.
2. THE ENGINEER MAY REQUIRE THAT ALL LANES BE RE-OPENED AT ANY TIME IF THE ROUTE IS NEEDED FOR EMERGENCY PURPOSES. THIS COULD INCLUDE INCIDENTS AT LOCATIONS OUTSIDE THE CONTRACT LIMITS.

LINE WIDTH

1. UNLESS AUTHORIZED BY THE ENGINEER, THE MINIMUM LANE WIDTHS FOR WORK ZONE TRAVEL LANES SHALL BE AS FOLLOWS: FREEWAYS AND/OR EXPRESSWAYS IS 3.35m. THE MINIMUM LANE WIDTH FOR ALL OTHER TYPES OF ROADWAYS IS 3.05m.

BARRIER/SHADOW VEHICLES

1. BARRIER AND SHADOW VEHICLES SHALL BE REQUIRED AS PER STANDARD SHEET TITLED "WORK ZONE TRAFFIC CONTROL LEGENDS AND NOTES".
2. NO WORK ACTIVITY, EQUIPMENT, VEHICLES AND/OR MATERIALS SHALL BE LOCATED BETWEEN THE BARRIER OR SHADOW VEHICLE AND THE ACTIVE WORK AREA (ROLL AHEAD DISTANCE).
3. THE CONTRACTOR MAY BE REQUIRED TO PROVIDE A BARRIER VEHICLE IN CONJUNCTION WITH POLICE PRESENCE IN THE WORK ZONE, TO BE INCLUDED IN THE UNIT BID PRICE FOR BASIC WORK ZONE TRAFFIC CONTROL.

REGIONAL WZTC NOTES:

1. THE CONTRACTOR SHALL PROVIDE THE ENGINEER, AT LEAST FOUR WEEKS IN ADVANCE, WITH ITS PROPOSED SCHEDULE FOR EACH WEEK PERTAINING TO LANE CLOSINGS AND RAMP CLOSINGS. THE PROPOSAL SHALL INCLUDE LAYOUT AND SIGNING FOR ANY METHODS AVAILABLE TO IMPROVE TRAFFIC FLOW NOT SHOWN ON THE TYPICAL WORK ZONE TRAFFIC CONTROL DETAILS. MODIFICATIONS ORDERED BY THE ENGINEER SHALL BE MADE BEFORE IMPLEMENTATION OF THESE CLOSURES.
2. THE CONTRACTOR SHALL IDENTIFY ALL CONSTRUCTION SIGNS WITH "CONTRACTOR'S NAME, CONTRACT NO. (D.....), NYSDOT" WRITTEN ON THE BACK OF EACH SIGN IN 75mm HIGH TEXT. THE CONTRACTOR SHALL SUBMIT THE SIZE AND LEGEND OF ALL CONSTRUCTION SIGNS TO THE ENGINEER PRIOR TO FABRICATION.
3. CONSTRUCTION SIGNS SHALL BE VISIBLE ONLY WHEN THE WORK THEY PERTAIN TO IS IN PROGRESS. CONSTRUCTION SIGNS HAVING CENTER HINGED SIGN PANELS OR FOLDING PORTABLE SIGN SUPPORTS SHALL BE FOLDED DOWN WHEN THE WORK THEY PERTAIN TO IS NOT IN PROGRESS. OTHER CONSTRUCTION SIGNS WHICH CANNOT BE FOLDED DOWN SHALL BE REMOVED OR COMPLETELY COVERED. THE COVERING OF SIGNS SHALL CONFORM TO THE REQUIREMENTS OF SECTIONS 619-2.02H3 AND 645-2.09 AND 645-3.09 OF THE N.Y.S.D.O.T STANDARD SPECIFICATIONS.
4. THE BOTTOM OF TEMPORARY CONSTRUCTION SIGNS SHALL BE A MINIMUM OF 2.1m ABOVE THE TRAVEL PAVEMENT AND THE EDGES OFFSET A MINIMUM OF .6m CLEAR OF THE TRAVEL LANE, AS SHOWN ON THE CONTRACT DOCUMENTS OR A.O.B.E. IF THE 0.6m HORIZONTAL CLEARANCE CANNOT BE MET, THE CONTRACTOR MAY USE THE EQUIVALENT RECTANGULAR SIGN. IF THE .6m CLEARANCE CANNOT BE MET USING RECTANGULAR SIGNS, THE SIGN SHALL BE MOUNTED A MINIMUM OF 4m ABOVE THE TRAVEL PAVEMENT. UNDER NO CIRCUMSTANCES WILL THE CLIPPING OF SIGNS OR USAGE OF PREVIOUSLY CLIPPED SIGNS BE ALLOWED.
5. WARNING SIGNS AND FLASHING ARROW PANELS SHALL BE LOCATED SO AS TO BE VISIBLE FOR AN ADEQUATE DISTANCE AND TO PROVIDE SUFFICIENT ADVANCE WARNING TO DRIVERS. THEY SHALL NOT BE BLOCKED BY FOLIAGE, ROADWAY FEATURES, OR OTHER SIGNS AND TRAFFIC CONTROL DEVICES. NO STATIONARY MOUNTING OF CONSTRUCTION SIGNS SHALL BE PERMITTED ON UTILITY POLES OR OTHER ROADSIDE ELEMENTS. (E.G. , ON NYPD OR ON FDNY FACILITIES). FOR NIGHTTIME CONSTRUCTION, SIGN PLACEMENT SHALL ALSO CONSIDER GLARE FROM LIGHT SOURCES BEHIND THE SIGN AND LOW BEAM HEADLIGHT PATTERNS. SIGNS SHALL BE ORIENTED ESSENTIALLY PERPENDICULAR TO THE DIRECTION OF TRAFFIC. WHERE THE SIGN CANNOT BE LOCATED SO AS TO BE VISIBLE UNDER HEADLIGHT ILLUMINATION, OTHER ILLUMINATION SHALL BE CONSIDERED TO ENHANCE VISIBILITY OR THE SIGN SHALL BE RELOCATED.
6. IN REFERENCE TO THE NATIONAL MUTCD THE FOLLOWING STIPULATIONS APPLY UNLESS OTHERWISE SPECIFIED BY THE ENGINEER:
 - A. WHERE WARNING SIGNS ARE SHOWN IN BOTH DIAMOND AND RECTANGULAR SHAPES, ONLY DIAMOND SHAPES SHALL BE PERMITTED, EXCEPT AS MODIFIED BY NOTE 6 ABOVE, OR A.O.B.E. THE SIZE OF DIAMOND-SHAPED TTC WARNING SIGNS IN FREEWAY AND EXPRESSWAY WORK ZONES MUST BE MINIMUM OF 48" X 48" (1200 X 1200 MM).
 - B. WHERE SIGNS ARE SHOWN IN ALTERNATE SIZES, THE LARGEST SIZE MUST BE USED, UNLESS OTHERWISE SHOWN ON THE PLANS OR A.O.B.E.
7. EXISTING SIGNS DESIGNATED FOR REMOVAL ARE NOT TO BE REMOVED BEFORE BEING REPLACED BY NEW SIGNS AT THE LOCATIONS CALLED FOR IN THE PLANS. IF EXISTING SIGNS ARE REMOVED BEFORE THE NEW ONES ARE INSTALLED, TEMPORARY SIGNS SHALL BE PROVIDED AT THESE LOCATIONS. THE REMOVAL OF EXISTING SIGNS THAT ARE NOT BEING REPLACED SHALL BE REMOVED ABOVE, FURNISHING, MOUNTING AND REMOVAL OF TEMPORARY SIGNS WILL BE PAID FOR UNDER ITEM 619.01 OR ITEM 619.0101.
8. THE CONTRACTOR SHALL NOT REMOVE EXISTING GROUND-MOUNTED GUIDE SIGNS UNTIL PROPOSED GUIDE SIGNS ARE INSTALLED TO THE SATISFACTION OF THE ENGINEER.

9. VACANT.
10. UNDER ITEM 619.01 - BASIC WORK ZONE TRAFFIC CONTROL, THE CONTRACTOR IS REQUIRED TO PERFORM MAINTENANCE CLEANING OF THE PAVEMENT AND SHOULDER AREAS WITHIN THE CONTRACT LIMITS WHEN ORDERED BY ENGINEER. MAINTENANCE CLEANING SHALL MEAN THE REMOVAL OF DEBRIS FROM ANY SOURCE, WHICH IN THE OPINION OF THE ENGINEER, IMPEDES TRAFFIC OR STORM WATER FLOW. THIS REQUIREMENT SHALL NOT BE CONSTRUED TO CHANGE THE PROVISIONS OF SECTION 619 - 1.02B SEASONAL OPERATIONS AND SNOW & ICE CONTROL OF THE N.S.D.O.T. STANDARD SPECIFICATIONS.
11. TO ENSURE A SAFE TRAFFIC FLOW AT ALL TIMES, STORAGE OF MATERIALS AND EQUIPMENT, INCLUDING EMPLOYEES' CARS, SHALL NOT BE PERMITTED WITHIN THE TRAVELED WAY OF THE HIGHWAY. STORAGE AREAS SHALL BE SEPARATED FROM THE TRAVELED WAY BY A CLEAR SPACE OF 10 METERS MINIMUM WIDTH, UNLESS SUCH STORAGE IS PLACED BEHIND TEMPORARY CONCRETE BARRIER OR OTHER PERMANENT ROADSIDE BARRIER INSTALLATION.
12. THE CONCURRENT CLOSURES OF LEFT AND RIGHT LANES SHALL NOT BE PERMITTED WITHIN A 3.2 KM DISTANCE BETWEEN CONSECUTIVE CLOSURES IN THE SAME TRAVEL DIRECTION, EXCEPT AS MODIFIED BY SPECIAL NOTES IN THE PROPOSAL.
13. THE CONTRACTOR SHALL PROVIDE A FLAGGER WITH APPROPRIATE SIGNING WHENEVER OPERATIONS INTERFERE WITH TRAFFIC. EXAMPLES INCLUDE, BUT ARE NOT LIMITED TO, DELIVERY/REMOVAL OF MATERIALS OR EQUIPMENT, LIFTING OPERATIONS, AND ANY OTHER ACTIVITIES SO DESIGNATED BY THE ENGINEER. THE COST FOR THIS WORK SHALL BE INCLUDED IN THE PRICE BID FOR ITEM 619.01 OR ITEM 619.0101. ALL FLAGGERS USED MUST BE FORMALLY TRAINED IN PROPER FLAGGING PROCEDURES AND FHWA CERTIFIED.
14. THE SOLE DUTY OF THE FLAGGER SHALL BE TO DIRECT TRAFFIC PROPERLY AT ALL TIMES. FLAGGERS SHALL NOT BE USED TO MOVE TEMPORARY SIGNS OR ASSIST IN OTHER WORK. REQUIREMENTS OF THE MUTCD FOR HAND SIGNALING DEVICES AND SIGNALING PROCEDURES SHALL BE MET. STOP/SLOW PADDLES SHALL BE USED. CLEAN, WELL-FITTING ORANGE VESTS AND HARD-HATS SHALL BE WORN BY ALL FLAGGERS. FLAGGING STATIONS SHALL BE LOCATED TO PROVIDE ADEQUATE SIGHT DISTANCE AND VISIBILITY. TO THE EXTENT POSSIBLE, AN ADEQUATE ESCAPE PATH SHALL BE PROVIDED FOR THE SAFETY OF THE FLAGGER AND THE SAFE RECOVERY OF VEHICLES THAT FAIL TO RESPOND TO FLAGGER DIRECTIONS. ANY FLAGGING DEFICIENCIES SHALL BE CORRECTED IMMEDIATELY, OR THE CONTRACTOR SHALL BE REQUIRED TO CEASE OPERATIONS UNTIL A COMPETENT FLAGGER IS OBTAINED.
15. FOR LONG-TERM OR OVERNIGHT OPERATIONS, THE CONTRACTOR SHALL NOT USE CONES. THE FIRST CHANNELIZING DEVICE AT THE BEGINNING OF LANE CLOSURES SHALL BE FITTED WITH A WARNING LIGHT IN ACCORDANCE WITH THE NATIONAL MUTCD. IN ADDITION, THE CONTRACTOR SHALL OBSERVE THE FOLLOWING:
 - ONE TYPE B (HIGH INTENSITY) FLASHING WARNING LIGHT IS TO BE USED AS THE FIRST LIGHT IN A LONGITUDINAL SERIES AND FOR MARKING ISOLATED HAZARDS.
 - TYPE C (LOW INTENSITY) STEADY BURNING WARNING LIGHTS MAY ONLY BE USED ON TEMPORARY CONCRETE BARRIER OR OTHER NON-REFLECTIVE FEATURES LOCATED CLOSE TO TRAVEL LANES. THEY ARE NOT TO BE USED ON REFLECTORIZED CHANNELIZING DEVICES UNLESS JUSTIFIED BY REDUCED VISIBILITY OR HEAVY FOG.
16. FOR NIGHTTIME OPERATIONS, THE LANE CLOSURES SHALL BE REINFORCED BY THE PLACEMENT OF TWO DRUMS IN THE CENTER OF THE CLOSED LANES. THE TWO DRUMS SHALL BE PLACED TRANSVERSELY ACROSS THE LANE WHILE PERMITTING THE PASSAGE OF CONSTRUCTION TRAFFIC. THEY MAY BE TEMPORARILY OMITTED, WHERE REQUIRED, TO ACCOMMODATE PAVING OR OTHER OPERATIONS.
17. FOR LOCAL STREET LANE CLOSURE DETAILS REFER TO THE NATIONAL MUTCD SECTION 6H.01 (PAGE NUMBERS 6H-40 TO 6H-67).
18. THE MAXIMUM SPACING OF CHANNELIZING DEVICES FOR LANE CLOSURE IN THE WORK ZONES AND 60 METERS UPSTREAM (IN ADVANCE OF WORKERS) SHALL BE 6 METERS.
19. TO ACCOMMODATE SPECIAL EVENT REQUIREMENTS, ON OR THROUGH THE JOB SITE, THE CONTRACTOR SHALL REMOVE ALL LANE CLOSURES AND WORK ZONE TRAFFIC CONTROL DEVICES AS ORDERED BY THE ENGINEER.
20. THE CONTRACTOR WILL BE REQUIRED TO PROVIDE PAVEMENT MARKING IN ACCORDANCE WITH THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) BEFORE OPENING TO TRAFFIC, IN THE AREA WHERE THE OLD PAVEMENT MARKING HAVE BEEN ERACED, DUE TO CONTRACT WORK OPERATIONS, AREAS NEWLY PAVED, OR ABOVE.

EVERY PLACE ITEM 619.01 APPEARS ON THESE DWGS, IT SHALL ALSO APPLY TO ITEM 619.0101



AS-BUILT REVISIONS
DESCRIPTION OF ALTERATIONS:

ITS TRAVEL TIME I-278 AND I-495

PS&E DATE: 06/21/2012

COUNTY: KINGS AND QUEENS

PIN X806.49

BRIDGES

CULVERTS

ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED

CONTRACT NUMBER

D262095

DRAWING NO. WZN- 01

SHEET NO. 13

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11

DOCUMENT NAME: X80649_cph_wzn_01.dgn

DOCUMENT NAME: X10339_cph_wztc_01.dgn

FILE NAME = X80649_cph_wztc.02.dgn
DATE/TIME = 03-AUG-2012 16:02
USER = kdesai

DESIGN SUPERVISOR RANDOLPH HUNT

JOB MANAGER MAREK CWIOK

DESIGN YELENA GRIG

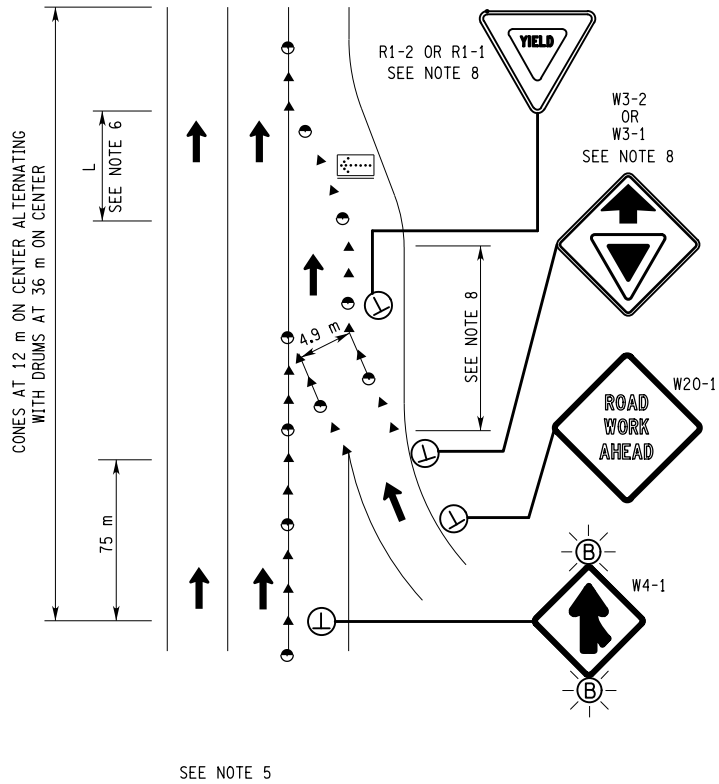
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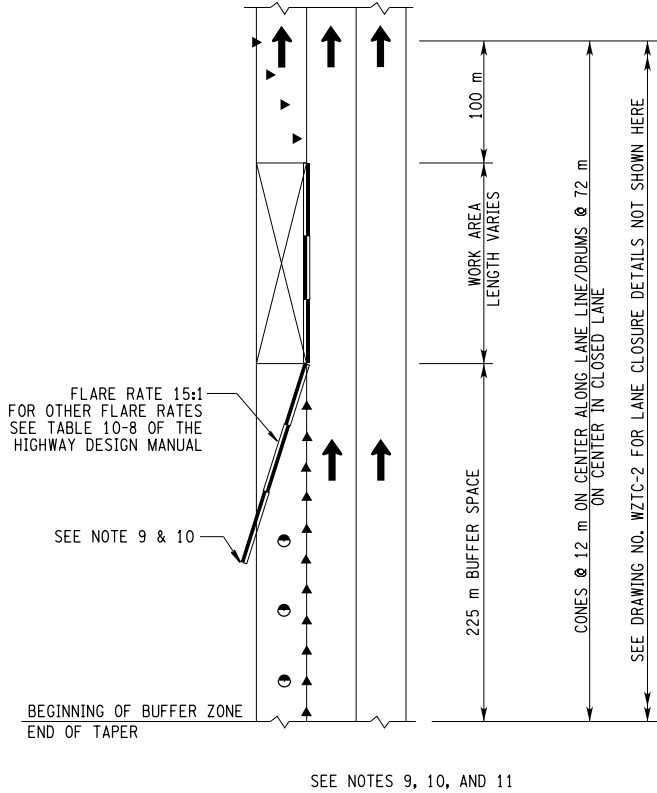
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PROJECT MANAGER RAJ AMIN

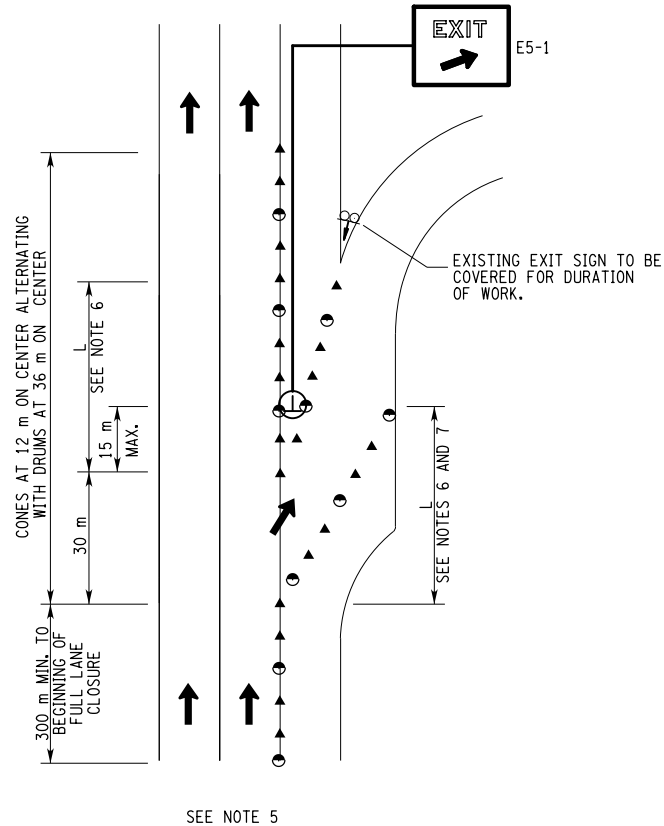
OPEN ENTRANCE RAMP WITH RIGHT LANE CLOSED



LEFT LANE CLOSURE WITH
TEMPORARY CONCRETE BARRIER



OPEN EXIT RAMP WITH RIGHT LANE CLOSED



NOTES:

1. THE WZTC SCHEMES AT EXIT AND ACCESS RAMP SHOWN ON SHEET WZTC-3 ARE THE REQUIRED TRAFFIC CONTROL AS CONFINED TO THE AREAS IN THE VICINITY OF ON/OFF RAMP. FOR ADVANCE WARNING SIGNS, DETOUR ROUTES AND SIGNAGE, AND OTHER TRAFFIC CONTROL ELEMENTS FOR THE SPECIFIC RAMP WITHIN THE PROJECT LIMITS, SEE OTHER WZTC DRAWINGS.
2. WHEN RAMP CLOSURES ARE ON A SHORT TERM DAILY BASIS, TYPE III LIGHTED BARRICADES MAY BE SUBSTITUTED WITH DRUMS.
3. DISTANCE X IS THE DECELERATION LENGTH OF VEHICLES CIRCUMVENTING THE ROAD CLOSURE. THIS DISTANCE SHALL BE KEPT FREE OF PARKED VEHICLES AND STORED MATERIALS AT ALL TIMES. DISTANCE X IS BASED ON TABLE 6C-2 OF THE SECTION 6C.06 OF MUTCD

SPEED MPH.	DISTANCE X METERS
30	60
40	95
50	130

4. VACANT
5. SEE DRAWING NO. WZTC-2 FOR LANE CLOSURES.
6. LANE CLOSURE TAPER LENGTH, L, SHALL BE IN ACCORDANCE WITH THE MUTCD SECTION 6C.08. FOR SHOULDER CLOSURE, THE TAPER LENGTH SHALL BE EQUAL TO 1/3L. FOR NIGHTTIME APPLICATIONS, L SHALL BE EQUAL TO 225 METERS.
7. AS THE LOCATION OF BEGINNING OF TAPER WILL VARY WITH THE LOCATION OF WORK AREA, THE REQUIRED TAPER LENGTH L SHALL BE PROVIDED. WHEN FIELD CONDITIONS PRECLUDE ACHIEVING THE REQUIRED TAPER LENGTH, L SHALL BE A.O.B.E.
8. THE YIELD SIGN SHALL BE REPLACED WITH A STOP SIGN IF NO ADEQUATE ACCELERATION LANE EXISTS FOR THE ENTERING TRAFFIC. THE STOP SIGN SHALL HAVE TWO TYPE B WARNING LIGHTS WITH RED LENSES.
9. THE TEMPORARY CONCRETE BARRIER SHALL BE LOCATED SO AS TO PREVENT VEHICLES FROM INTRUDING BEHIND THE BARRIER WHILE MAINTAINING ACCESS TO CONSTRUCTION VEHICLES.
10. THE APPROACH END OF THE TEMPORARY CONCRETE BARRIER SHALL EITHER BE SHIELDED BEHIND THE DEFLECTION CLEARANCE OF EXISTING BARRIER, BURIED IN THE BACK SLOPE, OR FLARED BACK TO THE END OF THE CLEAR ZONE FOR THE CONSTRUCTION CONDITIONS AND TERMINATED WITH A TAPERED END SECTION.

WHEN SPACE RESTRICTIONS PRECLUDE TERMINATING THE BARRIER APPROACH END AS DESCRIBED, A TAPERED END SECTION MAY STILL BE USED IF THE FULL HEIGHT BARRIER WOULD BE 3.7 METERS OR MORE REMOVED (TRANSVERSELY) FROM THE APPROACH TRAFFIC. AN IMPACT ATTENUATOR SHALL BE USED IF THE BARRIER IS WITHIN 3.7 METERS FROM APPROACH TRAFFIC AND POSTED SPEED IS GREATER THAN 45 mph.

11. THE CONTRACTOR SHALL PROVIDE AND MAINTAIN DELINEATION ON THE TEMPORARY CONCRETE BARRIER. UNLESS THE PLANS INDICATE THE USE OF A SPECIFIC TYPE OF DELINEATION, THE CONTRACTOR SHALL HAVE THE OPTION OF USING ONE OR MORE OF THE VARIOUS TYPES SPECIFIED IN THE N.Y.S.D.O.T. STANDARD SPECIFICATIONS.

AFFIX SEAL: ON:	ALTERED BY: ON:

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	ITS TRAVEL TIME I-278 AND I-495	PIN X806.49	BRIDGES	CULVERTS	ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED	CONTRACT NUMBER	
	PS&E DATE: 06/21/2012					D262095	
						DRAWING NO. WZTC-02	
	COUNTY: KINGS AND QUEENS					SHEET NO. 15	

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NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11
DOCUMENT NAME: X80649_cph_wztc.02.dgn



AFFIX SEAL:
ON: _____

ALTERED BY:
ON: _____

AS-BUILT REVISIONS
DESCRIPTION OF ALTERATIONS:

ITS TRAVEL TIME I-278 AND I-495

PS&E DATE: 06/21/2012

COUNTY: KINGS AND QUEENS

PIN X806.49

BRIDGES

CULVERTS

ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED

CONTRACT NUMBER

WORK ZONE TRAFFIC CONTROL DETAILS - 3

D262095

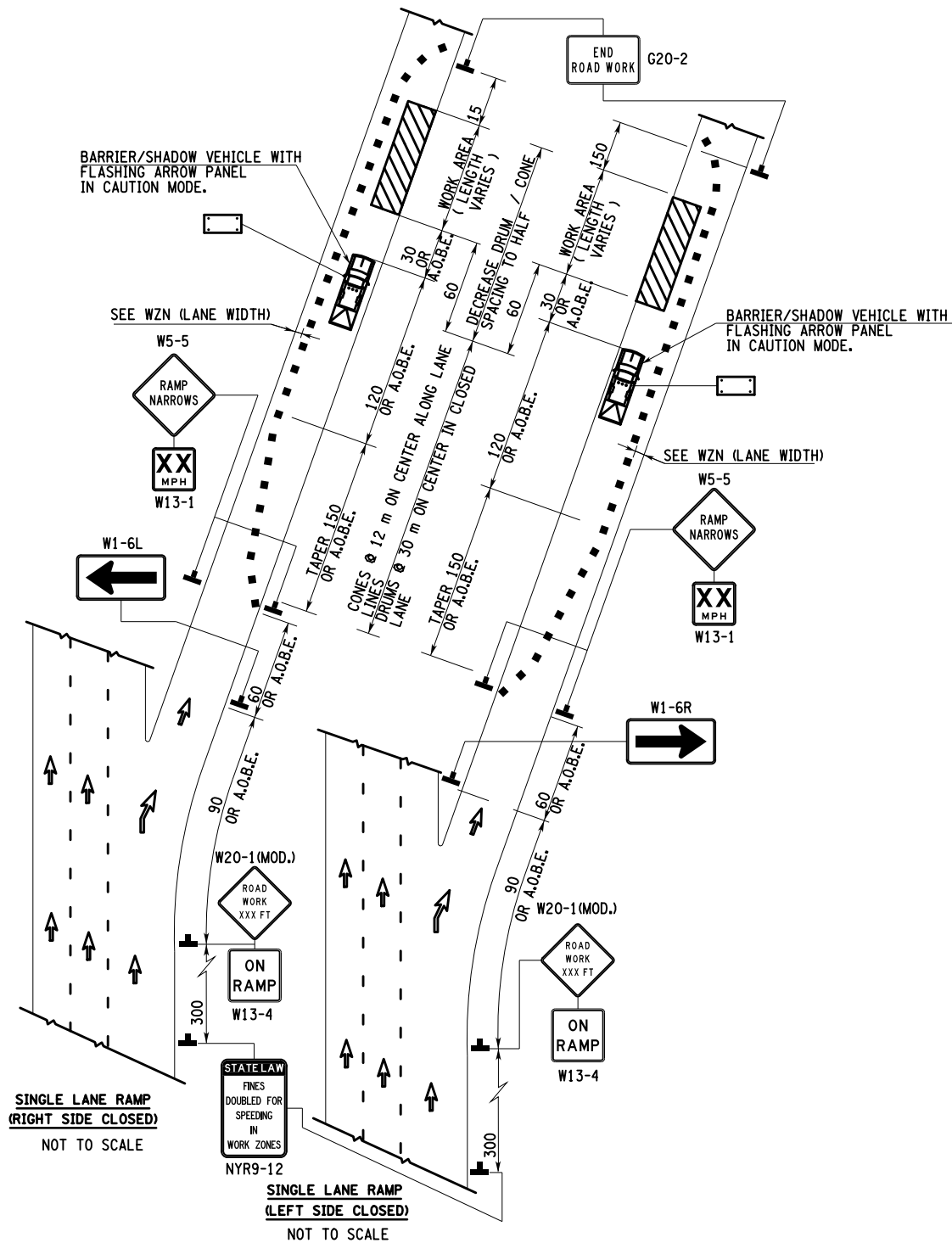
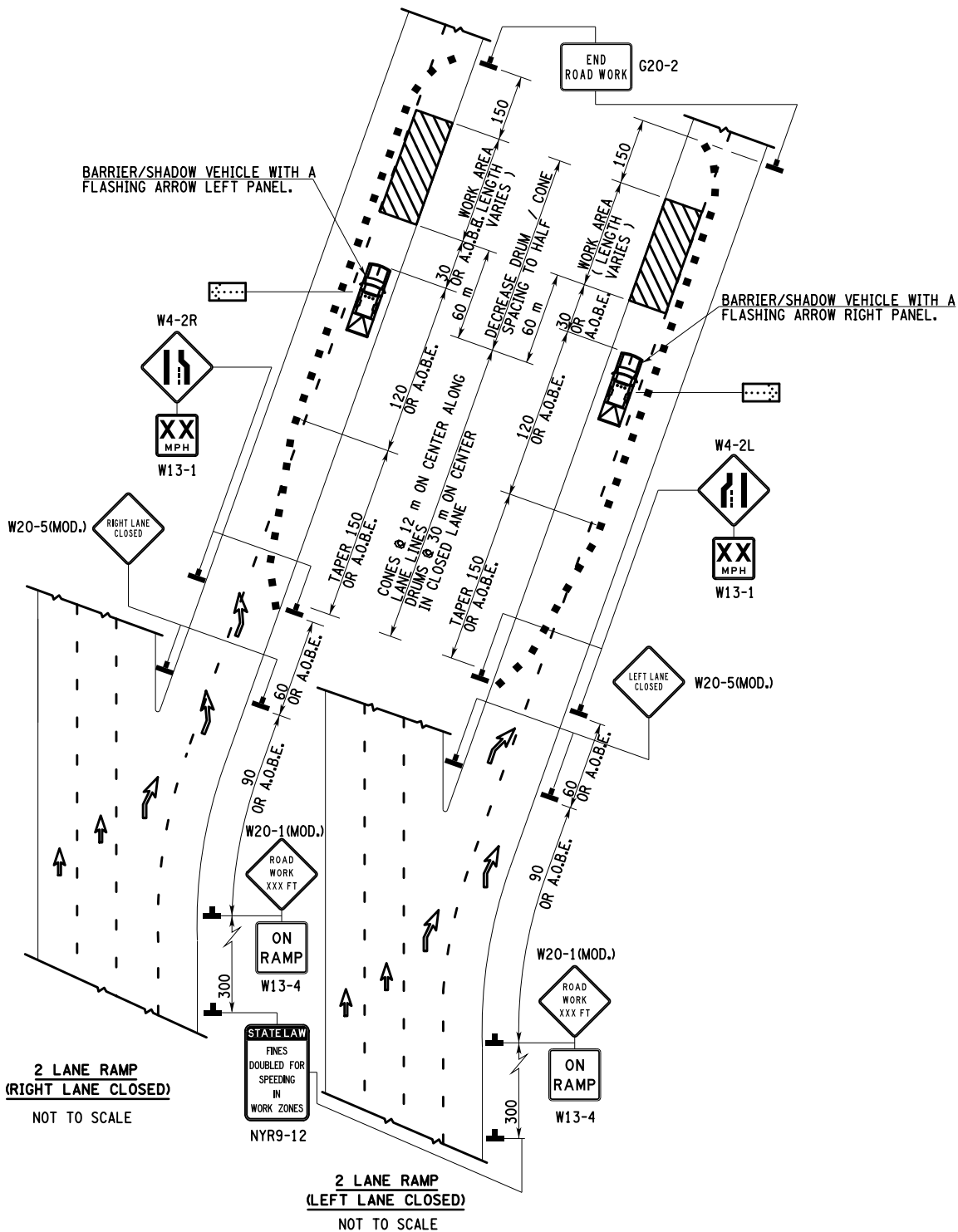
DRAWING NO. WZTC-03

SHEET NO. 16

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NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11

DOCUMENT NAME: X80649_cph_wztc.03.dgn



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DATE/TIME = 03-AUG-2012 16:03
USER = kdesai

DESIGN SUPERVISOR RANDOLPH HUNT

JOB MANAGER MAREK CWIOK

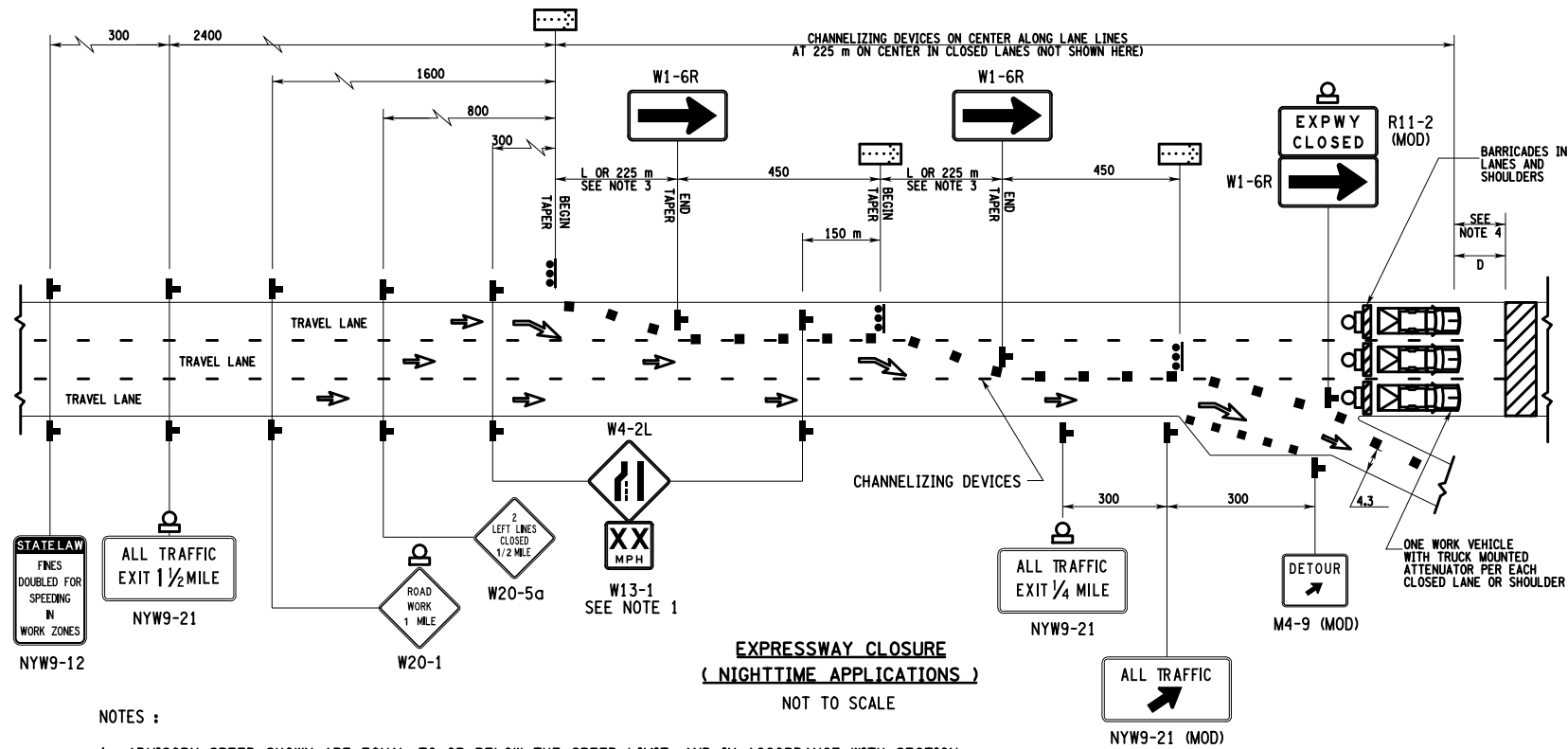
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CHECK MAREK CWIOK

DRAFTING YELENA GRIG

CHECK MAREK CWIOK

PROJECT MANAGER RAJ AMIN

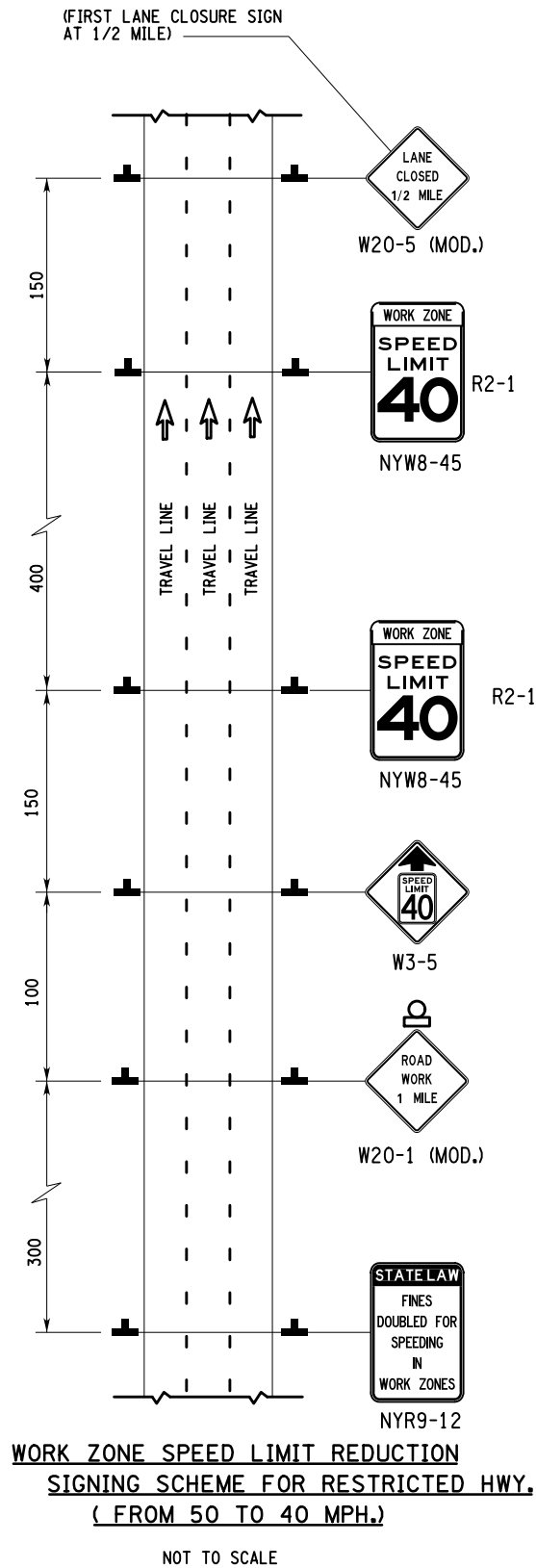
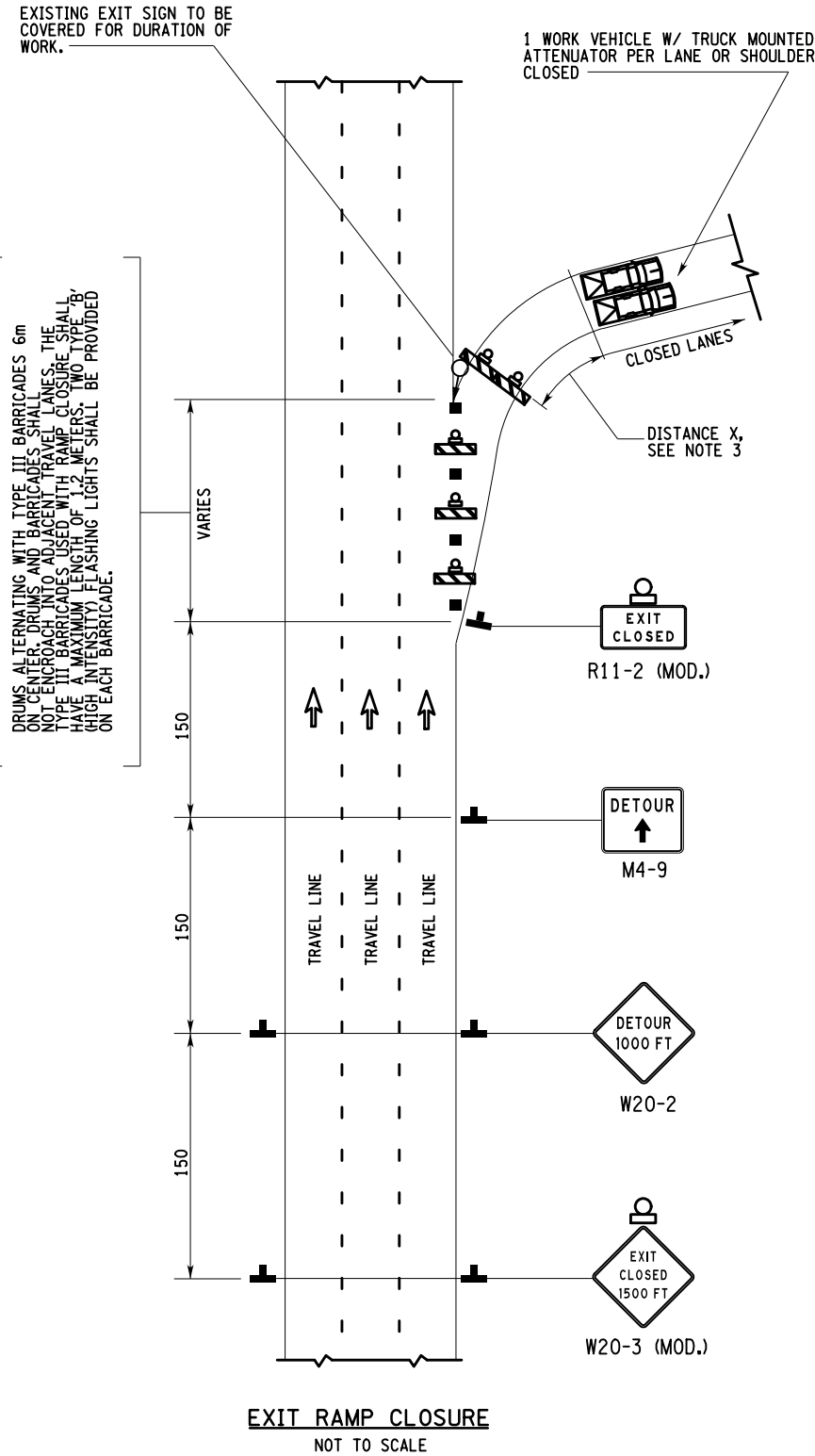
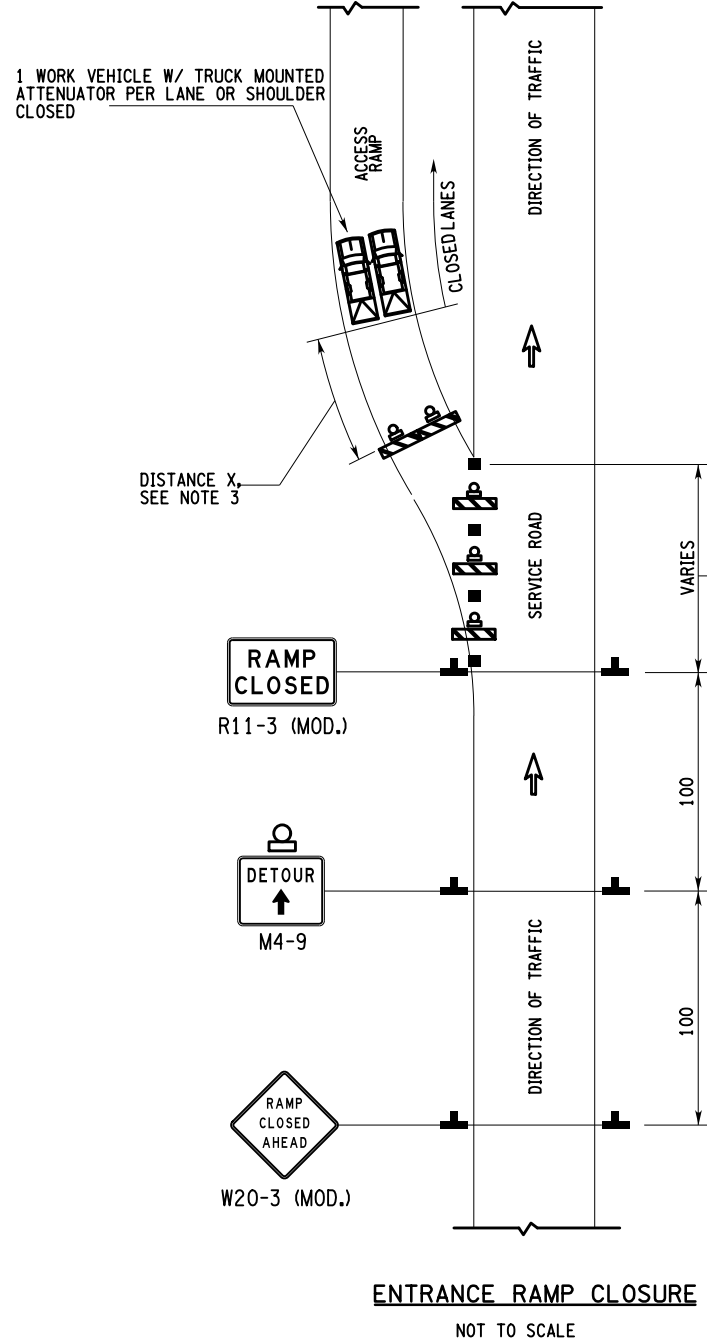


NOTES :


1. ADVISORY SPEED SHOWN ARE EQUAL TO OR BELOW THE SPEED LIMIT, AND IN ACCORDANCE WITH SECTION 2B.13 THRU 2B-18 OF THE NATIONAL MUTCD.
2. TEMPORARAY EXPRESSWAY CLOSURE MAY BE MODIFIED FOR TWO OR FOUR LANE ONE WAY ROADWAYS.
3. THE LANE CLOSURE TAPER LENGTH, L, SHALL BE IN ACCORDANCE WITH THE NATIONAL MUTCD SECTION 6C.08 (TABLE 6H-4 ON STANDARD SHEET M619-11). FOR SHOULDER CLOSURE, THE TAPER LENGTH SHALL BE EQUAL TO 1/3L. FOR NIGHTTIME APPLICATIONS, L SHALL BE EQUAL TO 225m.
4. THE BARRIER VEHICLE WITH A FLASHING ARROW PANEL SHALL BE LOCATED A DISTANCE D UPSTREAM OF THE WORK AREA, ACCORDING TO TABLE NY2-A PLACEMENT DISTANCE FOR BARRIER VEHICLES (FOUND ON STANDARD SHEET M619-11). THIS DISTANCE SHALL BE KEPT FREE OF PARKED VEHICLES AND STORED MATERIALS AT ALL TIMES.

AFFIX SEAL: ON:	ALTERED BY: ON:


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	PS&E DATE: 06/21/2012					D262095	
						DRAWING NO. WZTC-04	
	COUNTY: KINGS AND QUEENS					SHEET NO. 17	
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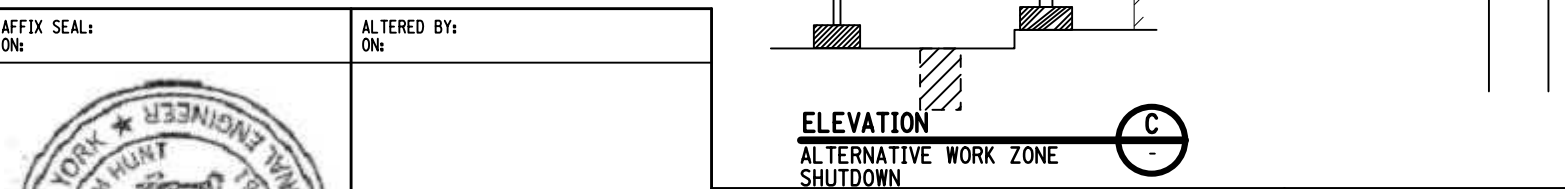
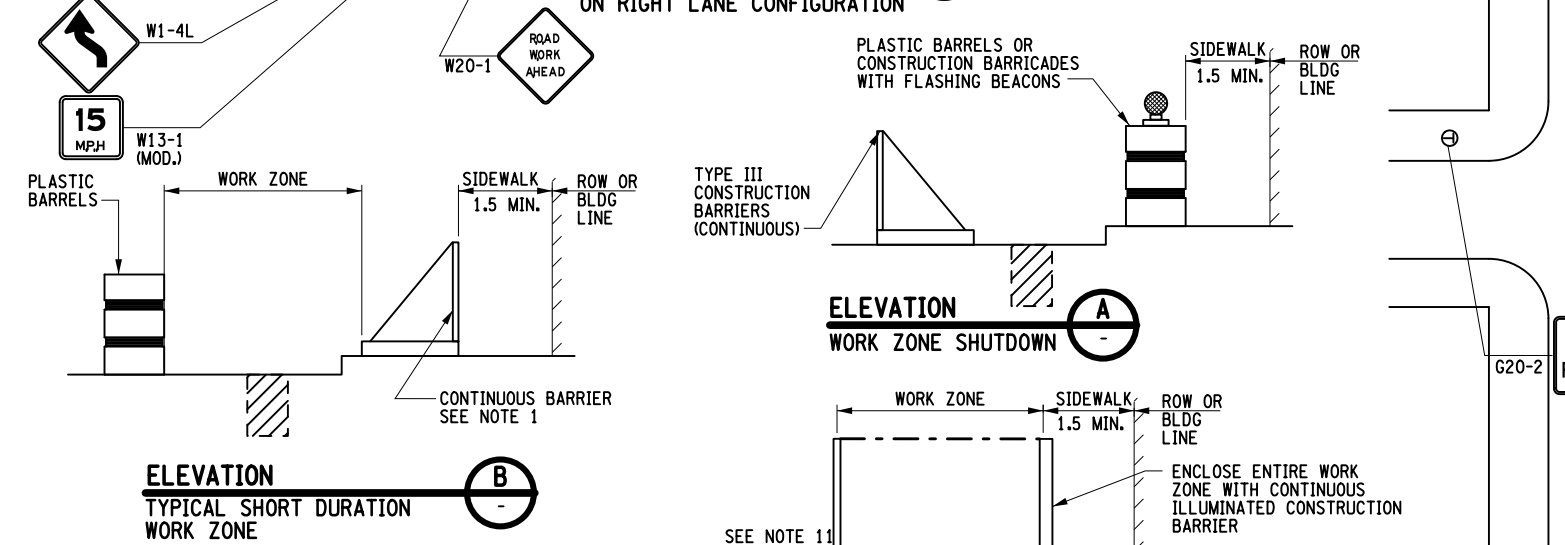
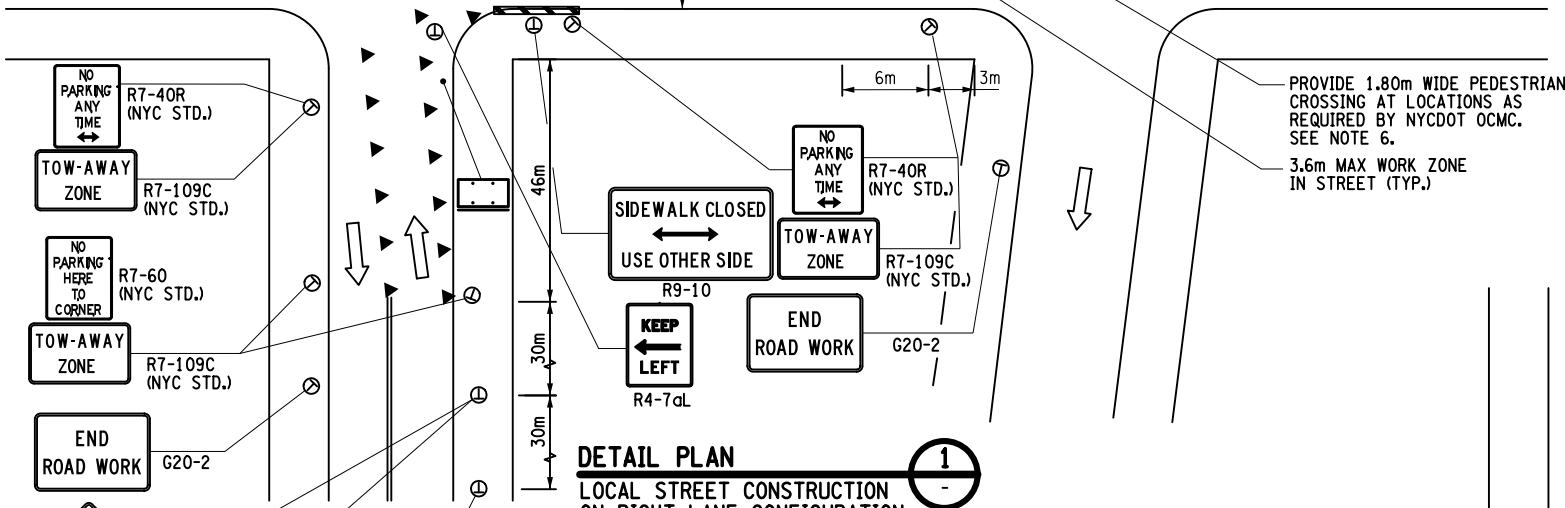
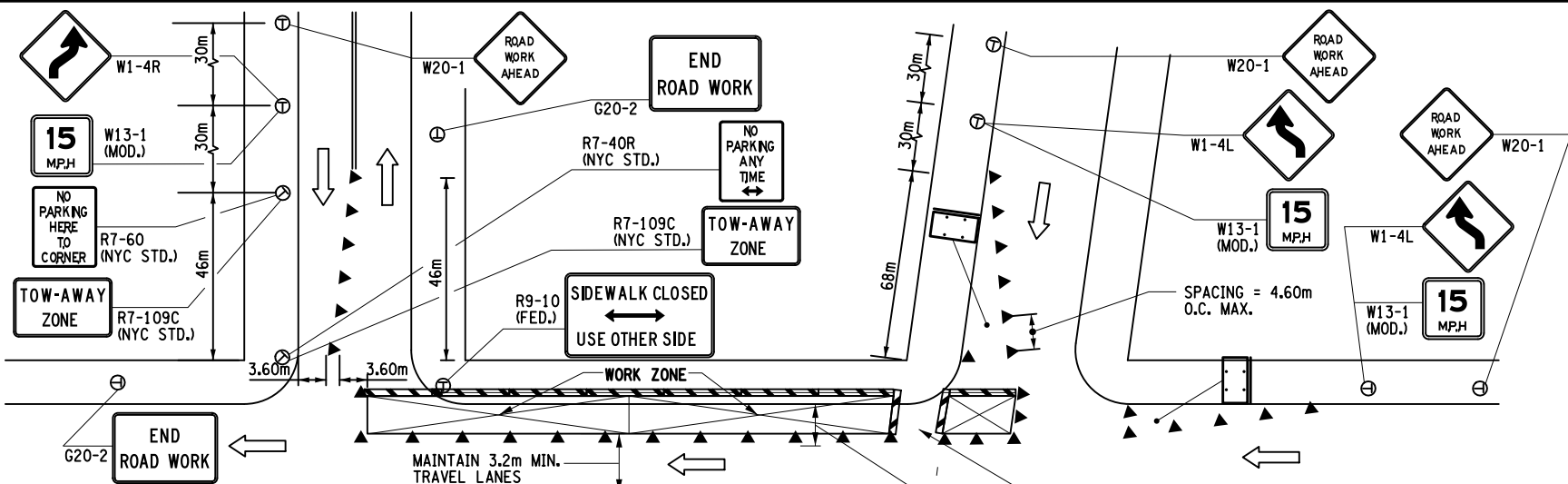


- NOTES:
1. THE WZTC SCHEMES AT ACCESS RAMPS SHOWN ON THIS SHEET ARE THE REQUIRED TRAFFIC CONTROL AS CONFINED TO THE AREAS IN THE VICINITY OF ON/OFF RAMPS. FOR ADVANCE WARNING SIGNS, DETOUR ROUTES AND SIGNAGE, AND OTHER TRAFFIC CONTROL ELEMENTS FOR THE SPECIFIC RAMPS WITHIN THE PROJECT LIMITS, SEE OTHER WZTC DRAWINGS.
 2. WHEN RAMP CLOSURES ARE ON A SHORT TERM DAILY BASIS, TYPE III LIGHTED BARRICADES MAY BE REPLACED BY DRUMS.
 3. DISTANCE X IS THE DECELERATION LENGTH OF VEHICLES CIRCUMVENTING THE ROAD CLOSURE. THIS DISTANCE SHALL BE KEPT FREE OF PARKED VEHICLES AND STORED MATERIALS AT ALL TIMES. DISTANCE X IS THE LONGITUDINAL BUFFER SPACE SHOWN ON TABLE 6C-2 OF SHEET M619-11, AND IN SECTION 6C.06 OF NATIONAL MUTCD

AFFIX SEAL: ON: 

ALTERED BY: ON:

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	ITS TRAVEL TIME I-278 AND I-495	PIN X806.49	BRIDGES	CULVERTS	ALL DIMENSIONS in m UNLESS OTHERWISE NOTED	CONTRACT NUMBER	
	PS&E DATE: 06/21/2012					D262095	
						DRAWING NO. WZTC-05	
						SHEET NO. 18	
	COUNTY: KINGS AND QUEENS						
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.					NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11 DOCUMENT NAME: X80649_cph_wztc_05.dgn		



NOTES:

- CONSTRUCTION BARRIER SHALL CONSIST OF ANY ONE OF THE FOLLOWING AS APPROVED BY THE ENGINEER:

CONTINUOUS ROW OF TYPE III CONSTRUCTION BARRIERS CONFORMING TO NYSDOT STD. SHT. M619-4 AND M619-5.

TIMBER BARRICADES WITH WOODEN CROSS MEMBERS, 1.8 m HIGH CONFORMING TO NYCDOT STD. DWG. H-1013 - FOR USE ALONG SIDEWALKS ONLY.

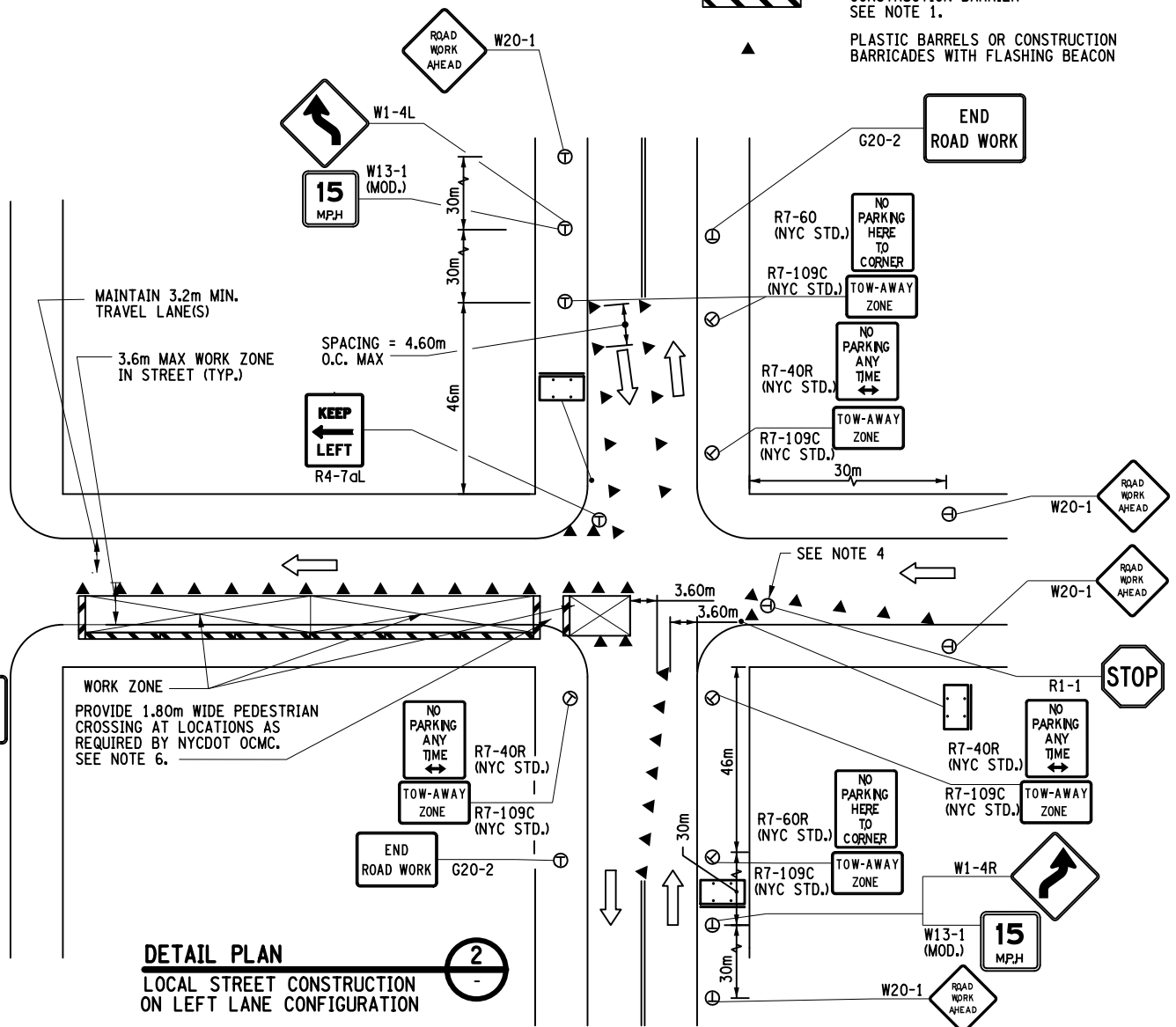
PORTABLE PLASTIC INTERLOCKING BARRIER MODULES 1.2 m HIGH, MODEL 2001 AS MANUFACTURED BY YODOCK WALL OR APPROVED SIMILAR.
- PEDESTRIAN ACCESS WILL BE MAINTAINED THROUGHOUT CONSTRUCTION AREA.
- FLASHING ARROW BOARD USE IS REQUIRED WHEN A TRAVEL LANE WILL BE CLOSED TO TRAFFIC. FLASHING ARROW BOARD IS NOT REQUIRED IN CASE OF LANE SHIFT(S) ONLY.
- IF THE INTERSECTION IS REGULATED BY A TRAFFIC SIGNAL, STOP SIGN R1-1 IS NOT REQUIRED.

- FOR SIGN TEXT DATA, SEE CSD DRAWINGS.
- AT LOCATIONS WHERE A CROSS WALK CLOSURE IS PERMITTED, CONTRACTOR SHALL CLEARLY MARK THE CLOSURE WITH R8-3C "SIDEWALK CLOSED" AND R6-2C (MOD) OR R6-3C (MOD) "USE OTHER SIDE" SIGNS AS APPLICABLE.
- SEE DWG. NO. WZTC-1 FOR LEGEND OF ADDITIONAL MAINTENANCE AND PROTECTION OF TRAFFIC SYMBOLS.
- CONTRACTOR TO USE SHADOW VEHICLE WITH TRUCK MOUNTED ATTENUATOR AND A FLASHING ARROW PANEL AT THE UPSTREAM END OF WORK ZONE LOCATED IN THE TRAVEL LANE. PAYMENT AS PART OF ITEM 619.01.
- PARKING SIGNS SHALL BE PLACED FACING APPROACHING TRAFFIC, ORIENTED AT 30 AND 45 DEGREES WITH LINE OF TRAFFIC.
- NO STEEL PLATING OVER EXCAVATIONS SHALL BE USED ON STATE HIGHWAYS AND CITY STREETS.
- APPLICABLE ONLY WHERE STREET/ SIDEWALK OCCUPANCY DURING PEAK HOURS IS ALLOWED.

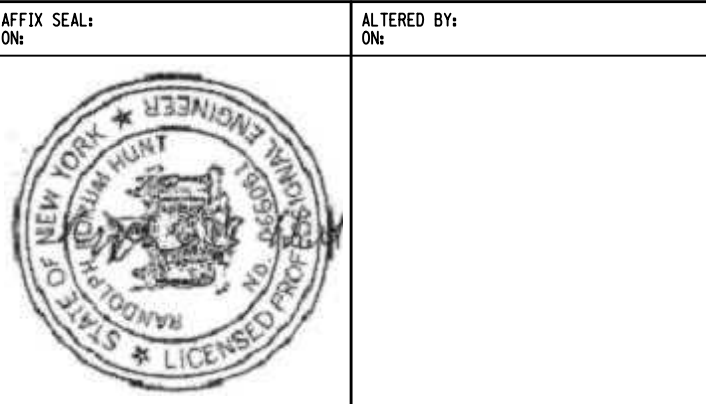
LEGEND:

(SEE NOTE 7)

- CONSTRUCTION BARRIER SEE NOTE 1.
- PLASTIC BARRELS OR CONSTRUCTION BARRICADES WITH FLASHING BEACON



DETAIL PLAN
LOCAL STREET CONSTRUCTION
ON LEFT LANE CONFIGURATION



AS-BUILT REVISIONS
DESCRIPTION OF ALTERATIONS:

ITS TRAVEL TIME I-278 AND I-495

PS&E DATE: 06/21/2012

COUNTY: KINGS AND QUEENS

PIN X806.49

BRIDGES

CULVERTS

ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED

WORK ZONE TRAFFIC CONTROL PLAN - 1
LOCAL STREETS SHEET 1 OF 2

CONTRACT NUMBER

D262095

DRAWING NO. WZTC-06









SHEET NO. 19




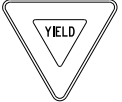






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NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11

DOCUMENT NAME: X80649_cph_wztc_06.dgn



SIGN	M.U.T.C.D. NUMBER	COLOR CODE	SIZE		
			CONVENTIONAL ROAD (SEE NOTE 3)	EXPRESSWAY	FREEWAY
	W20-5 L	A	900x900 (36"x36")	1200x1200 (48"x48")	1200x1200 (48"x48")
	W20-5 R	A	900x900 (36"x36")	1200x1200 (48"x48")	1200x1200 (48"x48")
	W20-1	A	900x900 (36"x36")	1200x1200 (48"x48")	1200x1200 (48"x48")
	NYR9-12	B	600x1050 (24"x42")	1200x2100 (48"x84")	1200x2100 (48"x84")
	W20-5a L	A	900x900 (36"x36")	1200x1200 (48"x48")	1200x1200 (48"x48")
	W20-5a R	A	900x900 (36"x36")	1200x1200 (48"x48")	1200x1200 (48"x48")
	W20-5 L	A	900x900 (36"x36")	1200x1200 (48"x48")	1200x1200 (48"x48")
	W20-5 R	A	900x900 (36"x36")	1200x1200 (48"x48")	1200x1200 (48"x48")

SIGN	M.U.T.C.D. NUMBER	COLOR CODE	SIZE		
			CONVENTIONAL ROAD (SEE NOTE 3)	EXPRESSWAY	FREEWAY
	W3-5	A SEE NOTE 5	900x900 (36"x36")	1200x1200 (48"x48")	1200x1200 (48"x48")
	NYW8-45	A	600x200 (24"x8")	900x300 (36"x12")	1200x400 (48"x16")
	R2-1	B	600x750 (24"x30")	900x1200 (36"x48")	1200x1500 (48"x60")
	R1-1	D	750x750 (30"x30")	900x900 (36"x36")	1200x1200 (48"x48")
	R1-2	E	900x900x900 (36"x36"x36")	1200x1200x1200 (48"x48"x48")	1500x1500x1500 (60"x60"x60")
	W3-2	A SEE NOTE 5	900x900 (36"x36")	1200x1200 (48"x48")	1200x1200 (48"x48")
	W4-1 R	A	900x900 (36"x36")	1200x1200 (48"x48")	1200x1200 (48"x48")
	W1-6 L	A	1200x600 (48"x24")	1200x600 (48"x24")	1200x600 (48"x24")
	W1-6 R	A	1200x600 (48"x24")	1200x600 (48"x24")	1200x600 (48"x24")
	W4-2 L	A	900x900 (36"x36")	1200x1200 (48"x48")	1200x1200 (48"x48")
	W4-2 R	A	900x900 (36"x36")	1200x1200 (48"x48")	1200x1200 (48"x48")

SIGN	M.U.T.C.D. NUMBER	COLOR CODE	SIZE		
			CONVENTIONAL ROAD (SEE NOTE 3)	EXPRESSWAY	FREEWAY
	W13-1	A	600x600 (24"x24")	600x600 (24"x24")	600x600 (24"x24")
	M1-1	F	600x600 (24"x24")	900x900 (36"x36")	900x900 (36"x36")
	M4-9 (MOD)	A	750x600 (30"x24")	1200x900 (48"x36")	1200x900 (48"x36")
	M4-9 (MOD)	A	750x600 (30"x24")	1200x900 (48"x36")	1200x900 (48"x36")
	M4-9 (MOD)	A	750x600 (30"x24")	1200x900 (48"x36")	1200x900 (48"x36")
	M4-8A	A	600x450 (24"x18")	600x450 (24"x18")	600x450 (24"x18")
	M4-8	A	600x300 (24"x12")	750x375 (30"x15")	750x375 (30"x15")
	M6-3	F	525x375 (21"x15")	750x525 (30"x21")	750x525 (30"x21")
	M6-1	F	525x375 (21"x15")	750x525 (30"x21")	750x525 (30"x21")
	M6-1	F	525x375 (21"x15")	750x525 (30"x21")	750x525 (30"x21")

ROADWAY DEFINITIONS

CONVENTIONAL ROAD - A STREET OR HIGHWAY OTHER THAN A FREEWAY OR AN EXPRESSWAY.
EXPRESSWAY - A DIVIDED HIGHWAY WITH PARTIAL CONTROL.
FREEWAY - A DIVIDED HIGHWAY WITH FULL CONTROL OF ACCESS.

NOTES:

1. ALL SIGNS SHALL BE INSTALLED IN ACCORDANCE WITH THE NATIONAL M.U.T.C.D. AND NEW YORK STATE SUPPLEMENT.

2. TEMPORARY CONSTRUCTION SIGNS SHALL BE PAID FOR UNDER ITEM 619.01, BASIC WORK ZONE TRAFFIC CONTROL.

3. FOR MULTILANE UNDIVIDED CONVENTIONAL ROADWAYS USE EXPRESSWAY SIGN SIZES.

4. DIMENSIONS ARE SHOWN IN MILLIMETERS FOLLOWED BY INCHES IN PARENTHESIS AND ARE SHOWN AS WIDTH X HEIGHT.


5. MULTICOLORED SYMBOL IMPOSED ON BLACK ON ORANGE SIGN.

6. ALL DIMENSIONS IN mm UNLESS OTHERWISE NOTED

COLOR CODE LEGEND	
CODE	DESCRIPTION
A	BLACK LEGEND AND BORDER ON A ORANGE BACKGROUND
B	BLACK LEGEND AND BORDER ON A WHITE BACKGROUND
C	WHITE LEGEND AND BORDER ON A GREEN BACKGROUND
D	WHITE LEGEND AND BORDER ON A RED BACKGROUND
E	RED LEGEND AND BORDER ON A WHITE BACKGROUND
F	WHITE LEGEND AND BORDER ON A BLUE BACKGROUND

AFFIX SEAL: ON:

ALTERED BY: ON:



AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:

ITS TRAVEL TIME I-278 AND I-495

PS&E DATE: 06/21/2012

COUNTY: KINGS AND QUEENS

PIN X806.49

BRIDGES

CULVERTS

ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED


CONSTRUCTION SIGN DATA SHEET

CONTRACT NUMBER

D262095

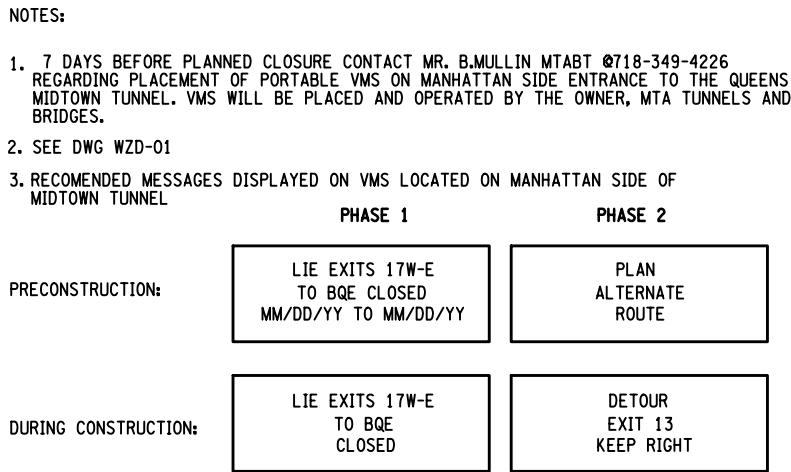
DRAWING NO. WZTC-08


SHEET NO. 21



IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY, IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11
DOCUMENT NAME: X80649_cph wztc.08.dgn



AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	ITS TRAVEL TIME I-278 AND I-495	PIN X806.49	BRIDGES	CULVERTS	ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED	CONTRACT NUMBER	
	PS&E DATE: 06/21/2012				DETOUR-GREENPOINT AVE AT LIE EB EXIT 17W-E CLOSURE	D262095	
						DRAWING NO. DTR-01	
	COUNTY: KINGS AND QUEEN						

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11

DOCUMENT NAME: X80649.cph.wz+c_dtr_01.dgn



1. MOUNT THE SIGNS ON EXISTING LAMPOST.
2. SEE DWG WZD-01



ALTERED BY:
ON:

[illegible]

DOCUMENT NAME: X80649_cph_wztc_dtr_03.dgn

FILE NAME = X80649_cph_wztc_dtr_04.dgn
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USER = kdesai

DESIGN SUPERVISOR RANDOLPH HUNT

JOB MANAGER MAREK CWTOK

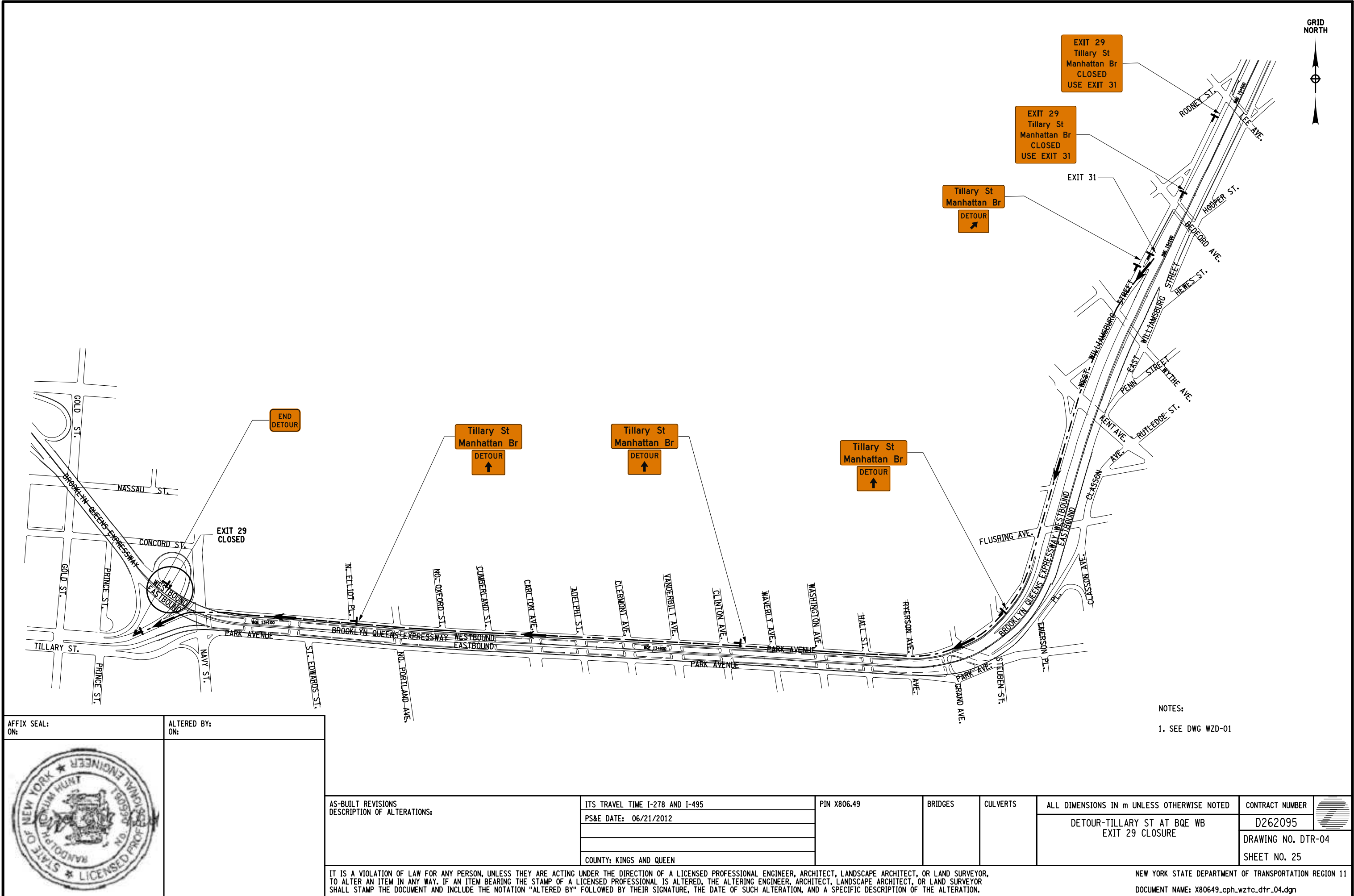
DESIGN YELENA GRIG

CHECK MAREK CWTOK

DRAFTING YELENA GRIG

CHECK MAREK CWTOK


PROJECT MANAGER RAJ AMIN



NOTES:
1. SEE DWG WZD-01

AFFIX SEAL:
ON:

ALTERED BY:
ON:

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	ITS TRAVEL TIME I-278 AND I-495	PIN X806.49	BRIDGES	CULVERTS	ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED DETOUR-TILLARY ST AT BQE WB EXIT 29 CLOSURE	CONTRACT NUMBER	
	PS&E DATE: 06/21/2012					D262095	
						DRAWING NO. DTR-04	
						SHEET NO. 25	
	COUNTY: KINGS AND QUEEN						
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.						NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11 DOCUMENT NAME: X80649_cph_wztc_dtr_04.dgn	

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JOB MANAGER MAREK CWTOK

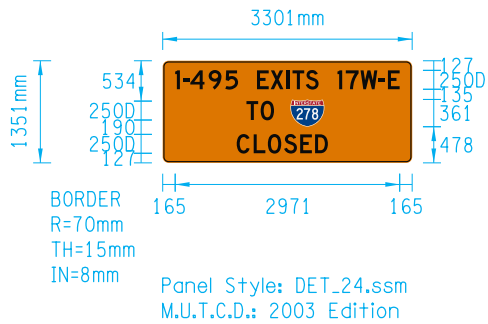
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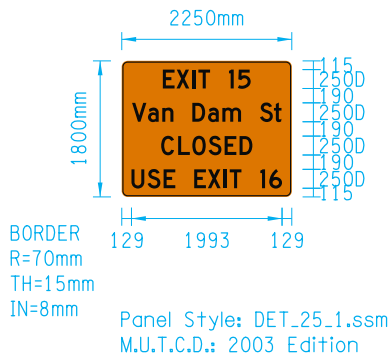
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CHECK

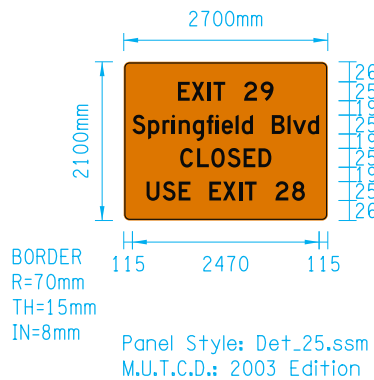
PROJECT MANAGER RAJ AMIN



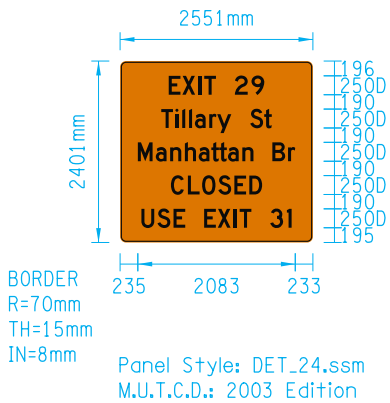
DETOUR-GREENPOINT AVE AT LIE EB
EXITS 17W-E CLOSURE



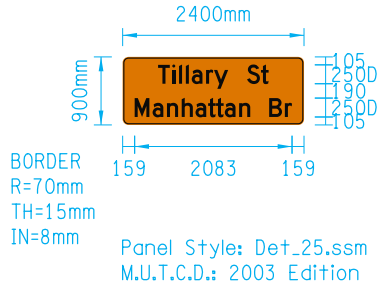
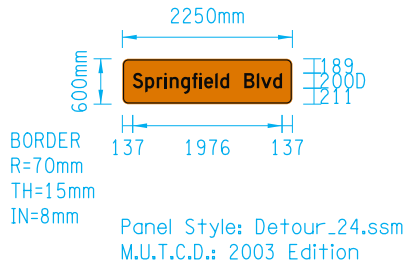
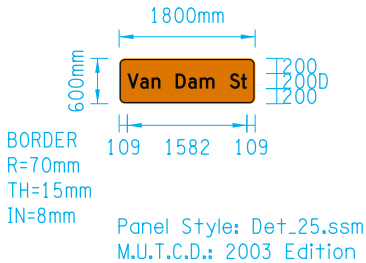
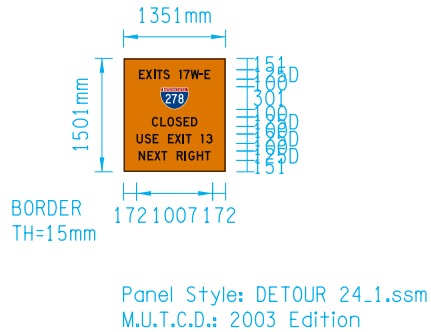
DETOUR-GREENPOINT AVE WB
EXIT 15 CLOSURE



DETOUR-EAST HAMPTON BLVD AT LIE WB
EXIT 29 CLOSURE



DETOUR-TILLARY ST AT BQE WB
EXIT 29 CLOSURE



AFFIX SEAL: ON:	ALTERED BY: ON:

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	ITS TRAVEL TIME I-278 AND I-495	PIN X806.49	BRIDGES	CULVERTS	ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED	CONTRACT NUMBER	
	PS&E DATE: 06/21/2012						
						SIGNIS CUSTOM	DRAWING NO. WZD-01
	COUNTY: KINGS						SHEET NO. 26
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.					NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11 DOCUMENT NAME: x80649_cph_wzd.01.dgn		

FILE NAME = x80649_cph_con_02.dgn
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USER = kdesai

DESIGN SUPERVISOR RANDOLPH HUNT

JOB MANAGER MAREK CWIOK

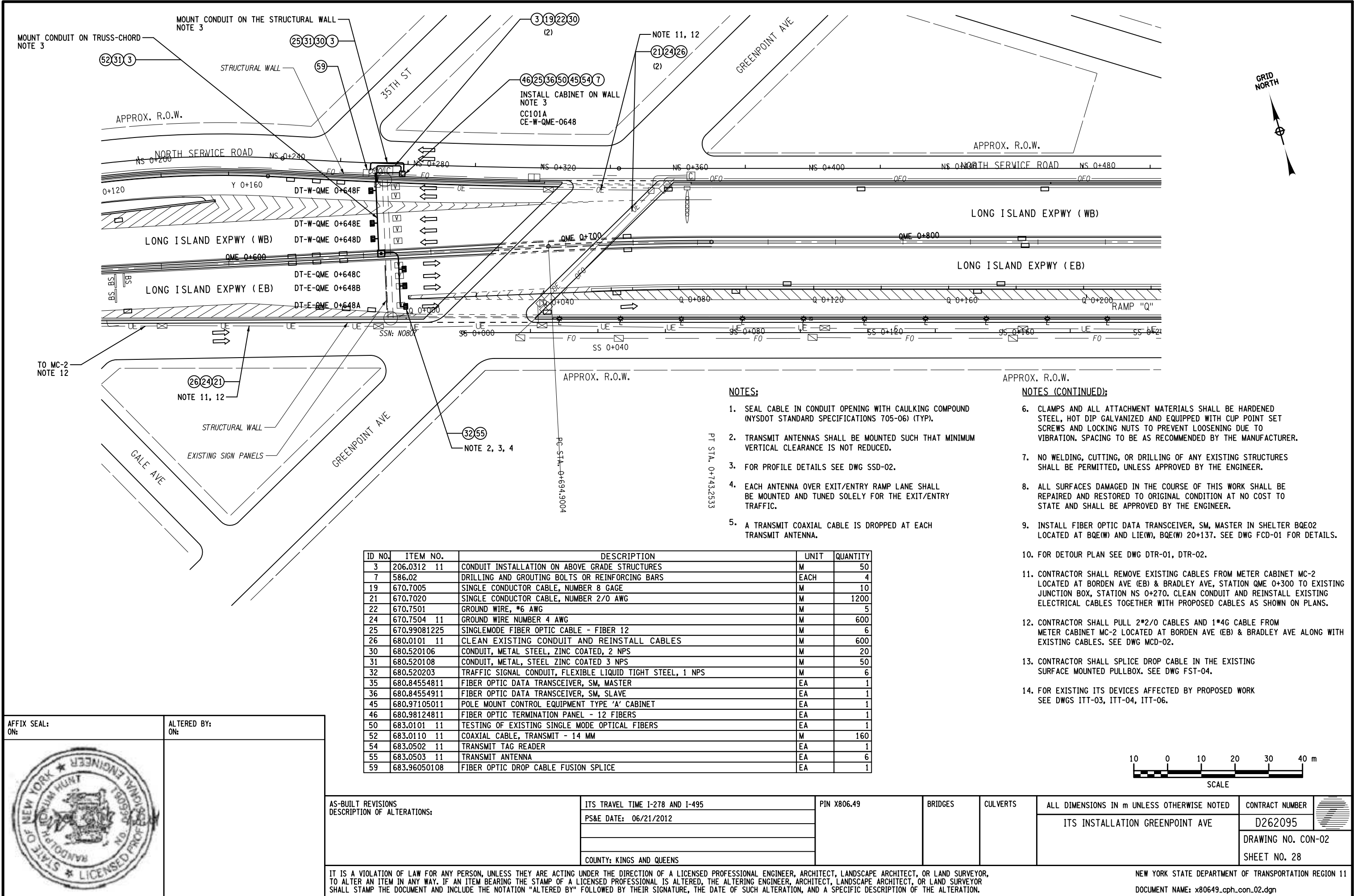
DESIGN KAUSHIK DESAI

CHECK MAREK CWIOK

DRAFTING KAUSHIK DESAI

CHECK MAREK CWIOK

PROJECT MANAGER RAJ AMIN



ID NO.	ITEM NO.	DESCRIPTION	UNIT	QUANTITY
3	206.0312 11	CONDUIT INSTALLATION ON ABOVE GRADE STRUCTURES	M	50
7	586.02	DRILLING AND GROUTING BOLTS OR REINFORCING BARS	EACH	4
19	670.7005	SINGLE CONDUCTOR CABLE, NUMBER 8 GAGE	M	10
21	670.7020	SINGLE CONDUCTOR CABLE, NUMBER 2/0 AWG	M	1200
22	670.7501	GROUND WIRE, #6 AWG	M	5
24	670.7504 11	GROUND WIRE NUMBER 4 AWG	M	600
25	670.99081225	SINGLEMODE FIBER OPTIC CABLE - FIBER 12	M	6
26	680.0101 11	CLEAN EXISTING CONDUIT AND REINSTALL CABLES	M	600
30	680.520106	CONDUIT, METAL STEEL, ZINC COATED, 2 NPS	M	20
31	680.520108	CONDUIT, METAL, STEEL ZINC COATED 3 NPS	M	50
32	680.520203	TRAFFIC SIGNAL CONDUIT, FLEXIBLE LIQUID TIGHT STEEL, 1 NPS	M	6
35	680.84554811	FIBER OPTIC DATA TRANSCEIVER, SM, MASTER	EA	1
36	680.84554911	FIBER OPTIC DATA TRANSCEIVER, SM, SLAVE	EA	1
45	680.97105011	POLE MOUNT CONTROL EQUIPMENT TYPE 'A' CABINET	EA	1
46	680.98124811	FIBER OPTIC TERMINATION PANEL - 12 FIBERS	EA	1
50	683.0101 11	TESTING OF EXISTING SINGLE MODE OPTICAL FIBERS	EA	1
52	683.0110 11	COAXIAL CABLE, TRANSMIT - 14 MM	M	160
54	683.0502 11	TRANSMIT TAG READER	EA	1
55	683.0503 11	TRANSMIT ANTENNA	EA	6
59	683.96050108	FIBER OPTIC DROP CABLE FUSION SPLICE	EA	1

AFFIX SEAL:
ON:

ALTERED BY:
ON:

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	ITS TRAVEL TIME I-278 AND I-495	PIN X806.49	BRIDGES	CULVERTS	ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED	CONTRACT NUMBER	
	PS&E DATE: 06/21/2012					D262095	
						DRAWING NO. CON-02	
	COUNTY: KINGS AND QUEENS					SHEET NO. 28	
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.							
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USER = kdesai

DESIGN SUPERVISOR RANDOLPH HUNT

JOB MANAGER MAREK CWIOK


DESIGN KAUSHIK DESAI


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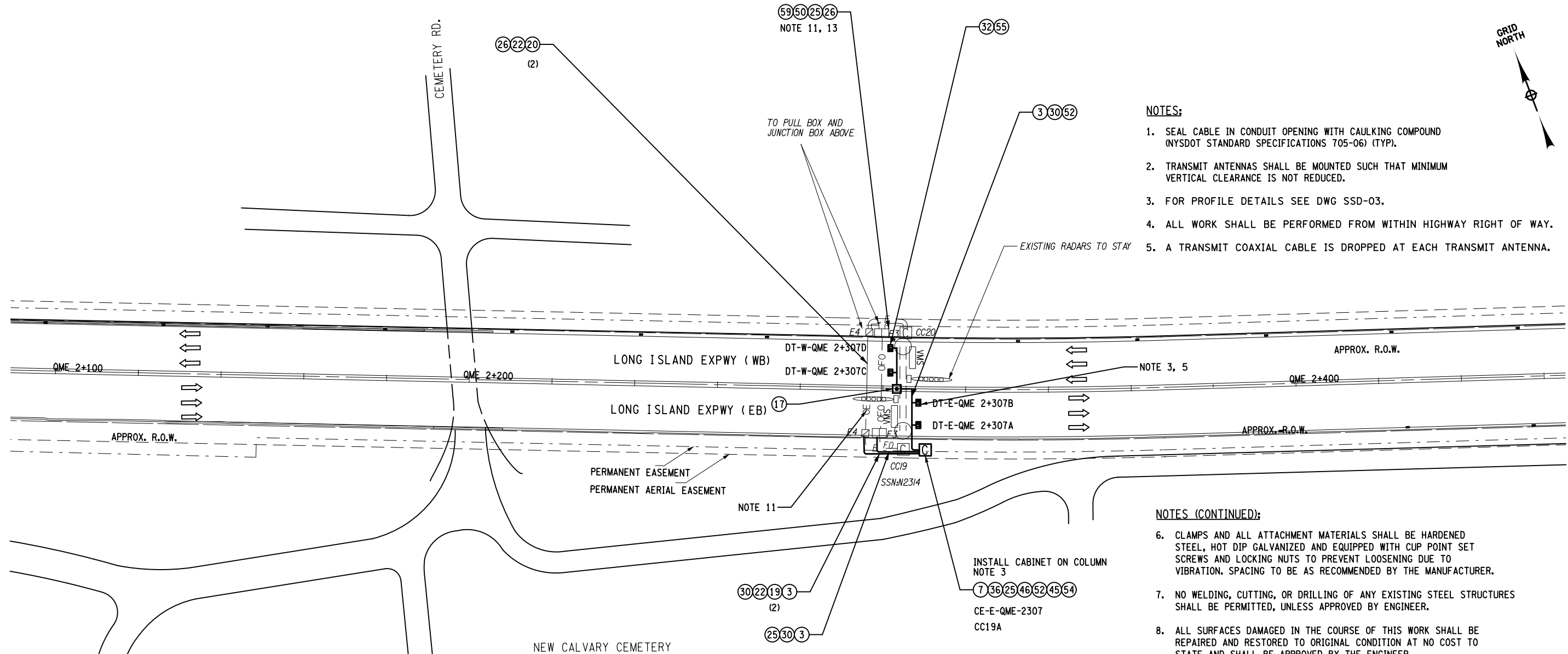
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CHECK MAREK CWIOK

PROJECT MANAGER RAJ AMIN

AFFIX SEAL: ON:	ALTERED BY: ON:
	

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	ITS TRAVEL TIME I-278 AND I-495	PIN X806.49	BRIDGES	CULVERTS	ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED	CONTRACT NUMBER	
	PS&E DATE: 06/21/2012					D262095	
						DRAWING NO. CON-03	
	COUNTY: KINGS AND QUEENS					SHEET NO. 29	
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.						NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11 DOCUMENT NAME: x80649_cph_con_03.dgn	



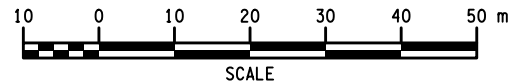
ID NO.	ITEM NO.	DESCRIPTION	UNIT	QUANTITY
3	206.0312 11	CONDUIT INSTALLATION ON ABOVE GRADE STRUCTURES	M	30
7	586.02	DRILLING AND GROUTING BOLTS OR REINFORCING BARS	EA	4
17	670.40	CAST IRON JUNCTION BOXES	EA	1
19	670.7005	SINGLE CONDUCTOR CABLE, NUMBER 8 GAGE	M	10
20	670.7010	SINGLE CONDUCTOR CABLE, NUMBER 1/0 GAGE	M	720
22	670.7501	GROUND WIRE, #6 AWG	M	5
25	670.99081225	SINGLEMODE FIBER OPTIC CABLE - FIBER 12	M	50
26	680.0101 11	CLEAN EXISTING CONDUIT AND REINSTALL CABLES	M	400
30	680.520106	CONDUIT, METAL STEEL, ZINC COATED, 2 NPS	M	30
32	680.520203	TRAFFIC SIGNAL CONDUIT, FLEXIBLE LIQUID TIGHT STEEL, 1 NPS	M	4
35	680.84554811	FIBER OPTIC DATA TRANSCEIVER, SM, MASTER	EA	1
36	680.84554911	FIBER OPTIC DATA TRANSCEIVER, SM, SLAVE	EA	1
45	680.97105011	POLE MOUNT CONTROL EQUIPMENT TYPE 'A' CABINET	EA	1
46	680.98124811	FIBER OPTIC TERMINATION PANEL - 12 FIBERS	EA	1
50	683.0101 11	TESTING OF EXISTING SINGLE MODE OPTICAL FIBERS	EA	1
52	683.0110 11	COAXIAL CABLE, TRANSMIT - 14 MM	M	70
54	683.0502 11	TRANSMIT TAG READER	EA	1
55	683.0503 11	TRANSMIT ANTENNA	EA	4
59	683.96050108	FIBER OPTIC DROP CABLE FUSION SPLICE	EA	1

NOTES:

1. SEAL CABLE IN CONDUIT OPENING WITH CAULKING COMPOUND (NYSOT STANDARD SPECIFICATIONS 705-06) (TYP).
2. TRANSMIT ANTENNAS SHALL BE MOUNTED SUCH THAT MINIMUM VERTICAL CLEARANCE IS NOT REDUCED.
3. FOR PROFILE DETAILS SEE DWG SSD-03.
4. ALL WORK SHALL BE PERFORMED FROM WITHIN HIGHWAY RIGHT OF WAY.
5. A TRANSMIT COAXIAL CABLE IS DROPPED AT EACH TRANSMIT ANTENNA.

NOTES (CONTINUED):


6. CLAMPS AND ALL ATTACHMENT MATERIALS SHALL BE HARDENED STEEL, HOT DIP GALVANIZED AND EQUIPPED WITH CUP POINT SET SCREWS AND LOCKING NUTS TO PREVENT LOOSENING DUE TO VIBRATION. SPACING TO BE AS RECOMMENDED BY THE MANUFACTURER.
7. NO WELDING, CUTTING, OR DRILLING OF ANY EXISTING STEEL STRUCTURES SHALL BE PERMITTED, UNLESS APPROVED BY ENGINEER.
8. ALL SURFACES DAMAGED IN THE COURSE OF THIS WORK SHALL BE REPAIRED AND RESTORED TO ORIGINAL CONDITION AT NO COST TO STATE AND SHALL BE APPROVED BY THE ENGINEER.
9. INSTALL FIBER OPTIC DATA TRANSCEIVER, SM, MASTER IN SHELTER BQE02 LOCATED AT LIE(W) AND BQE(W). SEE DWG FCD-02 FOR DETAILS.
10. VACANT
11. CONTRACTOR SHALL REMOVE EXISTING CABLES FROM METER CABINET MC-4 (@ QME 2+575), LOCATED AT 58TH ST AND N. SERVICE RD, TO EXISTING PULLBOX E4 (@ QME (E) 2+307). CLEAN CONDUIT AND REINSTALL EXISTING ELECTRICAL CABLES TOGETHER WITH PROPOSED CABLES 2*1/0 AND 1*6G. SEE DWG MCD-03 AND DWG LIE-14L OF D259712.
12. VACANT
13. CONTRACTOR SHALL SPLICE TERMINATION/DROP CABLE IN THE EXISTING SURFACE MOUNTED PULLBOX LOCATED AT UPPER LEVEL (@ QME 2+300). SEE DWG FST-01.
14. FOR EXISTING ITS DEVICES AFFECTED BY PROPOSED WORK SEE DWG ITT-03, ITT-04.



ID NO.	ITEM NO.	DESCRIPTION	UNIT	QUANTITY
25	670.99081225	SINGLEMODE FIBER OPTIC CABLE - FIBER 12	M	90
28	680.51500111	ELECTRICAL PULLBOX (610MM X 457MM X 914MM)	EA	1
29	680.5196 11	FURNISH AND INSTALL CONCRETE FIBER OPTIC PULLBOX	EA	1
30	680.520106	CONDUIT, METAL STEEL, ZINC COATED, 2 NPS	M	120
31	680.520108	CONDUIT, METAL, STEEL ZINC COATED 3 NPS	M	35
32	680.520203	TRAFFIC SIGNAL CONDUIT, FLEXIBLE LIQUID TIGHT STEEL, 1 NPS	M	5
35	680.84554811	FIBER OPTIC DATA TRANSCEIVER, SM, MASTER	EA	1
36	680.84554911	FIBER OPTIC DATA TRANSCEIVER, SM, SLAVE	EA	1
40	680.95533211	METER CABINET	EA	1
45	680.97105011	POLE MOUNT CONTROL EQUIPMENT TYPE 'A' CABINET	EA	1
46	680.98124811	FIBER OPTIC TERMINATION PANEL - 12 FIBERS	EA	1
50	683.0101 11	TESTING OF EXISTING SINGLE MODE OPTICAL FIBERS	EA	1
51	683.0109 11	COAXIAL CABLE, TRANSMIT - 10 MM	M	55
52	683.0110 11	COAXIAL CABLE, TRANSMIT - 14 MM	M	145
54	683.0502 11	TRANSMIT TAG READER	EA	1
55	683.0503 11	TRANSMIT ANTENNA	EA	5
58	680.95644811	PROPERTY LINE BOX (610mm X 508mm X 914mm)	EA	1
59	683.96050108	FIBER OPTIC DROP CABLE FUSION SPLICER	EA	1

NOTES:

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- | | | | |
|----|--------------|----|-----------------------------------|
| 50 | 683.0101 | 11 | TESTING OF EXISTING SINGLE MODE |
| 51 | 683.0109 | 11 | COAXIAL CABLE, TRANSMIT - 10 MM |
| 52 | 683.0110 | 11 | COAXIAL CABLE, TRANSMIT - 14 MM |
| 54 | 683.0502 | 11 | TRANSMIT TAG READER |
| 55 | 683.0503 | 11 | TRANSMIT ANTENNA |
| 58 | 680.95644811 | | PROPERTY LINE BOX (610mm X 508mm) |
| 59 | 683.96050108 | | FIBER OPTIC DROP CABLE FUSION SPL |
- | | | | |
|----|--------------|----|-----------------------------------|
| 50 | 683.0101 | 11 | TESTING OF EXISTING SINGLE MODE |
| 51 | 683.0109 | 11 | COAXIAL CABLE, TRANSMIT - 10 MM |
| 52 | 683.0110 | 11 | COAXIAL CABLE, TRANSMIT - 14 MM |
| 54 | 683.0502 | 11 | TRANSMIT TAG READER |
| 55 | 683.0503 | 11 | TRANSMIT ANTENNA |
| 58 | 680.95644811 | | PROPERTY LINE BOX (610mm X 508mm) |
| 59 | 683.96050108 | | FIBER OPTIC DROP CABLE FUSION SPL |
- | | | | |
|----|--------------|----|-----------------------------------|
| 50 | 683.0101 | 11 | TESTING OF EXISTING SINGLE MODE |
| 51 | 683.0109 | 11 | COAXIAL CABLE, TRANSMIT - 10 MM |
| 52 | 683.0110 | 11 | COAXIAL CABLE, TRANSMIT - 14 MM |
| 54 | 683.0502 | 11 | TRANSMIT TAG READER |
| 55 | 683.0503 | 11 | TRANSMIT ANTENNA |
| 58 | 680.95644811 | | PROPERTY LINE BOX (610mm X 508mm) |
| 59 | 683.96050108 | | FIBER OPTIC DROP CABLE FUSION SPL |
- | | | | |
|----|--------------|----|-----------------------------------|
| 50 | 683.0101 | 11 | TESTING OF EXISTING SINGLE MODE |
| 51 | 683.0109 | 11 | COAXIAL CABLE, TRANSMIT - 10 MM |
| 52 | 683.0110 | 11 | COAXIAL CABLE, TRANSMIT - 14 MM |
| 54 | 683.0502 | 11 | TRANSMIT TAG READER |
| 55 | 683.0503 | 11 | TRANSMIT ANTENNA |
| 58 | 680.95644811 | | PROPERTY LINE BOX (610mm X 508mm) |
| 59 | 683.96050108 | | FIBER OPTIC DROP CABLE FUSION SPL |

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	ITS TRAVEL TIME I-278 AND I-495	PIN X806.49	BRIDGES	CULVERTS	ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED	CONTRACT NUMBER	
	PS&E DATE: 06/21/2012				ITS INSTALLATION 63RD ST	D262095	
						DRAWING NO. CON-04	
COUNTY: KINGS AND QUEENS				SSN: N0795	SHEET NO. 30		

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11

DOCUMENT NAME: x80649_cph_con_04.dgn

FILE NAME = x80649_cph_con_05.dgn
DATE/TIME = 03-AUG-2012 16:03
USER = kdesai

DESIGN SUPERVISOR RANDOLPH HUNT

JOB MANAGER MAREK CWIOK

DESIGN KAUSHIK DESAI

CHECK MAREK CWIOK

DRAFTING KAUSHIK DESAI


CHECK MAREK CWIOK


PROJECT MANAGER RAJ AMIN

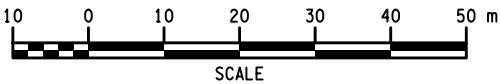
NOTES:

1. SEAL CABLE IN CONDUIT OPENING WITH CAULKING COMPOUND (NYSOT STANDARD SPECIFICATIONS 705-06) (TYP).
2. CLAMPS AND ALL ATTACHMENT MATERIALS SHALL BE HARDENED STEEL, HOT DIPPED GALVANIZED AND EQUIPPED WITH CUP POINT SET SCREWS AND LOCKING NUTS TO PREVENT LOOSENING DUE TO VIBRATION. SPACING TO BE AS RECOMMENDED BY THE MANUFACTURER.
3. ALL SURFACES DAMAGED IN THE COURSE OF THIS WORK SHALL BE REPAIRED AND RESTORED TO ORIGINAL CONDITION AT NO COST TO STATE AND SHALL BE APPROVED BY THE ENGINEER.
4. FOR CONDUIT EXCAVATION INFORMATION, SEE STANDARD SHEET M680-12.
5. FOR DETOUR INFORMATION SEE DWG DTR-03.
6. INSTALL 144 FIBERS CABLE IN EXISTING 4 NPS CONDUIT.
7. EXTEND EXISTING 144 FIBERS BY SPLICING PROPOSED 144 FIBERS CABLE TO THE EXISTING TERMINATED 144 FIBERS IN NEW SPLICE ENCLOSURE IN UNDERGROUND PULLBOX 157A.
8. EXTEND EXISTING 72 FIBERS BY SPLICING PROPOSED 72 FIBERS CABLE TO THE EXISTING 72 FIBERS IN PROPOSED UNDERGROUND PULLBOX 159A.
9. INSTALL PROPOSED CONDUIT ON BARRIER.

ID NO.	ITEM NO.	DESCRIPTION	UNIT	QUANTITY
1	206.03	CONDUIT EXCAVATION AND BACKFILL INCLUDING SURFACE RESTORATION	M	5
2	206.0301 10	CONDUIT EXCAVATION AND BACKFILL - (RESTORING OF TOP SURFACES NOT INCLUDED)	M	115
3	206.0312 11	CONDUIT INSTALLATION ON ABOVE GRADE STRUCTURES	M	20
5	206.05	TEST PIT EXCAVATION	EA	1
6	502.2001 18	FULL-DEPTH PORTLAND CEMENT CONCRETE SAW CUTS	M	10
29	680.5196 11	FURNISH AND INSTALL CONCRETE FIBER OPTIC PULLBOX	EA	3
37	680.92020410	MULTI-CELL GALVANIZED STEEL CONDUIT, 4 DUCTS	M	140
38	680.92124411	SINGLE MODE FIBER OPTIC TRUNK CABLE 144 FIBER (IN CONDUIT)	M	145
39	680.92127211	SINGLE MODE FIBER OPTIC TRUNK CABLE 72 FIBER (IN CONDUIT)	M	135
42	680.96156011	FIBER OPTIC CABLE SPLICE - MAINLINE 156 FIBERS	EA	1
44	680.9684 11	FIBER OPTIC CABLE SPLICE - MAINLINE 84 FIBERS	EA	1

AFFIX SEAL: ON:	ALTERED BY: ON:
	

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	ITS TRAVEL TIME I-278 AND I-495	PIN X806.49	BRIDGES	CULVERTS	ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED ITS INSTALLATION EAST HAMPTON - 1	CONTRACT NUMBER	
	PS&E DATE: 06/21/2012					D262095	
						DRAWING NO. CON-05	
	COUNTY: KINGS AND QUEENS					SHEET NO. 31	
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.							
NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11 DOCUMENT NAME: x80649_cph_con_05.dgn							



FILE NAME = x80649_cph_con_06.dgn
DATE/TIME = 03-AUG-2012 16:03
USER = kdesai

DESIGN SUPERVISOR RANDOLPH HUNT

JOB MANAGER MAREK CWTOK


DESIGN KAUSHIK DESAI

CHECK MAREK CWTOK

DRAFTING KAUSHIK DESAI

CHECK MAREK CWTOK

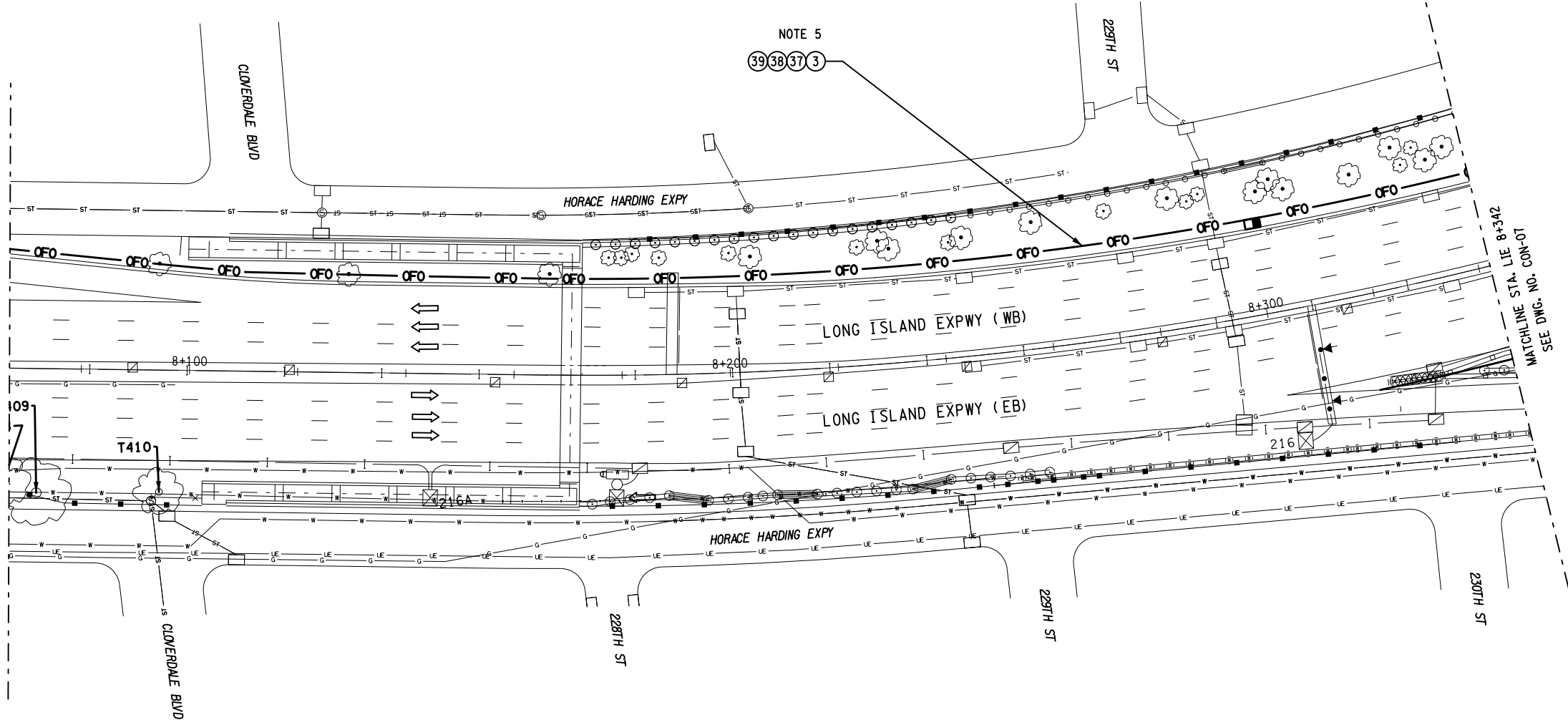
PROJECT MANAGER RAJ AMIN

AFFIX SEAL: ON:	ALTERED BY: ON:
	

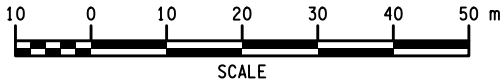
NOTES:


1. SEAL CABLE IN CONDUIT OPENING WITH CAULKING COMPOUND (NYSDOT STANDARD SPECIFICATIONS 705-06) (TYP).
2. CLAMPS AND ALL ATTACHMENT MATERIALS SHALL BE HARDENED STEEL, HOT DIPPED GALVANIZED AND EQUIPPED WITH CUP POINT SET SCREWS AND LOCKING NUTS TO PREVENT LOOSENING DUE TO VIBRATION. SPACING TO BE AS RECOMMENDED BY THE MANUFACTURER.
3. ALL SURFACES DAMAGED IN THE COURSE OF THIS WORK SHALL BE REPAIRED AND RESTORED TO ORIGINAL CONDITION AT NO COST TO STATE AND SHALL BE APPROVED BY THE ENGINEER.
4. VACANT
5. INSTALL PROPOSED CONDUIT ON BARRIER.

MATCHLINE STA. LIE 8+067
SEE DWG. NO. CON-05



ID NO.	ITEM NO.	DESCRIPTION	UNIT	QUANTITY
3	206.0312 11	CONDUIT INSTALLATION ON ABOVE GRADE STRUCTURES	M	275
37	680.92020410	MULTI-CELL GALVANIZED STEEL CONDUIT, 4 DUCTS	M	275
38	680.92124411	SINGLE MODE FIBER OPTIC TRUNK CABLE 144 FIBER (IN CONDUIT)	M	275
39	680.92127211	SINGLE MODE FIBER OPTIC TRUNK CABLE 72 FIBER (IN CONDUIT)	M	275



AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	ITS TRAVEL TIME I-278 AND I-495	PIN X806.49	BRIDGES	CULVERTS	ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED ITS INSTALLATION EAST HAMPTON - 2 LIE STA8+067 TO STA8+342	CONTRACT NUMBER	
	PS&E DATE: 06/21/2012					D262095	
						DRAWING NO. CON-06	
	COUNTY: KINGS AND QUEENS					SHEET NO. 32	
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.							
NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11						DOCUMENT NAME: x80649_cph_con_06.dgn	

NOTES:

1. SEAL CABLE IN CONDUIT OPENING WITH CAULKING COMPOUND (NYS DOT STANDARD SPECIFICATIONS 705-06) (TYP).
2. TRANSMIT ANTENNAS SHALL BE MOUNTED SUCH THAT MINIMUM VERTICAL CLEARANCE IS NOT REDUCED.
3. FOR PROFILE DETAILS SEE DWG SSD-05.
4. A TRANSMIT COAXIAL CABLE SHALL BE DROPPED AT EACH TRANSMIT ANTENNA.
5. CLAMPS AND ALL ATTACHMENT MATERIALS SHALL BE HARDENED STEEL, HOT DIPPED GALVANIZED AND EQUIPPED WITH CUP POINT SET SCREWS AND LOCKING NUTS TO PREVENT LOOSENING DUE TO VIBRATION. SPACING TO BE AS RECOMMENDED BY THE MANUFACTURER.
6. NO WELDING, CUTTING, OR DRILLING OF ANY EXISTING STEEL STRUCTURES SHALL BE PERMITTED, UNLESS APPROVED BY ENGINEER.
7. ALL SURFACES DAMAGED IN THE COURSE OF THIS WORK SHALL BE REPAIRED AND RESTORED TO ORIGINAL CONDITION AT NO COST TO STATE AND SHALL BE APPROVED BY THE ENGINEER.
8. FOR CONDUIT EXCAVATION INFORMATION, SEE STANDARD SHEET M680-12.
9. REMOVE EXISTING CORRUGATED BEAM GUIDE RAIL AND EXISTING CHAIN LINK FENCE. SEE STANDARD SHEET M606-6, M607-2, AND M607-10. REINSTATE GUIDE RAIL AND FENCE AFTER COMPLETION OF WORK. INSTALL SINGLE LEAF GATE ITEM 607.4016.
10. CONCRETE BASE FOR METER CABINET AND CONTROL CABINET SHALL BE CONSTRUCTED AS ONE COMPONENT, CONCRETE BASE SHALL INCLUDE 100 mm THICK WORK PAD FOR CABINETS AS PER STANDARD SHEETS M680-4. CONCRETE BASE SIZE SHALL BE DETERMINED IN THE FIELD, AS REQUIRED. BASE AND POLE FOUNDATION SHALL BE PAID UNDER ITEM 680.95533211, METER CABINET.
11. INSTALL PROPOSED CONDUIT ON THE EXISTING BARRIER.
12. FOR EXISTING ITS DEVICES AFFECTED BY PROPOSED WORK SEE DWG ITT-01.
13. INSTALL METER CABINET AND SERVICE BOX AT A DISTANCE GREATER THAN STANDARD DEFLECTION DISTANCE FOR CORRUGATED WEAK POST GUIDE RAIL (2.5 METER)
14. CONTRACTOR SHALL INFORM CON ED FACILITY, TWO WEEKS IN ADVANCE, OF PROPOSED SPLICE IN CON ED SERVICE POINT BOX. WORK OF CONNECTION SHALL BE PERFORMED BY CON ED, AND IT SHALL BE PAID UNDER ITEM 680.95606011, UTILITY CONNECTION COSTS.
15. INSTALL CONDUITS ON BRICK VENEER AS NEAR AS POSSIBLE TO GROUND SUCH THAT CONDUITS ARE NOT CLEARLY VISIBLE.
16. INSTALL FIBER OPTICS CABLES CONNECTORS (AS REQUIRED) AT CABINET END. WORK SHALL BE INCLUDED IN ITEM 670.99081225.

NOTES (CONTINUED):

17. CONTRACTOR SHOULD BE AWARE OF THE UNDERGROUND EXISTING UTILITIES IN THE VICINITY OF THE WORK. CONTRACTOR SHALL NOTIFY ALL UTILITIES WELL IN ADVANCE OF ANY EXCAVATION WORK. CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS TO PROTECT EXISTING UNDERGROUND UTILITIES PER 16 NYCRR 753.
18. EACH ANTENNA OVER EXIT/ENTRY RAMP LANE SHALL BE MOUNTED AND TUNED SOLELY FOR THE EXIT/ENTRY TRAFFIC.
19. IF EXISTING CONDITION RESTRICTS THE USE OF ZINC COATED STEEL CONDUIT, CONTRACTOR MAY USE SAME SIZE FLEXIBLE STEEL CONDUIT.



AS-BUILT REVISIONS
DESCRIPTION OF ALTERATIONS:

ITS TRAVEL TIME I-278 AND I-495

PS&E DATE: 06/21/2012

COUNTY: KINGS AND QUEENS

PIN X806.49

BRIDGES
1055340

CULVERTS

ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED
ITS INSTALLATION EAST HAMPTON - 3
LIE STA 8+342 TO STA 8+672

CONTRACT NUMBER

D262095

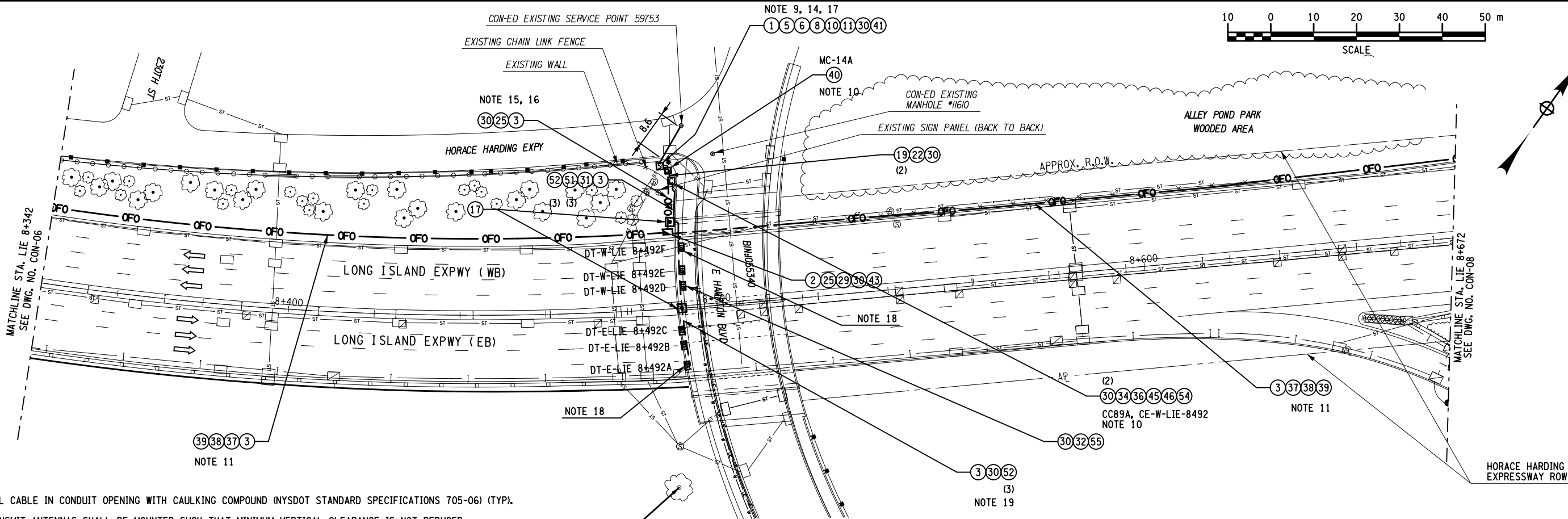
DRAWING NO. CON-07

SHEET NO. 33

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11

DOCUMENT NAME: x80649_cph_con_07.dgn



ID NO.	ITEM NO.	DESCRIPTION	UNIT	QUANTITY
1	206.03	CONDUIT EXCAVATION AND BACKFILL INCLUDING SURFACE RESTORATION	M	35
2	206.0301 10	CONDUIT EXCAVATION AND BACKFILL - (RESTORING OF TOP SURFACES NOT INCLUDED)	M	5
3	206.0312 11	CONDUIT INSTALLATION ON ABOVE GRADE STRUCTURES	M	370
5	206.05	TEST PIT EXCAVATION	EA	1
6	502.2001 18	FULL-DEPTH PORTLAND CEMENT CONCRETE SAW CUTS	M	70
8	606.51	RESETTING CORRUGATED BEAM GUIDE RAILING	M	5
10	607.4016	OPTIONAL FENCE GATE (SINGLE LEAF 1220 mm OPENING 1830 mm HIGH)	EA	1
11	607.97 08	REMOVE AND RESET EXISTING FENCE	M	5
17	670.40	CAST IRON JUNCTION BOXES	EA	2
19	670.7005	SINGLE CONDUCTOR CABLE, NUMBER 8 GAGE	M	10
22	670.7501	GROUND WIRE, #6 AWG	M	5
25	670.99081225	SINGLEMODE FIBER OPTIC CABLE - FIBER 12	M	15
29	680.5196 11	FURNISH AND INSTALL CONCRETE FIBER OPTIC PULLBOX	EA	1
30	680.520106	CONDUIT, METAL STEEL, ZINC COATED, 2 NPS	M	35
31	680.520108	CONDUIT, METAL, STEEL ZINC COATED 3 NPS	M	40
32	680.520203	TRAFFIC SIGNAL CONDUIT, FLEXIBLE LIQUID TIGHT STEEL, 1 NPS	M	6
34	680.6717	TRAFFIC SIGNAL POLE-POST TOP MOUNT 1.7 METER MOUNTING HEIGHT	EA	1
36	680.84554911	FIBER OPTIC DATA TRANSCEIVER, SM, SLAVE	EA	1
37	680.92020410	MULTI-CELL GALVANIZED STEEL CONDUIT, 4 DUCTS	M	340
38	680.92124411	SINGLE MODE FIBER OPTIC TRUNK CABLE 144 FIBER (IN CONDUIT)	M	340
39	680.92127211	SINGLE MODE FIBER OPTIC TRUNK CABLE 72 FIBER (IN CONDUIT)	M	340
40	680.95533211	METER CABINET	EA	1
41	680.95644811	PROPERTY LINE BOX (610mm X 508mm X 914mm)	EA	1
43	680.9646 11	FIBER OPTIC CABLE SPLICE	EA	1
45	680.97105011	POLE MOUNT CONTROL EQUIPMENT TYPE 'A' CABINET	EA	1
46	680.98124811	FIBER OPTIC TERMINATION PANEL - 12 FIBERS	EA	1
51	683.0109 11	COAXIAL CABLE, TRANSMIT - 10 MM	M	90
52	683.0110 11	COAXIAL CABLE, TRANSMIT - 14 MM	M	145
54	683.0502 11	TRANSMIT TAG READER	EA	1
55	683.0503 11	TRANSMIT ANTENNA	EA	6

FILE NAME = x80649_cph_con_08.dgn
DATE/TIME = 03-AUG-2012 16:03
USER = kdesai

DESIGN SUPERVISOR RANDOLPH HUNT

JOB MANAGER MAREK CWTOK

DESIGN KAUSHIK DESAI

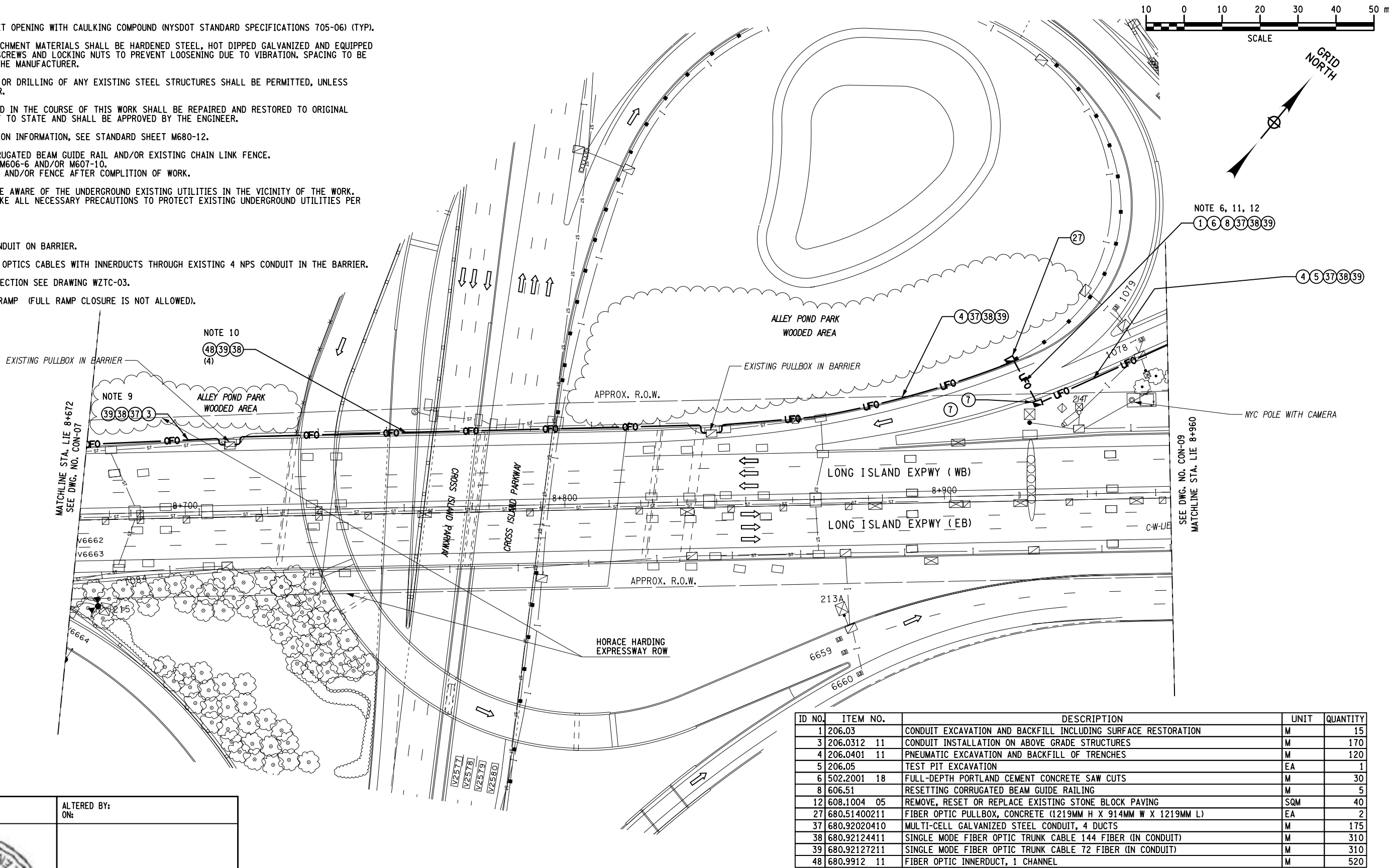
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PROJECT MANAGER RAJ AMIN

- NOTES:
1. SEAL CABLE IN CONDUIT OPENING WITH CAULKING COMPOUND (NYS DOT STANDARD SPECIFICATIONS 705-06) (TYP).
 2. CLAMPS AND ALL ATTACHMENT MATERIALS SHALL BE HARDENED STEEL, HOT DIPPED GALVANIZED AND EQUIPPED WITH CUP POINT SET SCREWS AND LOCKING NUTS TO PREVENT LOOSENING DUE TO VIBRATION. SPACING TO BE AS RECOMMENDED BY THE MANUFACTURER.
 3. NO WELDING, CUTTING, OR DRILLING OF ANY EXISTING STEEL STRUCTURES SHALL BE PERMITTED, UNLESS APPROVED BY ENGINEER.
 4. ALL SURFACES DAMAGED IN THE COURSE OF THIS WORK SHALL BE REPAIRED AND RESTORED TO ORIGINAL CONDITION AT NO COST TO STATE AND SHALL BE APPROVED BY THE ENGINEER.
 5. FOR CONDUIT EXCAVATION INFORMATION, SEE STANDARD SHEET M680-12.
 6. REMOVE EXISTING CORRUGATED BEAM GUIDE RAIL AND/OR EXISTING CHAIN LINK FENCE. SEE STANDARD SHEET M606-6 AND/OR M607-10. REINSTATE GUIDE RAIL AND/OR FENCE AFTER COMPLETION OF WORK.
 7. CONTRACTOR SHOULD BE AWARE OF THE UNDERGROUND EXISTING UTILITIES IN THE VICINITY OF THE WORK. CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS TO PROTECT EXISTING UNDERGROUND UTILITIES PER 16 NYCRR 753.
 8. VACANT
 9. INSTALL PROPOSED CONDUIT ON BARRIER.
 10. PULL PROPOSED FIBER OPTICS CABLES WITH INNERDUCTS THROUGH EXISTING 4 NPS CONDUIT IN THE BARRIER.
 11. FOR WORK ZONE PROTECTION SEE DRAWING WZTC-03.
 12. TRENCHING THROUGH RAMP (FULL RAMP CLOSURE IS NOT ALLOWED).



ID NO.	ITEM NO.	DESCRIPTION	UNIT	QUANTITY
1	206.03	CONDUIT EXCAVATION AND BACKFILL INCLUDING SURFACE RESTORATION	M	15
3	206.0312 11	CONDUIT INSTALLATION ON ABOVE GRADE STRUCTURES	M	170
4	206.0401 11	PNEUMATIC EXCAVATION AND BACKFILL OF TRENCHES	M	120
5	206.05	TEST PIT EXCAVATION	EA	1
6	502.2001 18	FULL-DEPTH PORTLAND CEMENT CONCRETE SAW CUTS	M	30
8	606.51	RESETTING CORRUGATED BEAM GUIDE RAILING	M	5
12	608.1004 05	REMOVE, RESET OR REPLACE EXISTING STONE BLOCK PAVING	SQM	40
27	680.51400211	FIBER OPTIC PULLBOX, CONCRETE (1219MM H X 914MM W X 1219MM L)	EA	2
37	680.92020410	MULTI-CELL GALVANIZED STEEL CONDUIT, 4 DUCTS	M	175
38	680.92124411	SINGLE MODE FIBER OPTIC TRUNK CABLE 144 FIBER (IN CONDUIT)	M	310
39	680.92127211	SINGLE MODE FIBER OPTIC TRUNK CABLE 72 FIBER (IN CONDUIT)	M	310
48	680.9912 11	FIBER OPTIC INNERDUCT, 1 CHANNEL	M	520



AFFIX SEAL:
ON:

ALTERED BY:
ON:

AS-BUILT REVISIONS
DESCRIPTION OF ALTERATIONS:

ITS TRAVEL TIME I-278 AND I-495

PS&E DATE: 06/21/2012

COUNTY: KINGS AND QUEENS

PIN X806.49

BRIDGES

CULVERTS

ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED
ITS INSTALLATION EAST HAMPTON - 4
LIE STA8+672 TO STA8+960

CONTRACT NUMBER

D262095

DRAWING NO. CON-08

SHEET NO. 34

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11

DOCUMENT NAME: x80649_cph_con_08.dgn

FILE NAME = x80649_cph_con_09.dgn
DATE/TIME = 03-AUG-2012 16:03
USER = kdesai

DESIGN SUPERVISOR RANDOLPH HUNT

JOB MANAGER MAREK CWIOK


DESIGN KAUSHIK DESAI

CHECK MAREK CWIOK

DRAFTING KAUSHIK DESAI

CHECK MAREK CWIOK

PROJECT MANAGER RAJ AMIN

AFFIX SEAL: ON:	ALTERED BY: ON:
	

AS-BUILT REVISIONS
DESCRIPTION OF ALTERATIONS:

ITS TRAVEL TIME I-278 AND I-495

PS&E DATE: 06/21/2012

COUNTY: KINGS AND QUEENS

ID NO.	ITEM NO.	DESCRIPTION	UNIT	QUANTITY
4	206.0401 11	PNEUMATIC EXCAVATION AND BACKFILL OF TRENCHES	M	60
12	608.1004 05	REMOVE, RESET OR REPLACE EXISTING STONE BLOCK PAVING	SQM	30
29	680.5196 11	FURNISH AND INSTALL CONCRETE FIBER OPTIC PULLBOX	EA	1
37	680.92020410	MULTI-CELL GALVANIZED STEEL CONDUIT, 4 DUCTS	M	60
38	680.92124411	SINGLE MODE FIBER OPTIC TRUNK CABLE 144 FIBER (IN CONDUIT)	M	30
39	680.92127211	SINGLE MODE FIBER OPTIC TRUNK CABLE 72 FIBER (IN CONDUIT)	M	60
47	680.98724811	FIBER OPTIC TERMINATION PANEL, 72 FIBERS	EA	1

ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED
ITS INSTALLATION EAST HAMPTON - 5
LIE STA8+960

CONTRACT NUMBER

D262095

DRAWING NO. CON-09

SHEET NO. 35

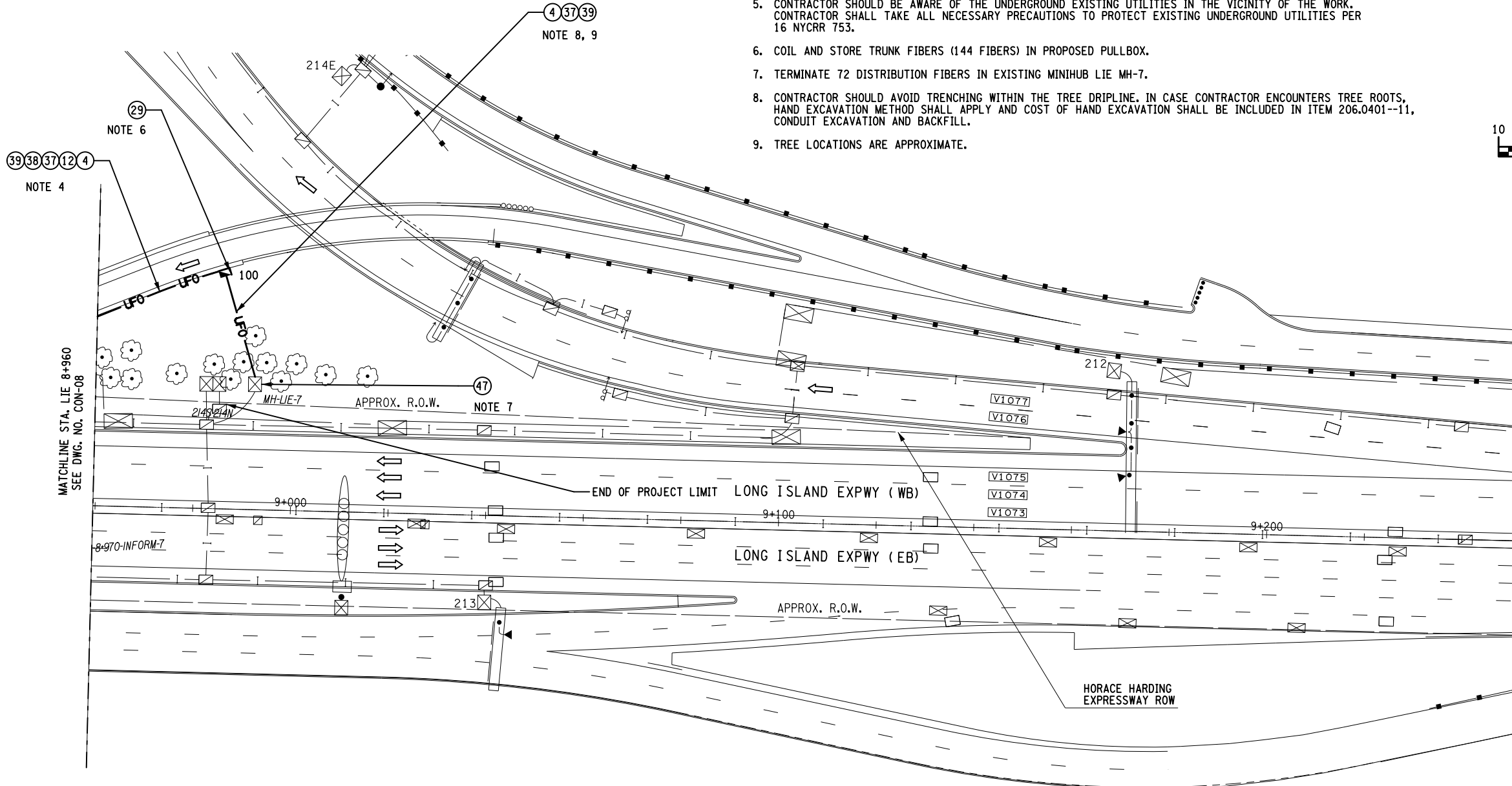
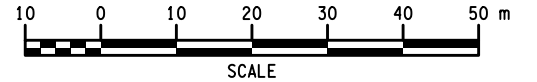
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11

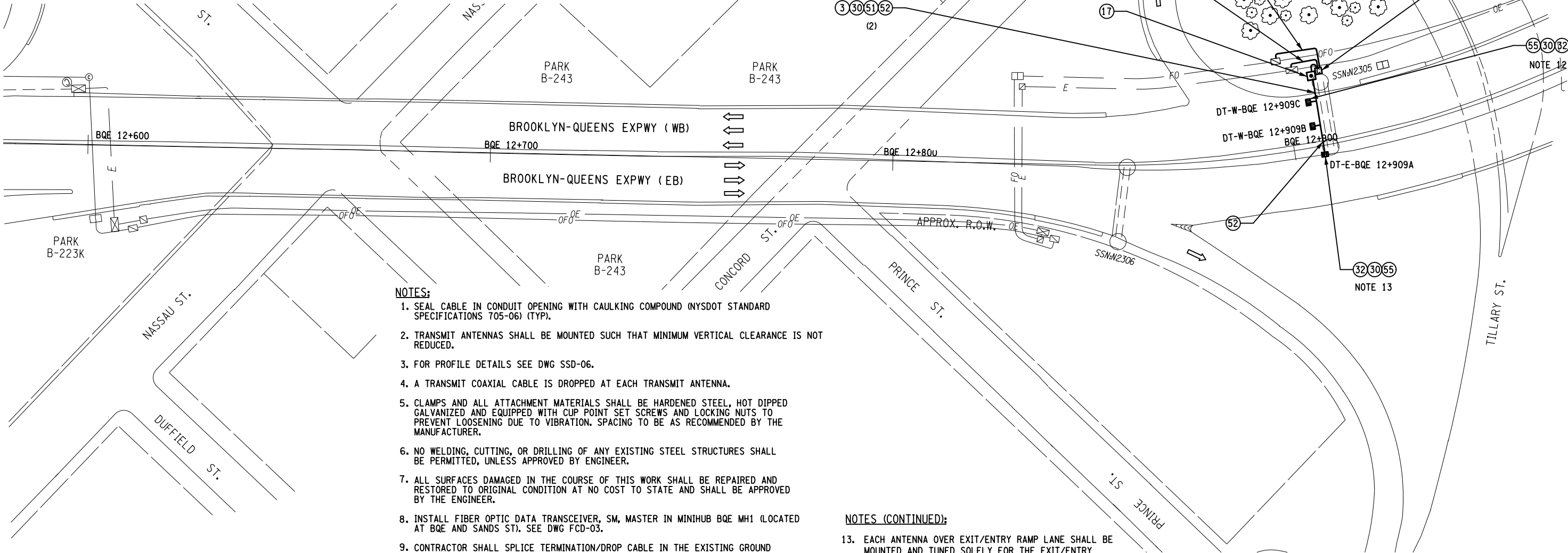
DOCUMENT NAME: x80649_cph_con_09.dgn

NOTES:

1. SEAL CABLE IN CONDUIT OPENING WITH CAULKING COMPOUND (NYS DOT STANDARD SPECIFICATIONS 705-06) (TYP).
2. CLAMPS AND ALL ATTACHMENT MATERIALS SHALL BE HARDENED STEEL, HOT DIPPED GALVANIZED AND EQUIPPED WITH CUP POINT SET SCREWS AND LOCKING NUTS TO PREVENT LOOSENING DUE TO VIBRATION. SPACING TO BE AS RECOMMENDED BY THE MANUFACTURER.
3. ALL SURFACES DAMAGED IN THE COURSE OF THIS WORK SHALL BE REPAIRED AND RESTORED TO ORIGINAL CONDITION AT NO COST TO STATE AND SHALL BE APPROVED BY THE ENGINEER.
4. FOR CONDUIT EXCAVATION INFORMATION, SEE STANDARD SHEET M680-12.
5. CONTRACTOR SHOULD BE AWARE OF THE UNDERGROUND EXISTING UTILITIES IN THE VICINITY OF THE WORK. CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS TO PROTECT EXISTING UNDERGROUND UTILITIES PER 16 NYCRR 753.
6. COIL AND STORE TRUNK FIBERS (144 FIBERS) IN PROPOSED PULLBOX.
7. TERMINATE 72 DISTRIBUTION FIBERS IN EXISTING MINIHUB LIE MH-7.
8. CONTRACTOR SHOULD AVOID TRENCHING WITHIN THE TREE DRIPLINE. IN CASE CONTRACTOR ENCOUNTERS TREE ROOTS, HAND EXCAVATION METHOD SHALL APPLY AND COST OF HAND EXCAVATION SHALL BE INCLUDED IN ITEM 206.0401--11, CONDUIT EXCAVATION AND BACKFILL.
9. TREE LOCATIONS ARE APPROXIMATE.



ID NO.	ITEM NO.	DESCRIPTION	UNIT	QUANTITY
3	206.0312 11	CONDUIT INSTALLATION ON ABOVE GRADE STRUCTURES	M	35
17	670.40	CAST IRON JUNCTION BOXES	EA	1
19	670.7005	SINGLE CONDUCTOR CABLE, NUMBER 8 GAGE	M	10
22	670.7501	GROUND WIRE, #6 AWG	M	5
25	670.99081225	SINGLEMODE FIBER OPTIC CABLE - FIBER 12	M	5
30	680.520106	CONDUIT, METAL STEEL, ZINC COATED, 2 NPS	M	25
32	680.520203	TRAFFIC SIGNAL CONDUIT, FLEXIBLE LIQUID TIGHT STEEL, 1 NPS	M	3
33	680.520206	TRAFFIC SIGNAL CONDUIT, FLEXIBLE LIQUID-TIGHT STEEL, 2 NPS	M	5
35	680.84554811	FIBER OPTIC DATA TRANSCEIVER, SM, MASTER	EA	1
36	680.84554911	FIBER OPTIC DATA TRANSCEIVER, SM, SLAVE	EA	1
46	680.98124811	FIBER OPTIC TERMINATION PANEL - 12 FIBERS	EA	1
51	683.0109 11	COAXIAL CABLE, TRANSMIT - 10 MM	M	45
52	683.0110 11	COAXIAL CABLE, TRANSMIT - 14 MM	M	25
53	683.0112 11	FIELD CABINET - TYPE T	EA	1
54	683.0502 11	TRANSMIT TAG READER	EA	1
55	683.0503 11	TRANSMIT ANTENNA	EA	3
59	683.96050108	FIBER OPTIC DROP CABLE FUSION SPLICE	EA	1




- NOTES:**
1. SEAL CABLE IN CONDUIT OPENING WITH CAULKING COMPOUND (NYS DOT STANDARD SPECIFICATIONS 705-06) (TYP).
 2. TRANSMIT ANTENNAS SHALL BE MOUNTED SUCH THAT MINIMUM VERTICAL CLEARANCE IS NOT REDUCED.
 3. FOR PROFILE DETAILS SEE DWG SSD-06.
 4. A TRANSMIT COAXIAL CABLE IS DROPPED AT EACH TRANSMIT ANTENNA.
 5. CLAMPS AND ALL ATTACHMENT MATERIALS SHALL BE HARDENED STEEL, HOT DIPPED GALVANIZED AND EQUIPPED WITH CUP POINT SET SCREWS AND LOCKING NUTS TO PREVENT LOOSENING DUE TO VIBRATION. SPACING TO BE AS RECOMMENDED BY THE MANUFACTURER.
 6. NO WELDING, CUTTING, OR DRILLING OF ANY EXISTING STEEL STRUCTURES SHALL BE PERMITTED, UNLESS APPROVED BY ENGINEER.
 7. ALL SURFACES DAMAGED IN THE COURSE OF THIS WORK SHALL BE REPAIRED AND RESTORED TO ORIGINAL CONDITION AT NO COST TO STATE AND SHALL BE APPROVED BY THE ENGINEER.
 8. INSTALL FIBER OPTIC DATA TRANSCEIVER, SM, MASTER IN MINIHUB BQE MH1 (LOCATED AT BQE AND SANDS ST). SEE DWG FCD-03.
 9. CONTRACTOR SHALL SPLICE TERMINATION/DROP CABLE IN THE EXISTING GROUND MOUNTED PULLBOX. SEE DWG FST-03.
 10. CONTRACTOR SHALL SPLICE PROPOSED 2*8 AND *6G CABLES IN THE EXISTING GROUND MOUNTED PULLBOX TO CIRCUIT *7. SEE DWG. MCD-04.
 11. FOR CONDUIT EXCAVATION INFORMATION, SEE STANDARD SHEET M680-12
 12. FOR DETOUR INFORMATION SEE DWG DTR-04.

- NOTES (CONTINUED):**
13. EACH ANTENNA OVER EXIT/ENTRY RAMP LANE SHALL BE MOUNTED AND TUNED SOLELY FOR THE EXIT/ENTRY TRAFFIC, WHERE APPLICABLE.
 14. INSTALL ANTENNA, AS PER MANUFACTURER'S RECOMMENDATIONS, ON STRUCTURE IN SIDE-FIRED FASHION AT HEIGHT OF 17 +/- 1 FT AND 5 FT AWAY FROM EDGE OF EASTBOUND LEFT LANE AT ANGLE (YAW ANGLE 30° +/- 2°, PITCH ANGLE 35° +/- 2°) SUCH THAT IT WILL CAPTURE BQE EAST BOUND TRAFFIC (1.5 LANES).

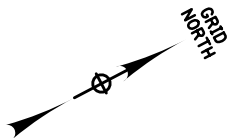
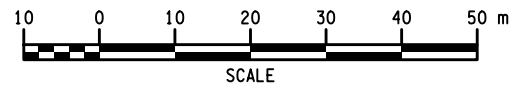
AFFIX SEAL:
ON:

ALTERED BY:
ON:


AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	ITS TRAVEL TIME I-278 AND I-495	PIN X806.49	BRIDGES	CULVERTS	ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED	CONTRACT NUMBER	
	PS&E DATE: 06/21/2012				D262095		
					DRAWING NO. CON-10		
					SHEET NO. 36		
	COUNTY: KINGS AND QUEENS						


IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11
DOCUMENT NAME: X80649_cph_con.10.dgn



1. SEAL CABLE IN CONDUIT OPENING WITH CAULKING COMPOUND (INSDOT STANDARD SPECIFICATIONS 705-06) (TYP).
2. TRANSMIT ANTENNAS SHALL BE MOUNTED SUCH THAT MINIMUM VERTICAL CLEARANCE IS NOT REDUCED.
3. FOR PROFILE DETAILS SEE DWG SSD-07.
4. A TRANSMIT COAXIAL CABLE IS DROPPED AT EACH TRANSMIT ANTENNA.
5. CLAMPS AND ALL ATTACHMENT MATERIALS SHALL BE HARDENED STEEL, HOT DIPPED GALVANIZED AND EQUIPPED WITH CUP POINT SET SCREWS AND LOCKING NUTS TO PREVENT LOOSENING DUE TO VIBRATION. SPACING TO BE AS RECOMMENDED BY THE MANUFACTURER.
6. NO WELDING, CUTTING, OR DRILLING OF ANY EXISTING STEEL STRUCTURES SHALL BE PERMITTED, UNLESS APPROVED BY ENGINEER.
7. ALL SURFACES DAMAGED IN THE COURSE OF THIS WORK SHALL BE REPAIRED AND RESTORED TO ORIGINAL CONDITION AT NO COST TO STATE AND SHALL BE APPROVED BY THE ENGINEER.
8. FOR CONDUIT EXCAVATION INFORMATION, SEE STANDARD SHEET M680-12.
9. INSTALL FIBER OPTIC DATA TRANSCEIVER, SM, MASTER IN MINIHUB BQE-02 LOCATED AT BQE 20+137. SEE DWG FCD-02.
10. CONTRACTOR SHALL SPLICE TERMINATION/DROP CABLE IN THE EXISTING GROUND MOUNTED PULLBOX. SEE DWG FST-02.
11. FOR EXISTING ITS DEVICES AFFECTED BY PROPOSED WORK SEE DWG ITT-05.
12. EACH ANTENNA OVER EXIT/ENTRY RAMP LANE SHALL BE MOUNTED AND TUNED SOLELY FOR THE EXIT/ENTRY TRAFFIC.
13. PULL PROPOSED CABLES, 2*8 AND 1*6G, FROM EXISTING PULLBOX TO PROPOSED CABINET THROUGH PROPOSED 2NPS CONDUIT AND PROPOSED FLEXIBLE CONDUIT. EXISTING CABLES SHALL STAY IN EXISTING CONDUIT.

AFFIX SEAL: ON:	ALTERED BY: ON:
 <p>The seal is circular with a double-lined border. The outer ring contains the text "STATE OF NEW YORK" at the top and "LICENSED PROFESSIONAL ENGINEER" at the bottom, separated by two stars. The inner circle features a central emblem depicting a Native American figure holding a bow and arrow, with a shield on his chest. Above the figure is a scroll with the motto "E PLURIBUS UNUM". The name "RANDOLPH" is inscribed in an arc above the central emblem.</p>	

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	ITS TRAVEL TIME I-278 AND I-495	PIN X806.49	BRIDGES	CULVERTS	ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED	CONTRACT NUMBER	
	PS&E DATE: 06/21/2012				ITS INSTALLATION 54TH AVE SSN#N0531	D262095	
						DRAWING NO. CON-11	
						SHEET NO. 37	
	COUNTY: KINGS AND QUEENS						
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.							NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11 DOCUMENT NAME: X80649_cph_con.11.dgn

NOTES:

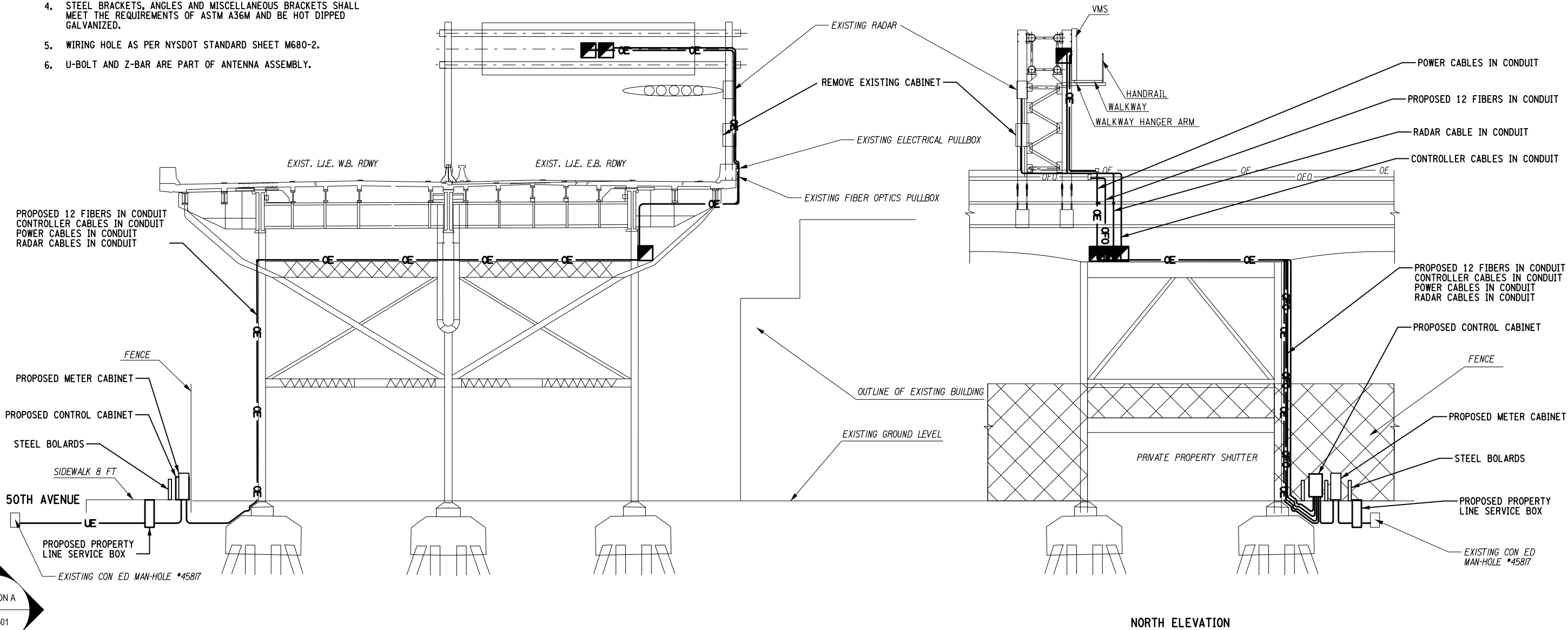
1. THE SUGGESTED METHOD OF TRANSMIT ANTENNA ATTACHMENT SHOWN ON DWG DTL-02 IS PROVIDED FOR INFORMATION ONLY. CONTRACTOR SHALL SUBMIT SHOP DRAWINGS FOR APPROVAL OF THE MEANS AND METHOD OF ATTACHMENT REQUIREMENTS FOR MOUNTING ANTENNA.
2. ALL NECESSARY HARDWARE AND COMPONENTS FOR SUPPORT AND MOUNTING BRACKETS SHALL BE INCLUDED IN THE PRICE BID FOR THE CONDUIT AND PAID UNDER CONDUIT ITEM.
3. ALL FASTENER HARDWARE SHALL BE STAINLESS STEEL AND CONFIRM TO SUB SELECTION 715-16 OF THE STANDARD SPECIFICATIONS. A LOCK WASHER OR APPROVED STOP NUT SHALL BE USED UNDER EACH NUT. A FLAT WASHER SHALL BE USED UNDER EACH HEAD WITH SLOTTED HOLES AND A FLAT WASHER SHALL BE USED UNDER LOCKWASHERS.
4. STEEL BRACKETS, ANGLES AND MISCELLANEOUS BRACKETS SHALL MEET THE REQUIREMENTS OF ASTM A36M AND BE HOT DIPPED GALVANIZED.
5. WIRING HOLE AS PER NYSDOT STANDARD SHEET M680-2.
6. U-BOLT AND Z-BAR ARE PART OF ANTENNA ASSEMBLY.

NOTES: (CONTINUED)

7. ALL STEEL SURFACES SHALL BE PREVENTED FROM COMING IN DIRECT CONTACT WITH THE ALUMINIUM BRACKETS BY MEANS OF EITHER AN APPROVED MASTIC OR 3mm THICK PAD PLACED BETWEEN THE DISSIMILAR METALS AS PER STANDARD SPECIFICATION 646-2.06.
8. SECONDARY TRUSS ELEMENTS ARE NOT SHOWN FOR CLARITY.

NOTES: (CONTINUED)

9. ALL PIPE ENDS SHOULD BE CLOSED BY WATER PROOF CAP (TYPICAL).
10. MINIMUM BENDING RADIUS 241mm FOR 2 NPS AND 330mm FOR 3 NPS PVC COATED GALVANIZED CONDUIT.
11. FOR CONSTRUCTION, SEE DWG CON-01.
12. PROPOSED CONDUIT ON TRUSS-CHORD SHALL BE TIED TO CHORD WITH STAINLESS STEEL STRAPS AT 1m ON CENTERS, MAXIMUM OR AOE.
13. INSTALL CABINET ON GROUND, SEE DWG DTL-01, DETAIL B.
14. FOR CONDUIT MOUNTING DETAIL, SEE DWG DTL-06, DETAIL C.



AFFIX SEAL: ON:	ALTERED BY: ON:

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	ITS TRAVEL TIME I-278 AND I-495	PIN X806.49	BRIDGES	CULVERTS	ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED	CONTRACT NUMBER	
	PS&E DATE: 06/21/2012					D262095	
						DRAWING NO. SSD-01	
	COUNTY: KINGS AND QUEENS					SHEET NO. 38	
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.					NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11 DOCUMENT NAME: x80649_cph_gnr_ssd_01.dgn		

FILE NAME = x80649_cph_gnr_ssd_02.dgn
DATE/TIME = 03-AUG-2012 16:11
USER = kdesai

DESIGN SUPERVISOR RANDOLPH HUNT

JOB MANAGER MAREK CWTOK

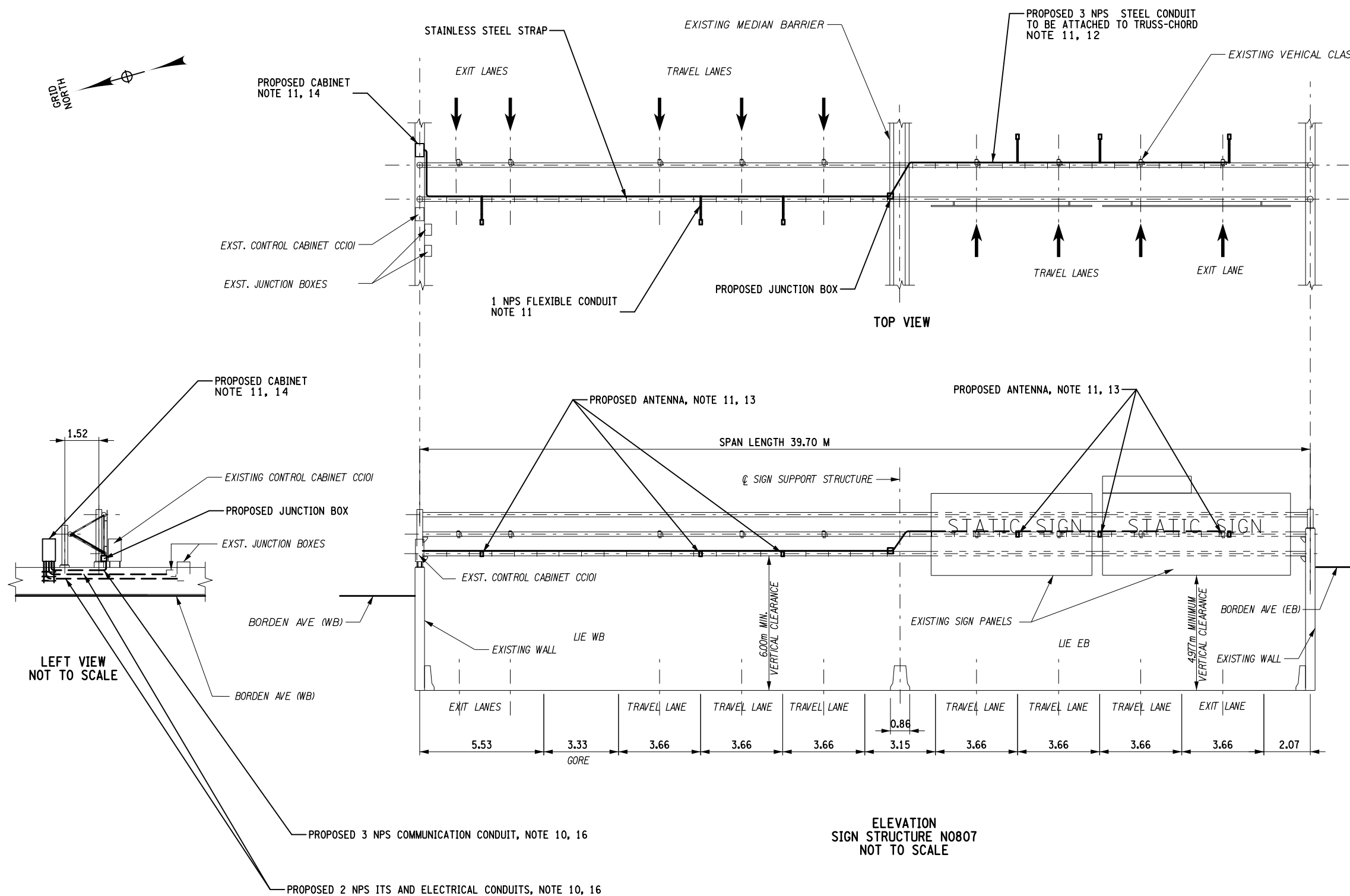
DESIGN KAUSHIK DESAI

CHECK MAREK CWTOK

DRAFTING KAUSHIK DESAI

CHECK MAREK CWTOK


PROJECT MANAGER RAJ AMIN

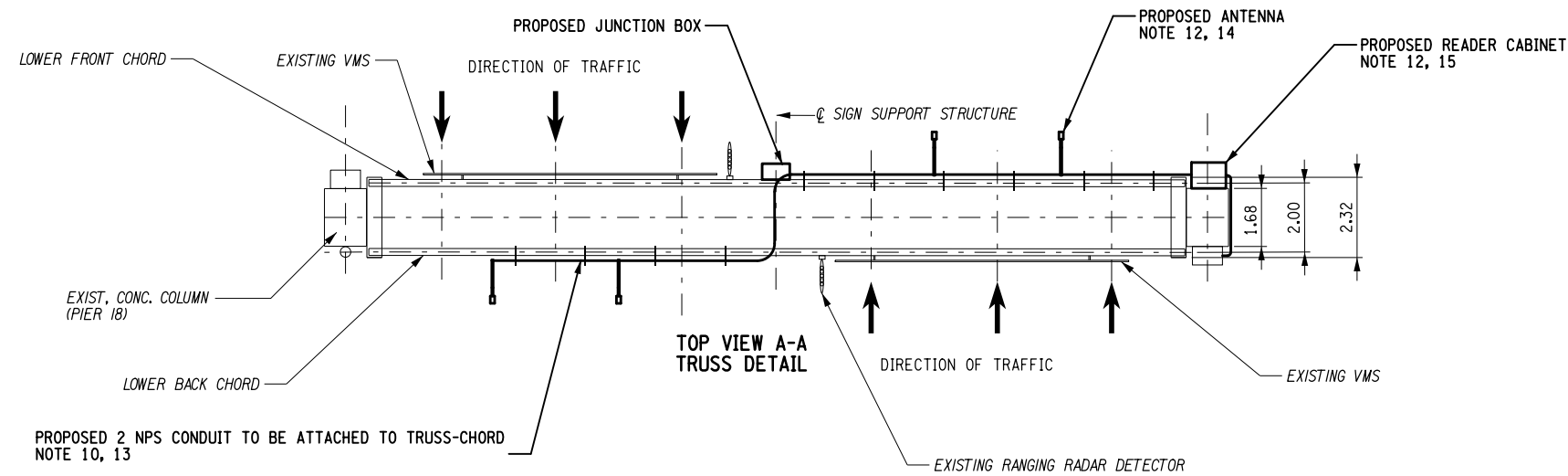


NOTES:

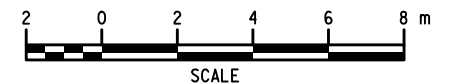
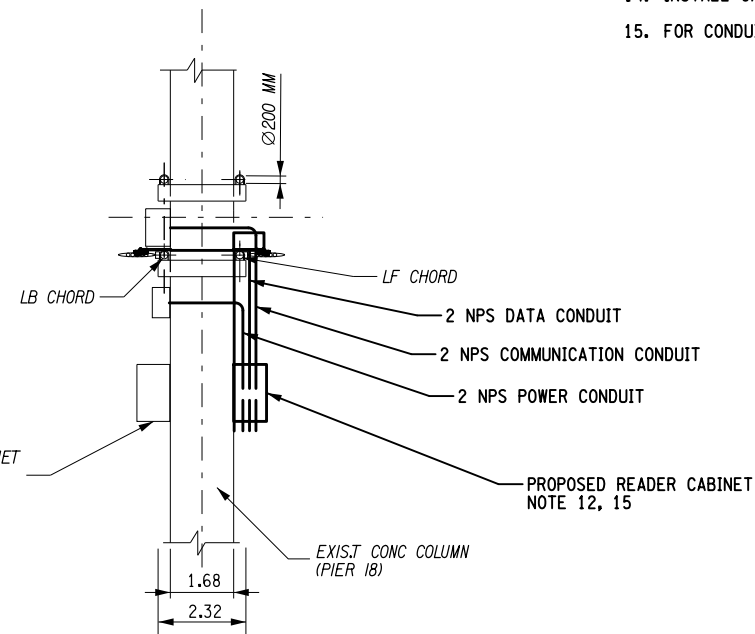
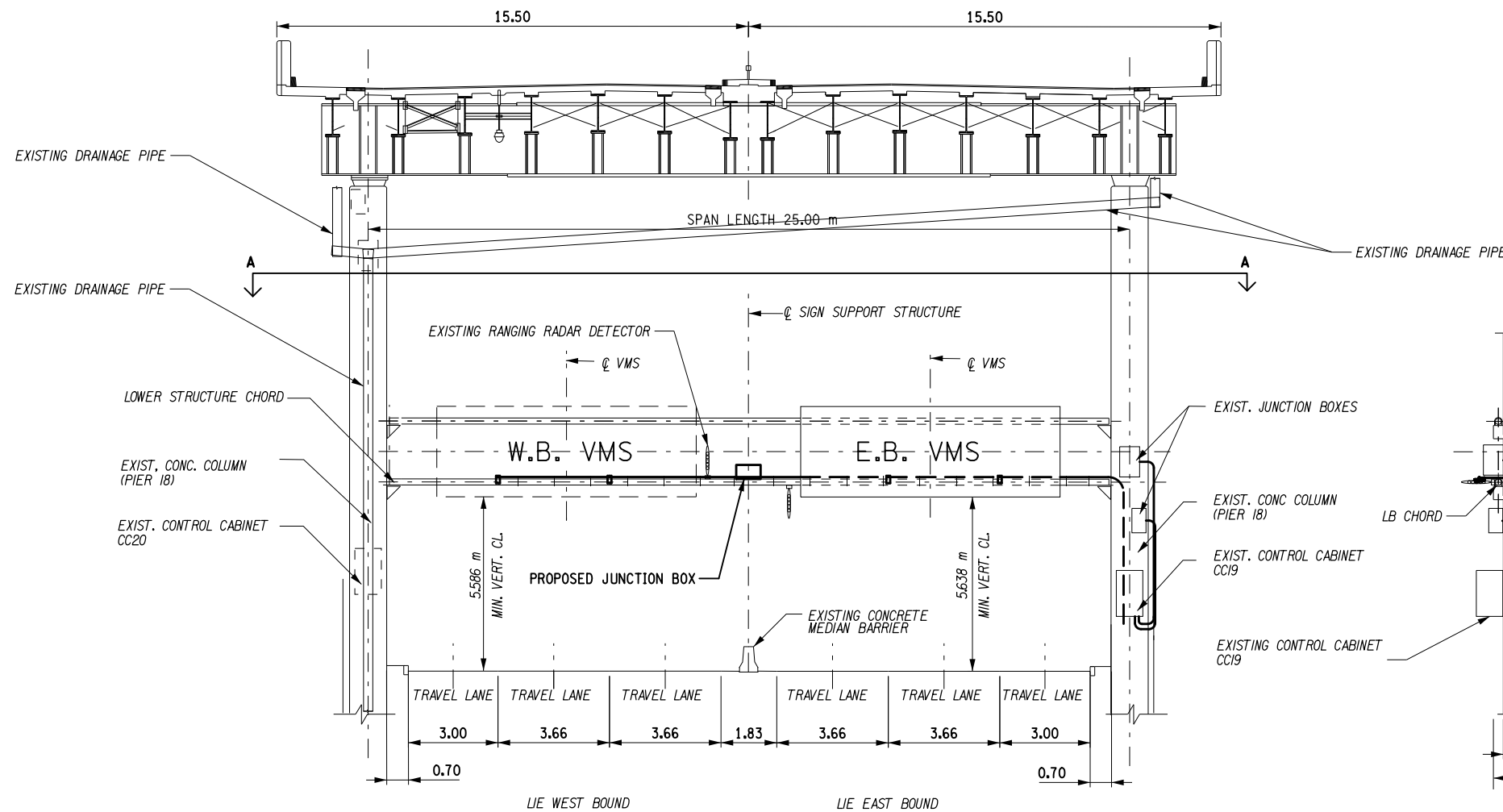
1. THE SUGGESTED METHOD OF TRANSMIT ANTENNA ATTACHMENT SHOWN ON DWG DTL-01 IS PROVIDED FOR INFORMATION ONLY. CONTRACTOR SHALL SUBMIT SHOP DRAWINGS FOR APPROVAL OF THE MEANS AND METHOD OF ATTACHMENT REQUIREMENTS FOR MOUNTING ANTENNA.
2. ALL NECESSARY HARDWARE AND COMPONENTS FOR SUPPORT AND MOUNTING BRACKETS SHALL BE INCLUDED IN THE PRICE BID FOR THE CONDUIT AND PAID UNDER CONDUIT ITEM.
3. ALL FASTENER HARDWARE SHALL BE STAINLESS STEEL AND CONFORM TO SUB SELECTION 715-16 OF THE STANDARD SPECIFICATIONS. A LOCK WASHER OR APPROVED STOP NUT SHALL BE USED UNDER EACH NUT. A FLAT WASHER SHALL BE USED UNDER EACH HEAD WITH SLOTTED HOLES AND A FLAT WASHER SHALL BE USED UNDER LOCKWASHERS.
4. STEEL BRACKETS, ANGLES AND MISCELLANEOUS BRACKETS SHALL MEET THE REQUIREMENTS OF ASTM A36M AND BE HOT DIPPED GALVANIZED.
5. WIRING HOLE AS PER NYSDOT STANDARD SHEET M680-2.
6. U-BOLT AND Z-BAR ARE PART OF ANTENNA ASSEMBLY.
7. ALL STEEL SURFACES SHALL BE PREVENTED FROM COMING IN DIRECT CONTACT WITH THE ALUMINIUM BRACKETS BY MEANS OF EITHER AN APPROVED MASTIC OR 3mm THICK PAD PLACED BETWEEN THE DISSIMILAR METALS AS PER STANDARD SPECIFICATION 646-2.06.
8. SECONDARY TRUSS ELEMENTS ARE NOT SHOWN FOR CLARITY.
9. ALL PIPE ENDS SHOULD BE CLOSED BY WATER PROOF CAP (TYPICAL).
10. MINIMUM BENDING RADIUS 241mm FOR 2 NPS AND 330mm FOR 3 NPS GALVANIZED CONDUIT.
11. MINIMUM BENDING RADIUS FOR COAXIAL CABLE SHALL BE 127mm.
12. FOR CONSTRUCTION, SEE DWG CON-02.
13. PROPOSED CONDUIT ON TRUSS-CHORD SHALL BE TIED TO CHORD WITH STAINLESS STEEL STRAPS AT 1m ON CENTERS, MAXIMUM OR AOB.
14. INSTALL CABINET ON EXISTING WALL, SEE DWG DTL-04, DETAIL A.
15. FOR CONDUIT MOUNTING DETAIL, SEE DWG DTL-06.
16. TRANSMIT ANTENNAS WILL BE MOUNTED ON THE SAME GANTRY WITH VEHICLE CLASSIFICATION DETECTORS (VCD), THE VEHICLE CLASSIFICATION DETECTORS SHALL BE STRICTLY ALLIGNED TO THE CENTER OF EACH TRAVEL LANE. THE OFFSET LENGTH OF TRANSMIT ANTENNAS FROM THE CENTER OF EACH LANE SHALL BE VERIFIED IN THE FIELD FOR FULL LANE COVERAGE.

AFFIX SEAL: ON:	ALTERED BY: ON:

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	ITS TRAVEL TIME I-278 AND I-495	PIN X806.49	BRIDGES	CULVERTS	ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED	CONTRACT NUMBER	
	PS&E DATE: 06/21/2012					D262095	
						DRAWING NO. SSD-02	
						SHEET NO. 39	
	COUNTY: KINGS AND QUEENS						
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.					NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11 DOCUMENT NAME: x80649_cph_gnr_ssd.02.dgn		



- NOTES:
1. THE SUGGESTED METHOD OF TRANSMIT ANTENNA ATTACHMENT SHOWN ON DWG DTL-02 IS PROVIDED FOR INFORMATION ONLY. CONTRACTOR SHALL SUBMIT SHOP DRAWINGS FOR APPROVAL OF THE MEANS AND METHOD OF ATTACHMENT REQUIREMENTS FOR MOUNTING ANTENNA.
 2. ALL NECESSARY HARDWARE AND COMPONENTS FOR SUPPORT AND MOUNTING BRACKETS SHALL BE INCLUDED IN THE PRICE BID FOR THE CONDUIT AND PAID UNDER CONDUIT ITEM.
 3. ALL FASTENER HARDWARE SHALL BE STAINLESS STEEL AND CONFIRM TO SUB SELECTION 715-16 OF THE STANDARD SPECIFICATIONS. A LOCK WASHER OR APROVED STOP NUT SHALL BE USED UNDER EACH NUT. A FLAT WASHER SHALL BE USED UNDER EACH HEAD WITH SLOTTED HOLES AND A FLAT WASHER SHALL BE USED UNDER LOCKWASHERS.
 4. STEEL BRACKETS, ANGLES AND MISCELLANEOUS BRACKETS SHALL MEET THE REQUIREMENTS OF ASTM A36M AND BE HOT DIPPED GALVANIZED.
 5. WIRING HOLE AS PER NYSDOT STANDARD SHEET M680-2.
 6. U-BOLT AND Z-BAR ARE PART OF ANTENNA ASSEMBLY.
 7. ALL STEEL SURFACES SHALL BE PREVENTED FROM COMING IN DIRECT CONTACT WITH THE ALUMINIUM BRACKETS BY MEANS OF EITHER AN APPROVED MASTIC OR 3mm THICK PAD PLACED BETWEEN THE DISSIMILAR METALS AS PER STANDARD SPECIFICATION 646-2.06.
 8. SECONDARY TRUSS ELEMENTS ARE NOT SHOWN FOR CLARITY.
 9. ALL PIPE ENDS SHOULD BE CLOSED BY WATER PROOF CAP (TYPICAL).
 10. MINIMUM BENDING RADIUS 241mm FOR 2 NPS AND 330mm FOR 3 NPS GALVANIZED CONDUIT.
 11. MINIMUM BENDING RADIUS FOR COAXIAL CABLE SHALL BE 127mm.
 12. FOR CONSTRUCTION, SEE DWG CON-03.
 13. PROPOSED CONDUIT ON TRUSS-CHORD SHALL BE TIED TO CHORD WITH STAINLESS STEEL STRAPS AT 1m ON CENTERS, MAXIMUM OR AOE.
 14. INSTALL CABINET ON EXISTING COLUMN, SEE DWG DTL-04, DETAIL A.
 15. FOR CONDUIT MOUNTING DETAIL, SEE DWG DTL-06.



AFFIX SEAL: ON:	ALTERED BY: ON:

ELEVATION SIGN STRUCTURE N2314		RIGHT VIEW		SCALE	
AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	ITS TRAVEL TIME I-278 AND I-495	PIN X806.49	BRIDGES	CULVERTS	ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED
	PS&E DATE: 06/21/2012				CONTRACT NUMBER
					D262095
	COUNTY: KINGS AND QUEENS				DRAWING NO. SSD-03
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.					PROFILE - 48TH ST (LOWER LEVEL) SHEET NO. 40
NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11					DOCUMENT NAME: x80649_cph_gnr_ssd.03.dgn



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USER = kdesai

DESIGN SUPERVISOR RANDOLPH HUNT

JOB MANAGER MAREK CMIK

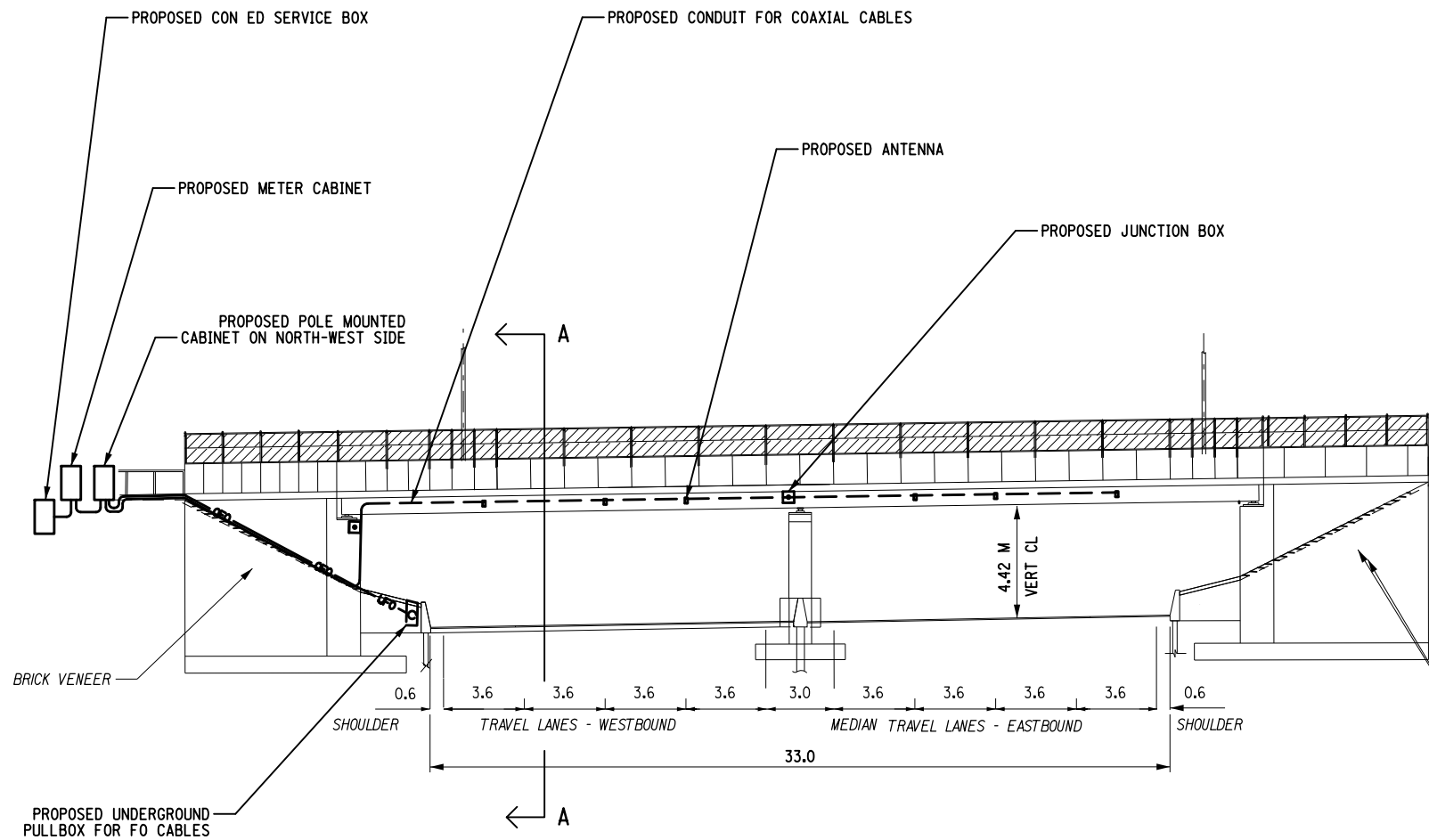
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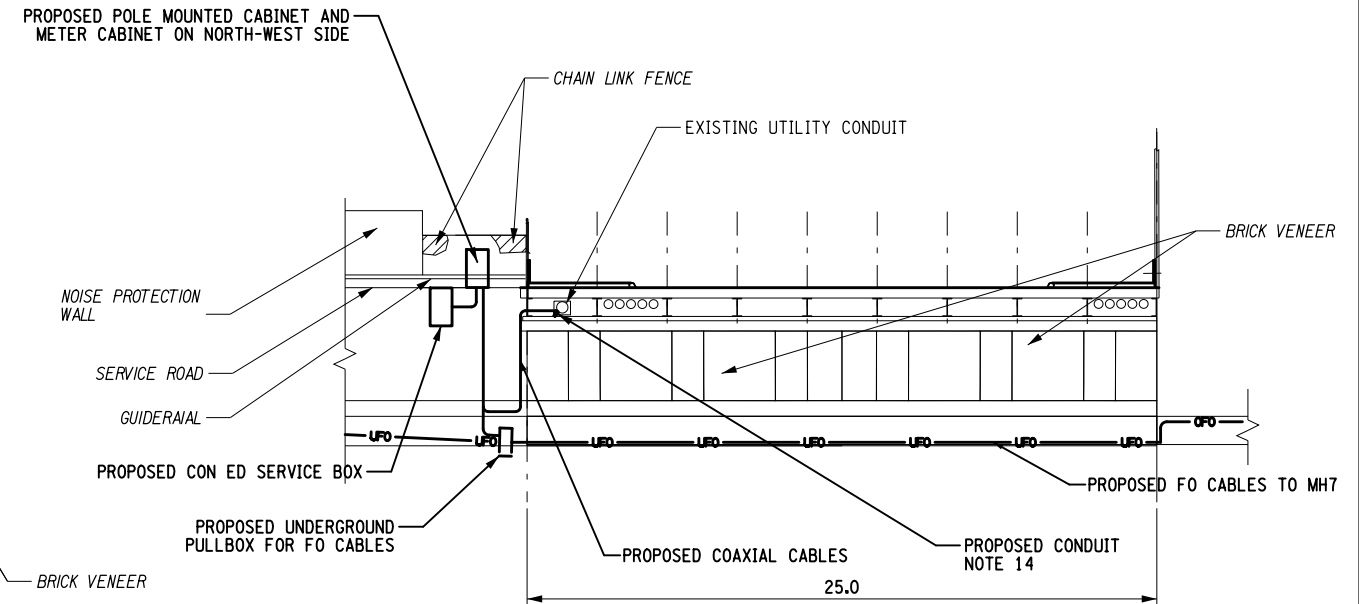
PROJECT MANAGER RAJ AMIN




WEST ELEVATION
BIN 1055340


- NOTES:
1. THE SUGGESTED METHOD OF TRANSMIT ANTENNA ATTACHMENT SHOWN ON DWG DTL-03 IS PROVIDED FOR INFORMATION ONLY. CONTRACTOR SHALL SUBMIT SHOP DRAWINGS FOR APPROVAL OF THE MEANS AND METHOD OF ATTACHMENT REQUIREMENTS FOR MOUNTING ANTENNA.
 2. ALL NECESSARY HARDWARE AND COMPONENTS FOR SUPPORT AND MOUNTING BRACKETS SHALL BE INCLUDED IN THE PRICE BID FOR THE CONDUIT AND PAID UNDER CONDUIT ITEM.
 3. ALL FASTENER HARDWARE SHALL BE STAINLESS STEEL AND CONFIRM TO SUB SELECTION 715-16 OF THE STANDARD SPECIFICATIONS. A LOCK WASHER OR APPROVED STOP NUT SHALL BE USED UNDER EACH NUT. A FLAT WASHER SHALL BE USED UNDER EACH HEAD WITH SLOTTED HOLES AND A FLAT WASHER SHALL BE USED UNDER LOCKWASHERS.
 4. STEEL BRACKETS, ANGLES AND MISCELLANEOUS BRACKETS SHALL MEET THE REQUIREMENTS OF ASTM A36M AND BE HOT DIPPED GALVANIZED.
 5. WIRING HOLE AS PER NYSDOT STANDARD SHEET M680-2.
 6. U-BOLT AND Z-BAR ARE PART OF ANTENNA ASSEMBLY.
 7. ALL STEEL SURFACES SHALL BE PREVENTED FROM COMING IN DIRECT CONTACT WITH THE ALUMINIUM BRACKETS BY MEANS OF EITHER AN APPROVED MASTIC OR 3mm THICK PAD PLACED BETWEEN THE DISSIMILAR METALS AS PER STANDARD SPECIFICATION 646-2.06.

- NOTES (CONTINUED):
8. ALL PIPE ENDS SHOULD BE CLOSED BY WATER PROOF CAP (TYPICAL).
 9. MINIMUM BENDING RADIUS 241mm FOR 2 NPS AND 330mm FOR 3 NPS GALVANIZED CONDUIT.
 10. MINIMUM BENDING RADIUS FOR COAXIAL CABLE SHALL BE 127mm.
 11. FOR CONSTRUCTION, SEE DWG CON-07.
 12. INSTALL CABINET ON PROPOSED POLE SEE DWG DTL-04, DETAIL "C".
 13. VACANT
 14. INSTALL PROPOSED CONDUIT IN EXISTING OPENING IN CROSS MEMBER ALONG WITH EXISTING UTILITY CONDUIT.



SECTION A-A

AFFIX SEAL: ON:	ALTERED BY: ON:
	

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	ITS TRAVEL TIME I-278 AND I-495	PIN X806.49	BRIDGES	CULVERTS	ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED	CONTRACT NUMBER	
	PS&E DATE: 06/21/2012					D262095	
						DRAWING NO. SSD-05	
	COUNTY: KINGS AND QUEENS					SHEET NO. 42	
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.						NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11 DOCUMENT NAME: X80649_cph_gnr_ssd_05.dgn	

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USER = kdesai

DESIGN SUPERVISOR RANDOLPH HUNT

JOB MANAGER MAREK CWIOK

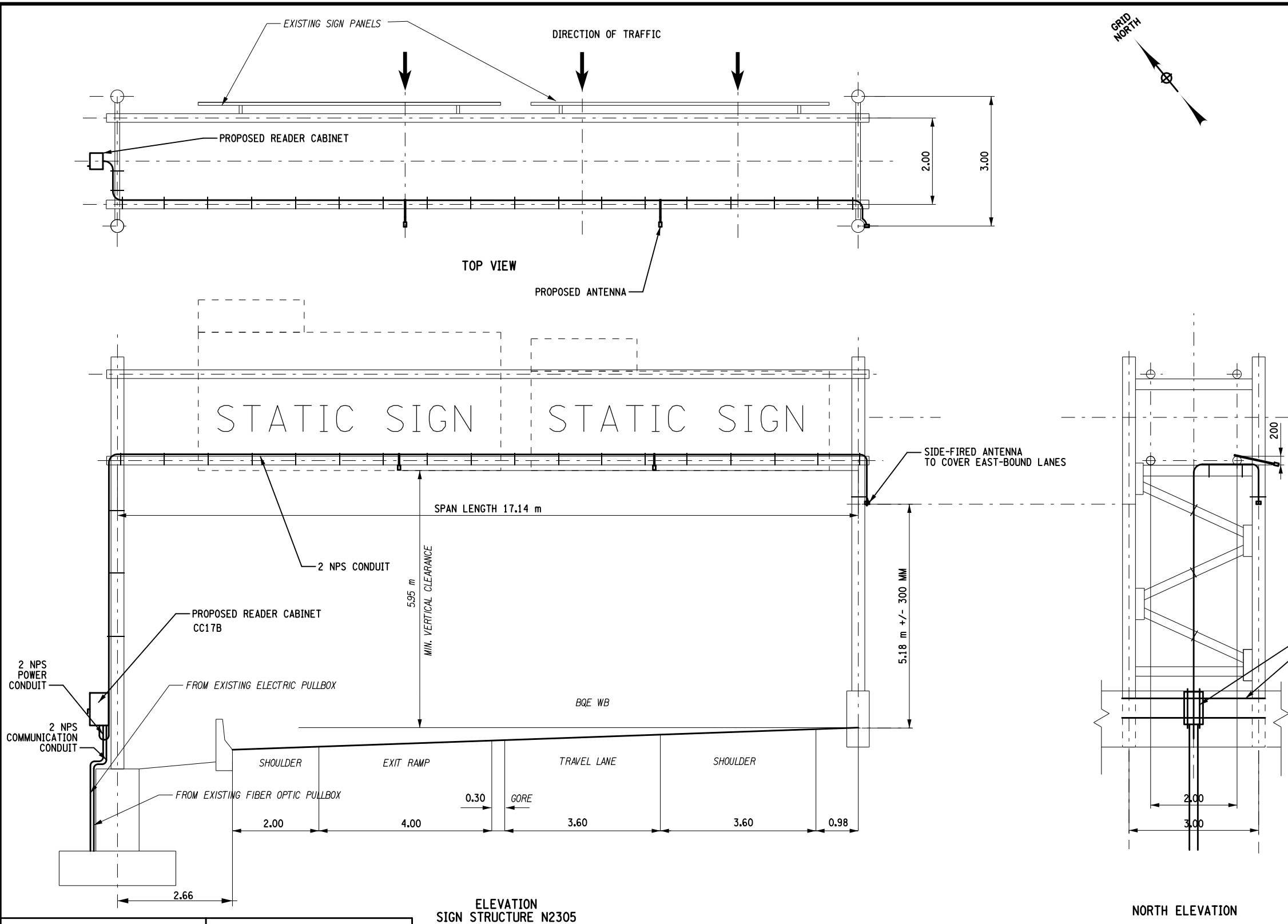
DESIGN KAUSHIK DESAI

CHECK MAREK CWIOK

DRAFTING KAUSHIK DESAI

CHECK MAREK CWIOK

PROJECT MANAGER RAJ AMIN



NOTES:

1. THE SUGGESTED METHOD OF TRANSMIT ANTENNA ATTACHMENT SHOWN ON DWG DTL-02 IS PROVIDED FOR INFORMATION ONLY. CONTRACTOR SHALL SUBMIT SHOP DRAWINGS FOR APPROVAL OF THE MEANS AND METHOD OF ATTACHMENT REQUIREMENTS FOR MOUNTING ANTENNA.
2. ALL NECESSARY HARDWARE AND COMPONENTS FOR SUPPORT AND MOUNTING BRACKETS SHALL BE INCLUDED IN THE PRICE BID FOR THE CONDUIT AND PAID UNDER CONDUIT ITEM.
3. ALL FASTENER HARDWARE SHALL BE STAINLESS STEEL AND CONFIRM TO SUB SELECTION 715-16 OF THE STANDARD SPECIFICATIONS. A LOCK WASHER OR APPROVED STOP NUT SHALL BE USED UNDER EACH NUT. A FLAT WASHER SHALL BE USED UNDER EACH HEAD WITH SLOTTED HOLES AND A FLAT WASHER SHALL BE USED UNDER LOCKWASHERS.
4. STEEL BRACKETS, ANGLES AND MISCELLANEOUS BRACKETS SHALL MEET THE REQUIREMENTS OF ASTM A36M AND BE HOT DIPPED GALVANIZED.
5. WIRING HOLE AS PER NYSDOT STANDARD SHEET M680-2.
6. U-BOLT AND Z-BAR ARE PART OF ANTENNA ASSEMBLY.
7. ALL STEEL SURFACES SHALL BE PREVENTED FROM COMING IN DIRECT CONTACT WITH THE ALUMINIUM BRACKETS BY MEANS OF EITHER AN APPROVED MASTIC OR 3mm THICK PAD PLACED BETWEEN THE DISSIMILAR METALS AS PER STANDARD SPECIFICATION 646-2.06.
8. SECONDARY TRUSS ELEMENTS ARE NOT SHOWN FOR CLARITY.
9. ALL PIPE ENDS SHOULD BE CLOSED BY WATER PROOF CAP (TYPICAL).
10. MINIMUM BENDING RADIUS 241mm FOR 2 NPS AND 330mm FOR 3 NPS GALVANIZED CONDUIT.
11. MINIMUM BENDING RADIUS FOR COAXIAL CABLE SHALL BE 127mm.
12. FOR CONSTRUCTION, SEE DWG CON-10.
13. PROPOSED CONDUIT ON TRUSS-CHORD SHALL BE TIED TO CHORD WITH STAINLESS STEEL STRAPS AT 1m ON CENTERS, MAXIMUM OR ABOVE.
14. INSTALL CABINET ON SIGN STRUCTURE, SEE DWG DTL-04, DETAIL C.
15. FOR CONDUIT MOUNTING DETAIL, SEE DWG DTL-06.

AFFIX SEAL: ON:	ALTERED BY: ON:

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	ITS TRAVEL TIME I-278 AND I-495	PIN X806.49	BRIDGES	CULVERTS	ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED	CONTRACT NUMBER	
	PS&E DATE: 06/21/2012					D262095	
						DRAWING NO. SSD-06	
	COUNTY: KINGS AND QUEEN					SHEET NO. 43	
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.					NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11		
					DOCUMENT NAME: x80649_cph_gnr_ssd_06.dgn		

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USER = douran

PROJECT MANAGER RAJENDRA AMIN

CHECK E. TSE

DRAFTING R. DRAISS

CHECK J. KOLODZINSKI

DESIGN E. TSE

JOB MANAGER J. KOLODZINSKI

DESIGN SUPERVISOR B. CRUDELE

NOTES:

PROPOSED STRUCTURE S.I.N: N2283 IS TO REPLACE THE EXISTING STRUCTURE S.I.N: N2728 ON L.I.E. EASTBOUND.

THIS STRUCTURE IS DESIGNED IN ACCORDANCE WITH NYSDOT STANDARD DESIGN SPECIFICATIONS FOR OVERHEAD SIGN STRUCTURES. ALL CONSTRUCTION AND MATERIALS SHALL BE IN ACCORDANCE WITH STANDARD SPECIFICATION SECTION 644. STEEL FABRICATION SHALL BE IN ACCORDANCE WITH THE NYS STEEL CONSTRUCTION MANUAL.

THE WEB MEMBERS IN EACH FACE OF THE TRUSS SHALL FORM CONTINUOUS TRUSSING FROM POST TO POST AND THERE SHALL BE AN EVEN NUMBER OF PANELS IN THE SPAN.

FOR VMS PANEL ATTACHMENT DETAILS AND SPACING OF VERTICAL HANGERS, SEE DWGS. ST-03 THRU ST-05.

INSTALL WIRE INLETS IN TOP CHORD (FRONT AND/OR BACK, AS NECESSARY) AT $\frac{1}{2}$ OF EACH SIGN AND AT FACE OF POST.

THE CONTRACTOR SHALL ESTABLISH THE TOP OF EXISTING BARRIER ELEVATIONS BASED ON THE INFORMATION SHOWN ON THE CONTRACT PLANS AND DATA DERIVED FROM FIELD SURVEYS. SPAN LENGTHS AND DIMENSIONS XL AND XR SHALL BE FIELD-VERIFIED PRIOR TO THE FABRICATION OF THE SIGN STRUCTURES. ELEVATIONS AND DIMENSIONS SHALL BE PRESENTED ON THE SHOP DRAWINGS. A NEGATIVE XL OR XR INDICATES THAT THE BOTTOM OF BASE PLATE IS ABOVE THE POINT OF MINIMUM VERTICAL CLEARANCE. IF FIELD-VERIFIED SPAN LENGTHS, XL OR XR DIFFER FROM INFORMATION SHOWN ON THE CONTRACT PLANS, THE ENGINEER SHALL BE CONSULTED TO DETERMINE WHETHER DESIGN CHANGES ARE NEEDED.

NOMINAL POST HEIGHT IS THE LARGER OF LEFT OR RIGHT POSTS.

THE SIGN STRUCTURE HEIGHT IS DESIGNED TO PROVIDE THE ACTUAL VERTICAL CLEARANCE INDICATED BETWEEN THE WALKWAY AND ROADWAY SURFACE, INCLUDING LIGHTING OR OTHER ATTACHMENTS, AT ANY POINT OVER THE ENTIRE ROADWAY WIDTH.

FOR CHORD SPLICE DETAILS, SEE DWG. NO. ST-09.

FOR DETAILS 1-3, SEE DWG. ST-06.

FOR DETAILS 4-6, SEE DWG. ST-07.

FOR DETAILS 7-12, SEE DWG. ST-08.

FOR TYPICAL POST CAP DETAILS, SEE DWG. ST-09.

FOR POST BASE PLATES CONNECTION DETAILS, SEE DWG. ST-02.

DETAILS ON THE DRAWINGS LABELED AS "NOT TO SCALE" ARE INTENTIONALLY DRAWN NOT TO SCALE FOR VISUAL CLARITY. ALL OTHER DETAILS, FOR WHICH NO SCALE IS SHOWN, ARE DRAWN PROPORTIONAL AND ARE FULLY DIMENSIONED.

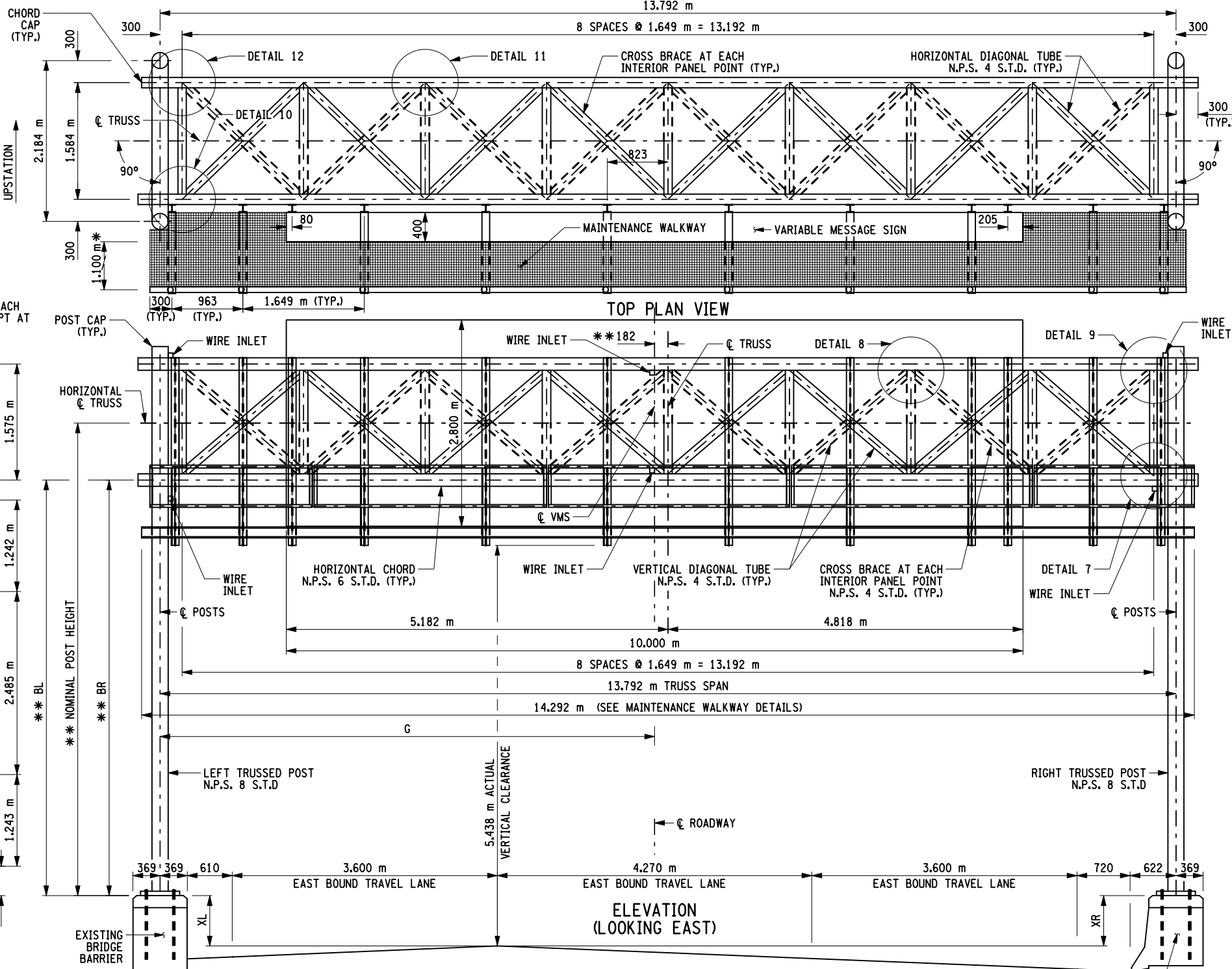
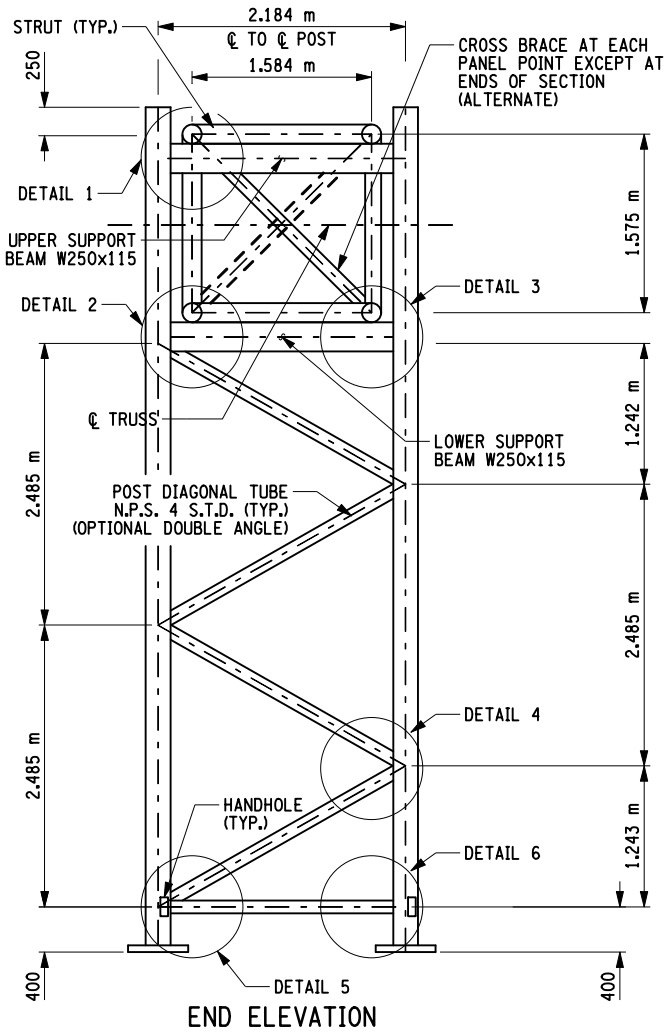
* MAINTENANCE WALKWAY NOT TO SCALE

** SIGN STRUCTURE IS ON A LONGITUDINAL AND TRANSVERSE GRADE. ADJUSTMENT HAS TO BE MADE TO INDIVIDUAL BL AND BR BASED ON FIELD MEASUREMENT SO THAT THE FABRICATED SIGN STRUCTURE IS LEVEL AND MEETING THE ACTUAL VERTICAL CLEARANCE.

VMS SIGN STRUCTURES

STRUCTURE TYPE	MAX. SPAN LENGTH (m)	MAX. SIGN AREA (m ²)	CHORD SPACING (m)	POST SPACING (m)
13.8-28-6.4	13.8	28	1.6	2.2

TRUSS		POST		
CHORD NPS	DIAGONAL NPS	MAX. NOMINAL POST HEIGHT (m)	POST NPS	OPTIONAL DIAGONAL ANGLE
6 STD	4 STD	6.4	8 STD	2L-102x76x9.5



VMS SIGN STRUCTURE TABLE

S.I.N.	ITEM NO.	TRUSS SPAN (m)	TOTAL VMS AREA (m ²)	NOMINAL POST HEIGHT (m)	MIN. VCL (m)	POSTS									VMS						
						LEFT					RIGHT										
						SUBSTRATE	BL (m)	XL (m)	OFF-SET (m)	FOUND. TYPE	SUBSTRATE	BR (m)	XR (m)	OFF-SET (m)	FOUND. TYPE	MIN. NO. Z-BARS (TYP.)	G (m)	MAX LENGTH (m)	MAX HEIGHT (m)	MAX THICKNESS (m)	MAX WEIGHT (kg)
N2283	644.4401	13.8	28	6.415	5.335	BARRIER	5.627	-0.683	0.979	SPECIAL	BARRIER	5.627	-0.683	1.342	SPECIAL	6	6.714	10	2.8	0.4	990



AS-BUILT REVISIONS
DESCRIPTION OF ALTERATIONS:

VARIABLE MESSAGE SIGN REPLACEMENT
ON LIE AT 23 ST
INTELLIGENT TRANSPORTATION SYSTEMS

COUNTY:

PIN x806.49

BRIDGES

CULVERTS

ALL DIMENSIONS IN mm UNLESS OTHERWISE NOTED
TRUSS PLAN AND ELEVATION
N.Y.S. LIE AT 23 ST.

CONTRACT NUMBER

D262095

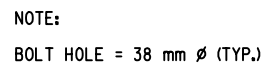
DRAWING NO. ST-01

SHEET NO. 45

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11

DOCUMENT NAME: x80649_cpb_70_pln.dgn

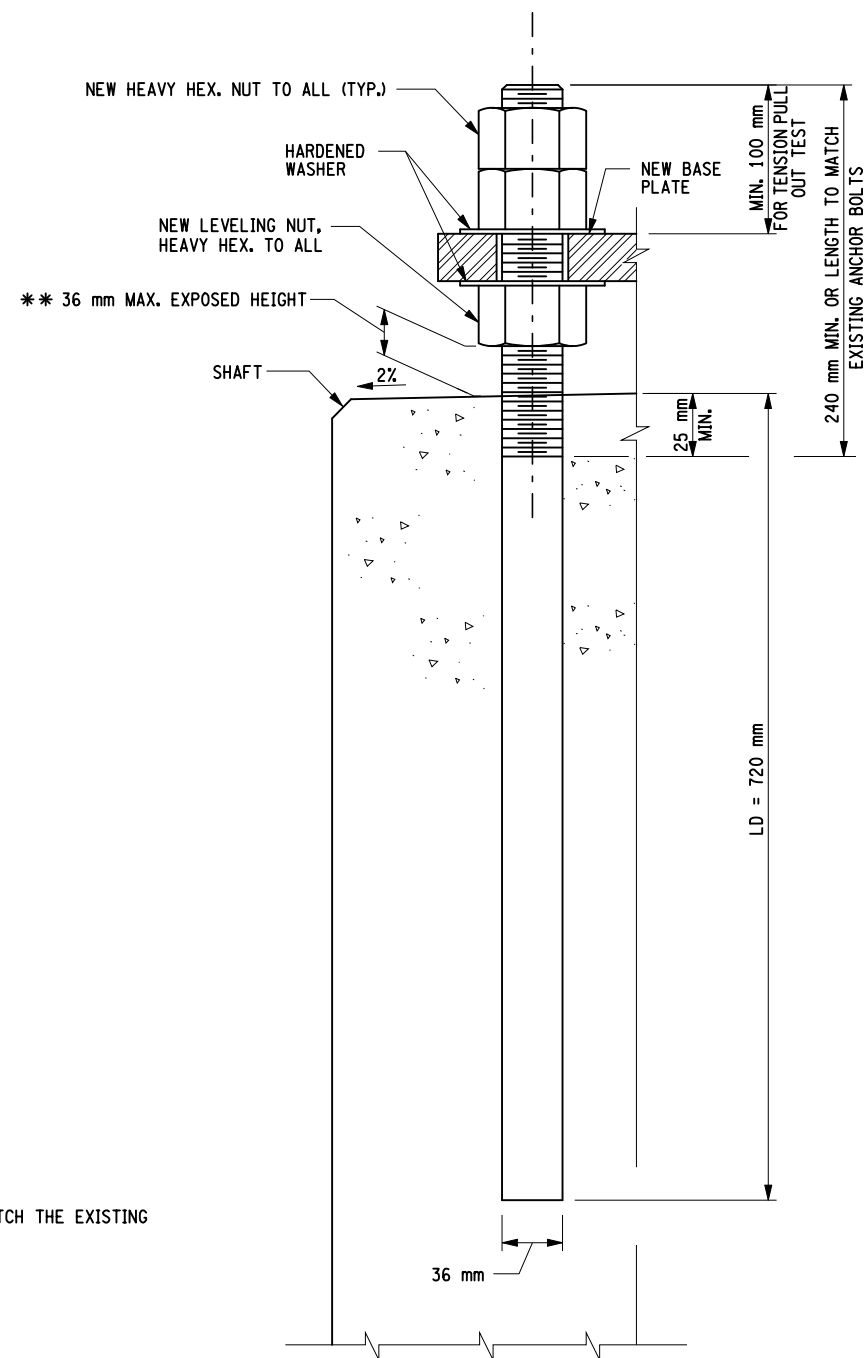


BASE PLATE DETAIL
8 BOLT ARRANGEMENT
NOT TO SCALE





- NOTES:
1. S.I.N. AND CONTRACT NO. SHALL BE STAMPED ON BASE PLATE, SEE SPECIFICATIONS.
 2. DETAILS ON DRAWINGS LABELED AS "NOT TO SCALE" ARE INTENTIONALLY DRAWN NOT TO SCALE FOR VISUAL CLARITY. ALL OTHER DETAILS, FOR WHICH NO SCALE IS SHOWN, ARE DRAWN PROPORTIONAL AND ARE FULLY DIMENSIONED.
 3. REMOVE EXISTING CONCRETE GROUT UNDER EXISTING BASE PLATE.
 4. NEW BASE PLATE/NEW WASHER/NEW NUT SHALL BE USED (PAID FOR UNDER ITEM 644.11).
 5. THE EMBEDMENT DEPTH SHOWN ON THE PLAN FOR ITEM 586.03 IS FOR ESTIMATING PURPOSES ONLY. THE ACTUAL EMBEDMENT DEPTH FOR ITEM 586.03 SHALL BE CALCULATED BASED ON THE SIZE OF THE ANCHOR BOLTS, ACTUAL EDGE DISTANCE OF THE BOLTS, THE SPACING OF THE BOLTS, AND THE CHOSEN GROUT MANUFACTURER'S RECOMMENDATIONS. EMBEDMENT DEPTH CALCULATIONS SHALL BE SUBMITTED TO E.I.C. FOR APPROVAL. PULL OUT TEST IS REQUIRED ON ITEM 586.03.
 6. NEW ANCHOR BOLTS SHALL BE ASTM A615M GR420 STEEL.

- * DIMENSIONS SHOULD BE FIELD VERIFIED TO MATCH THE EXISTING ANCHOR BOLT LAYOUT
- ** FIELD VERIFICATION TO PROPOSED POST HEIGHT TO MEET THIS TOLERANCE



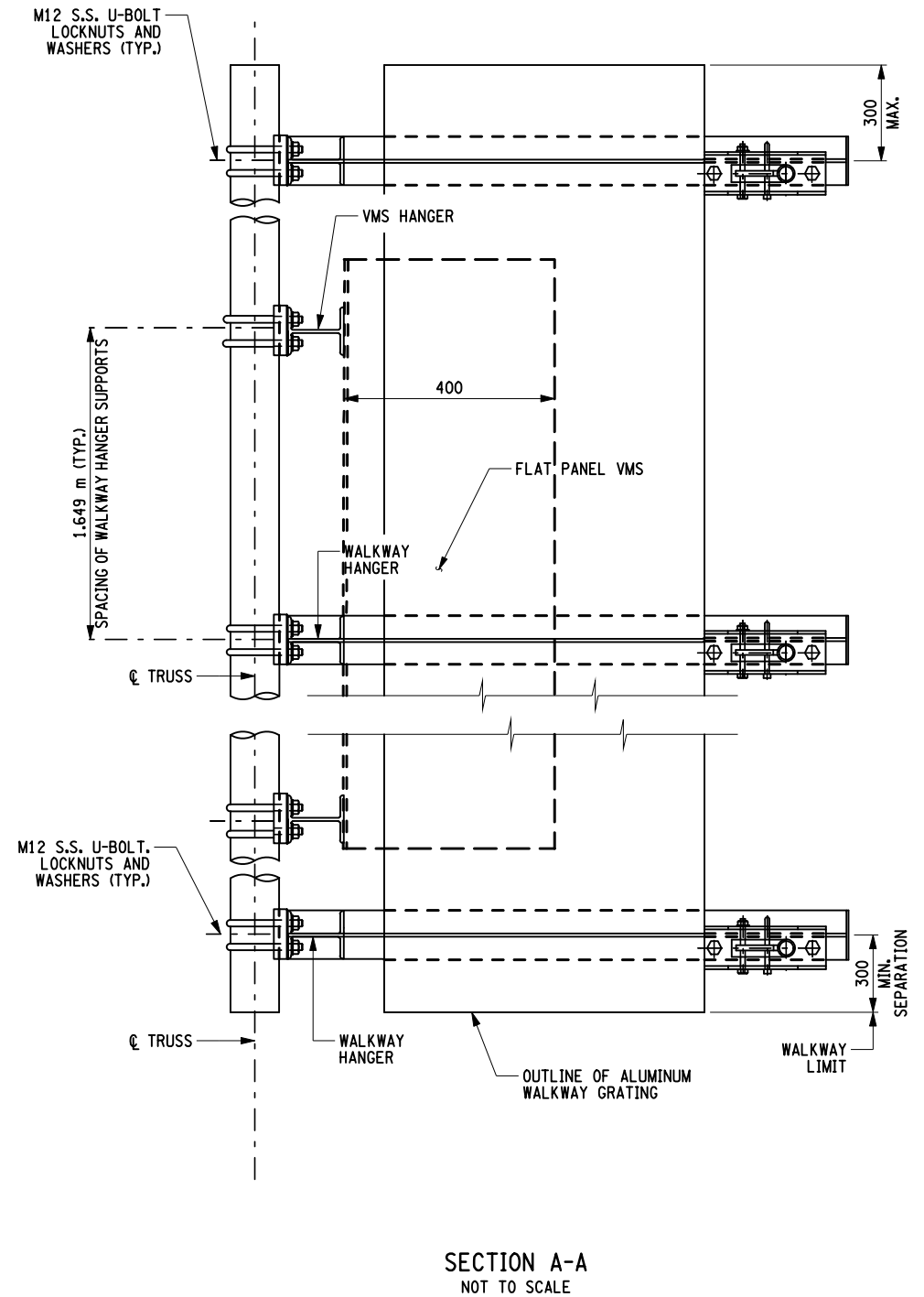
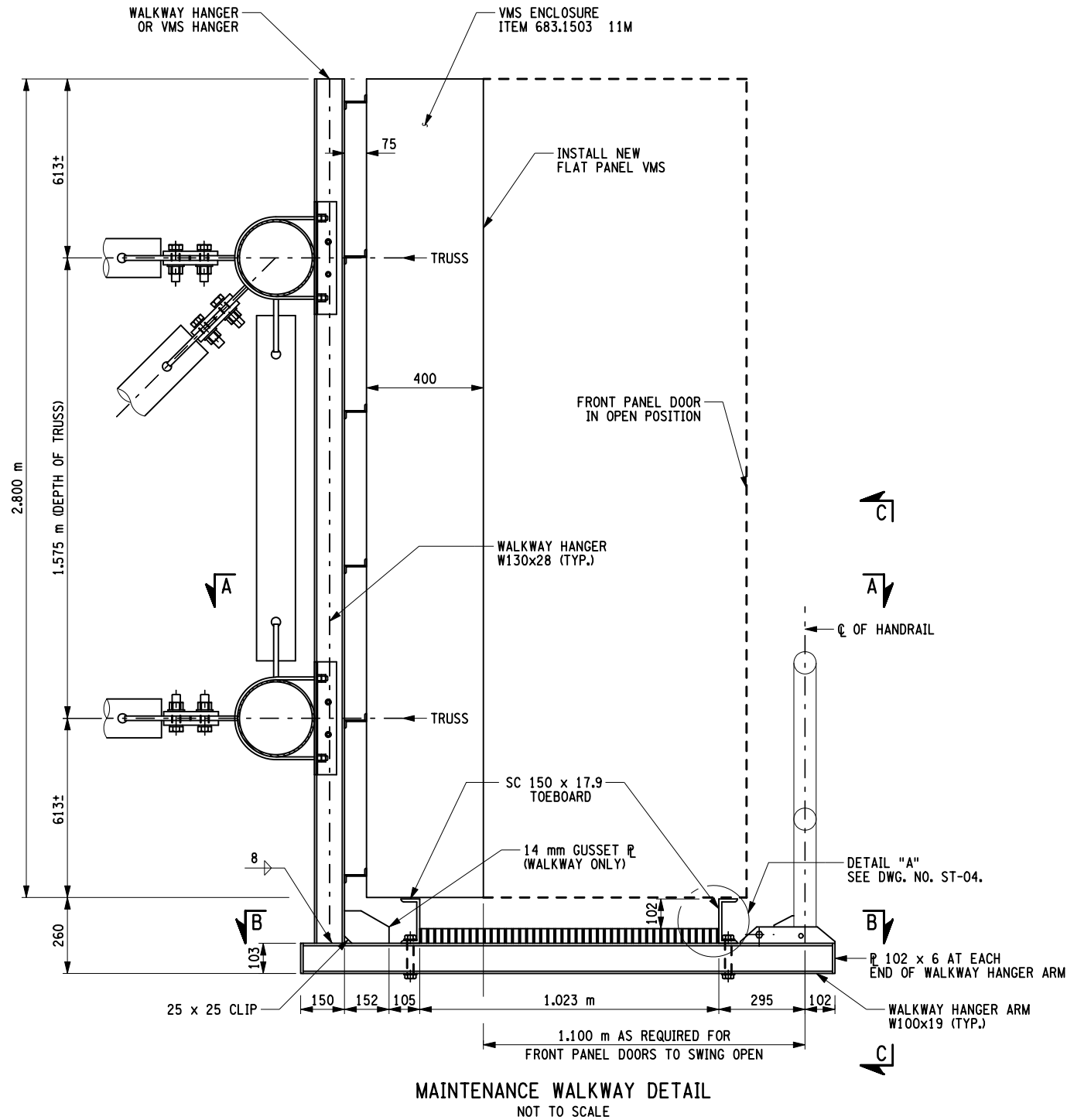
DRILL AND GROUT ANCHOR BOLT DETAIL
(ITEM 586.03)
NOT TO SCALE

AFFIX SEAL: ON:	ALTERED BY: ON:
	

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	VARIABLE MESSAGE SIGN REPLACEMENT	PIN x806.49	BRIDGES	CULVERTS	ALL DIMENSIONS IN mm UNLESS OTHERWISE NOTED	CONTRACT NUMBER		
	ON LIE AT 23 ST				BASE PLATE CONNECTION DETAILS	D262095		
	INTELLIGENT TRANSPORTATION SYSTEMS					DRAWING NO. ST-02		
								SHEET NO. 46
	COUNTY:							
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.							NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11	
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USER = douran

DESIGN SUPERVISOR B. CRUDELE
JOB MANAGER J. KOLODZINSKI
DESIGN E. TSE
CHECK J. KOLODZINSKI
DRAFTING R. DRAISS
CHECK E. TSE
PROJECT MANAGER RAJENDRA AMIN



NOTE:
1. SEE DWG. NO. ST-04 FOR NOTES.



AS-BUILT REVISIONS
DESCRIPTION OF ALTERATIONS:

VARIABLE MESSAGE SIGN REPLACEMENT
ON LIE AT 23 ST
INTELLIGENT TRANSPORTATION SYSTEMS
COUNTY:

PIN x806.49

BRIDGES

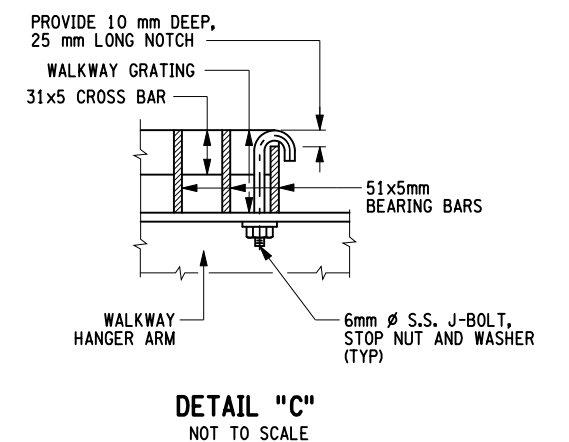
CULVERTS

ALL DIMENSIONS IN mm UNLESS OTHERWISE NOTED
VARIABLE MESSAGE SIGN DETAILS
(1 OF 3)
N.Y.S. LIE AT 23 ST.



CONTRACT NUMBER
D262095
DRAWING NO. ST-03
SHEET NO. 47

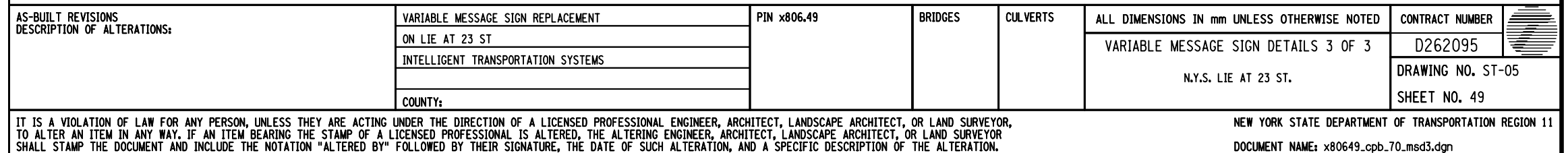
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

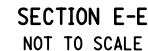
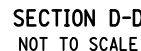
NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11
DOCUMENT NAME: x80649_cpb_70_msd.dgn



- ## NOTES:
1. ALL MATERIAL SHALL BE ALUMINUM ALLOY 6061-T6 UNLESS OTHERWISE NOTED.
 2. ALL STEEL BLOCKS, BOLTS, J-BOLTS, WASHERS AND NUTS SHALL BE STAINLESS STEEL CONFORMING TO ASTM SPECIFICATION A320/A320M GRADE B8, CLASS 1.
 3. WELDING OF ALUMINUM SHALL BE AS SPECIFIED IN THE CONSTRUCTION SPECIFICATIONS.
 4. THE THICKNESS OF THE STEEL BLOCK MAY BE MODIFIED TO AVOID INTERFERENCE AT CHORD SPLICE PLATES.
 5. SEE ELEVATION DWGS FOR HORIZONTAL LIMITS OF WALKWAY.
 6. MINIMUM SECTION MODULUS OF ALUMINUM WALKWAY GRATING SHALL BE 2150 mm³ PER METER WIDTH. WALKWAY GRATING SHALL NOT CANTILEVER MORE THAN 300 mm BEYOND HANGER ARMS AT EACH END OF THE MAIN WALKWAY. WALKWAY GRATING SHALL BE CONTINUOUS OVER A MINIMUM OF TWO SPANS.
 7. ALTERNATE PIPE FITTINGS FOR HANDRAIL MAY BE SUBMITTED TO THE ENGINEER FOR APPROVAL.
 8. ALL PIPES FOR HANDRAIL SHALL BE 38 mm Ø WITH 5 mm WALL THICKNESS.
 9. MAXIMUM LENGTH OF HANDRAIL SECTION SHALL BE 3300 mm AND SHALL INCLUDE TWO POSTS.
 10. ALTERNATE HANDRAIL HINGE DETAILS MAY BE SUBMITTED TO THE ENGINEER FOR APPROVAL.
 11. WALKWAY HANGERS SHALL PROJECT 75 mm ABOVE THE TOP OF THE TOP CHORD.
 12. MAINTENANCE WALKWAY, RAILING AND SUPPORTS SHALL BE PROVIDED CONTINUOUSLY FOR THE LENGTH SHOWN ON THE PLAN VIEWS.
 13. VERTICAL I-BEAM (DEPTH = 131 mm, WEB AND FLANGE THICKNESS = 6.86 mm AND 10.9 mm RESPECTIVELY, FLANGE WIDTH = 128 mm).
 14. PAID FOR AS MISCELLANEOUS METALS - ITEM 656.01.

AFFIX SEAL: ON:		ALTERED BY: ON:		SECTION B-B NOT TO SCALE													
		AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:		VARIABLE MESSAGE SIGN REPLACEMENT		PIN x806.49		BRIDGES		CULVERTS		ALL DIMENSIONS IN mm UNLESS OTHERWISE NOTED		CONTRACT NUMBER			
				ON LIE AT 23 ST								VARIABLE MESSAGE SIGN DETAILS 2 OF 3		D262095			
				INTELLIGENT TRANSPORTATION SYSTEMS								N.Y.S. LIE AT 23 ST.		DRAWING NO. ST-04			
				COUNTY:													
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.																	
NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11 DOCUMENT NAME: x80649_cpb_70_msd2.dgn																	



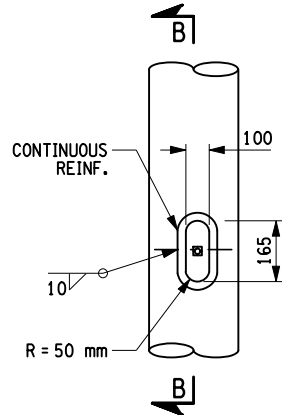


1. SPLICE PLATES SHALL BE 8 mm THICK AT TOP BEAM CONNECTIONS, 22 mm THICK AT LOWER BEAM CONNECTIONS; HOLES IN WEB PLATES AT TOP BEAM AND HOLES IN LOWER BEAM CONNECTION SHALL BE STANDARD. HOLES IN TOP BEAM SPLICE PLATES MAY BE OVERSIZE.
2. SEE DWG. NO. ST-01 FOR LOCATION OF DETAILS 1, 2 & 3.
3. SADDLE ASSEMBLIES SHALL BE SHIMMED AS REQUIRED TO PROVIDE LEVEL SEAT FOR CHORD WHEN ALL DEAD LOADS ARE APPLIED. VARIABLE THICKNESS SHIM PACKS SHALL BE PROVIDED. ALL SHIMS SHALL BE GALVANIZED STEEL.
4. ALL BOLTS SHALL BE A325 HIGH STRENGTH 22 mm IN STANDARD HOLES, UNLESS OTHERWISE NOTED.
5. GUSSET AND SPLIT TUBE TAB PLATES SHALL BE 13 mm THICK UNLESS OTHERWISE NOTED.
6. TOP SUPPORT BEAM SHALL BE INSTALLED AFTER TRUSS HAS BEEN ERRECTED AND LEVELED.
7. DETAILS ON DRAWINGS LABELED AS "NOT TO SCALE" ARE INTENTIONALLY DRAWN NOT TO SCALE FOR VISUAL CLARITY. ALL OTHER DETAILS, FOR WHICH NO SCALE IS SHOWN, ARE DRAWN PROPORTIONAL AND ARE FULLY DIMENSIONED.

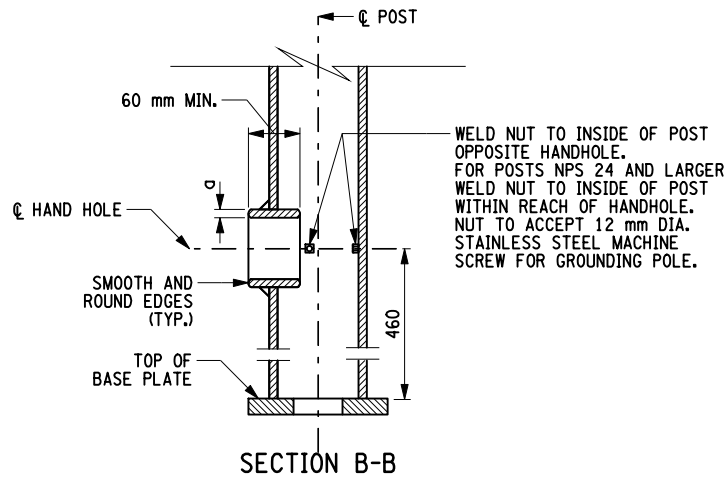
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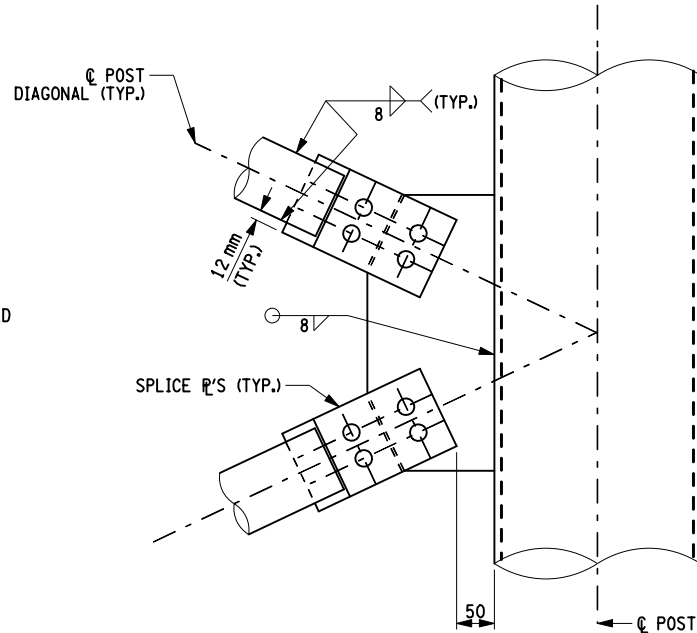
PROJECT MANAGER RAJENDRA AMIN
CHECK E. TSE
DRAFTING R. DRAISS
CHECK J. KOLODZINSKI
DESIGN E. TSE
JOB MANAGER J. KOLODZINSKI
DESIGN SUPERVISOR B. CRUDELE



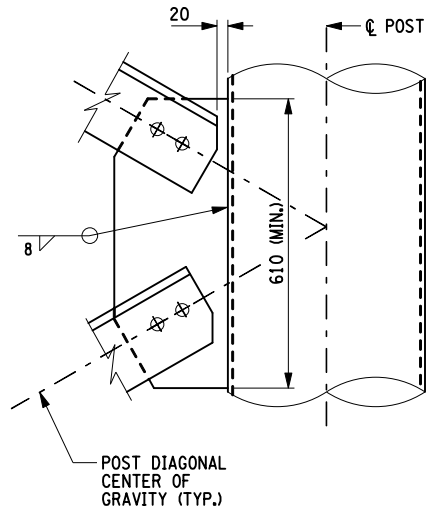
DETAILS OF THE COVER TO BE DETERMINED BY THE FABRICATOR TO FIT TIGHTLY, EXCLUDE WATER, AND BE REMOVABLE WITH A WRENCH. ALL FASTENERS, NUTS, AND WASHERS SHALL BE STAINLESS STEEL,
COVER



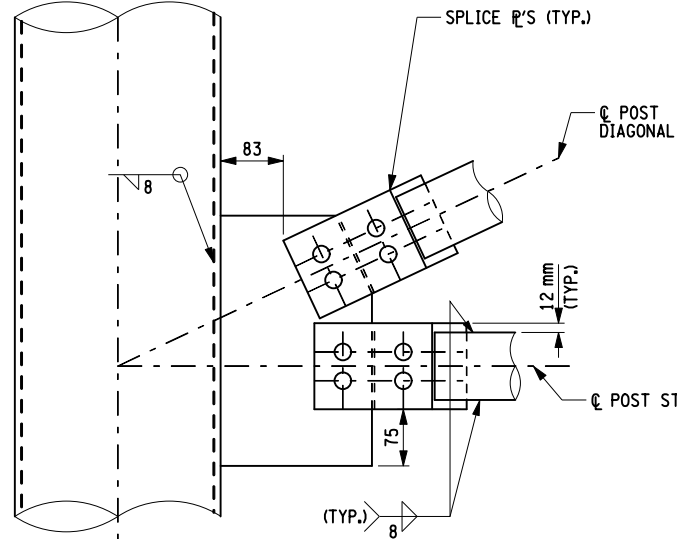
POST HANDHOLE DETAIL
NOT TO SCALE



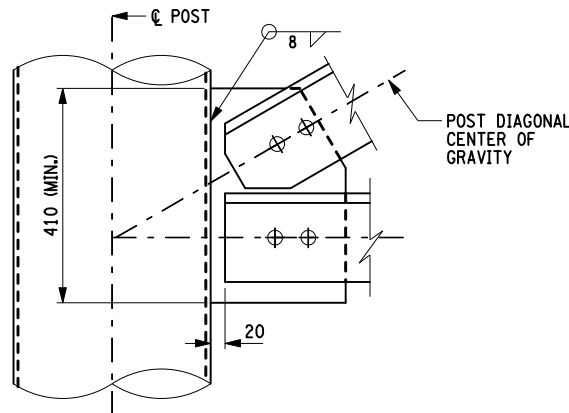
DETAIL 4
NOT TO SCALE



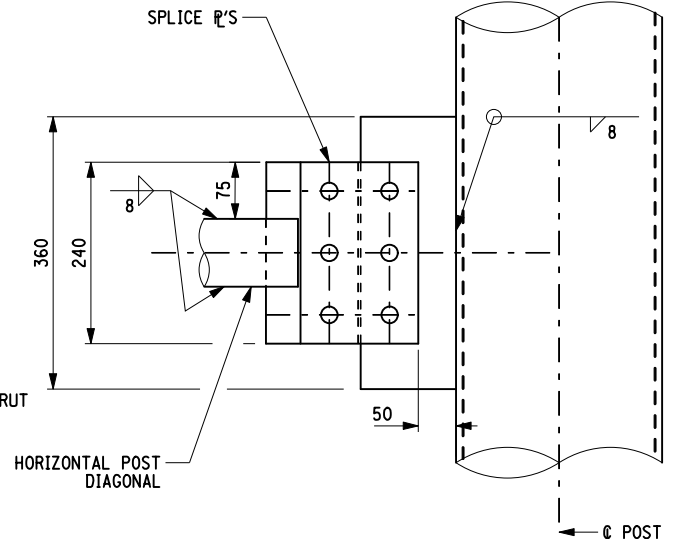
OPTIONAL DOUBLE ANGLE CONNECTION
(FOR DETAIL 4)
NOT TO SCALE



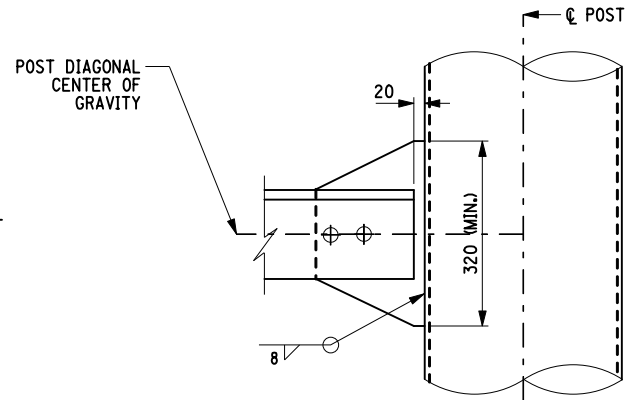
DETAIL 5
NOT TO SCALE



OPTIONAL DOUBLE ANGLE CONNECTION
(FOR DETAIL 5)
NOT TO SCALE



DETAIL 6
NOT TO SCALE



OPTIONAL DOUBLE ANGLE CONNECTION
(FOR DETAIL 6)
NOT TO SCALE

NOTES:

SEE DWG. NO. ST-01 FOR LOCATION OF DETAILS 4 THRU 6.

PLACE HANDHOLES OPPOSITE TRAFFIC.

ALL BOLTS SHALL BE HIGH STRENGTH M22 IN STANDARD HOLES, UNLESS OTHERWISE NOTED.

GUSSET AND SPLIT TUBE TAB PLATES SHALL BE 12 mm THICK UNLESS OTHERWISE NOTED.

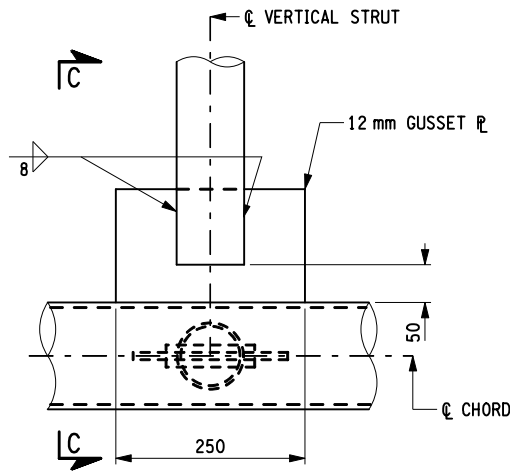
DETAILS ON DRAWINGS LABELED AS "NOT TO SCALE" ARE INTENTIONALLY DRAWN NOT TO SCALE FOR VISUAL CLARITY. ALL OTHER DETAILS, FOR WHICH NO SCALE IS SHOWN, ARE DRAWN PROPORTIONAL AND ARE FULLY DIMENSIONED.

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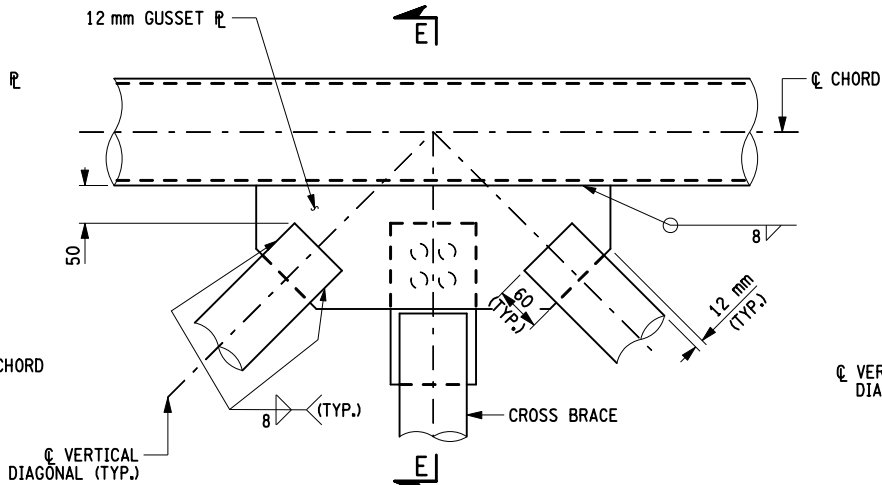
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	ON LIE AT 23 ST					D262095
	INTELLIGENT TRANSPORTATION SYSTEMS					DRAWING NO. ST-07
	COUNTY:					SHEET NO. 51
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.						NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11 DOCUMENT NAME: x080812.cpb_70_dtl_post2.dgn

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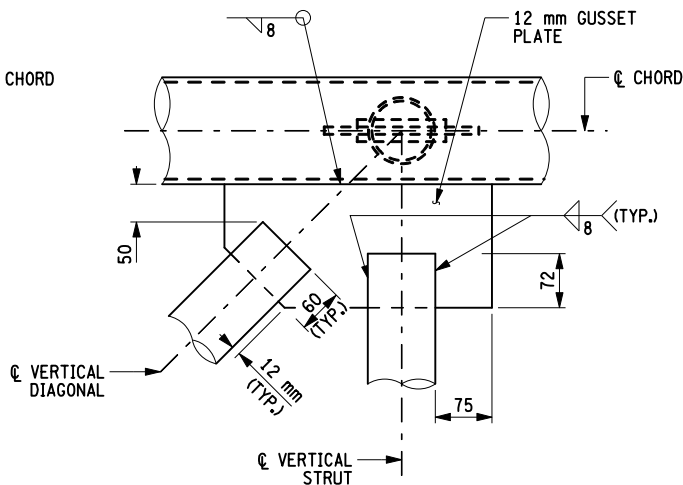
PROJECT MANAGER RAJENDRA AMIN
CHECK E. TSE
DRAFTING R. DRAISS
CHECK J. KOLODZINSKI
DESIGN E. TSE
JOB MANAGER J. KOLODZINSKI
DESIGN SUPERVISOR B. CRUDELE



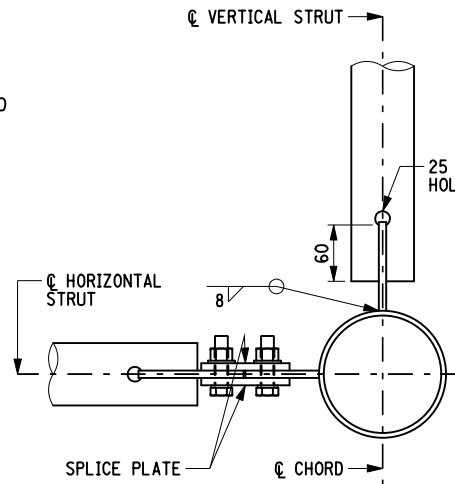
DETAIL 7
NOT TO SCALE



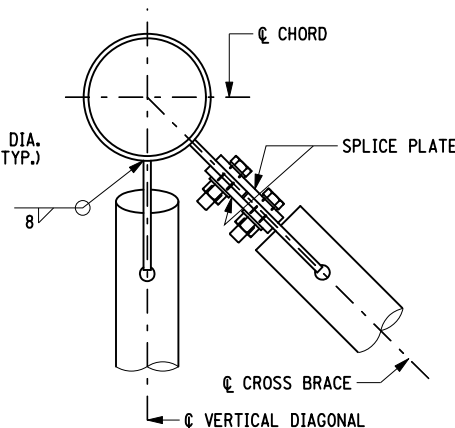
DETAIL 8
NOT TO SCALE



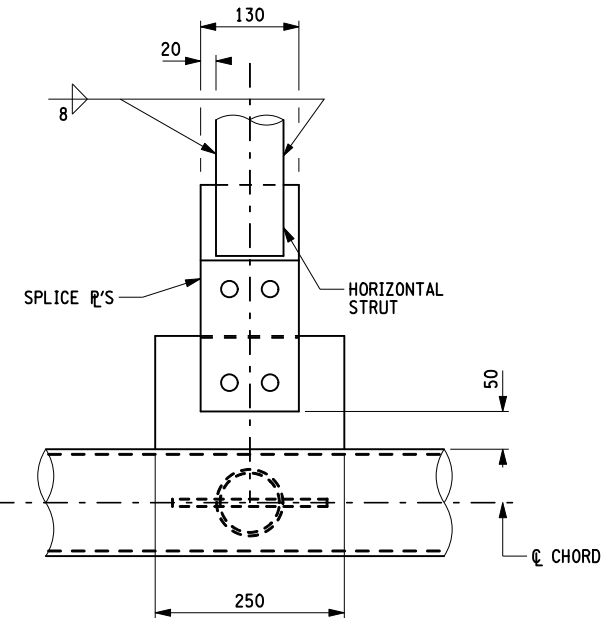
DETAIL 9
NOT TO SCALE



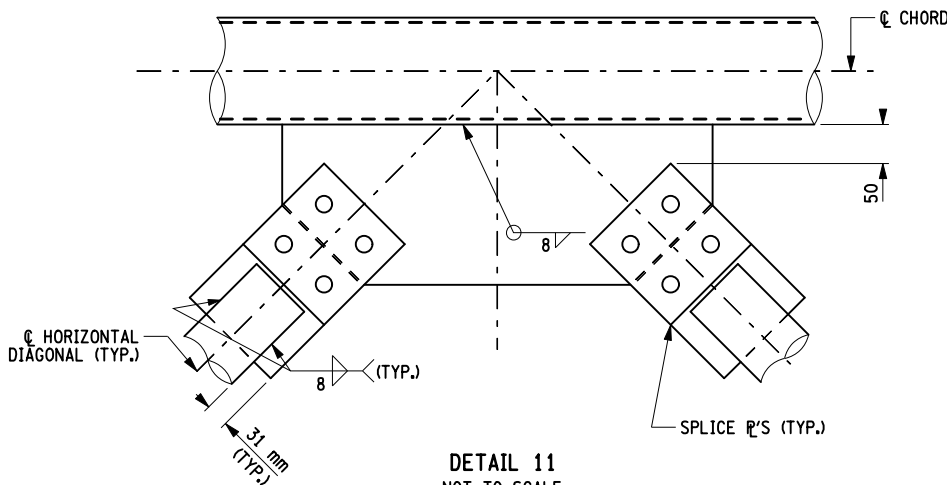
SECTION C-C
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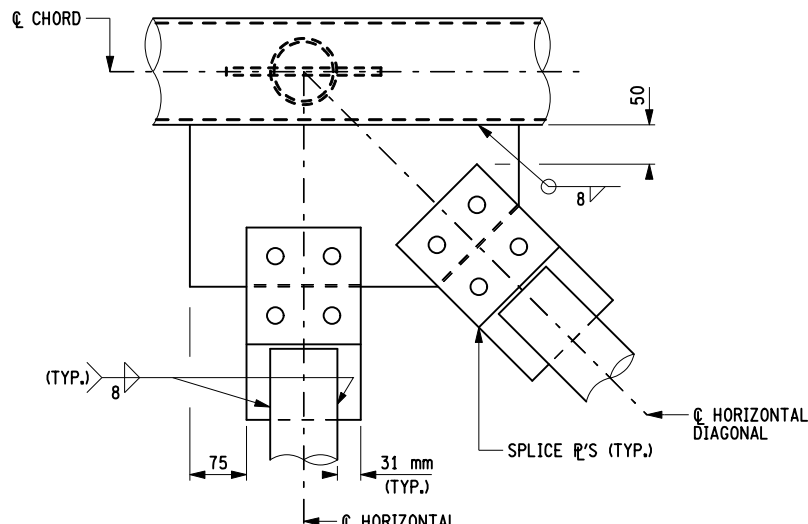
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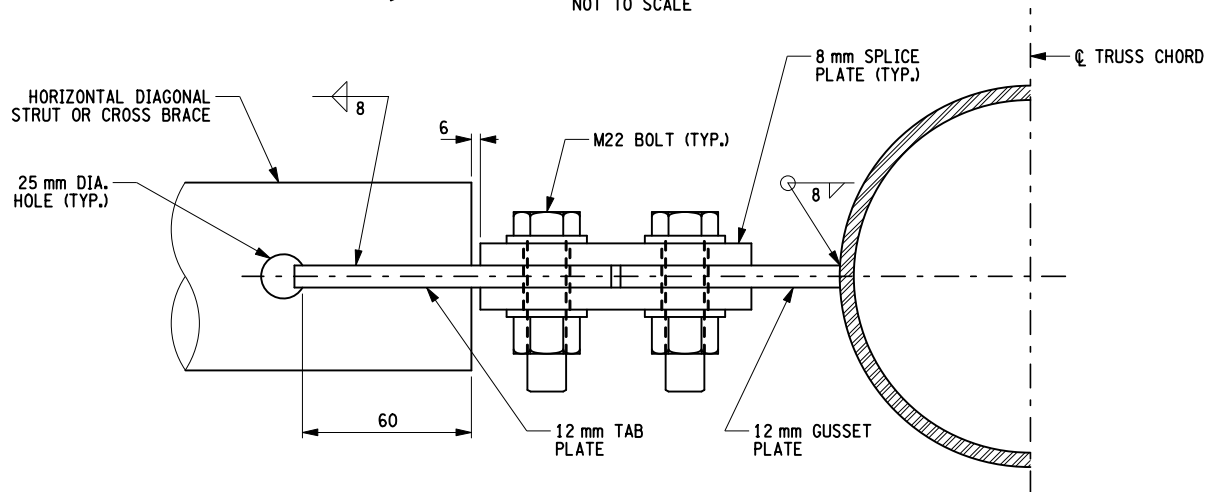
DETAIL 10
NOT TO SCALE



DETAIL 11
NOT TO SCALE



DETAIL 12
NOT TO SCALE




SPLIT TUBE BOLTED CONNECTION DETAIL
(TYPICAL FOR ALL BOLTED TRUSS CONNECTIONS)
NOT TO SCALE

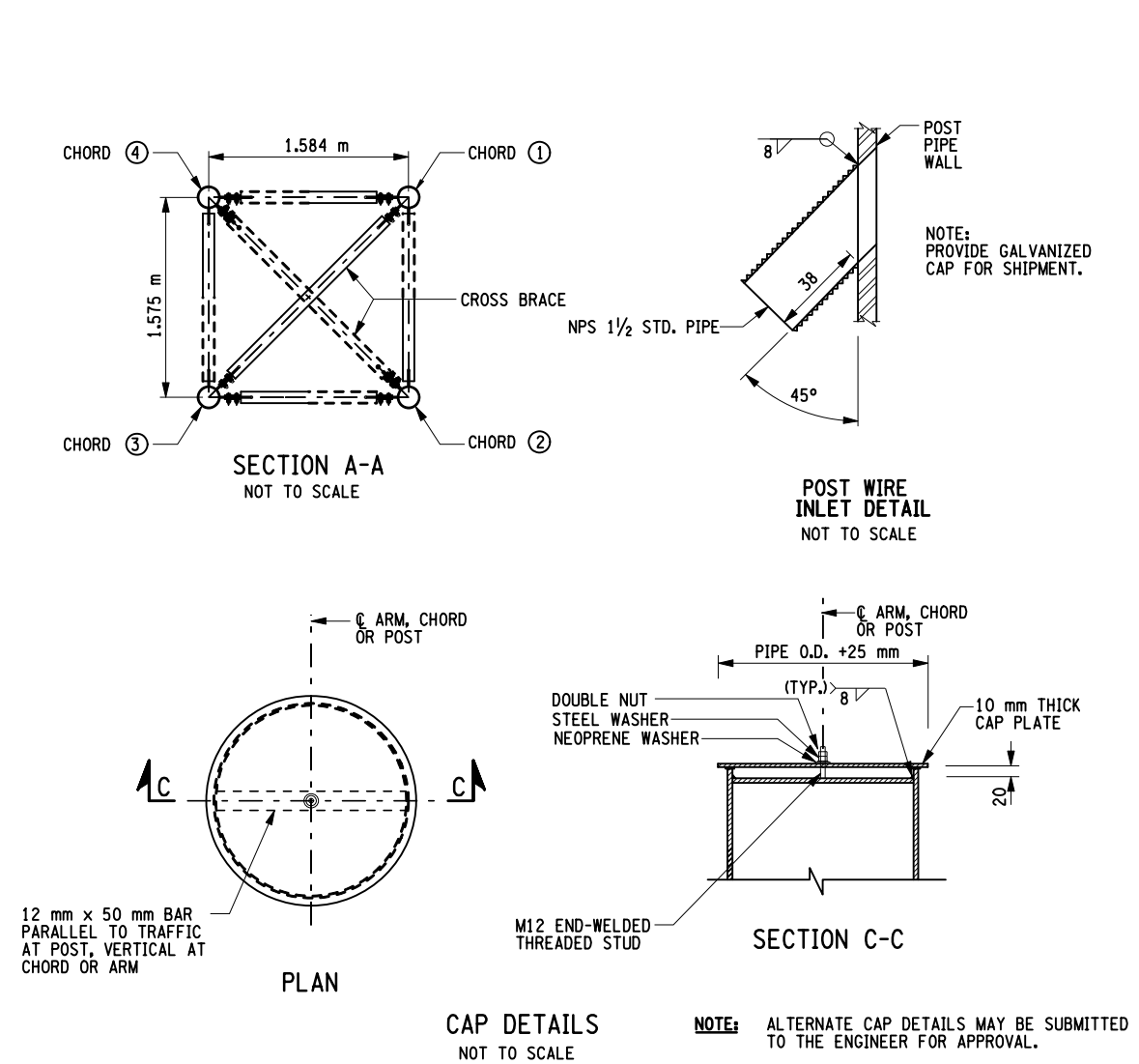
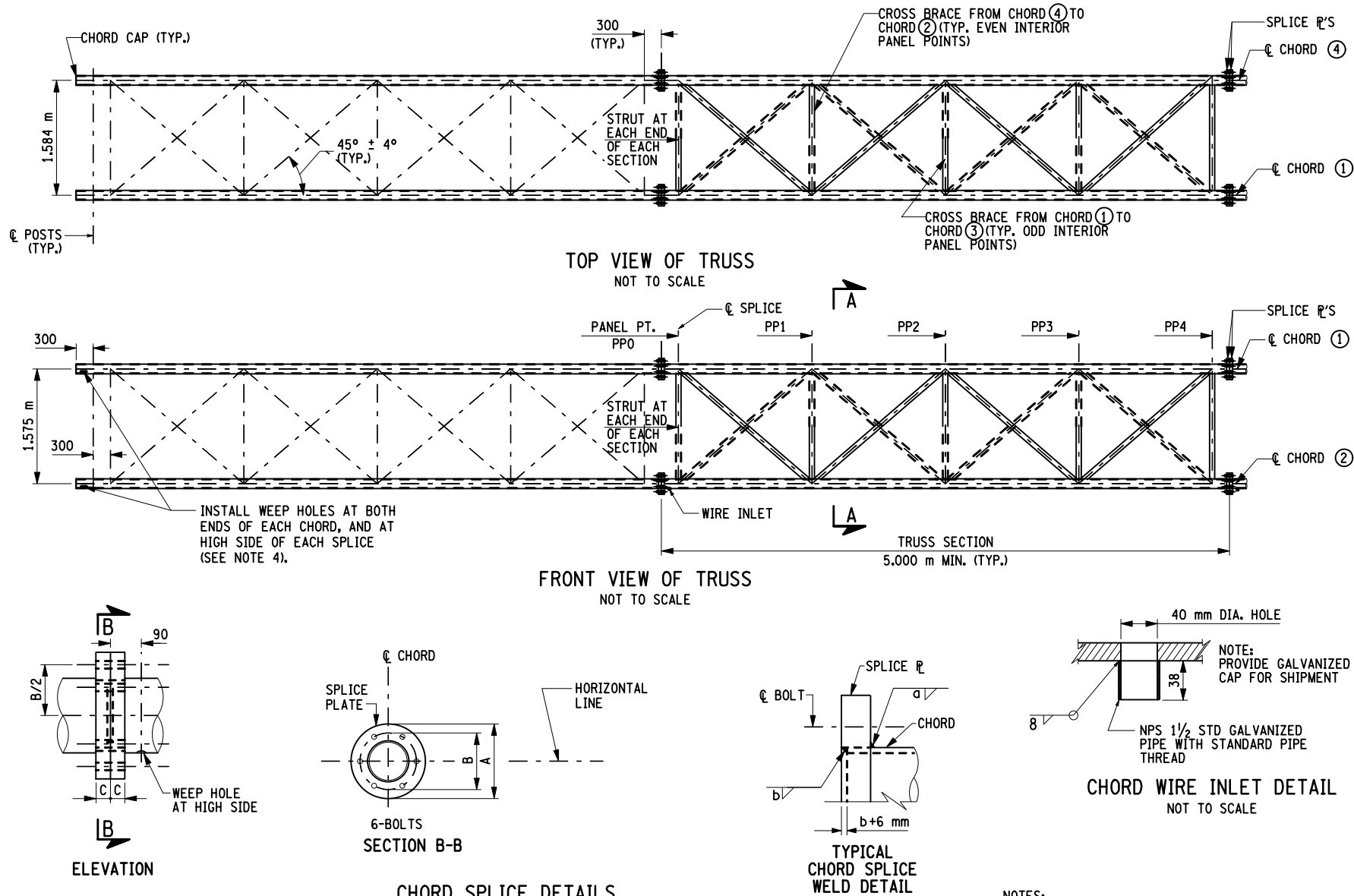
NOTES:

1. ALL BOLTED CONNECTIONS SHALL BE MADE WITH M22 HIGH STRENGTH BOLTS FULLY TIGHTENED. HOLES IN GUSSET AND TAB PLATES SHALL BE STANDARD. HOLES IN BOLTED CONNECTION SPLICE PLATES OR DOUBLE ANGLES MAY BE OVERSIZE.
2. FOR LOCATIONS OF DETAILS, SEE DWG. NO. ST-01.
3. ALL VERTICAL TRUSS CONNECTIONS SHALL BE WELDED. DOUBLE ANGLES MAY NOT BE SUBSTITUTED FOR VERTICAL TRUSS MEMBERS.
4. FULLY WELDED SPLIT TUBES MAY BE USED IN LIEU OF THE BOLTED CONNECTIONS SHOWN IN DETAILS 10, 11 AND 12, IF GALVANIZING TANKS PERMIT.
5. DETAIL 10 MAY BE SUBSTITUTED FOR DETAIL 7 AT THE FABRICATOR'S DISCRETION.

DETAILS ON DRAWINGS LABELED AS "NOT TO SCALE" ARE INTENTIONALLY DRAWN NOT TO SCALE FOR VISUAL CLARITY. ALL OTHER DETAILS, FOR WHICH NO SCALE IS SHOWN, ARE DRAWN PROPORTIONAL AND ARE FULLY DIMENSIONED.

AFFIX SEAL: ON:	ALTERED BY: ON:

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	VARIABLE MESSAGE SIGN REPLACEMENT	PIN x806.49	BRIDGES	CULVERTS	ALL DIMENSIONS IN mm UNLESS OTHERWISE NOTED	CONTRACT NUMBER	
	ON LIE AT 23 ST					D262095	
	INTELLIGENT TRANSPORTATION SYSTEMS						
	COUNTY:						
					TRUSS CONNECTION DETAILS		
					N.Y.S. LIE AT 23 ST.		
						DRAWING NO. ST-08	
						SHEET NO. 52	
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.							
NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11							
DOCUMENT NAME: x80649_cpb_70_dtl_connections.dgn							



AFFIX SEAL: ON:

ALTERED BY: ON:

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	VARIABLE MESSAGE SIGN REPLACEMENT	PIN x806.49	BRIDGES	CULVERTS	ALL DIMENSIONS IN mm UNLESS OTHERWISE NOTED	CONTRACT NUMBER	
	ON LIE AT 23 ST					D262095	
	INTELLIGENT TRANSPORTATION SYSTEMS					DRAWING NO. ST-09	
	COUNTY:					SHEET NO. 53	
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.							
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DESIGN SUPERVISOR RANDOLPH HUNT

JOB MANAGER MAREK CWIOK

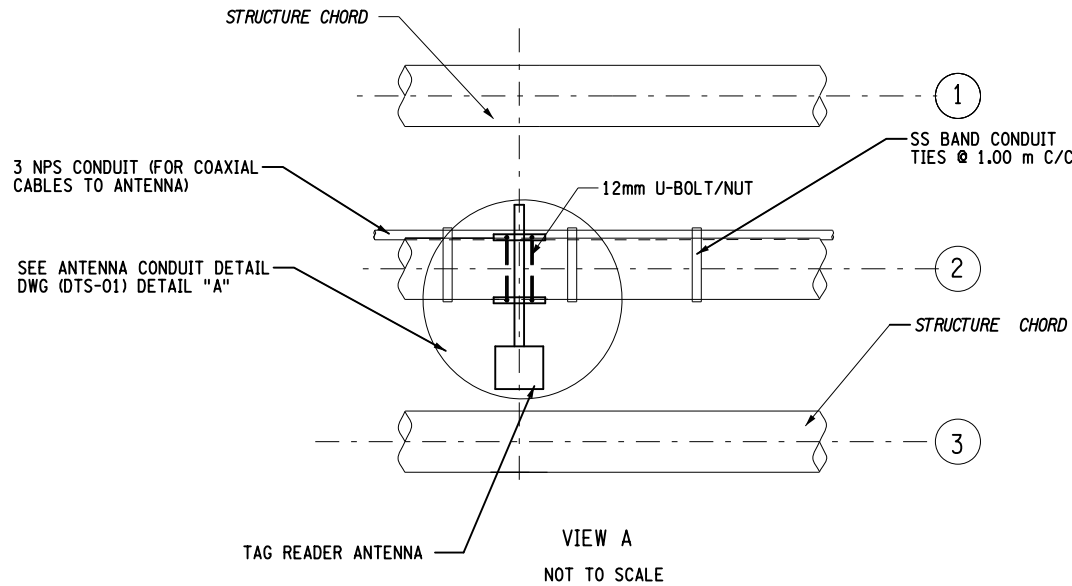
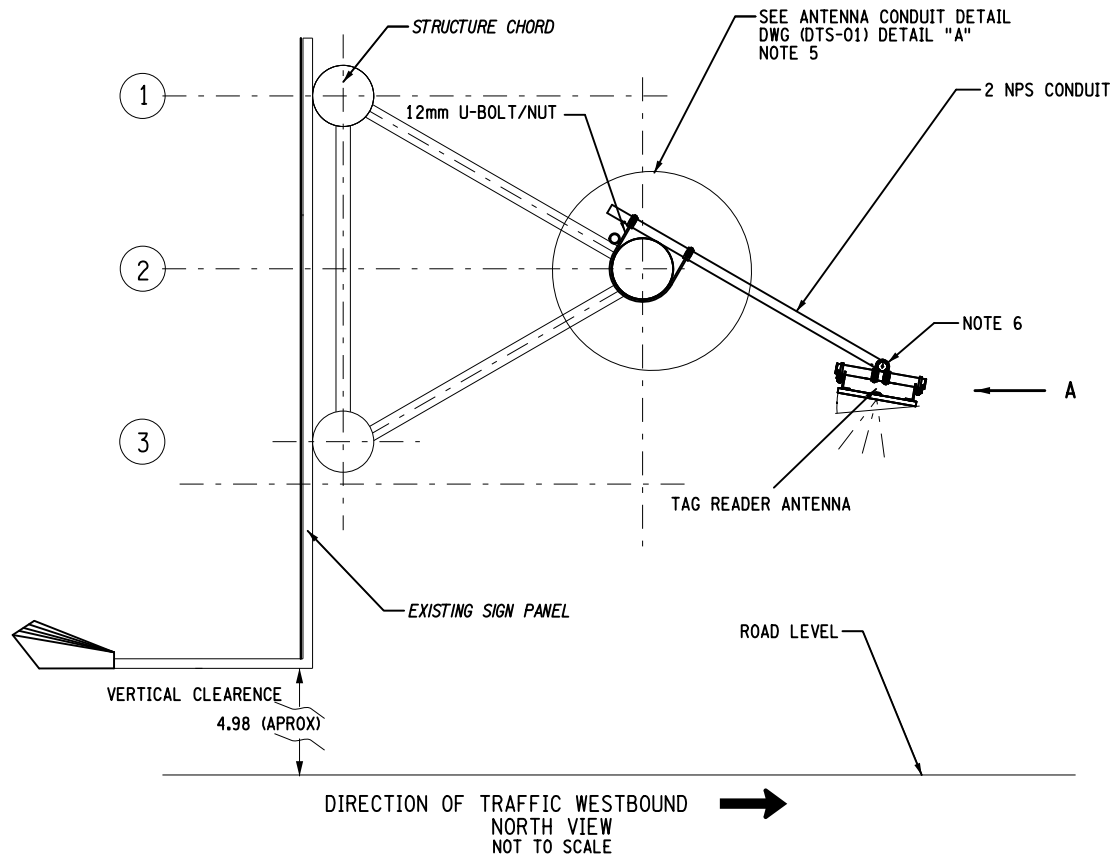
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CHECK MAREK CWIOK

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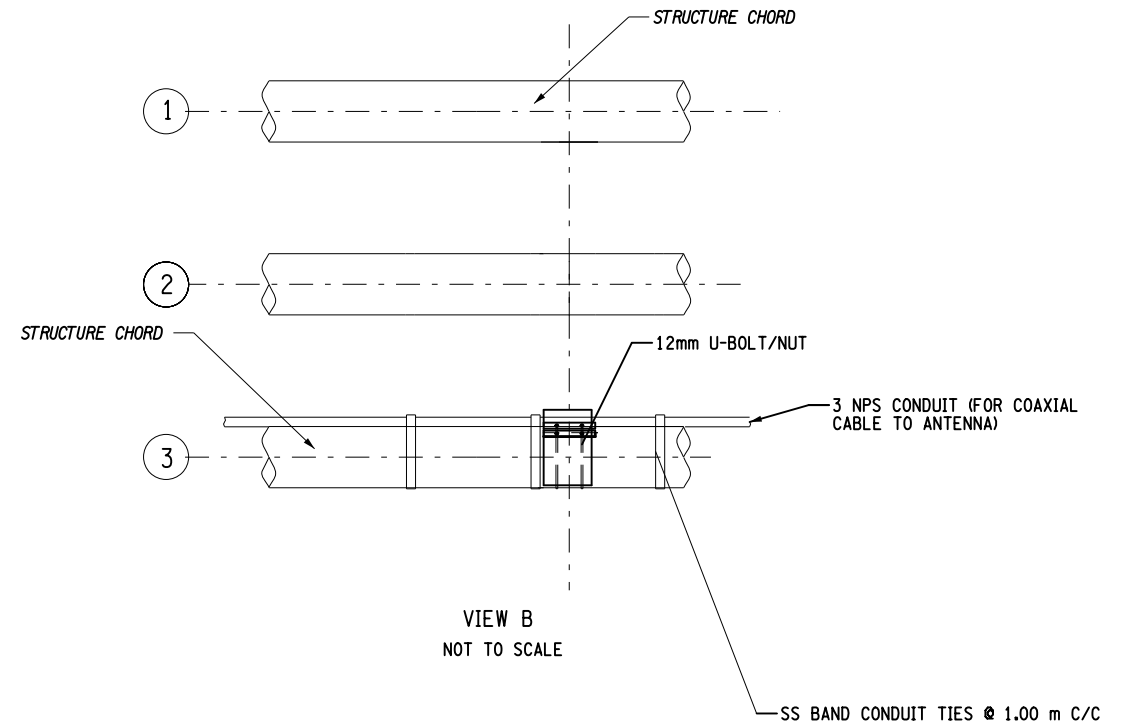
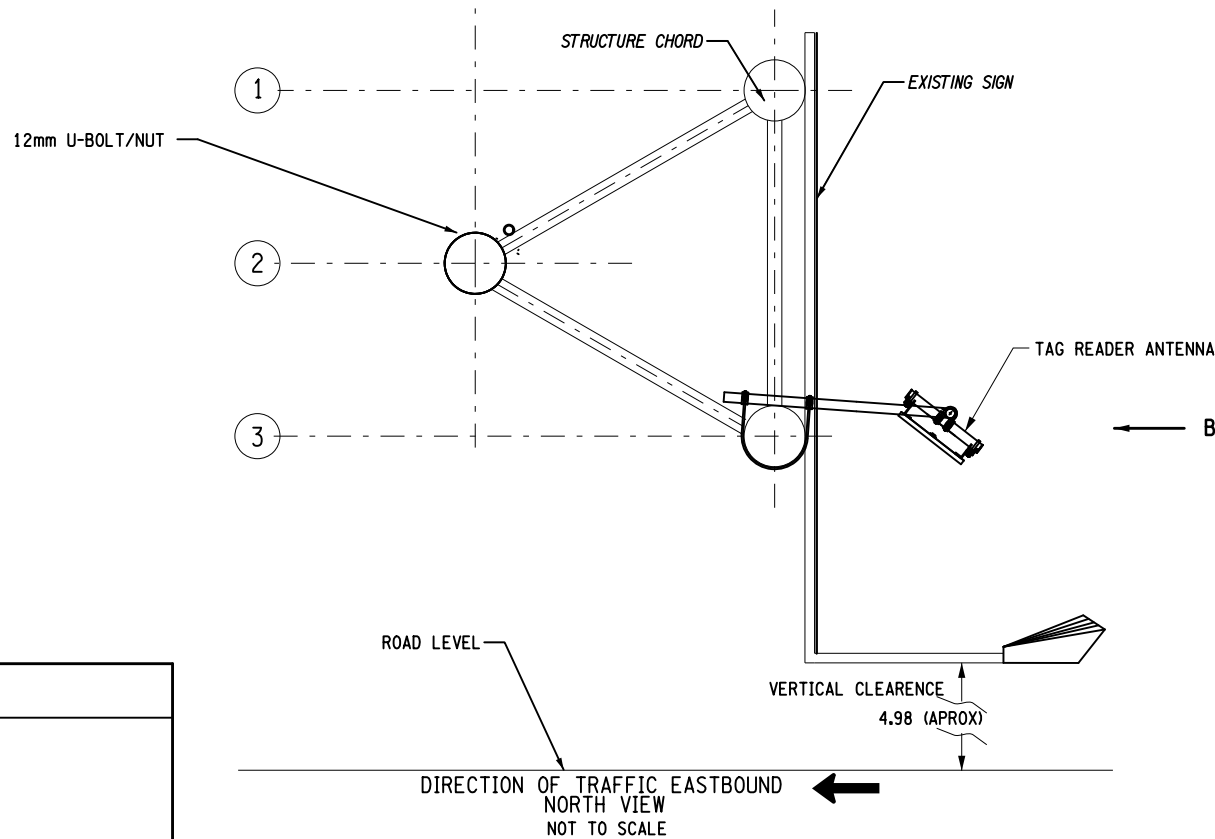
CHECK MAREK CWIOK


PROJECT MANAGER RAJ AMIN



NOTES:

1. THE SUGGESTED METHOD OF TRANSMIT ANTENNA ATTACHMENT SHOWN ON THIS DRAWING IS PROVIDED FOR INFORMATION ONLY. CONTRACTOR SHALL SUBMIT SHOP DRAWINGS FOR APPROVAL OF THE MEANS AND METHOD OF ATTACHMENT REQUIREMENTS FOR MOUNTING ANTENNA.
2. ALL FASTENER HARDWARE SHALL BE STAINLESS STEEL AND CONFIRM TO SUB SELECTION 715-16 OF THE STANDARD SPECIFICATIONS. A LOCK WASHER OR APPROVED STOP NUT SHALL BE USED UNDER EACH NUT. A FLAT WASHER SHALL BE USED UNDER EACH HEAD WITH SLOTTED HOLES AND A FLAT WASHER SHALL BE USED UNDER LOCKWASHERS.
3. STEEL BRACKETS ANGLES AND MISCELLANEOUS BRACKETS SHALL MEET THE REQUIREMENTS OF ASTM A36M AND BE HOT DIPPED GALVANIZED.
4. WIRING HOLE AS PER NYSDOT STANDARD SHEET M680-2.
5. U-BOLT AND Z-BAR ARE PART OF ANTENNA ASSEMBLY.
6. ALL STEEL SURFACES SHALL BE PREVENTED FROM COMING IN DIRECT CONTACT WITH THE ALUMINIUM BRACKETS BY MEANS OF EITHER AN APPROVED MASTIC OR 3mm THICK PAD PLACED BETWEEN THE DISSIMILAR METALS AS PER STANDARD SPECIFICATION 646-2.06
7. SECONDARY TRUSS ELEMENTS ARE NOT SHOWN FOR CLARITY
8. ALL PIPE ENDS SHOULD BE CLOSED BY WATER PROOF CAP (TYPICAL)



AFFIX SEAL: ON:	ALTERED BY: ON:
	

AS-BUILT REVISIONS
DESCRIPTION OF ALTERATIONS:

ITS TRAVEL TIME I-278 AND I-495

PS&E DATE: 06/21/2012

COUNTY: KINGS AND QUEEN

PIN X806.49

BRIDGES

CULVERTS

ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED
TRANSMIT INSTALLATION DETAILS - 1
ON TRI-CHORD SS

CONTRACT NUMBER

D262095

DRAWING NO. DTL-01

SHEET NO. 54

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11

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USER = kdesai

DESIGN SUPERVISOR RANDOLPH HUNT

JOB MANAGER MAREK CWIOK

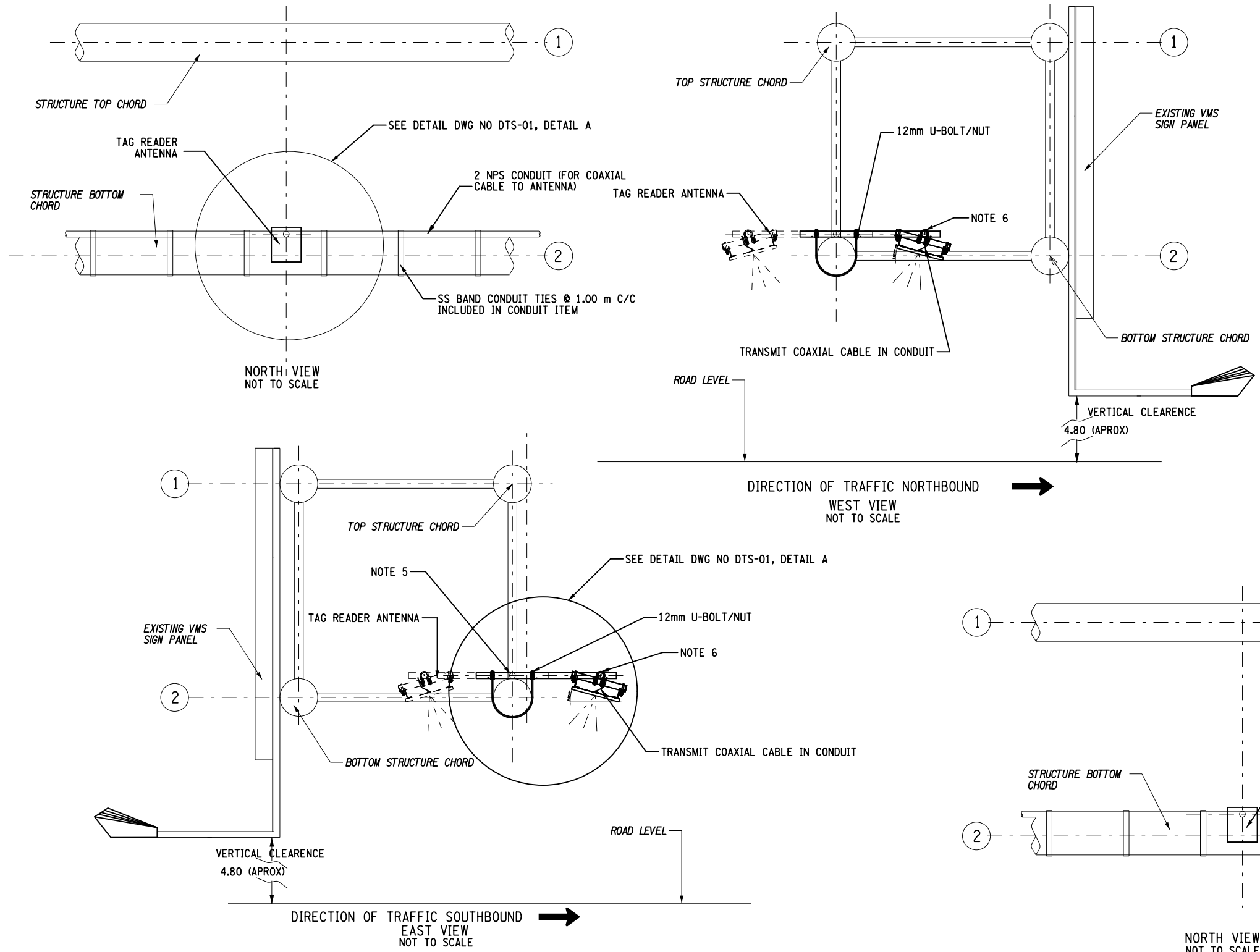
DESIGN RAISA STARTSEVA

CHECK MAREK CWIOK

DRAFTING RAISA STARTSEVA


CHECK MAREK CWIOK

PROJECT MANAGER RAJ AMIN



NOTES:

1. THE SUGGESTED METHOD OF TRANSMIT ANTENNA ATTACHMENT SHOWN ON THIS DRAWING IS PROVIDED FOR INFORMATION ONLY. CONTRACTOR SHALL SUBMIT SHOP DRAWINGS FOR APPROVAL OF THE MEANS AND METHOD OF ATTACHMENT REQUIREMENTS FOR MOUNTING ANTENNA.
2. ALL FASTENER HARDWARE SHALL BE STAINLESS STEEL AND CONFIRM TO SUB SELECTION 715-16 OF THE STANDARD SPECIFICATIONS. A LOCK WASHER OR APPROVED STOP NUT SHALL BE USED UNDER EACH NUT. A FLAT WASHER SHALL BE USED UNDER EACH HEAD WITH SLOTTED HOLES AND A FLAT WASHER SHALL BE USED UNDER LOCKWASHERS.
3. STEEL BRACKETS ANGLES AND MISCELLANEOUS BRACKETS SHALL MEET THE REQUIREMENTS OF ASTM A36M AND BE HOT DIPPED GALVANIZED.
4. WIRING HOLE AS PER NYS DOT STANDARD SHEET M680-2.
5. U-BOLT AND Z-BAR ARE PART OF ANTENNA ASSEMBLY.
6. ALL STEEL SURFACES SHALL BE PREVENTED FROM COMING IN DIRECT CONTACT WITH THE ALUMINIUM BRACKETS BY MEANS OF EITHER AN APPROVED MASTIC OR 3mm THICK PAD PLACED BETWEEN THE DISSIMILAR METALS AS PER STANDARD SPECIFICATION 646-2.06
7. SECONDARY TRUSS ELEMENTS ARE NOT SHOWN FOR CLARITY
8. ALL PIPE ENDS SHOULD BE CLOSED BY WATER PROOF CAP (TYPICAL)

AFFIX SEAL: ON:	ALTERED BY: ON:
	

AS-BUILT REVISIONS
DESCRIPTION OF ALTERATIONS:

ITS TRAVEL TIME I-278 AND I-495

PS&E DATE: 06/21/2012

COUNTY: KINGS AND QUEEN

PIN X806.49

BRIDGES

CULVERTS

ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED

CONTRACT NUMBER

TRANSMIT INSTALLATION DETAILS - 2

D262095

DRAWING NO. DTL-02

SHEET NO. 55

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11

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USER = kdesai

DESIGN SUPERVISOR RANDOLPH HUNT

JOB MANAGER MAREK Cwik

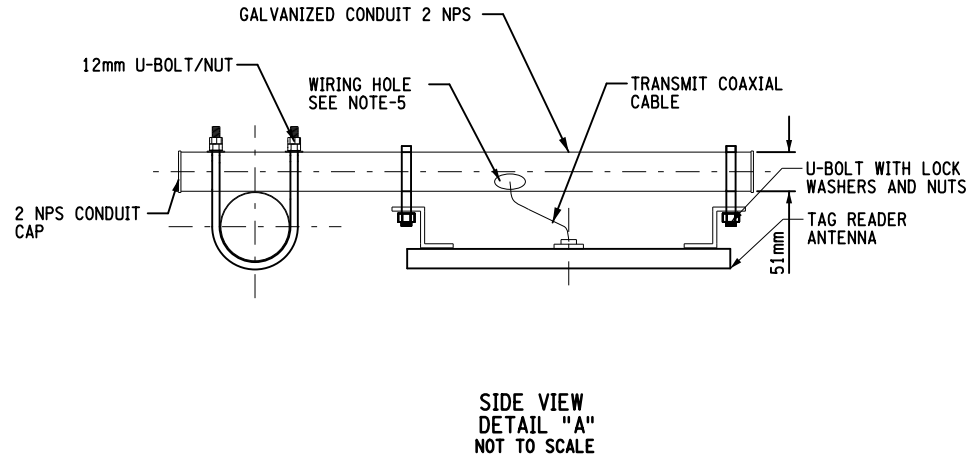
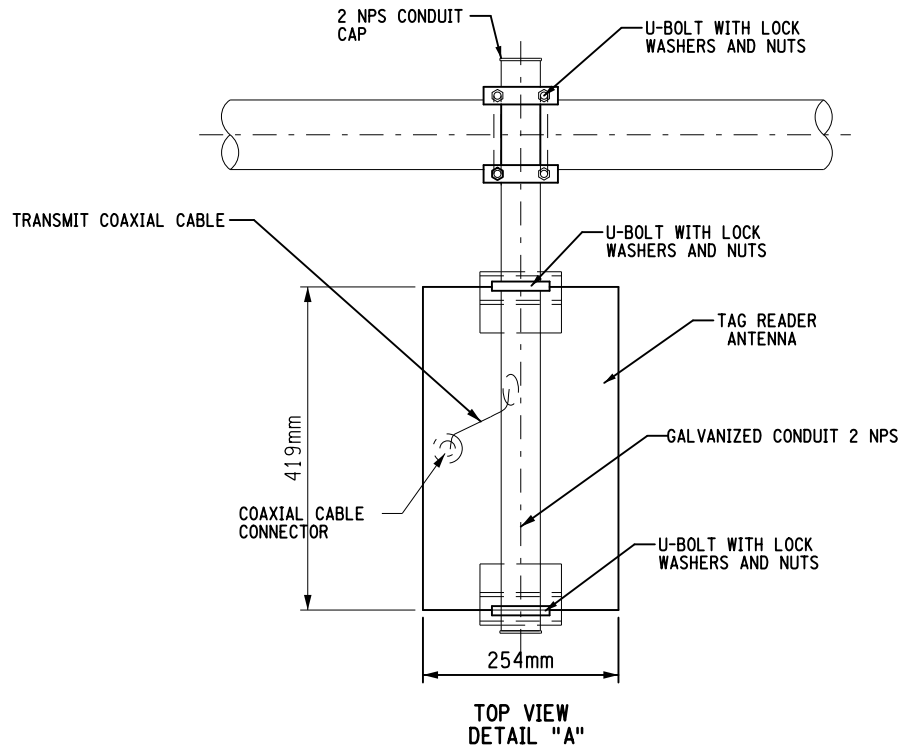
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CHECK MAREK Cwik

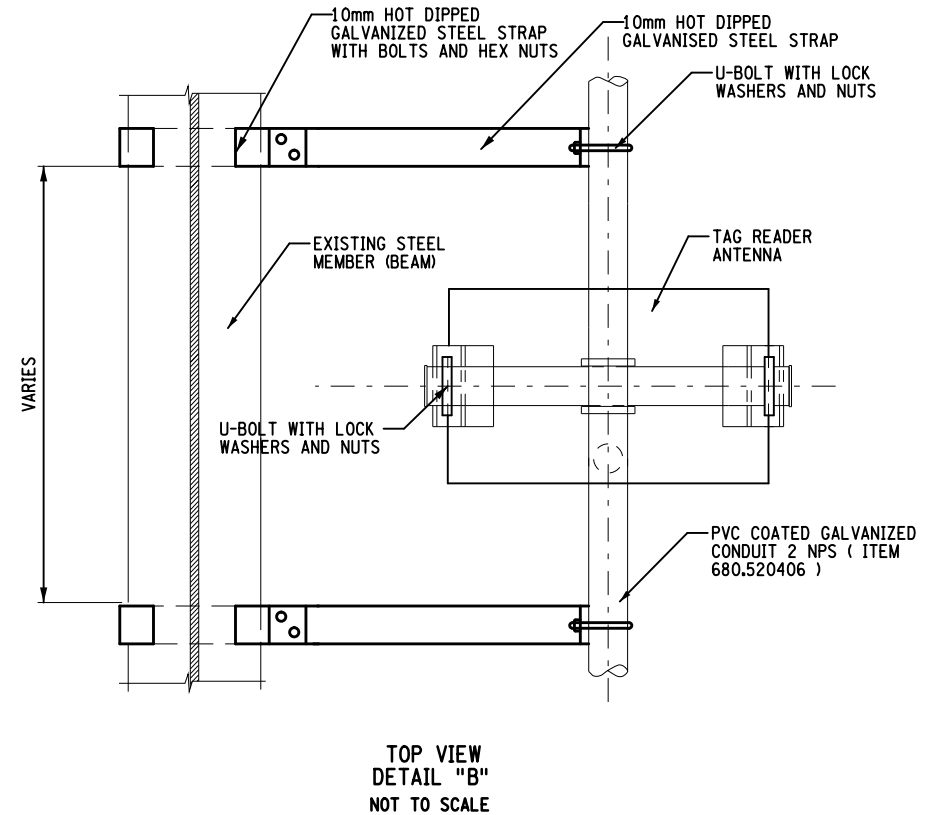
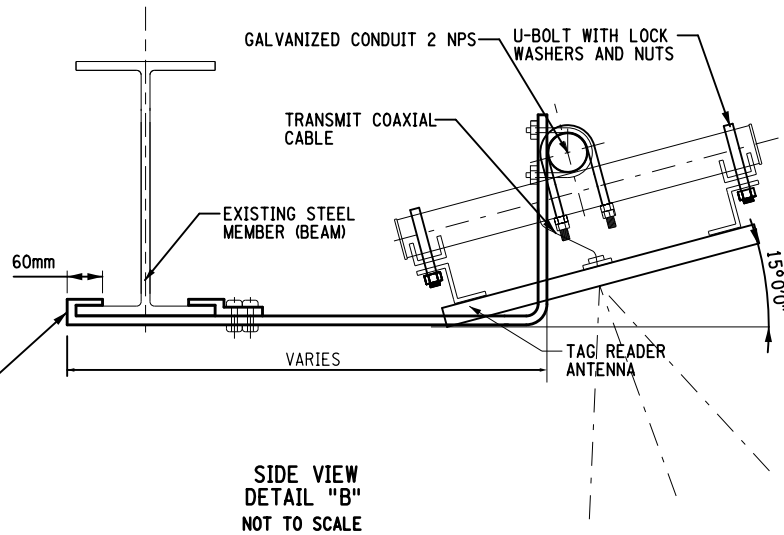
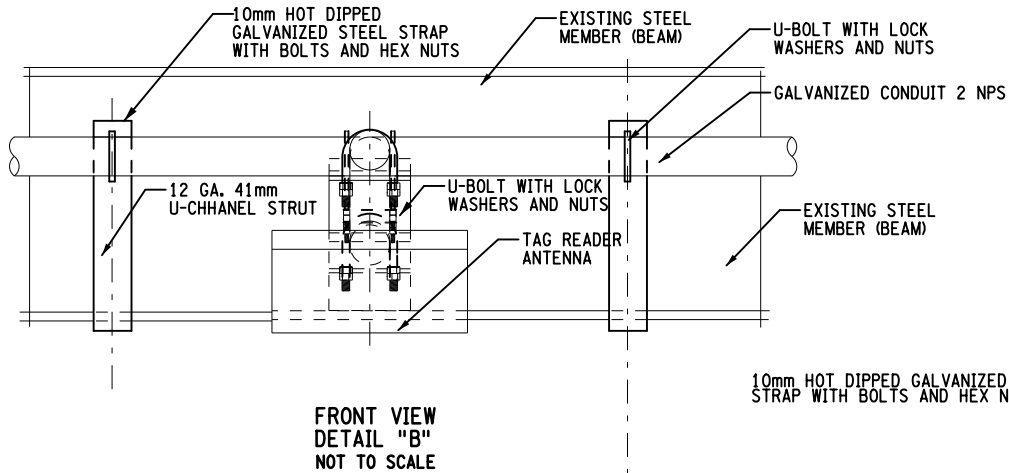
DRAFTING RAISA STARTSEVA

CHECK MAREK Cwik

PROJECT MANAGER RAJ AMIN



CONDUIT SUPPORT STRUCTURE CHORD MOUNTED




NOTES:

1. THE SUGGESTED METHOD OF TRANSMIT ANTENNA ATTACHMENT SHOWN ON THIS DRAWING IS PROVIDED FOR INFORMATION ONLY. CONTRACTOR SHALL SUBMIT SHOP DRAWINGS FOR APPROVAL OF THE MEANS AND METHOD OF ATTACHMENT REQUIREMENTS FOR MOUNTING ANTENNA.
2. ALL NECESSARY HARDWARE AND COMPONENTS FOR SUPPORT AND MOUNTING BRACKETS SHALL BE INCLUDED IN THE PRICE BID FOR THE APPLICATION OF ANTENNA AND PAY UNDER ITEM 680.99820111.
3. ALL FASTENER HARDWARE SHALL BE STAINLESS STEEL AND CONFIRM TO SUB SELECTION 715-16 OF THE STANDARD SPECIFICATIONS. A LOCK WASHER OR APPROVED STOP NUT SHALL BE USED UNDER EACH NUT. A FLAT WASHER SHALL BE USED UNDER EACH HEAD WITH SLOTTED HOLES AND A FLAT WASHER SHALL BE USED UNDER LOCKWASHERS.
4. STEEL BRACKETS ANGLES AND MISCELLANEOUS BRACKETS SHALL MEET THE REQUIREMENTS OF ASTM A36M AND BE HOT DIPPED GALVANIZED.
5. WIRING HOLE AS PER NYSDOT STANDARD SHEET M680-2
6. U-BOLT AND Z-BAR ARE PART OF ANTENNA ASSEMBLY
7. ALL STEEL SURFACES SHALL BE PREVENTED FROM COMING IN DIRECT CONTACT WITH THE ALUMINIUM BRACKETS BY MEANS OF EITHER AN APPROVED MASTIC OR 3mm THICK PAD PLACED BETWEEN THE DISSIMILAR METALS AS PER STANDARD SPECIFICATION 646-2.06
8. SECONDARY TRUSS ELEMENTS ARE NOT SHOWN FOR CLARITY
9. ALL PIPE ENDS SHOULD BE CLOSED BY WATER PROOF CAP (TYPICAL)

AFFIX SEAL: ON:	ALTERED BY: ON:

CONDUIT SUPPORT BRIDGE BEAM SUPPORT

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	ITS TRAVEL TIME I-278 AND I-495	PIN X806.49	BRIDGES	CULVERTS	ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED	CONTRACT NUMBER	
	PS&E DATE: 06/21/2012				TAG READER ANTENNA DETAILS	D262095	
						DRAWING NO. DTL-03	
						SHEET NO. 56	
	COUNTY: KINGS AND QUEEN						
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.						NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11	
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DESIGN SUPERVISOR RANDOLPH HUNT

JOB MANAGER MAREK CWTOK

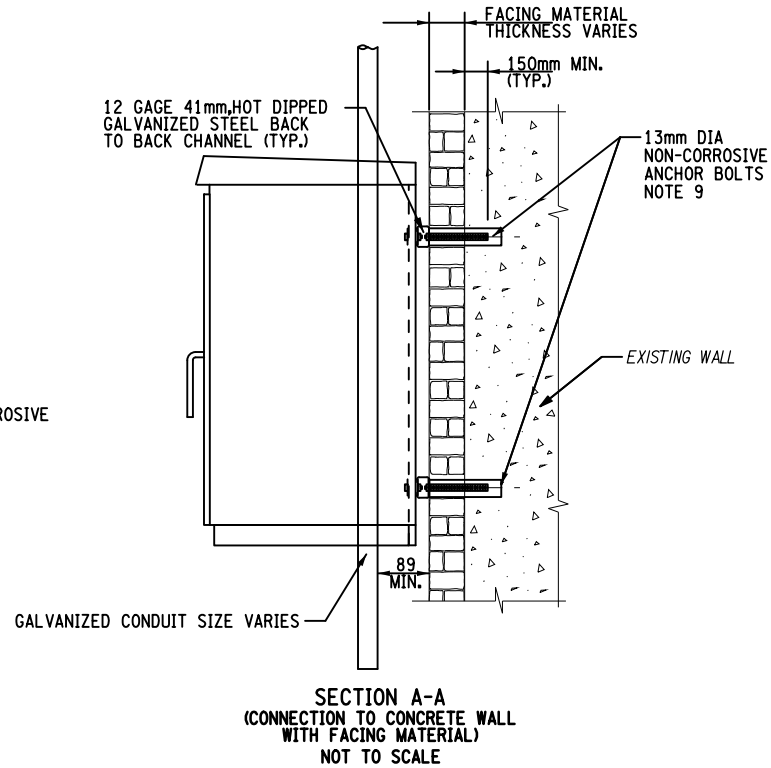
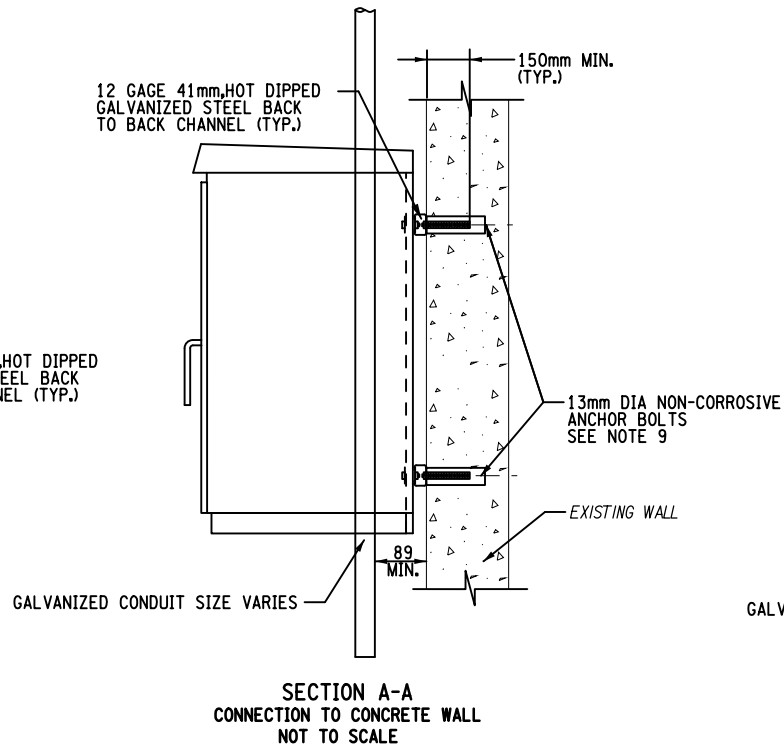
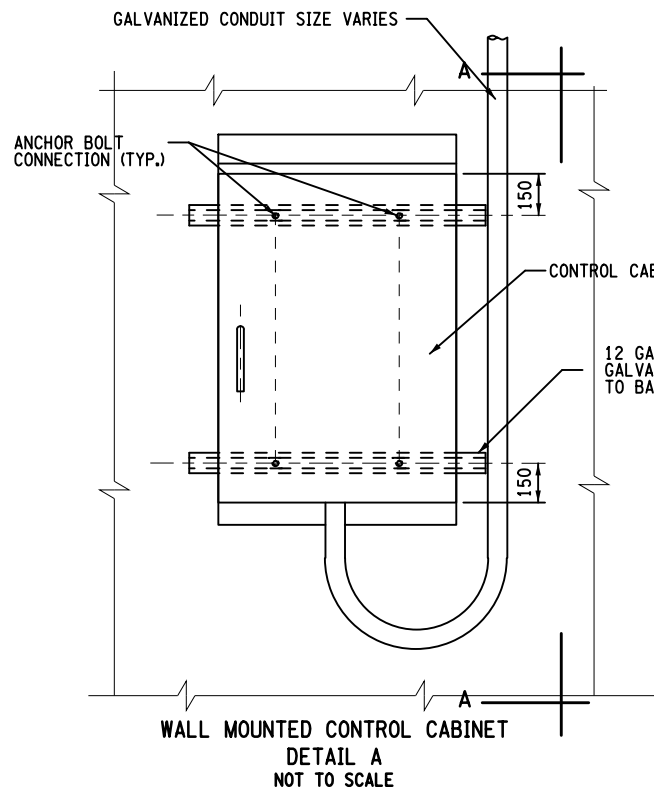
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CHECK MAREK CWTOK

DRAFTING RAISA STARTSEVA

CHECK MAREK CWTOK

PROJECT MANAGER RAJ AMIN



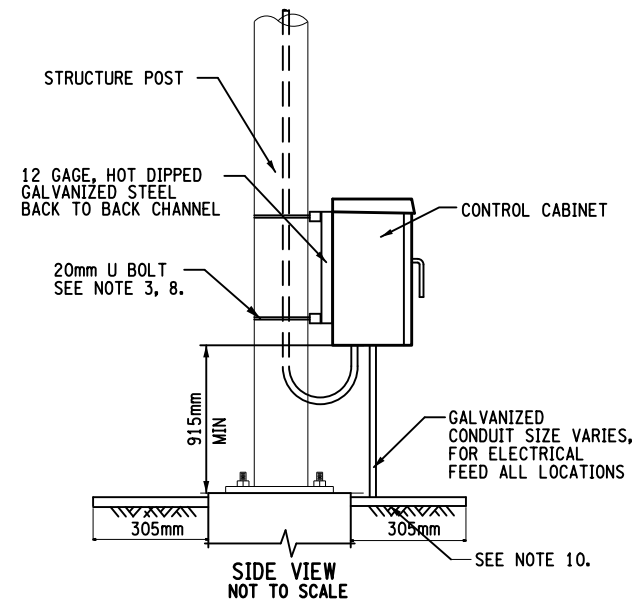
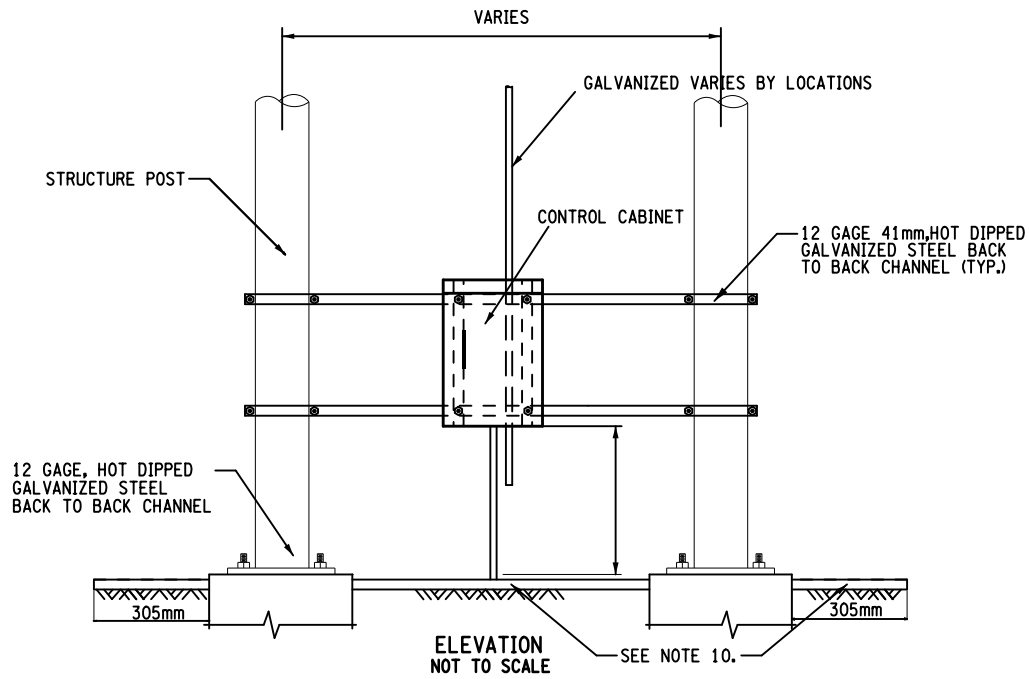
NOTES:


1. ALL FASTENER HARDWARE SHALL BE STAINLESS STEEL AND CONFIRM TO SUB SECTION 715-16 OF THE STANDARD SPECIFICATIONS. A LOCK WASHER OR APPROVED STOP NUT SHALL BE USED UNDER EACH NUT. A FLAT WASHER SHALL BE USED UNDER EACH HEAD WITH SLOTTED HOLES AND A FLAT WASHER SHALL BE USED UNDER LOCKWASHERS.
2. STEEL BRACKETS ANGELS AND MISCELLANEOUS BRACKETS SHALL MEET THE REQUIREMENTS OF ASTM A36M AND BE HOT DIPPED GALVANIZED.
3. ALL STEEL SURFACES SHALL BE PREVENTED FROM COMING IN DIRECT CONTACT WITH THE ALUMINIUM BRACKETS BY MEANS OF EITHER AN APPROVED MASTIC OR 3MM THICK PAD PLACED BETWEEN THE DISSIMILAR METALS AS PER STANDARD SPECIFICATION 646-2.06
4. SECONDARY TRUSS ELEMENTS ARE NOT SHOWN FOR CLARITY
5. THE BOTTOM OF CABINET SHALL BE MINIMUM 915mm HIGH ABOVE FINISHED SURFACE.
6. ALL MATERIALS AND LABOR SHALL BE PAID UNDER RESPECTIVE CONTROL READER CABINET ITEM, UNLESS OTHERWISE NOTED.
7. ALL SUPPORTS, BRACKETS, STRAPS, AND HARDWARE SHALL BE PAID FOR UNDER RESPECTIVE CONTROL READER CABINET ITEM.
8. ALL HARDWARE SHALL BE STAINLESS STEEL AND NON-MAGNETIC, UNLESS OTHERWISE NOTED.
9. THE CONTRACTOR SHALL FURNISH AND INSTALL NON-CORROSIVE ANCHOR BOLTS BY DRILLING AND GROUTING, IN ACCORDANCE WITH SPECIFICATION AND PAID UNDER ITEM 586.02M.


10. 75mm THICK ASPHALT MOWING STRIP SHALL BE PAID UNDER ITEM 608.020101M, WHERE APPLICABLE.

NIC

11. MOUNT CLAMP ON STRUCTURE AT EVERY 1m MINIMUM.



AFFIX SEAL: ON:	ALTERED BY: ON:
	

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	ITS TRAVEL TIME I-278 AND I-495	PIN X806.49	BRIDGES	CULVERTS	ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED	CONTRACT NUMBER	
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IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.						NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11 DOCUMENT NAME: x80649_cph_gnd_dtl_04.dgn	

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DESIGN SUPERVISOR RANDOLPH HUNT

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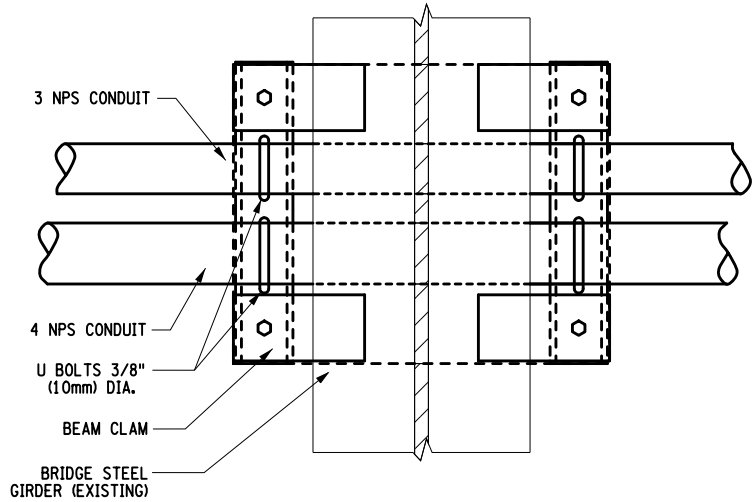
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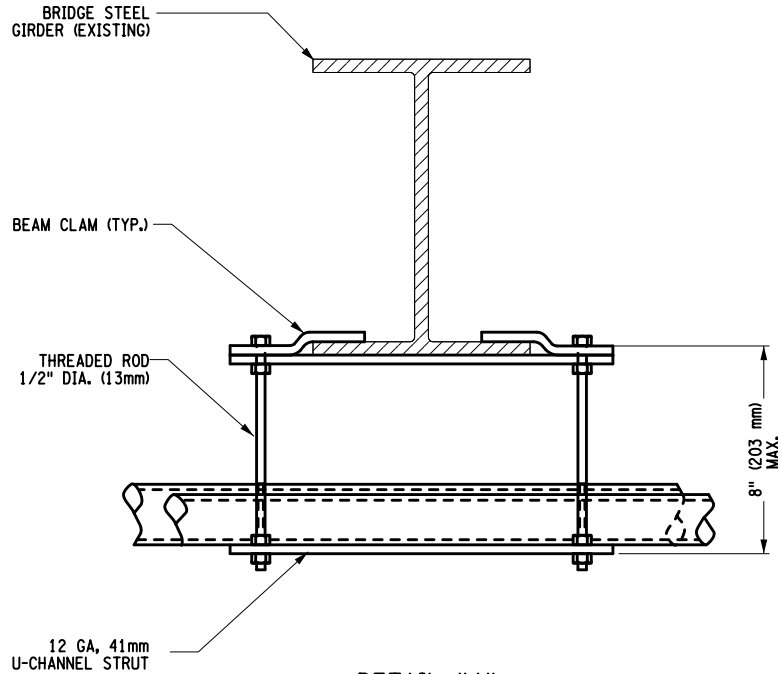
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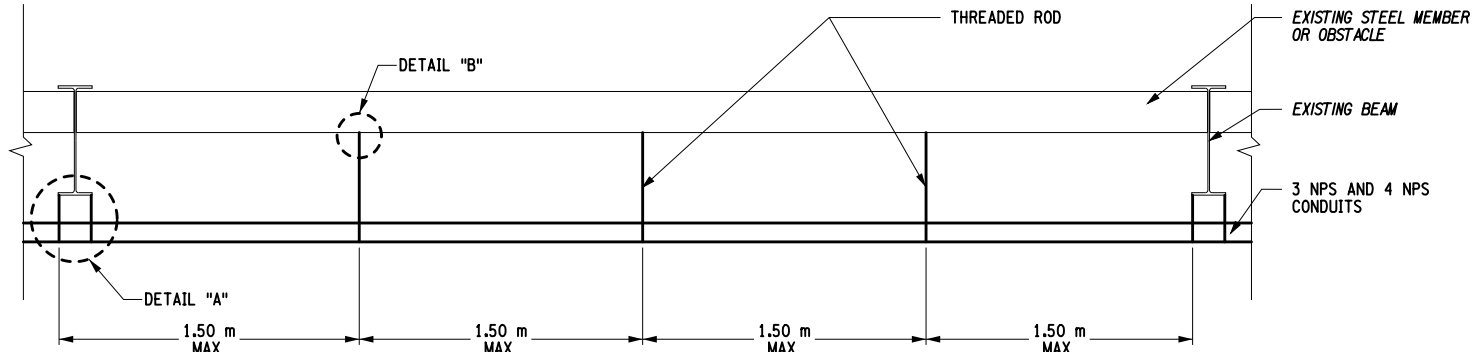
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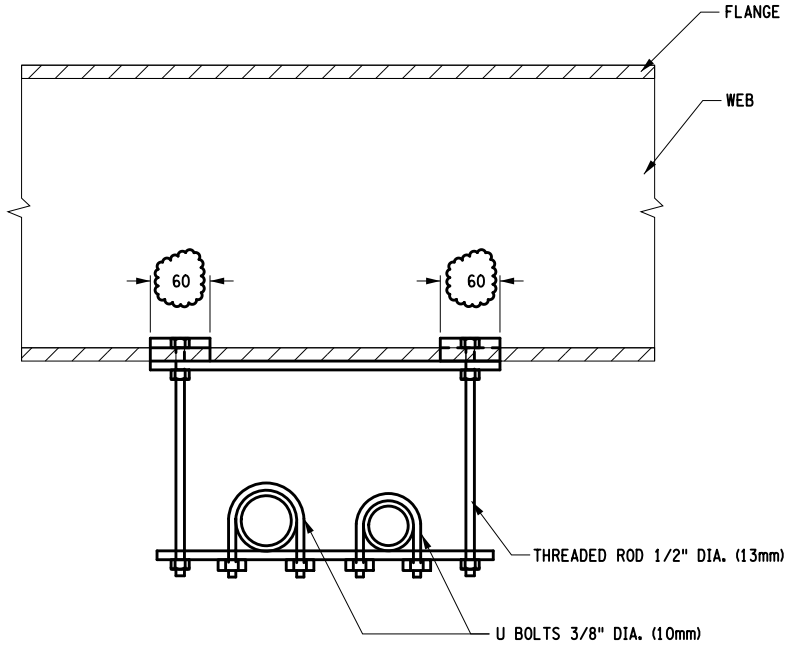
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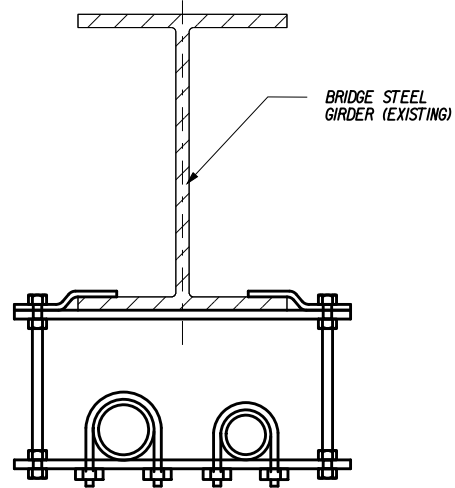
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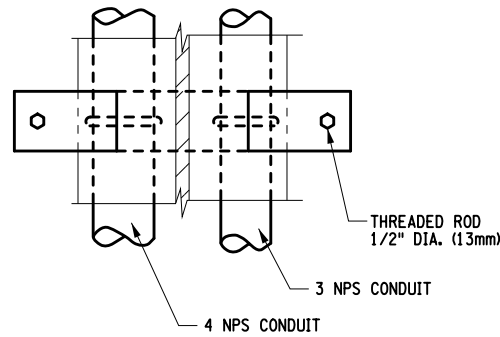
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DETAIL "A"
SIDE VIEW
N.T.S.



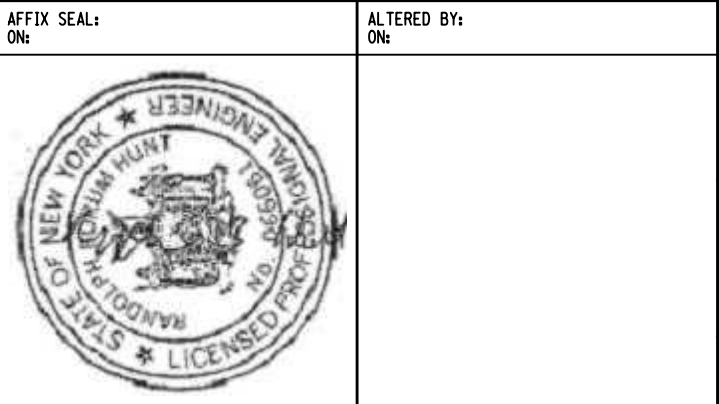
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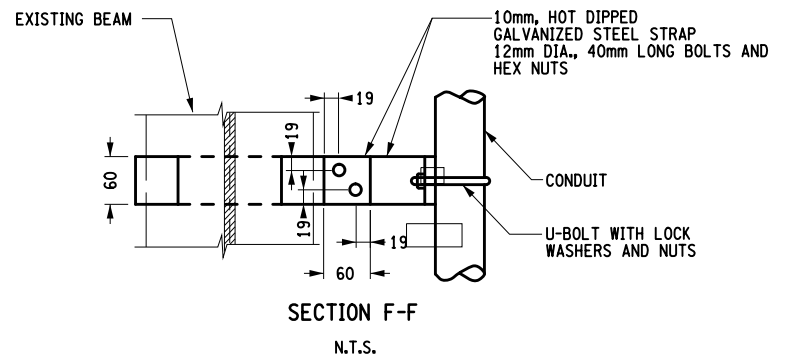
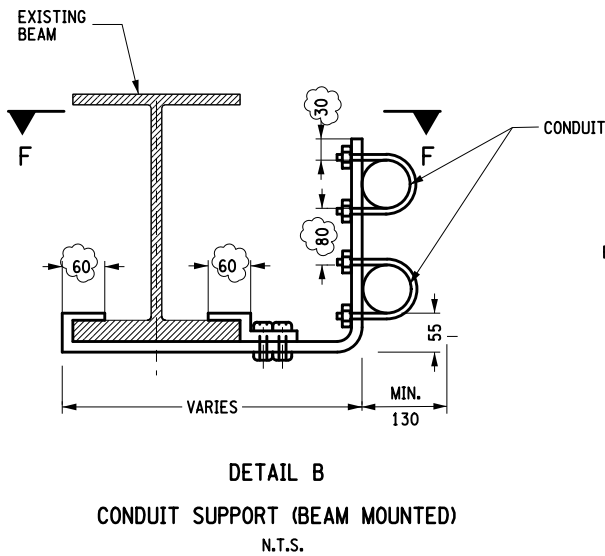
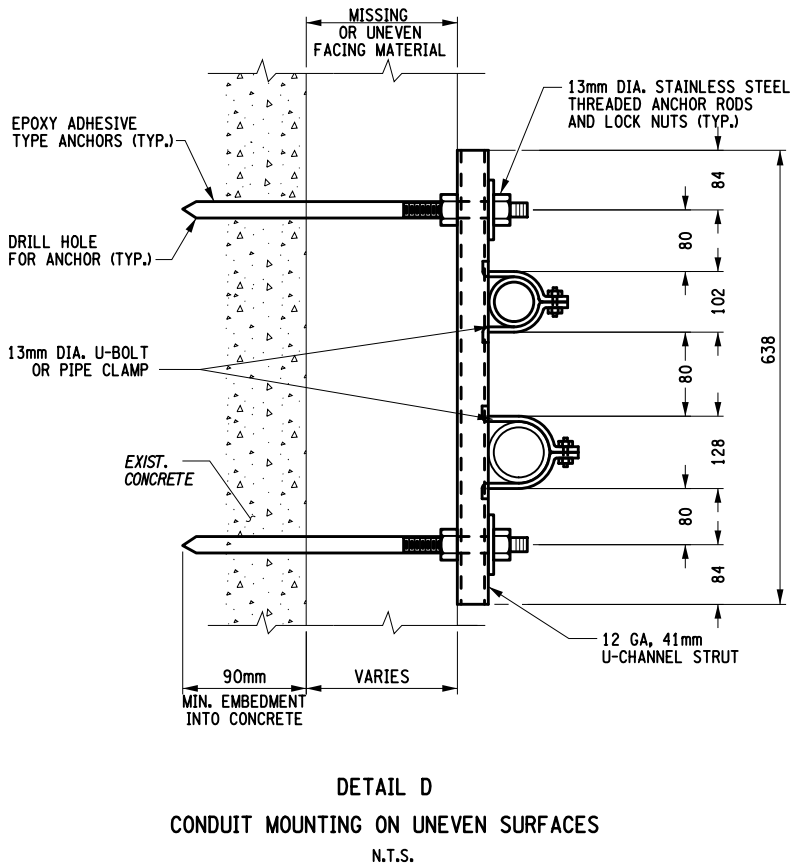
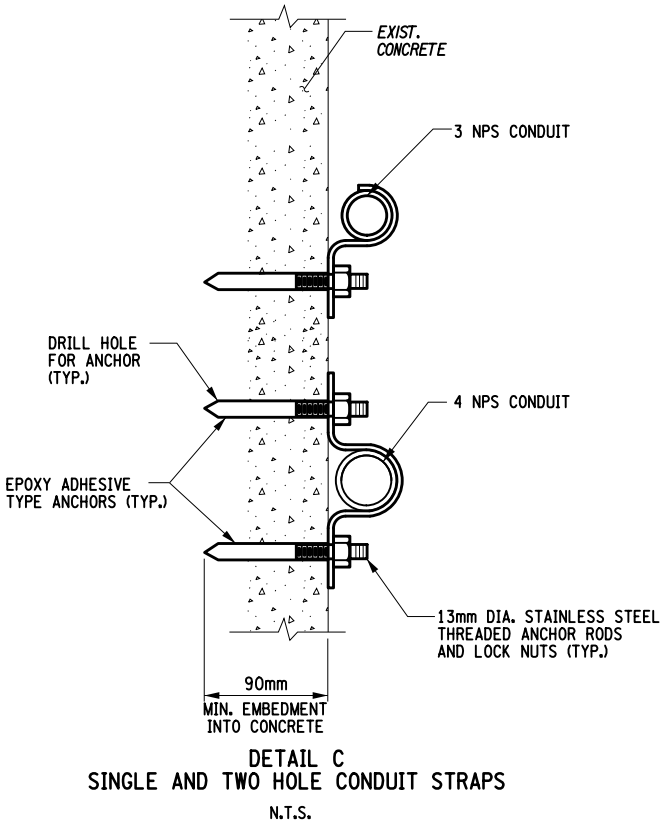
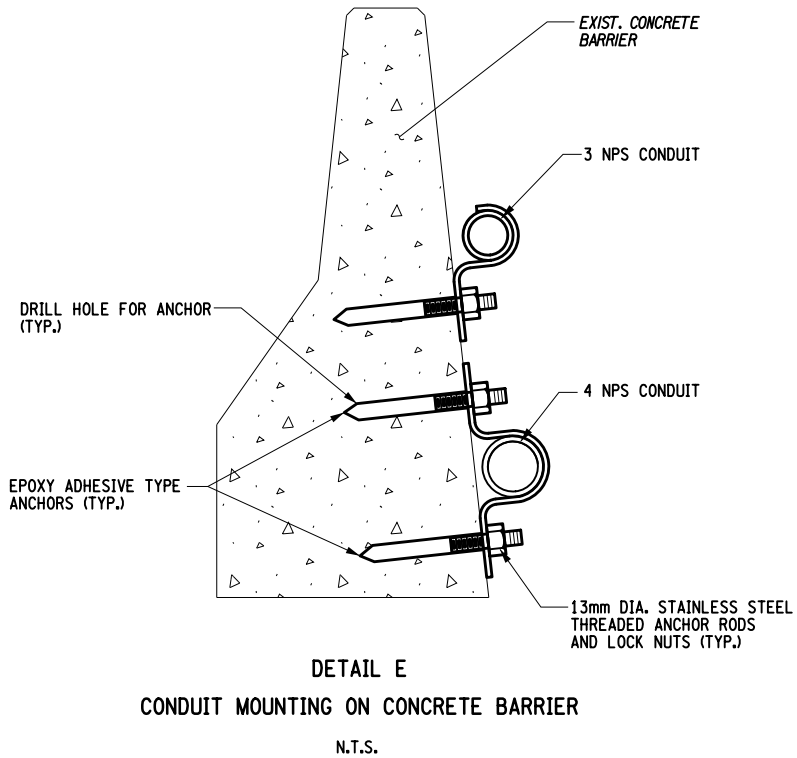
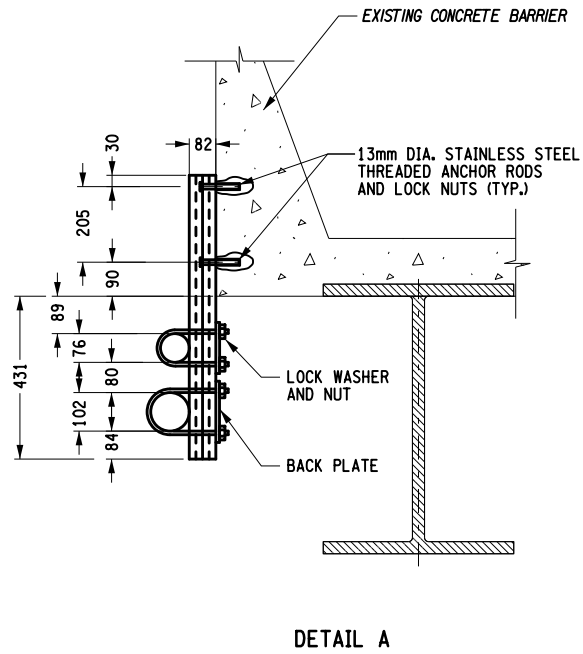
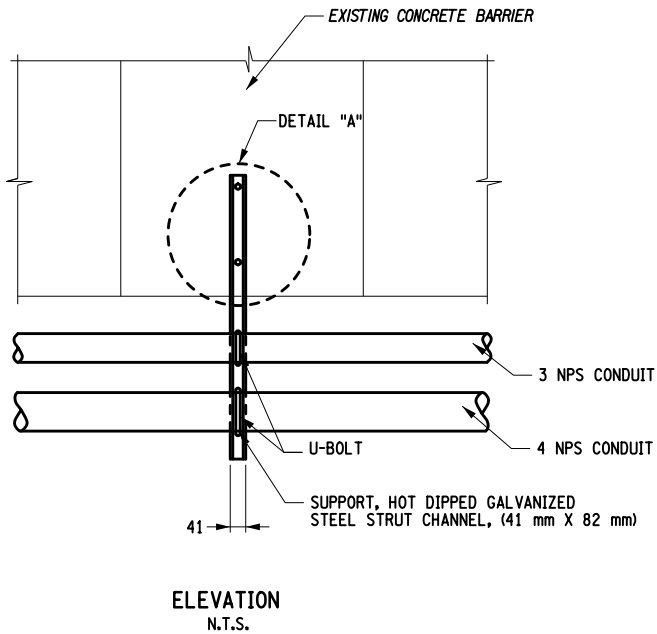
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CONDUIT SUPPORT ON BEAM MOUNTED
N.T.S.

NOTES:

- CONDUIT STRAPS SHALL BE MADE OF HOT DIPPED GALVANIZED OR ALLOY STEEL CLAMPS, ATTACHED TO WALLS AND OTHER CONCRETE STRUCTURES USING EPOXY ADHESIVE TYPE ANCHORS WITH STAINLESS STEEL BOLTS. THE EPOXY ADHESIVE SHALL BE IN ACCORDANCE WITH SUBSECTION 701-07 OF THE STANDARD SPECIFICATIONS. DRILLING OF HOLES FOR THE ANCHOR BOLT ASSEMBLIES SHALL BE IN ACCORDANCE WITH SUBSECTION 586 OF THE STANDARD SPECIFICATIONS. EXPANSION TYPE ANCHORS ARE NOT ACCEPTABLE.
- C-CLAMPS SHALL BE HARDENED STEEL, HOT DIP GALVANIZED, OR MALLEABLE IRON CASTING, AND EQUIPPED WITH CUP POINT SET SCREWS AND LOCKING NUTS TO PREVENT LOOSENING DUE TO VIBRATION. RESTRAINING STRAP SHALL BE HOT DIP GALVANIZED STEEL, 11 GA. THREADED HANGER ROD SHALL BE 13mm MIN. DIA. STAINLESS STEEL. HINGED OR EXTENSION SPLIT PIPE CLAMPS FOR ATTACHMENT TO THE THREADED ROD SHALL BE OF GALV. STEEL OR MALLEABLE IRON.
- U-CANNEL STRUTS SHALL BE ROLL FORMED, COMMERCIALY AVAILABLE 41 mm CHANNELS OF 12 GA., HOT DIPPED GALVANIZED STEEL. BEAM CLAMPS SHALL BE HOT DIPPED GALVANIZED STEEL, 6.3mm THICK. CONDUIT STRAPS SHALL BE MADE OF HOT DIPPED GALVANIZED OR ALLOY STEEL CLAMPS COMPATIBLE FOR USE WITH U-CANNEL STRUT SYSTEM, OR 13mm DIA. U-BOLTS.
- ALL FASTENER HARDWARE, INCLUDING BOLTS, NUTS AND WASHERS, SHALL BE STAINLESS STEEL AND SHALL CONFORM TO SUBSECTION 715-16 OF THE STANDARD SPECIFICATIONS.
- A LOCKWASHER OR AN APPROVED STOP NUT SHALL BE USED UNDER EACH NUT. A FLAT WASHER SHALL BE USED UNDER EACH HEAD WITH SLOTTED HOLES AND A FLAT WASHER SHALL BE USED UNDER LOCK WASHERS.
- CONDUIT STRAPS SHALL BE INSTALLED AT A SPACING OF 1m ON CENTERS OR AS APPROVED BY THE ENGINEER. A CONDUIT STRAP SHALL BE INSTALLED WITHIN 0.91M OF EACH PULLBOX.
- CONDUIT SUPPORTS SHALL BE INSTALLED AT A MINIMUM SPACING OF 3.05m ON CENTERS OR AS PER MANUFACTURERS' RECOMMENDATION, WHICHEVER IS LESS. A CONDUIT SUPPORT SHALL BE INSTALLED WITHIN 0.91M OF EACH PULLBOX.
- INSTALLATION OF ALL CONDUITS MOUNTED ONTO WALLS AND OTHER STRUCTURES SHALL BE PAID FOR UNDER CONDUIT INSTALLATION ON ABOVE GRADE STRUCTURES (ITEM 206.031211). THE COST TO DRILL HOLES AND FURNISH AND INSTALL ALL ANCHORS, SUPPORTS, CLAMPS, STRAPS, STRUTS, MOUNTING HARDWARE, AND OTHER ACCESSORIES NECESSARY TO COMPLETE THE WORK SHALL BE INCLUDED IN THE PRICE BID FOR ITEM 206.031211.
- THE SUGGESTED METHODS OF MOUNTING SHOWN ON THIS DRAWING ARE PROVIDED FOR INFORMATION ONLY. THE CONTRACTOR SHALL PREPARE A SHOP DRAWING SUBMITTAL WHICH WILL INCLUDE COPIES OF DESCRIPTIVE LITERATURE FOR CONDUIT STRAPS, MOUNTING STRUTS, CLAMPS, DEFLECTION AND EXPANSION COUPLINGS, AND OTHER NECESSARY COMPONENTS. THE SUBMITTAL SHALL INCLUDE A COMPLETE INSTALLATION PLAN AND CONDUIT LAYOUT DRAWINGS FOR EXPOSED CONDUIT SYSTEMS WHICH SHALL DETAIL CONSTRUCTION METHODS TO BE UTILIZED THROUGHOUT THE PROJECT CORRIDORS. THE CONDUIT LAYOUT DRAWINGS SHALL SHOW ALL PULLBOX AND JUNCTION BOX LOCATIONS, CONDUIT BENDS, EXPANSION COUPLINGS AND DEFLECTION COUPLINGS.
- ALL EXPOSED CONDUIT AND FITTINGS SHALL BE HOT DIP GALVANIZED STEEL. ALL FITTINGS, SWEEPS (SHOP AND FIELD BENT), EXPANSION COUPLINGS, DEFLECTION COUPLINGS, TERMINATION FITTINGS AND PLUGS SHALL BE INCLUDED FOR PAYMENT UNDER THE APPLICABLE RIGID GALVANIZED STEEL CONDUIT ITEM.





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AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	ITS TRAVEL TIME I-278 AND I-495		PIN X806.49	BRIDGES	CULVERTS	ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED	CONTRACT NUMBER		
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							DRAWING NO. DTL-05		
							SHEET NO. 58		
	COUNTY: KINGS AND QUEEN								
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NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11									
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8. INSTALLATION OF ALL CONDUITS MOUNTED ONTO WALLS AND OTHER STRUCTURES SHALL BE PAID FOR UNDER CONDUIT INSTALLATION ON ABOVE GRADE STRUCTURES (ITEM 206.0312--11). THE COST TO DRILL HOLES AND FURNISH AND INSTALL ALL ANCHORS, SUPPORTS, CLAMPS, STRAPS, STRUTS, MOUNTING HARDWARE, AND OTHER ACCESSORIES NECESSARY TO COMPLETE THE WORK.
9. THE SUGGESTED METHODS OF MOUNTING SHOWN ON THIS DRAWING ARE PROVIDED FOR INFORMATION ONLY. THE CONTRACTOR SHALL PREPARE A SHOP DRAWING SUBMITTAL WHICH WILL INCLUDE COPIES OF DESCRIPTIVE LITERATURE FOR CONDUIT STRAPS, MOUNTING STRUTS, CLAMPS, DEFLECTION AND EXPANSION COUPLINGS, AND OTHER NECESSARY COMPONENTS. THE SUBMITTAL SHALL INCLUDE A COMPLETE INSTALLATION PLAN AND CONDUIT LAYOUT DRAWINGS FOR EXPOSED CONDUIT SYSTEMS WHICH SHALL DETAIL CONSTRUCTION METHODS TO BE UTILIZED THROUGHOUT THE PROJECT CORRIDORS. THE CONDUIT LAYOUT DRAWINGS SHALL SHOW ALL PULLBOX AND JUNCTION BOX LOCATIONS, CONDUIT BENDS, EXPANSION COUPLINGS AND DEFLECTION COUPLINGS.
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11. ALL DIMENSIONS IN MILLIMETERS UNLESS OTHERWISE NOTED.

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AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	ITS TRAVEL TIME I-278 AND I-495	PIN X806.49	BRIDGES	CULVERTS	ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED	CONTRACT NUMBER	
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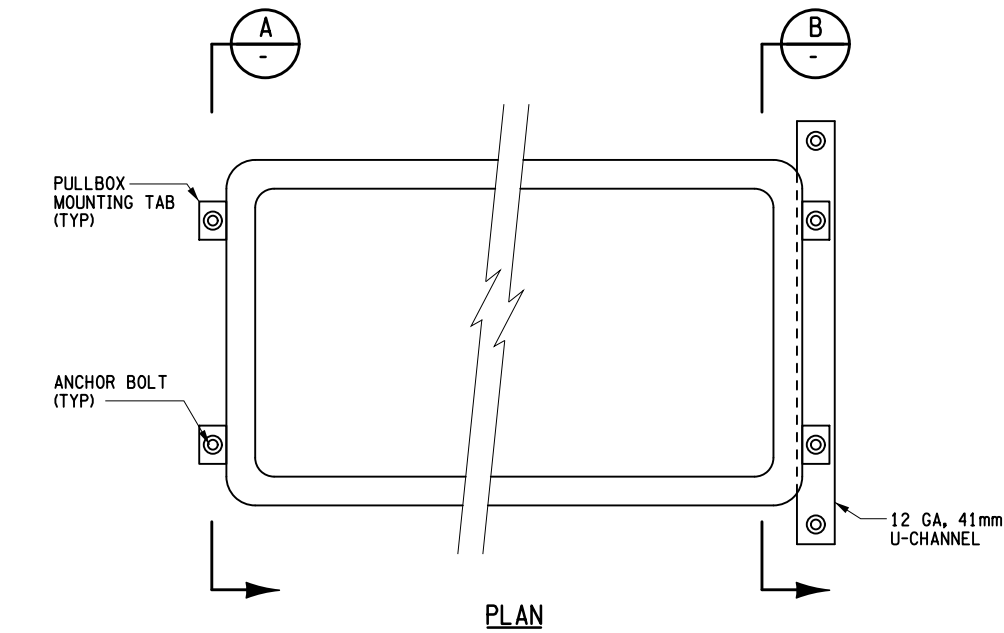
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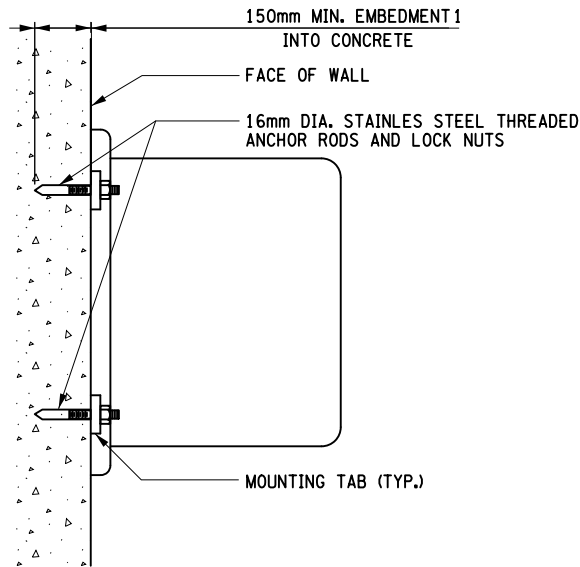
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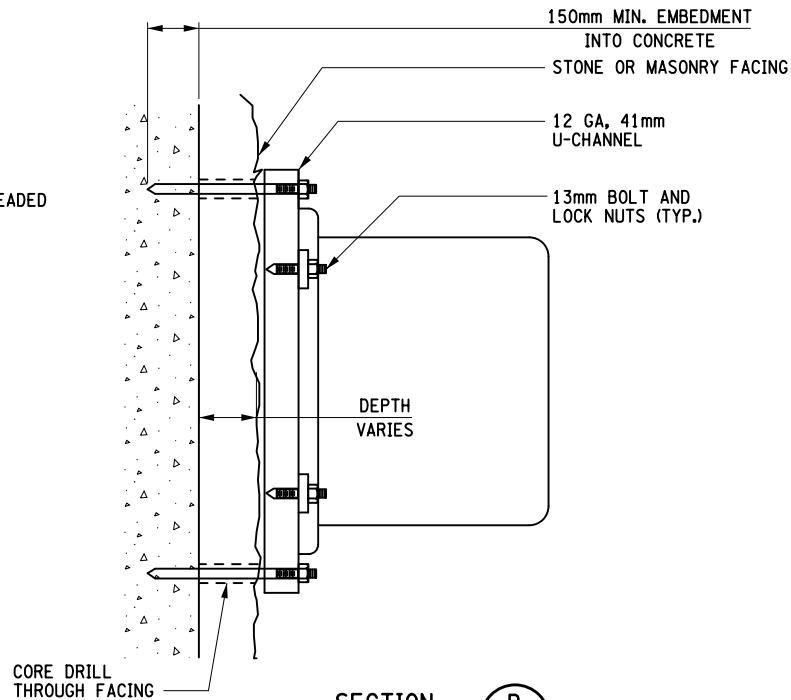


PLAN



SECTION A

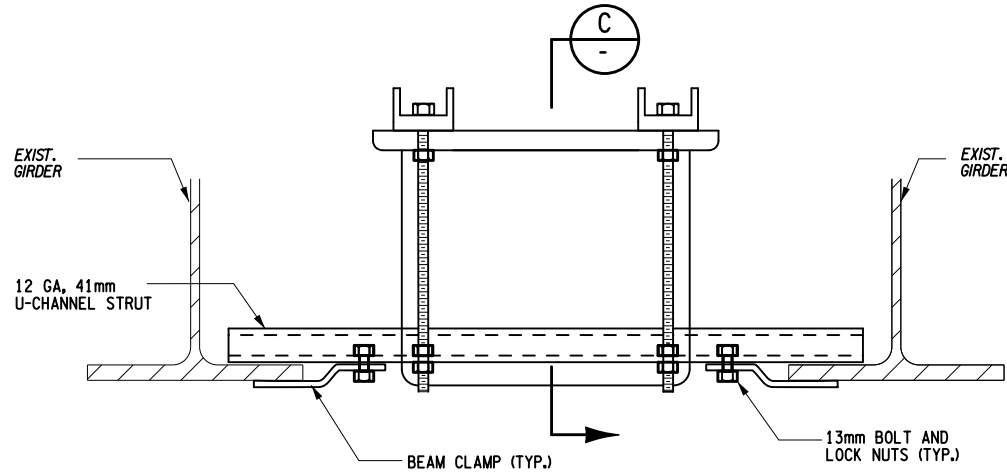
DIRECT FIXATION
TO SMOOTH WALLS



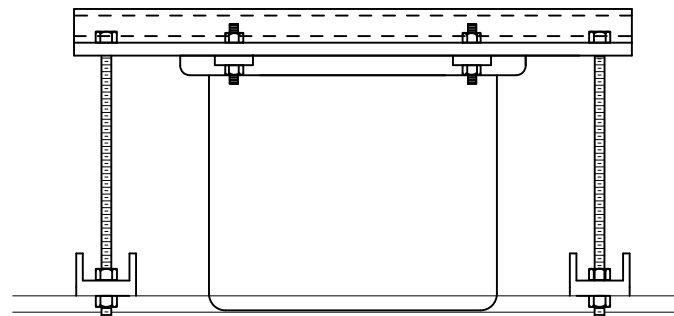
SECTION B

MOUNTING ON
UNEVEN WALL
SURFACES

SURFACE MOUNTED PULLBOX



PULLBOX MOUNTED IN
BETWEEN GIRDERS




SECTION C


PULLBOX MOUNTED FROM SUPERSTRUCTURES

NOTES:

- PULLBOXES SHALL BE ATTACHED TO WALLS AND OTHER CONCRETE STRUCTURES USING EPOXY ADHESIVE TYPE ANCHORS WITH STAINLESS STEEL BOLTS. THE EPOXY ADHESIVE SHALL BE IN ACCORDANCE WITH SUBSECTION 701-07 OF THE STANDARD SPECIFICATIONS. DRILLING OF HOLES FOR THE ANCHOR BOLT ASSEMBLIES SHALL BE IN ACCORDANCE WITH SUBSECTION 586 OF THE STANDARD SPECIFICATIONS. EXPANSION TYPE ANCHORS ARE NOT ACCEPTABLE.
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- INSTALLATION OF PULLBOXES ONTO WALLS, BRIDGE SUPERSTRUCTURES AND OTHER STRUCTURES IS INCLUDED IN THE PAYMENT FOR THE APPLICABLE PULLBOX ITEM, INCLUDING THE COST TO DRILL HOLES AND TO FURNISH AND INSTALL ALL ANCHORS, SUPPORTS, STRAPS, STRUTS, MOUNTING HARDWARE, AND OTHER ACCESSORIES NECESSARY TO COMPLETE THE WORK.
- THE SUGGESTED METHODS OF MOUNTING SHOWN ON THIS DRAWING ARE PROVIDED FOR INFORMATION ONLY. IN ACCORDANCE WITH THE SPECIFICATIONS, THE CONTRACTOR SHALL PREPARE A SHOP DRAWING SUBMITTAL WHICH WILL INCLUDE COPIES OF DESCRIPTIVE LITERATURE FOR THE PULL BOX AND ALL MOUNTING HARDWARE TO BE PROVIDED. IN ADDITION, THE SUBMITTAL SHALL INCLUDE A COMPLETE INSTALLATION PLAN WHICH SHALL DETAIL CONSTRUCTION METHODS TO BE UTILITIZED. REFER TO SPECIFICATION ITEM 11680.5160.

AFFIX SEAL: ON:	ALTERED BY: ON:
	

NOT TO SCALE

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	ITS TRAVEL TIME I-278 AND I-495	PIN X806.49	BRIDGES	CULVERTS	ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED	CONTRACT NUMBER	
	PS&E DATE: 06/21/2012					D262095	
						DRAWING NO. DTL-07	
	COUNTY: KINGS AND QUEEN					SHEET NO. 60	
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DESIGN SUPERVISOR RANDOLPH HUNT

JOB MANAGER MAREK CMIK

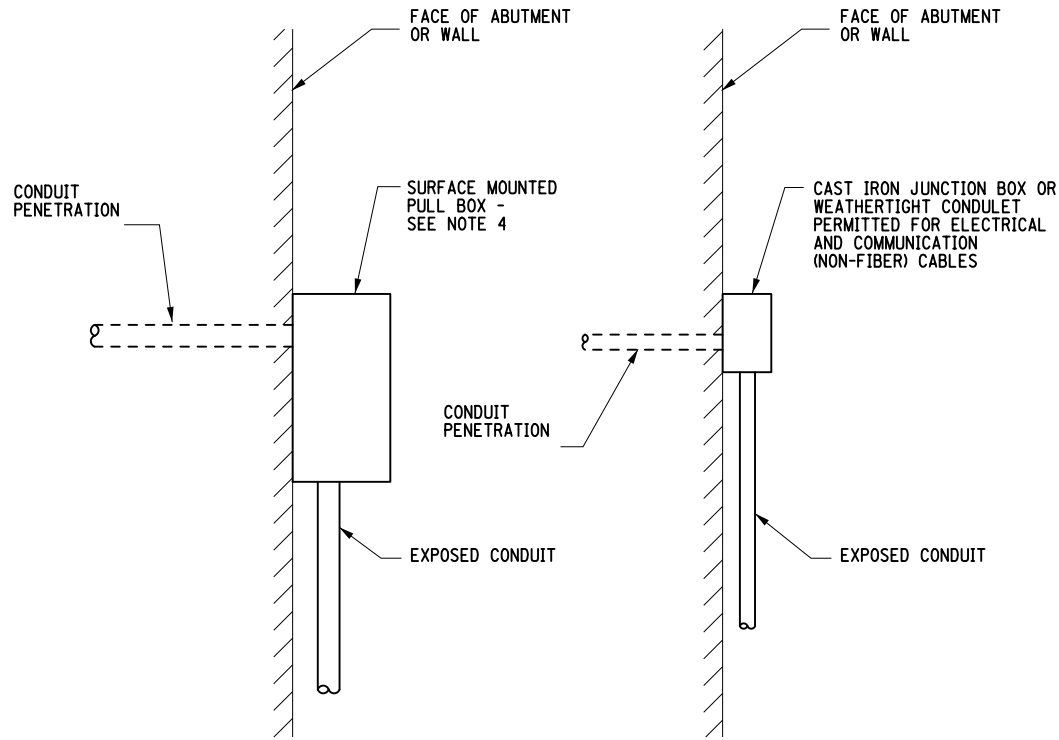
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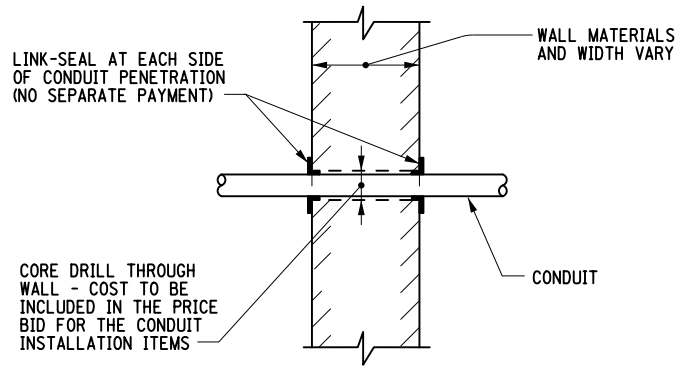
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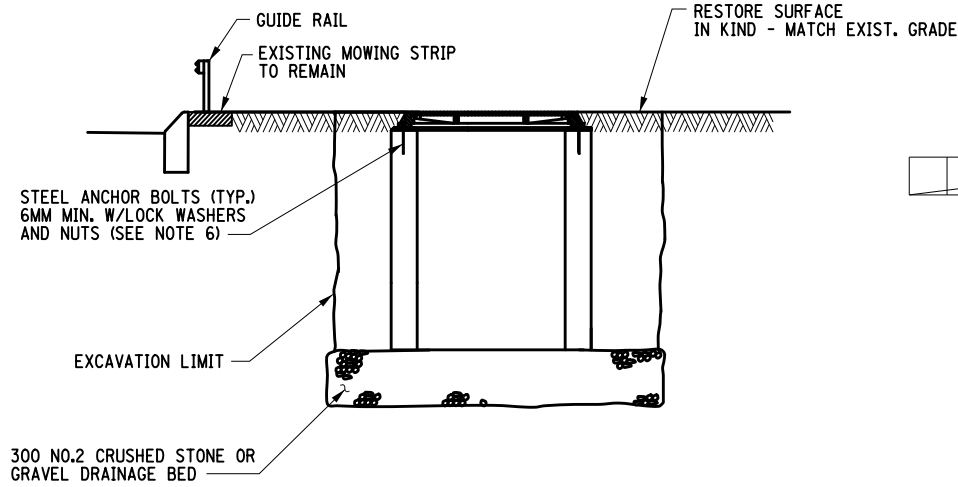
CONDUIT FOR FIBER OPTIC CABLE

CONDUIT FOR CONDUCTORS

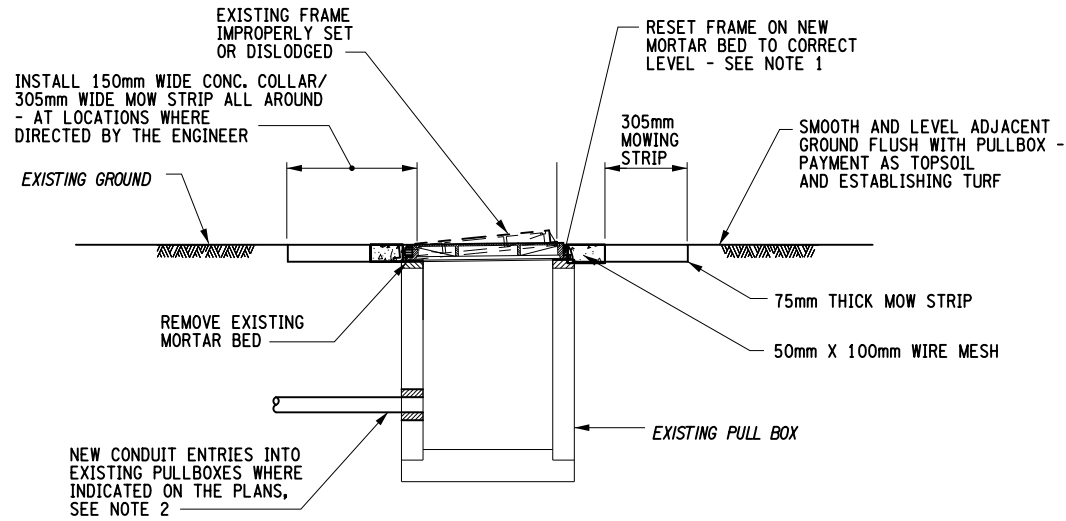
CONDUIT RISER DETAIL AT WALL PENETRATIONS



DETAIL
CONDUIT WALL PENETRATIONS

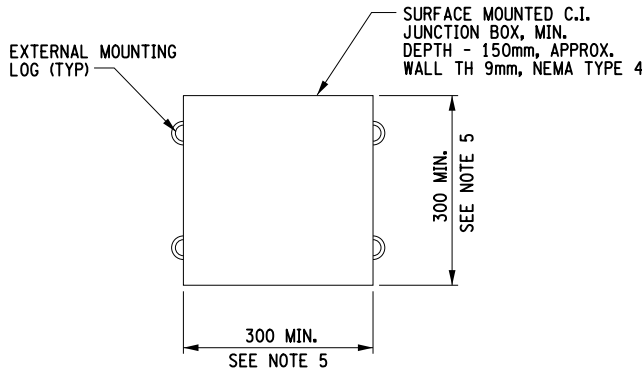


PULLBOX INSTALLATION IN LANDSCAPE AREA
TYPICAL INSTALLATION

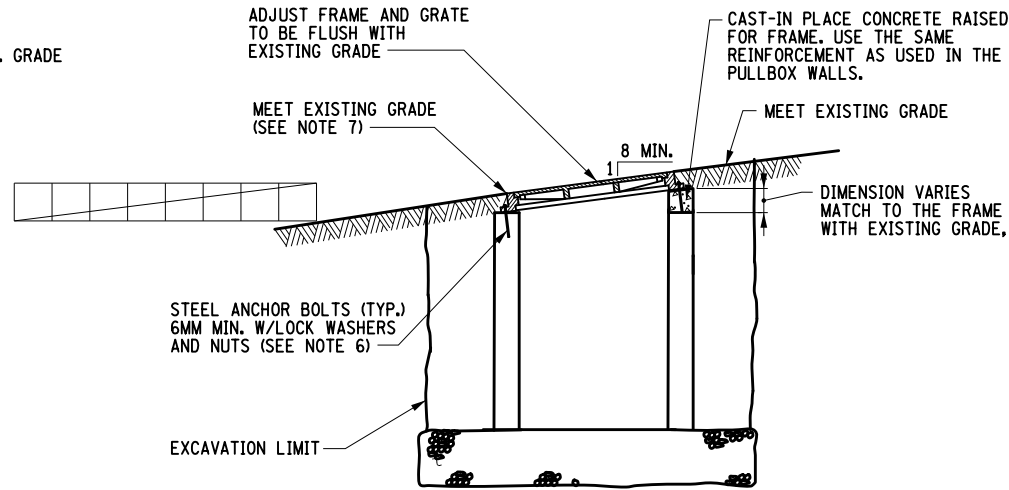


RESET EXISTING PULL BOX COVER

N.T.S.



CAST IRON JUNCTION BOX



PULLBOX INSTALLATION IN LANDSCAPE AREA
ON GENTLE SLOPES
(SEE NOTE 6)

NOTES:

- SOME OF THE EXISTING ITS PULLBOXES MAY BE MIS-IDENTIFIED BY THE EXISTING COVER LEGEND. OTHER COVERS OR FRAMES MAY BE DAMAGED AND REQUIRE REPLACEMENT. THE CONTRACTOR SHALL REPLACE DAMAGED CASTINGS AND COVERS WITH INCORRECT LEGENDS. PAYMENT FOR REPLACEMENT COVERS AND FRAMES SHALL BE UNDER ITEM 655.0101 - FRAMES AND GRATES (CASTINGS). ANY EXISTING COVERS THAT AS A RESULT ARE NO LONGER NEEDED SHALL BE RETURNED TO NYSDOT.
- THE CONTRACTOR SHALL CUT REQUIRED OPENINGS INTO EXISTING PULLBOXES FOR THE ENTRANCE OF NEW CONDUIT AS INDICATED ON THE DRAWINGS AND AS ORDERED BY THE ENGINEER. CONDUITS SHALL EXTEND COMPLETELY THROUGH THE WALL AND TERMINATE APPROXIMATELY 75 mm BEYOND THE INSIDE WALL FACE. THE OPENING SHALL BE FILLED WITH CONCRETE AND MORTAR PRIOR TO BACKFILLING. THE COST OF CONDUIT ENTRIES INTO EXISTING PULLBOXES SHALL BE INCLUDED IN THE BID PRICE FOR THE APPLICABLE CONDUIT ITEM.
- EXISTING PULLBOXES DESIGNATED FOR WORK (NEW CONDUIT ENTRY, RESETTING FRAME AND COVER, AND/OR INSTALLATION OF CONCRETE COLLAR) SHALL BE CLEANED AND MAINTAINED CLEAN BY THE CONTRACTOR FOR THE CONTRACT DURATION - COST TO BE INCLUDED IN THE PRICE BID FOR THE VARIOUS ITEMS.
- THE MINIMUM SIZE OF C.I. JUNCTION BOXES (ITEM 670.40) USED FOR FIBER OPTIC CABLES SHALL BE 600x600x200.
- DEPENDING UPON THE SIZE, NUMBER AND CONFIGURATION OF CONDUITS ENTERING THE JUNCTION BOX LARGER BOX SIZE MAY BE REQUIRED ALL JUNCTION BOXES SHALL BE SIZED IN ACCORDANCE WITH THE NATIONAL ELECTRICAL CODE.
- SECURE THE COVER FRAME WITH FOUR (4) ANCHOR BOLTS (9mm MIN. DIA.) DOWELED INTO THE PULLBOX WALLS WITH 150mm MIN. EMBEDMENT.

NOT TO SCALE



AFFIX SEAL: ON: _____

ALTERED BY: ON: _____

AS-BUILT REVISIONS
DESCRIPTION OF ALTERATIONS:

ITS TRAVEL TIME I-278 AND I-495

PS&E DATE: 06/21/2012

COUNTY: KINGS AND QUEEN

PIN X806.49

BRIDGES

CULVERTS

ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED

CONTRACT NUMBER

MISCELLANEOUS CONSTRUCTION DETAILS

D262095

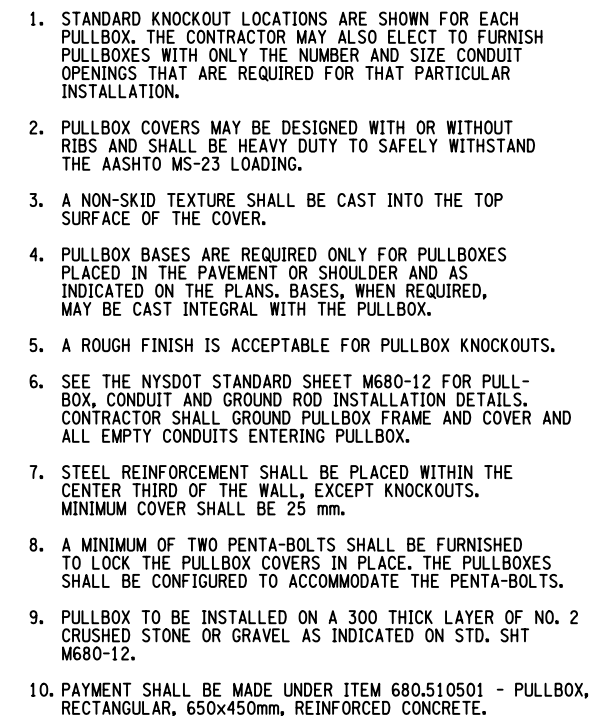
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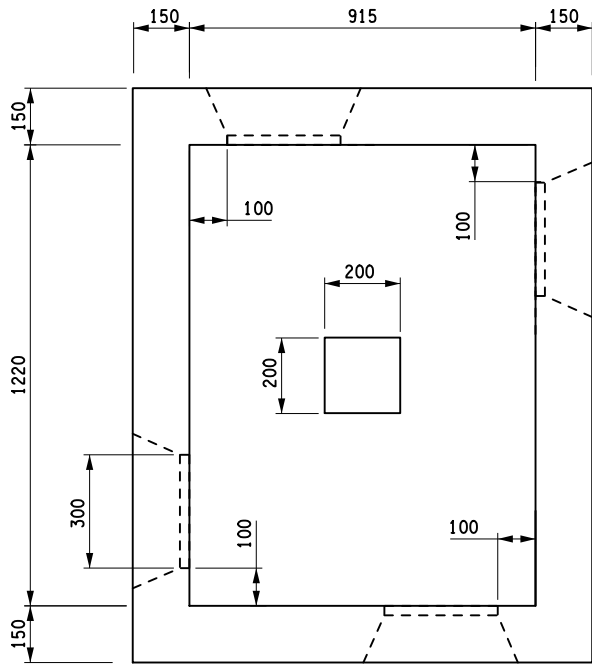
SHEET NO. 61

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

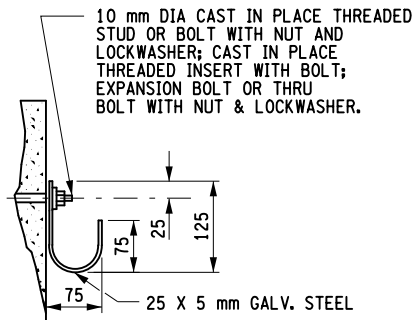
NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11

DOCUMENT NAME: x80649_cph_gnd_dtl_08.dgn

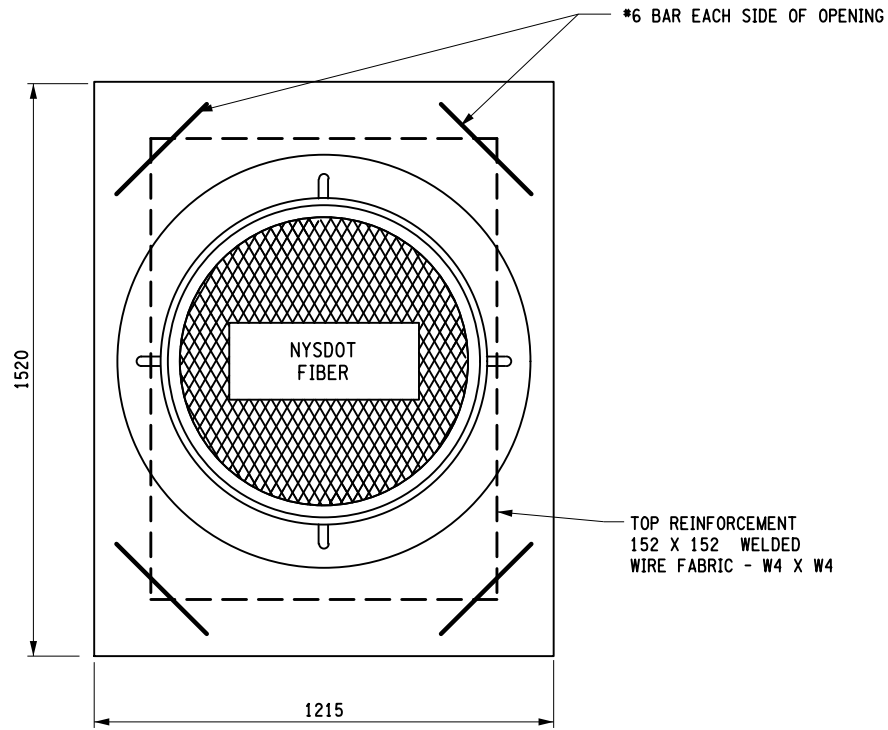




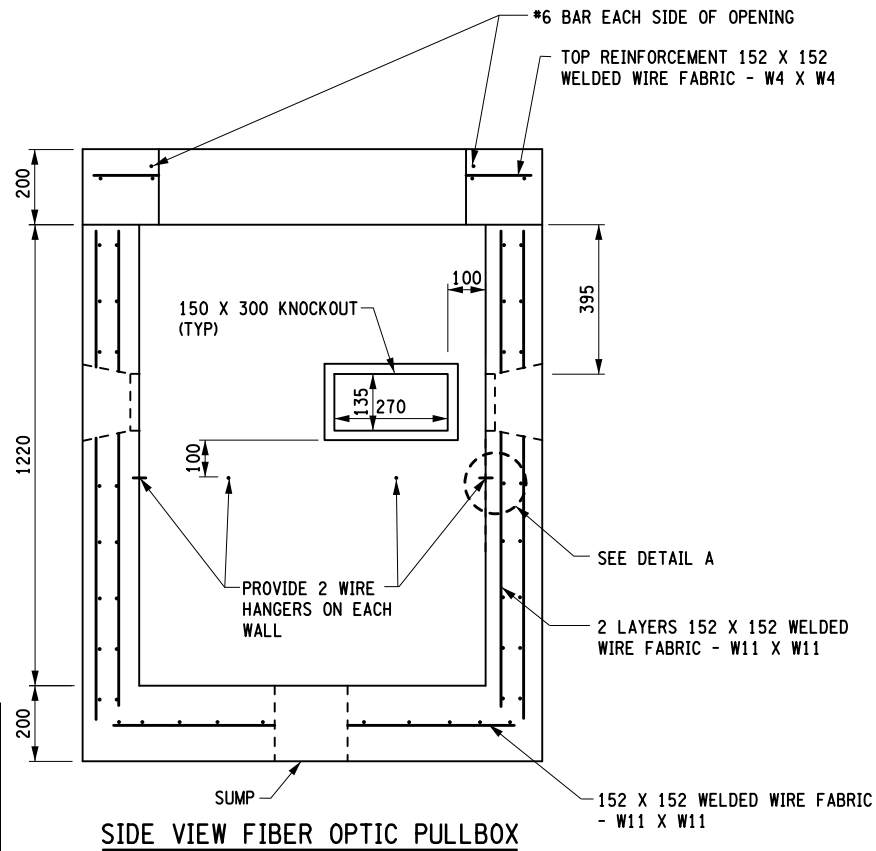
TOP VIEW FIBER OPTIC PULLBOX



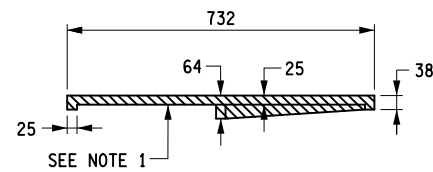
WIRE HANGER - DETAIL A



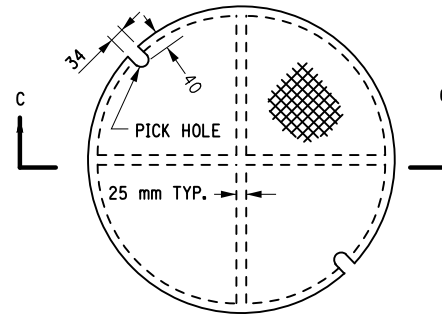
FIBER OPTIC PULLBOX TOP SLAB



SIDE VIEW FIBER OPTIC PULLBOX

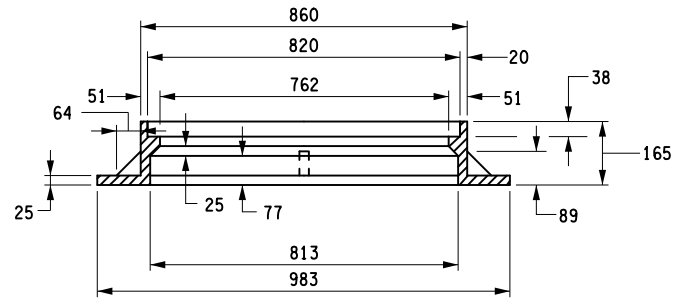


SECTION C-C

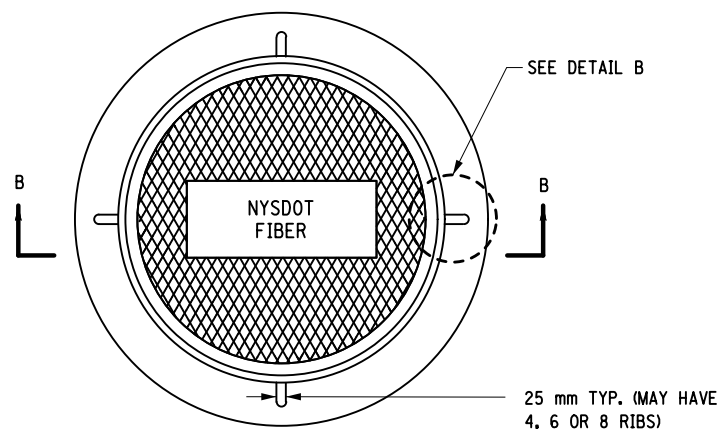


COVER UNDERSIDE DETAILS

APPROX. MASS 110 kg



SECTION B-B



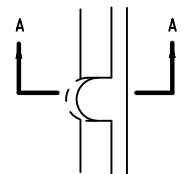
COVER DETAILS - FIBER OPTIC PULLBOX (TYPE 'F1')

NOTES:

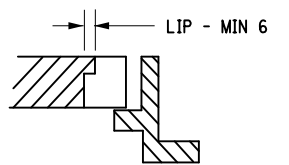
- PULLBOX COVERS MAY BE DESIGNED WITH OR WITHOUT RIBS AND SHALL BE HEAVY DUTY TO SAFELY WITHSTAND THE AASHTO MS-23 LOADING.
- A NON-SKID TEXTURE SHALL BE CAST INTO THE TOP SURFACE OF THE COVER.
- SEE THE NYSDOT STANDARD SHEET M680-12 FOR PULLBOX, CONDUIT AND GROUND ROD INSTALLATION DETAILS. CONTRACTOR SHALL GROUND PULLBOX FRAME AND COVER AND ALL EMPTY CONDUITS ENTERING PULLBOX.
- A MINIMUM OF TWO- PENTA BOLTS SHALL BE FURNISHED TO LOCK THE PULLBOX COVERS IN PLACE. THE PULLBOXES SHALL BE CONFIGURED TO ACCOMMODATE THE PENTA-BOLTS.
- STEEL REINFORCEMENT SHALL BE PLACED WITHIN THE CENTER THIRD OF THE WALL, MINIMUM COVER SHALL BE 25 mm.
- BASE IS REQUIRED FOR ALL TYPE F1 PULLBOXES REGARDLESS OF INSTALLATION LOCATION. PULLBOX IS TO BE INSTALLED ON A 300 THICK LAYER OF NO. 2 CRUSHED STONE OR GRAVEL AS INDICATED ON STD SHT M680-12.
- PAYMENT SHALL BE MADE UNDER ITEM 680.5196--11 - FURNISH AND INSTALL CONCRETE FIBER OPTIC PULLBOX.

SPECIAL NOTE:

- THIS DRAWING IS PROVIDED FOR INFORMATION REGARDING EXISTING PULLBOXES, AS PART OF THE WORK, THE CONTRACTOR SHALL PROVIDE NEW CONDUIT ENTRIES INTO EXISTING PULLBOXES AT LOCATIONS AS INDICATED ON THE PLANS.

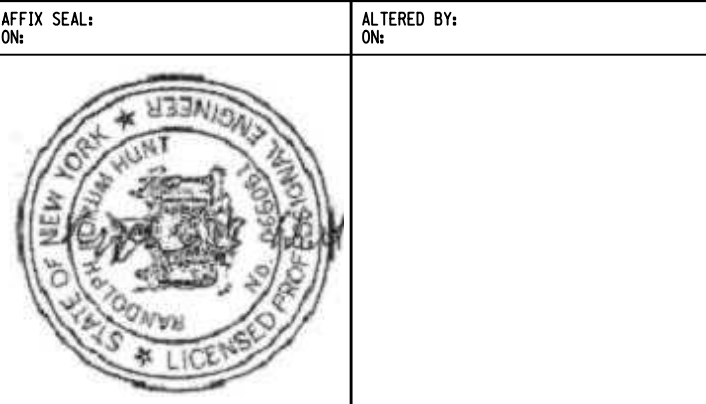


DETAIL B



SECTION A-A

LIFTING NOTCH OR EQUAL AS APPROVED BY THE EIC



AFFIX SEAL:
ON:

ALTERED BY:
ON:

AS-BUILT REVISIONS
DESCRIPTION OF ALTERATIONS:

ITS TRAVEL TIME I-278 AND I-495

PS&E DATE: 06/21/2012

COUNTY: KINGS AND QUEEN

PIN X806.49

BRIDGES

CULVERTS

ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED

REINFORCED CONCRETE PULLBOXES - 2

CONTRACT NUMBER

D262095

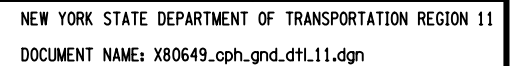
DRAWING NO. DTL-10

SHEET NO. 63

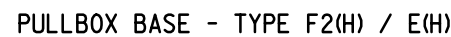
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11


DOCUMENT NAME: x80649_cph_gnd.dtl.10.dgn

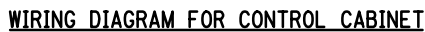
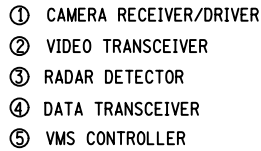


1. HEIGHT "H" WILL VARY DEPENDING ON THE SLOPE OF GRADE.
2. VERTICAL REINFORCEMENT IN PREFABRICATED BOXES SHOULD EXTEND OUT 1.675m FOR COMMUNICATIONS PULLBOX (TYPE F1) AND 760mm FOR COMMUNICATIONS (TYPE F2)/ELECTRIC (TYPE E1). BARS WILL BE CUT TO LENGTH REQUIRED IN FIELD FOR CAST-IN-PLACE CONCRETE.
3. FLANGE OF THE FRAME TO BE ANCHORED TO THE CONCRETE TOP.
4. REPLACEMENT OF PAVED AND UNPAVED AREAS SHALL MATCH EXISTING OR AS DIRECTED BY THE ENGINEER.
5. A NON-SKID TEXTURE SHALL BE CAST INTO THE TOP SURFACE OF THE COVER
6. A MINIMUM OF TWO-PENTA BOLTS SHALL BE FURNISHED TO LOCK PULLBOX, ITEM 680.5160 11, COVER IN PLACE. THE PULLBOXESSES SHALL BE CONFIGURED TO ACCOMODATE THE PENTA-BOLTS.
7. ITEM 680.51500111 AND TYPE E02 PULLBOX COVER SHALL BE BOLTED TO FRAME WITH FOUR RECESSED 13mm STAINLESS STEEL BOLTS IN DIAGONALLY OPPOSITE CORNERS OF COVER.
8. CONTRACTOR SHALL GROUND PULLBOX FRAME AND CONDUITS ENTERING THE PULLBOX.
9. PULLBOX SHALL CONTAIN STAINLESS STEEL CABLE SUPPORT HOOKS WITH LATCH RETAINER FOR LOOPING AND SECURING CABLE IN PLACE.
10. CONSTRUCTOR SHALL INSTALL GROUND ROD THROUGH THE CENTER OF PULLBOX DRAIN HOLE LOCATED AT THE MIDDLE OF BASE.
11. WIRING OTHER THEN GROUND WIRES, SHALL BE SECURED TO CABLE SUPPORT HOOKS AND HELD CLEAR OF BOTTOM OF PULLBOX.
12. CONTRUCTOR SHALL PROVIDE 10mm BOLTS TO SECURE CABLE SUPPORT HOOKS TO PULLBOX WALLS.
13. ALL EXCAVATION, RESTORATION AND BROKEN STONE SHALL BE PAID FOR UNDER RESPECTIVE PULLBOX ITEM.



COMMUNICATIONS PULLBOX - TYPE F2(H)
OR
ELECTRICAL PULLBOX - TYPE E(H)

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	ITS TRAVEL TIME I-278 AND I-495	PIN X806.49	BRIDGES	CULVERTS	ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED	CONTRACT NUMBER	
	PS&E DATE: 06/21/2012				PULLBOX ON STEEP SLOPE	D262095	
						DRAWING NO. DTL-12	
	COUNTY: KINGS AND QUEEN						
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY:" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.						NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11 DOCUMENT NAME: X80649_cph_gnd_dtl12.dgn	



AS-BUILT REVISIONS
DESCRIPTION OF ALTERATIONS:

ITS TRAVEL TIME I-278 AND I-495

PS&E DATE: 06/21/2012

COUNTY: KINGS AND QUEEN

PIN X806.49

BRIDGES

CULVERTS

ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED

CONTRACT NUMBER

CONTROL CABINET WIRING DIAGRAM

D262095

DRAWING NO. CCD-01

SHEET NO. 66

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NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11

DOCUMENT NAME: x80649_cph_dia_ccd_01.dgn

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DATE/TIME = 03-AUG-2012 16:12
USER = kdesai

DESIGN SUPERVISOR RANDOLPH HUNT

JOB MANAGER MAREK CMIK

DESIGN RAISA STARTSEVA

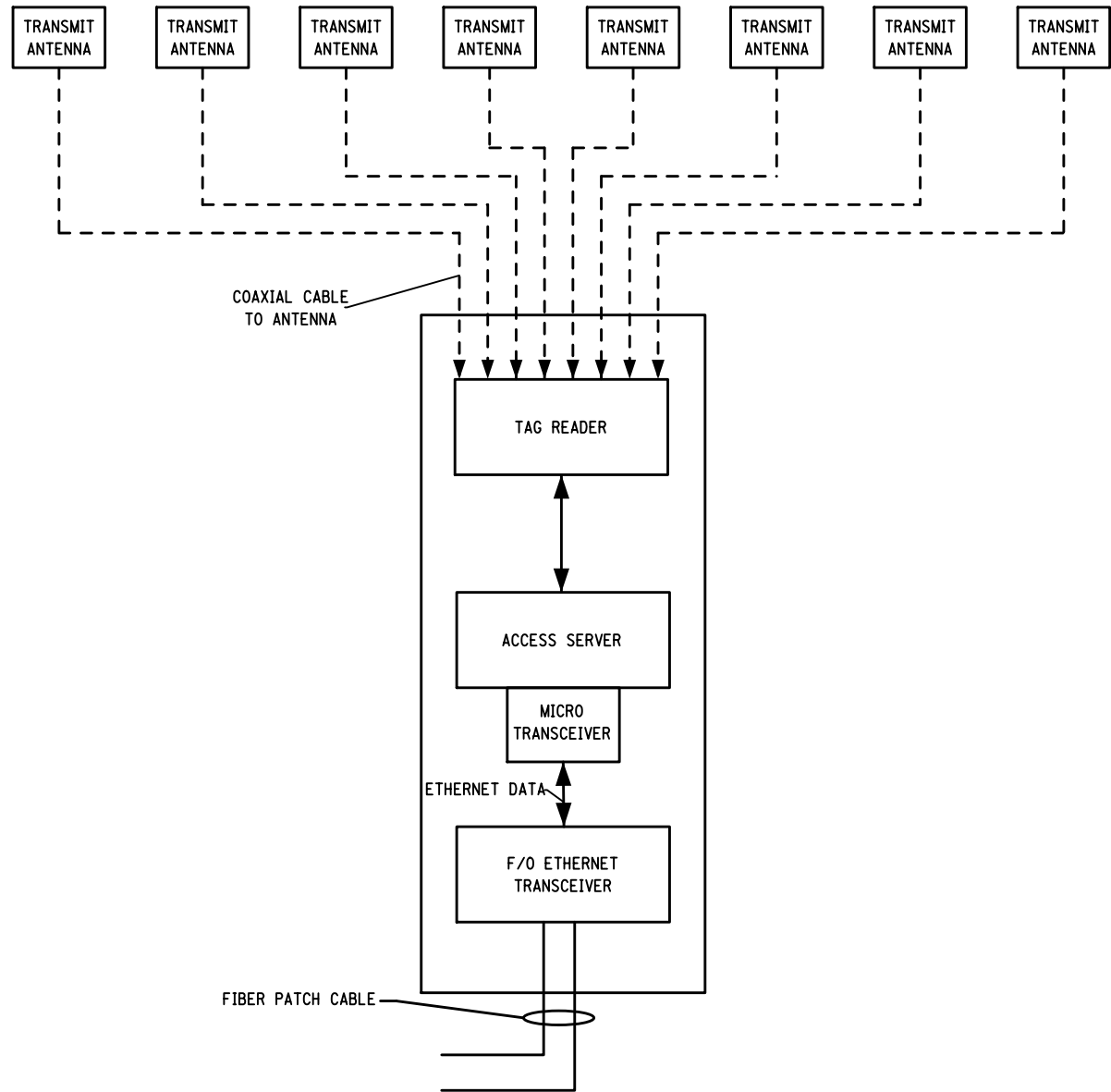
CHECK MAREK CMIK

DRAFTING RAISA STARTSEVA

CHECK MAREK CMIK

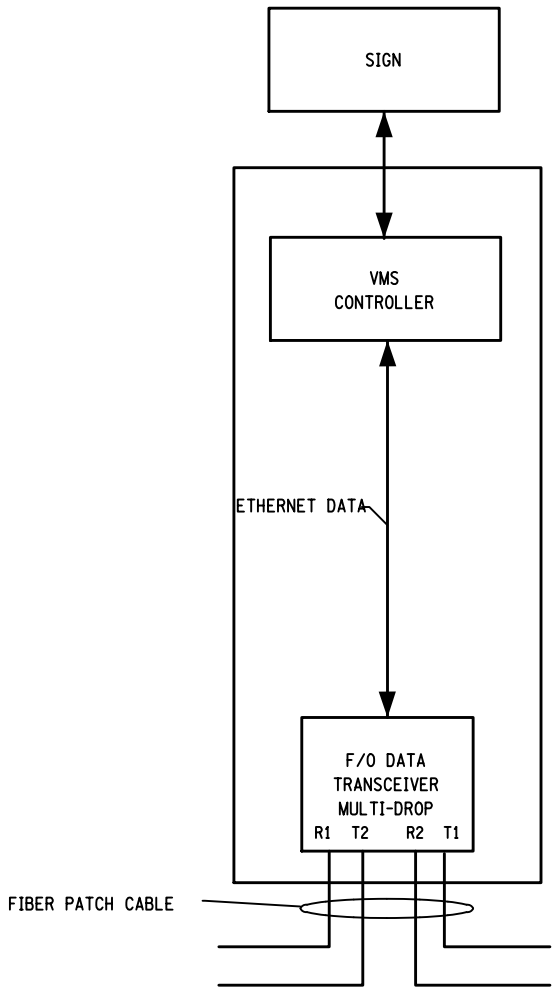
PROJECT MANAGER RAJ AMIN

TRANSMIT DETECTOR BLOCK DIAGRAM





TRANSMIT DETECTOR WITH FIBER OPTIC COMMUNICATIONS

VMS BLOCK DIAGRAM



VARIABLE MESSAGE SIGN WITH FIBER OPTIC COMMUNICATIONS

AFFIX SEAL: ON:	ALTERED BY: ON:
	

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	ITS TRAVEL TIME I-278 AND I-495	PIN X806.49	BRIDGES	CULVERTS	ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED TRANSMIT DETECTOR AND VMS BLOCK DIAG	CONTRACT NUMBER	
	PS&E DATE: 06/21/2012					D262095	
						DRAWING NO. DIA-01	
	COUNTY: KINGS AND QUEEN					SHEET NO. 67	
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.						NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11 DOCUMENT NAME: x80649_cph_dia_01.dgn	

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USER = kdesai

DESIGN SUPERVISOR RANDOLPH HUNT

JOB MANAGER MAREK CWTOK

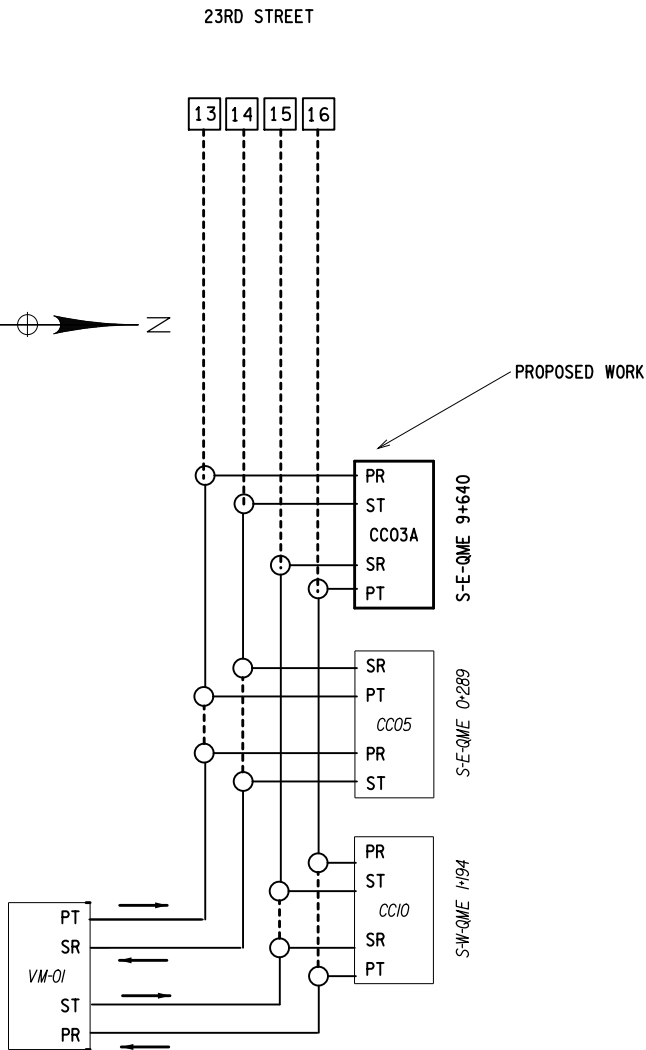
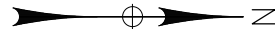
DESIGN MAREK CWTOK

CHECK MAREK CWTOK

DRAFTING RAISA STARTSEVA

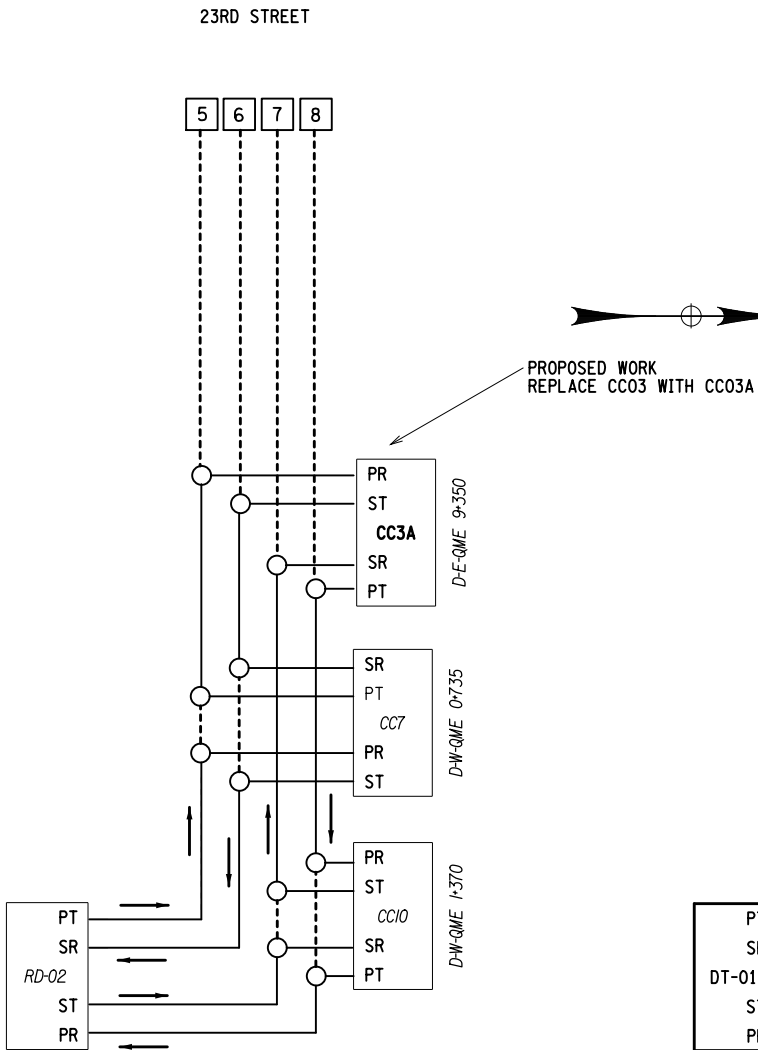
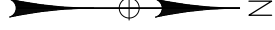
CHECK MAREK CWTOK

PROJECT MANAGER RAJ AMIN



CIRCUIT LIE-SHI-VM-01

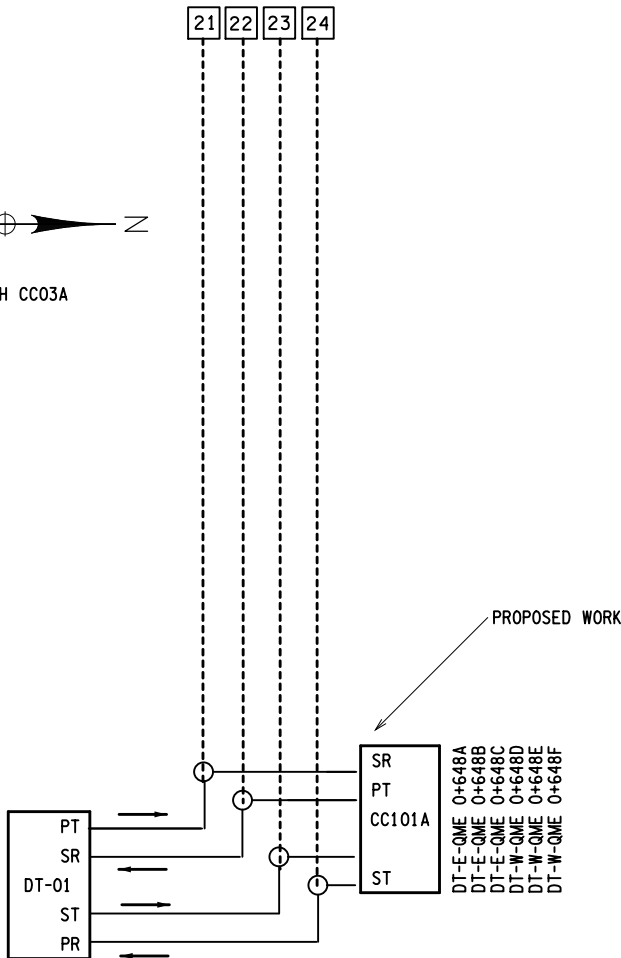
THREE FIBER OPTIC, SLAVE MODEM WITH SHR
LOCATION: DWG. CON-01
REPORTS TO: SH BQ-02



CIRCUIT LIE-SHI-RD-02

THREE FIBER OPTIC, SLAVE MODEM WITH SHR
LOCATION: DWG. CON-01
REPORTS TO: SH BQ-02

GREENPOINT AVE



CIRCUIT LIE-SH1-DT-01

ONE FIBER OPTIC, SLAVE MODEM WITH SHR
LOCATION: CON-02
REPORTS TO: SH BQ-02

NOTES:

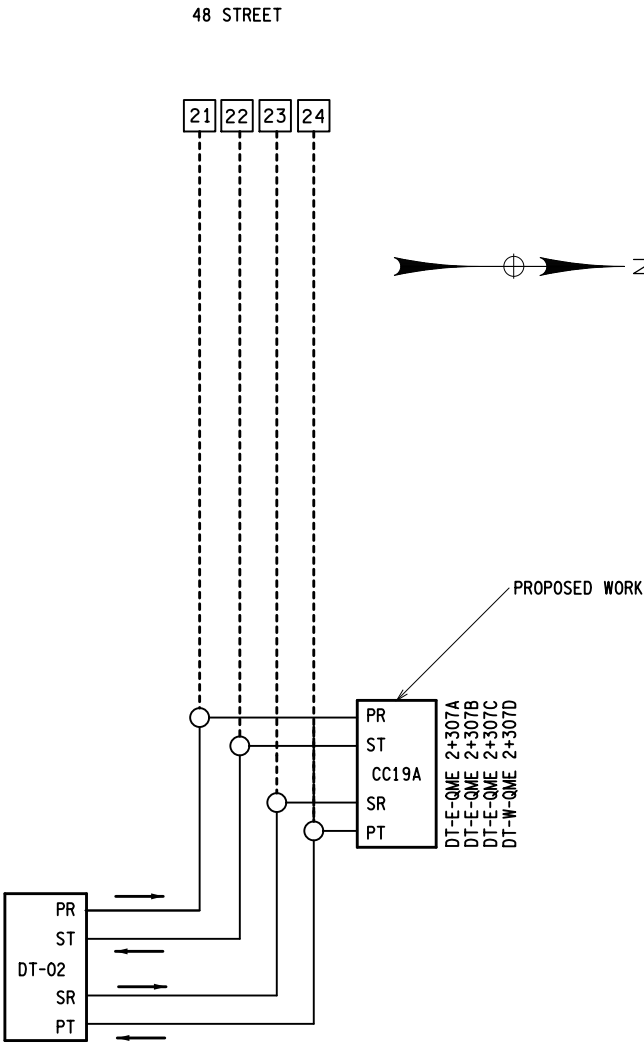
1. THIS DIAGRAM IS PROVIDED FOR ILLUSTRATION PURPOSES ONLY. IN CASE OF CONFLICT, SPECIFICATIONS TO GOVERN. ALL MULTIPLEXERS TO BE SIZED AND INSTALLED BASED ON MAXIMUM NUMBER OF CHANNELS REQUIRED.
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4. CONTRACTOR SHALL ASSIGN LOCAL ADDRESSES FOR ITS EQUIPMENT, SUBJECT TO THE APPROVAL OF THE ENGINEER.

LEGEND:

- PT - PRIMARY TRANSMIT
PR - PRIMARY RECEIVE
ST - SECONDARY TRANSMIT
SR - SECONDARY RECEIVE
- - EXISTING
■ - PROPOSED

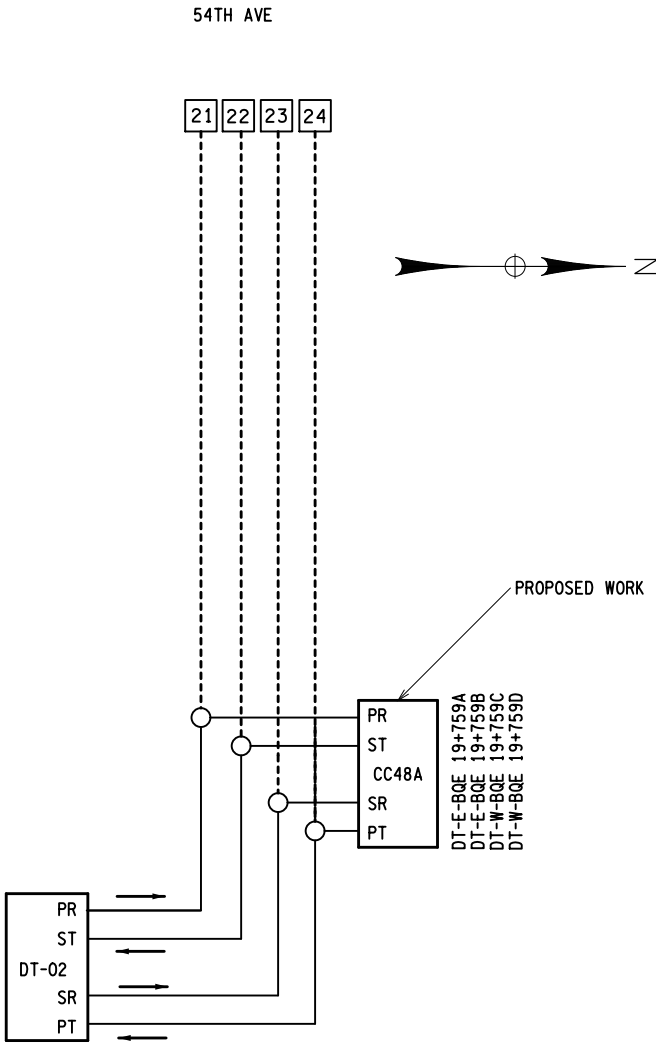
AFFIX SEAL: ON:	ALTERED BY: ON:

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	ITS TRAVEL TIME I-278 AND I-495	PIN X806.49	BRIDGES	CULVERTS	ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED	CONTRACT NUMBER	
	PS&E DATE: 06/21/2012					D262095	
						DRAWING NO. FCD-01	
	COUNTY: KINGS AND QUEEN					SHEET NO. 68	
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.						NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11 DOCUMENT NAME: X80649_cph_1fd_fcd_01.dgn	



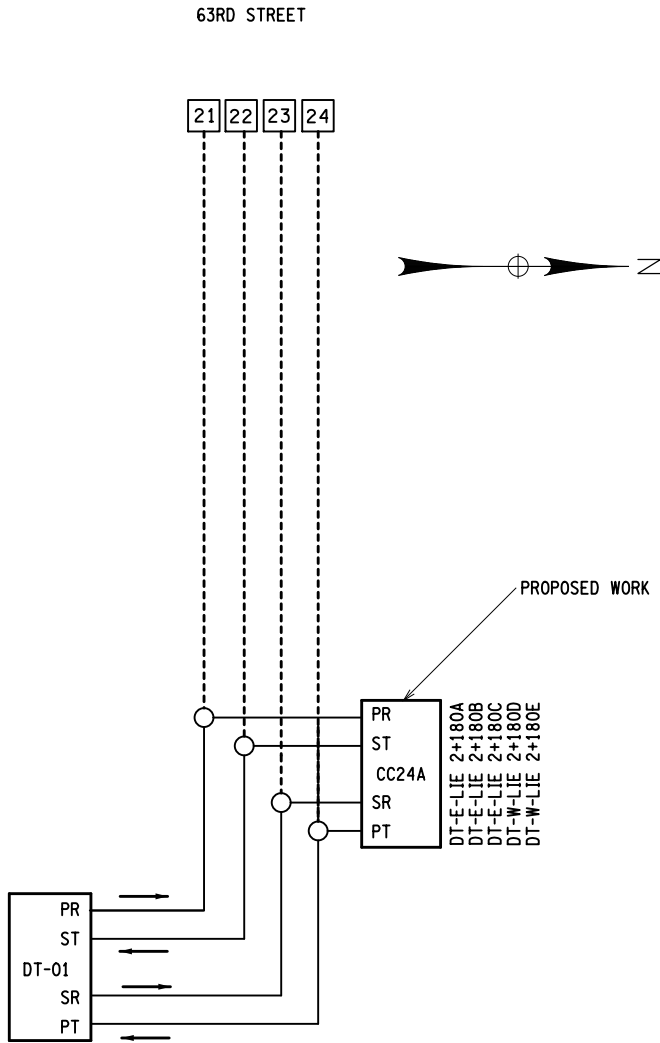
CIRCUIT LIE-SH1-DT-02

ONE FIBER OPTIC, SLAVE MODEM WITH SHR
LOCATION: DWG. CON-04
REPORTS TO: SH BQ-02



CIRCUIT BQE-SH1-DT-01

ONE FIBER OPTIC, SLAVE MODEM WITH SHR
LOCATION: CON-07
REPORTS TO: SH BQ-02



CIRCUIT LIE-MH2-DT-01

ONE FIBER OPTIC, SLAVE MODEM WITH SHR
LOCATION: DWG. CON-05
REPORTS TO: MINIHUB LIE-2

NOTES:

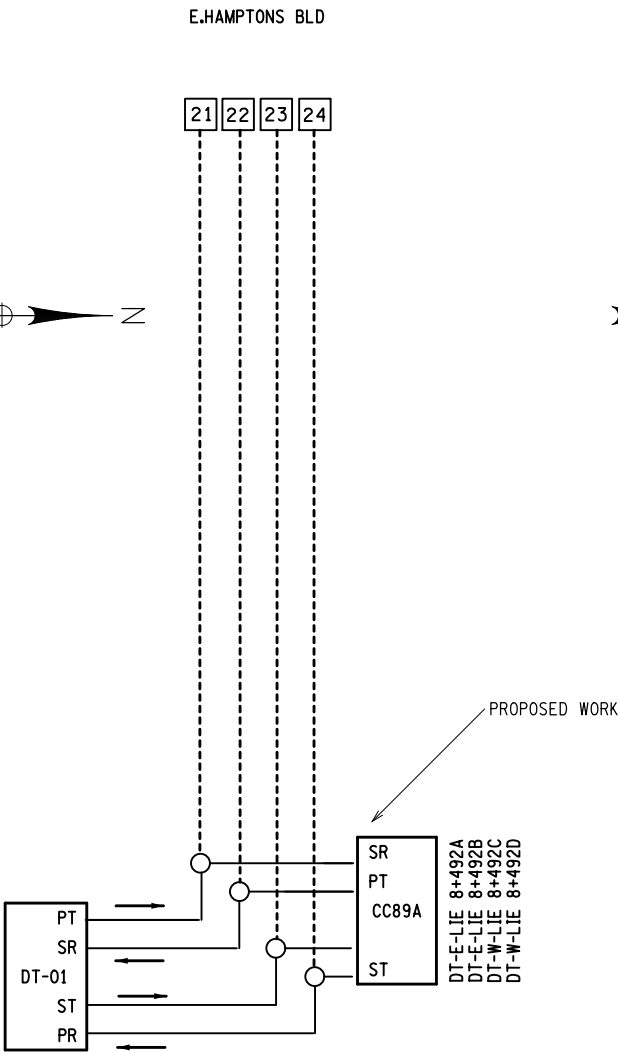
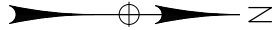
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4. CONTRACTOR SHALL ASSIGN LOCAL ADDRESSES FOR ITS EQUIPMENT, SUBJECT TO THE APPROVAL OF THE ENGINEER.
5. THIS DRAWING CONTAINS PART OF PS&E PACKAGE OF X22865, D259712, AND IS PROVIDED FOR INFORMATION ONLY.
6. BOTH RACKS HAVE 37 RU USABLE SPACE, NOT VERIFIED IN THE FIELD.

LEGEND:

- PT - PRIMARY TRANSMIT
PR - PRIMARY RECEIVE
ST - SECONDARY TRANSMIT
SR - SECONDARY RECEIVE
□ - EXISTING

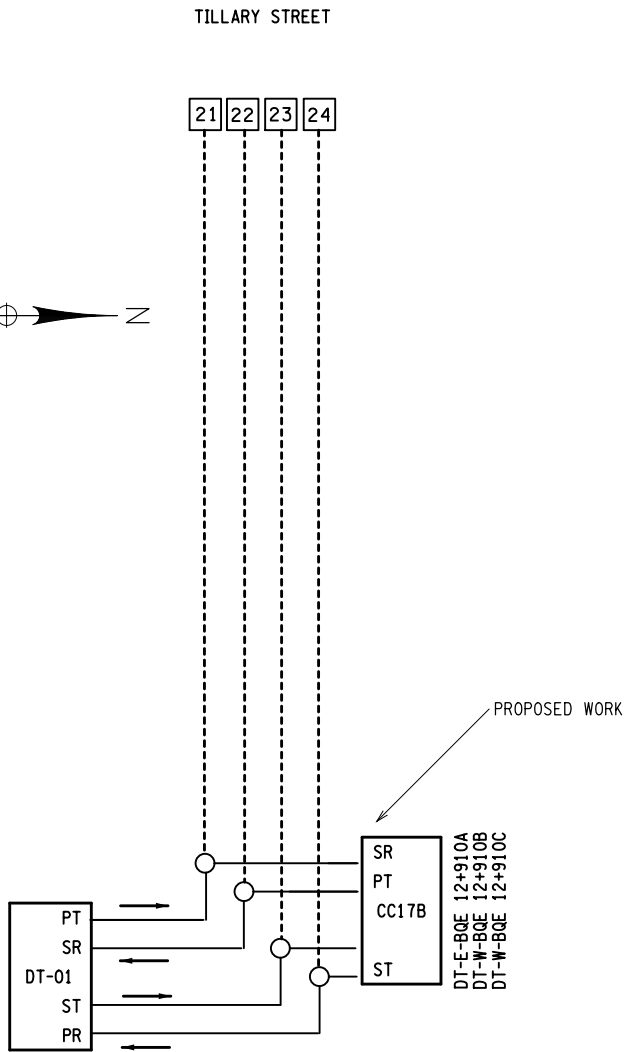
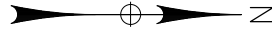
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AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	ITS TRAVEL TIME I-278 AND I-495	PIN X806.49	BRIDGES	CULVERTS	ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED	CONTRACT NUMBER	
	PS&E DATE: 06/21/2012					D262095	
						DRAWING NO. FCD-02	
	COUNTY: KINGS AND QUEEN					SHEET NO. 69	
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CIRCUIT LIE-MH7-DT-01

ONE FIBER OPTIC, SLAVE MODEM WITH SHR
LOCATION: CON-05
REPORTS TO: LIE-MH-LIE-07



CIRCUIT BQE-MH1-DT-01

ONE FIBER OPTIC, SLAVE MODEM WITH SHR
LOCATION: CON-06
REPORTS TO: BQE MH-1

NOTES:

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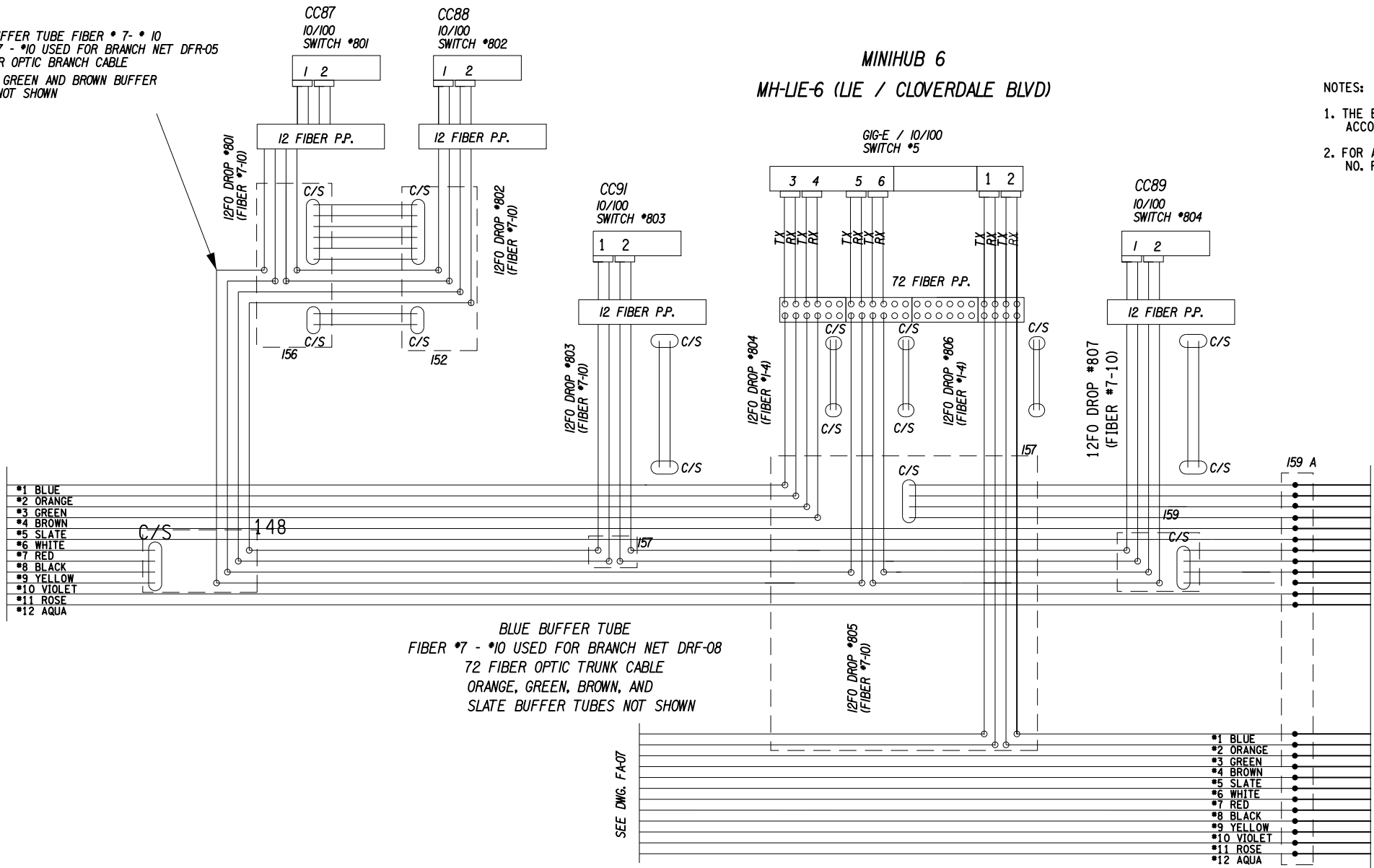
LEGEND:

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PR - PRIMARY RECEIVE
ST - SECONDARY TRANSMIT
SR - SECONDARY RECEIVE
- EXISTING
 - PROPOSED

AFFIX SEAL: ON:		ALTERED BY: ON:															
		AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:		ITS TRAVEL TIME I-278 AND I-495		PIN X806.49		BRIDGES		CULVERTS		ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED		CONTRACT NUMBER			
				PS&E DATE: 06/21/2012								FIBER CIRCUIT DIAGRAMS - 3		D262095			
														DRAWING NO. FCD-03			
				COUNTY: KINGS AND QUEEN										SHEET NO. 70			
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.																	
NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11 DOCUMENT NAME: X80649_cph_1.fcd_03.dgn																	

BL	=	BLUE	RD	=	RED
O	=	ORANGE	BK	=	BLACK
G	=	GREEN	Y	=	YELLOW
BR	=	BROWN	V	=	VIOLET
S	=	SLATE	RS	=	ROSE
W	=	WHITE	A	=	AQUA
C/S	=	COILED AND SPARED			
●	=	SPLICE POINT PROPOSED			
○	=	SPLICE POINT EXISTING			
TX	=	TRANSMIT			
RX	=	RECEIVE			

SEE DWG. FA-09



1. THE BASIC FIBER COLOR CONFIGURATION SHALL BE IN ACCORDANCE WITH EIA/TIA-598.
2. FOR ADDITIONAL INFORMATION REFER TO ITS DRAWINGS NO. FSD-06-01, FSD-07-01 AND FSD-07.

SEE DWG. FSD-07

WHITE BUFFER TUBE
FIBER #1 - #4 USED FOR GIGABIT NETWORK
72 FIBER OPTIC TRUNK CABLE



CONTRACT NUMBER	
-----------------	--

PS&E DATE: 06/21/2012

D262095

COUNTY: KINGS AND QUEEN

RAWING NO. FSD-01

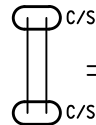
HEET NO. 71

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11

DOCUMENT NAME: x80649_cph_itd_fsd-01.dgn

SYMBOL



SYMBOL LEGEND

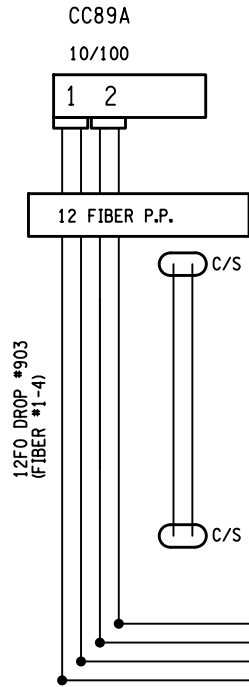
EX = EXISTING
PB = PULLBOX
F = FIBER
VIDS = VIDEO DETECTION
— = PROPOSED FIBER OPTIC CABLE
— = EXISTING FIBER OPTIC CABLE

FIBER LEGEND

BL = BLUE RD = RED
O = ORANGE BK = BLACK
G = GREEN Y = YELLOW
BR = BROWN V = VIOLET
S = SLATE RS = ROSE
W = WHITE A = AQUA

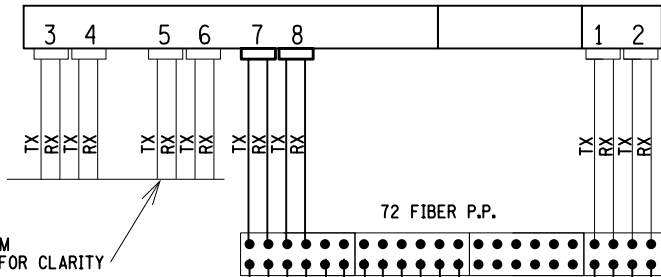
C/S = COILED AND SPARED
• = SPLICE POINT
TX = TRANSMIT
RX = RECEIVE

SEE DWG. FSD-06



EXISTING CONNECTION TO INFORM
FIBERS TO REMAIN NOT SHOWN FOR CLARITY
SEE DRAWING NO. FSD-07-01

MINIHUB 7
MH-LIE-7 (LIE / CIP)



REMOVE EXISTING INFORM CABLE AND CONNECT
TO 72 DISTRIBUTION FIBER OPTIC TRUNK CABLE

BLUE BUFFER TUBE
72 FIBER OPTIC TRUNK CABLE

ORANGE, GREEN, BROWN, AND
SLATE BUFFER TUBES NOT SHOWN

WHITE BUFFER TUBE
FIBER #1 - #4 USED FOR GIGABIT NETWORK
72 FIBER OPTIC TRUNK CABLE

ITS TRAVEL TIME I-278 AND I-495
PS&E DATE: 06/21/2012

- NOTES:
1. THE BASIC FIBER COLOR CONFIGURATION SHALL BE IN ACCORDANCE WITH EIA/TIA-598.
 2. FOR ADDITIONAL INFORMATION REFER TO ITS DRAWINGS NO. FSD-06-01, FSD-07-01 AND FSD-06.

AFFIX SEAL: ON:	ALTERED BY: ON:

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	ITS TRAVEL TIME I-278 AND I-495	PIN X806.49	BRIDGES	CULVERTS	ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED FIBER SPLICE DIAGRAM - LIE MH-7 PROP	CONTRACT NUMBER	
	PS&E DATE: 06/21/2012					D262095	
						DRAWING NO. FSD-02	
	COUNTY: KINGS AND QUEEN					SHEET NO. 72	
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.						NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11 DOCUMENT NAME: x80649_cph.ltd.fsd-02.dgn	

FILE NAME = x80649_cph_ltd_fsd-03.dgn
DATE/TIME = 03-AUG-2012 16:12
USER = kdesai

DESIGN SUPERVISOR RANDOLPH HUNT

JOB MANAGER MAREK CWTOK

DESIGN MAREK CWTOK

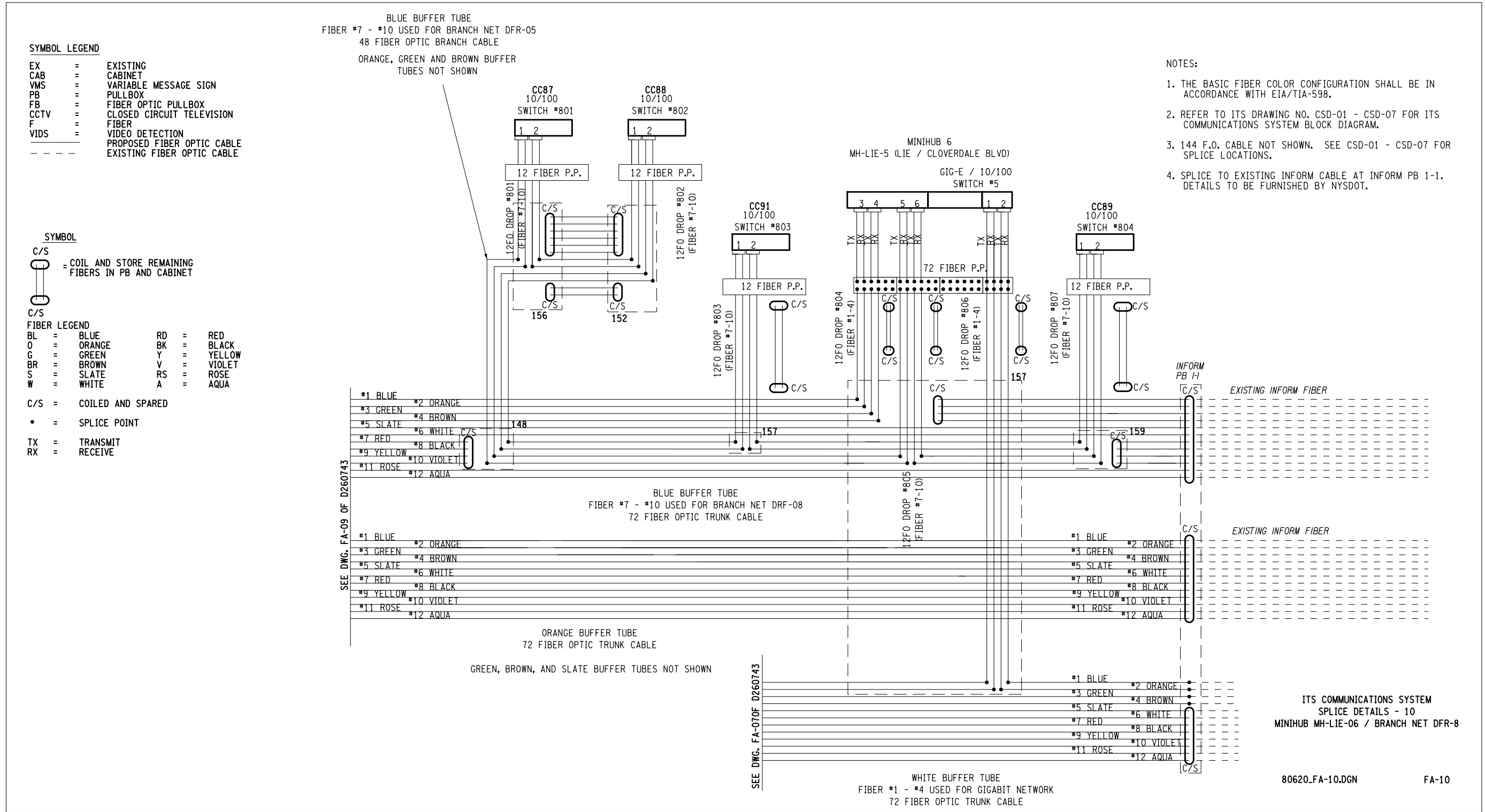
CHECK MAREK CWTOK

DRAFTING RAISA STARTSEVA

CHECK MAREK CWTOK

PROJECT MANAGER RAJ AMIN

FOR INFORMATION ONLY



- NOTES:
1. THE BASIC FIBER COLOR CONFIGURATION SHALL BE IN ACCORDANCE WITH EIA/TIA-598.
 2. REFER TO ITS DRAWING NO. CSD-01 - CSD-07 FOR ITS COMMUNICATIONS SYSTEM BLOCK DIAGRAM.
 3. 144 F.O. CABLE NOT SHOWN. SEE CSD-01 - CSD-07 FOR SPLICE LOCATIONS.
 4. SPLICE TO EXISTING INFORM CABLE AT INFORM PB 1-1. DETAILS TO BE FURNISHED BY NYSDOT.

- NOTES:
1. THIS DRAWING WAS EXTRACTED FROM CONTRACT NO. D 260743
DRAWING NO. FA-10 SHEET NO. 114



ALTERED BY:
ON:

AS-BUILT REVISIONS
DESCRIPTION OF ALTERATIONS:

ITS TRAVEL TIME I-278 AND I-495

PS&E DATE: 06/21/2012

COUNTY: KINGS AND QUEEN

PIN X806.49

BRIDGES

CULVERTS

ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED

CONTRACT NUMBER

FIBER SPLICE DIAGRAM - LIE MH-6 EXIS

D262095

DRAWING NO. FSD-03

SHEET NO. 73

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11
DOCUMENT NAME: x80649_cph_ltd_fsd-03.dgn

FILE NAME = x80649_cph.ltd.fsd-04.dgn
DATE/TIME = 03-AUG-2012 16:12
USER = kdesai

DESIGN SUPERVISOR RANDOLPH HUNT

JOB MANAGER MAREK CWIOK

DESIGN MAREK CWIOK

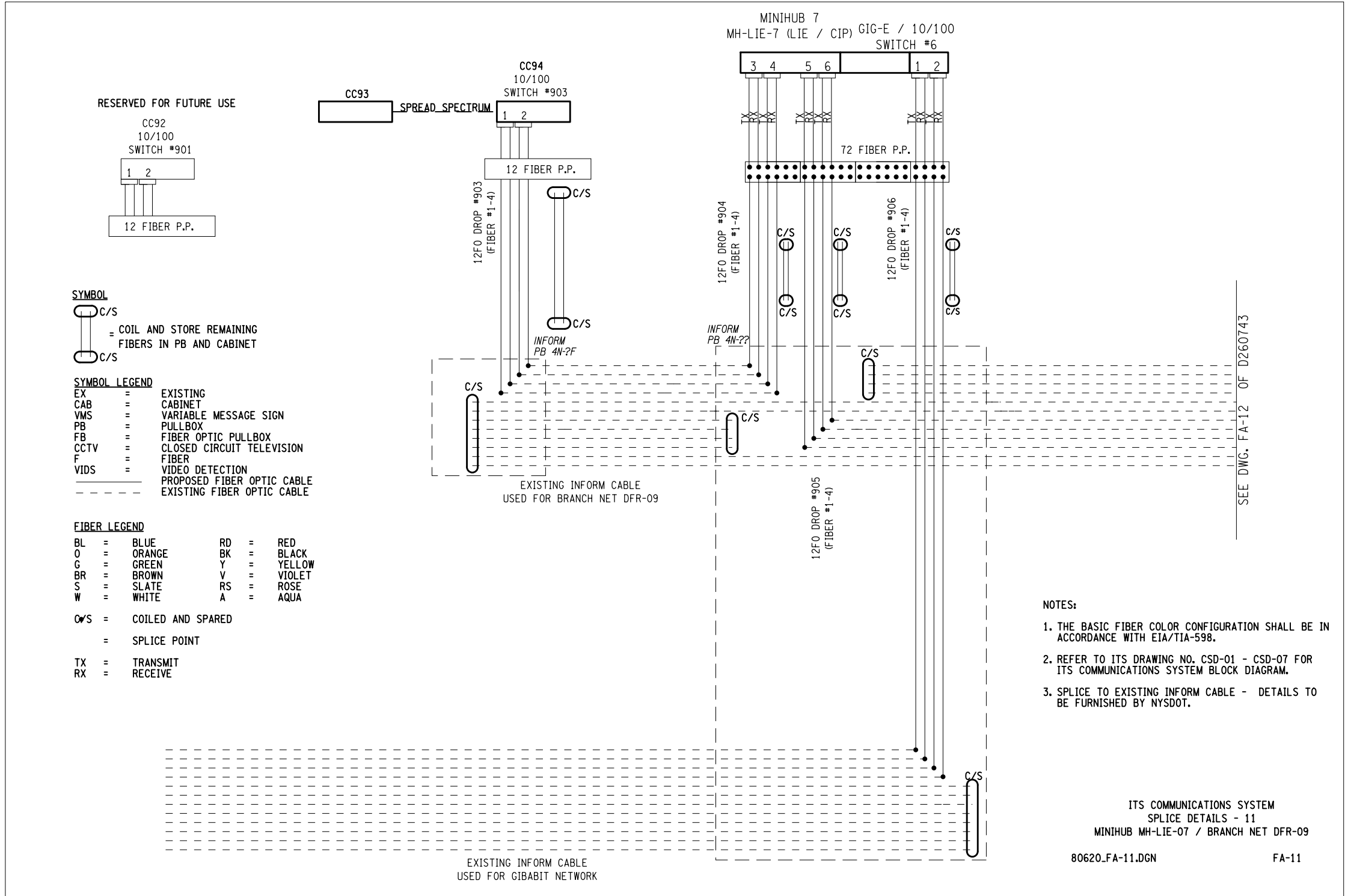
CHECK MAREK CWIOK

DRAFTING RAISA STARTSEVA


CHECK MAREK CWIOK

PROJECT MANAGER RAJ AMIN

FOR INFORMATION ONLY



AFFIX SEAL: ON:	ALTERED BY: ON:

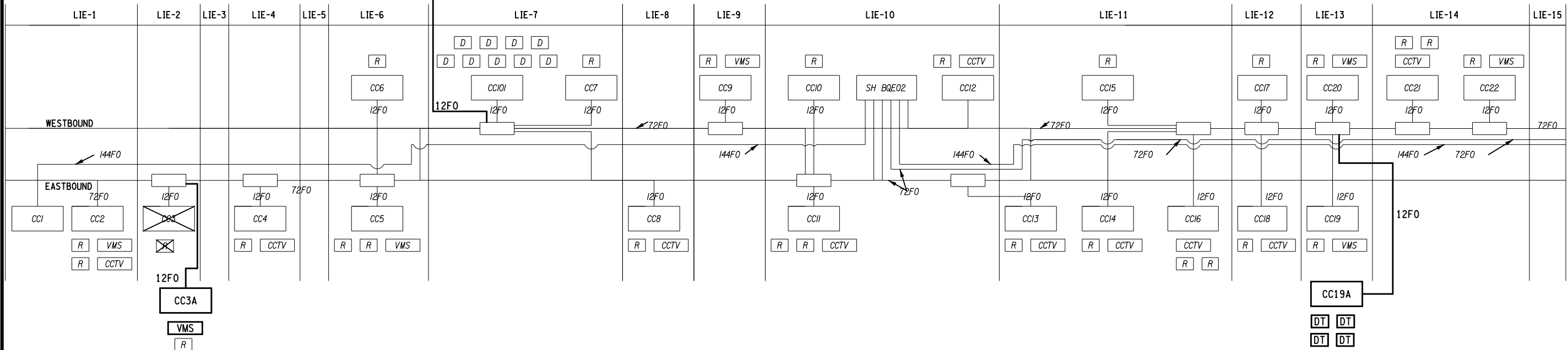
AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	ITS TRAVEL TIME I-278 AND I-495	PIN X806.49	BRIDGES	CULVERTS	ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED	CONTRACT NUMBER	
	PS&E DATE: 06/21/2012				FIBER SPLICE DIAGRAM - LIE MH-7 EXIS	D262095	
						DRAWING NO. FSD-04	
						SHEET NO. 74	
	COUNTY: KINGS AND QUEEN						
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.						NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11 DOCUMENT NAME: x80649_cph_ltd.fsd-04.dgn	


NOTES:


1. THIS DRAWING WAS EXTRACTED FROM CONTRACT NO. D 260743
DRAWING NO. FA-11 SHEET NO. 114

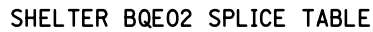
A diagram showing a 3x3 grid of blocks, each labeled 'DT'. Below the grid is a larger block labeled 'CC101A'. A line connects the bottom-left 'DT' block to the 'CC101A' block.

CC NO.	CONTROL CABINET	CC NO.	CONTROL CABINET
VMS	VMS SIGN	SH NO.	COMMUNICATIONS SHELTER
DT	TRANSMIT DETECTOR	CCTV	CCTV CAMERA
R	RADAR DETECTOR	D	CLASSIFICATION DETECTOR
XX	EXISTING		

[illegible]

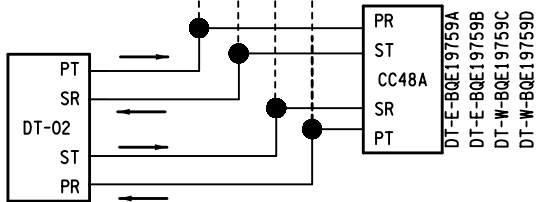
AFFIX SEAL: ON:	ALTERED BY: ON:
 <p>The seal is circular with a double-lined border. The outer ring contains the text "STATE OF NEW YORK" at the top and "LICENSED PROFESSIONAL ENGINEER" at the bottom, separated by two stars. The inner circle features a shield with a plow, a sheaf of wheat, and a sailing ship. Above the shield is an eagle with spread wings. The words "EUREKA HUNT" are inscribed in an arc above the shield, and "RANDOLPH" is inscribed in an arc below it.</p>	

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	ITS TRAVEL TIME I-278 AND I-495	PIN X80649	BRIDGES	CULVERTS	ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED	CONTRACT NUMBER	
	PS&E DATE: 06/21/2012				FIBER OPTIC SPLICING TABLE - 1	D262095	
						DRAWING NO. FST-01	
						SHEET NO. 75	
	COUNTY: KINGS AND QUEEN						
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.					NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11 DOCUMENT NAME: x80649_cph.itd.fst.01.dgn		



21 22 23 24

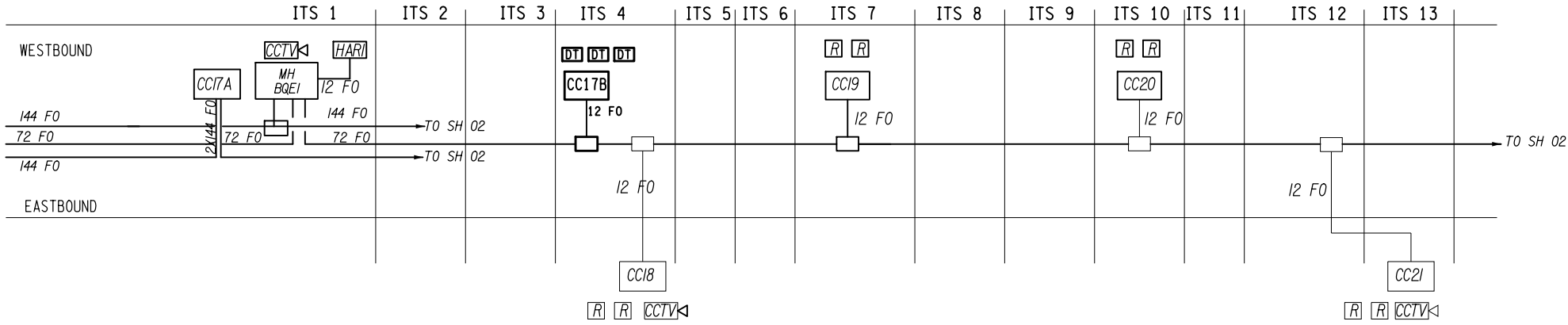
2. FOR SUMMARY OF ITS COMMUNICATION EQUIPMENTS SEE DWG. ITT-06.



ONE FIBER OPTIC SLAVE MODEM WITH SHR
AT BQE AND 54TH AVE

SHEET NO. 76

NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11
DOCUMENT NAME: x80649_cph_itd_fst_02.dgn



MINIHUB BQE1 SPLICE TABLE

FIBER No		FUNCTION	APPLICATION	D-E-BQE1/2985A D-E-BQE1/2985B C-E-BQE1/2985	D-W-BQE1/3470A D-W-BQE1/3470B	D-W-BQE1/4070A D-W-BQE1/4070B	D-W-BQE1/4325A D-W-BQE1/4325B C-E-BQE1/4325	DT-E-BQE12909A DT-W-BQE12909B DT-W-BQE12909C
1	BLUE	PRI TX	DATA CIRCUIT *1 USUALLY VEHICLE DETECTOR	3W	7W	3W	7W	21W
2	ORANGE	SEC RX		4W	8W	4W	8W	22W
3	GREEN	PRI RX		3E	7E	1W	5W	23W
4	BROWN	SEC TX		4E	8E	2W	6W	24W
5	SLATE	PRI TX	DATA CIRCUIT *2 USUALLY MESSAGE SIGN					
6	WHITE	SEC RX						
7	RED	PRI RX						
8	BLACK	SEC TX						
9	YELLOW	VIDEO	CCTV VIDEO					
10	VIOLET	VIDEO BU						
11	ROSE	VIDEO	CCTV VIDEO	25E			26E	
12	AQUA	VIDEO BU		25W			26W	
				CC18	CC19	CC20	CC21	CC17B

EQUIPMENT

- DT

TRANSMIT DETECTOR
- CC

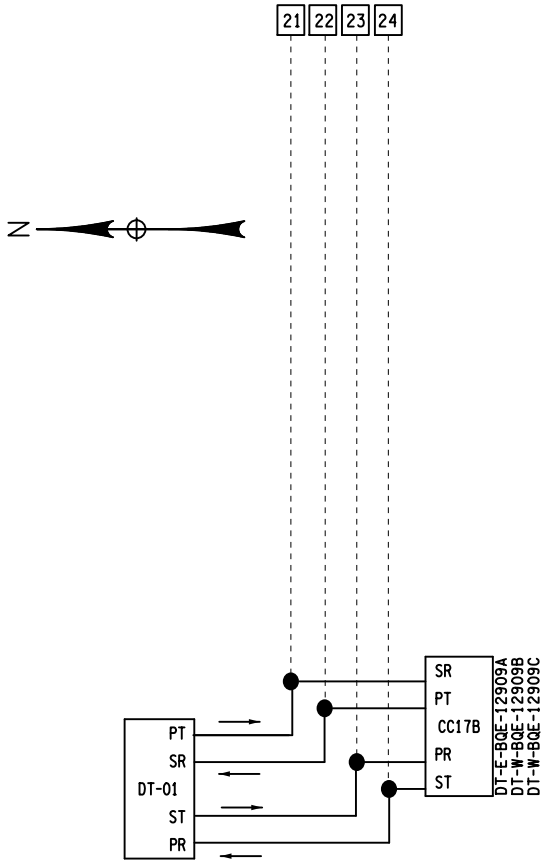
CONTROL CABINET
- PROPOSED
- CCTV

CCTV CAMERA
- R

RADAR DETECTOR
- CC

CONTROL CABINET
- MH


MINI HUB
- EXISTING



CIRCUIT BQE-H1-DT-01
ONE FIBER OPTIC, SLAVE MODEM WITH SHR

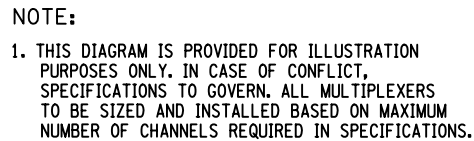
AFFIX SEAL:
ON:

ALTERED BY:
ON:


AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	ITS TRAVEL TIME I-278 AND I-495	PIN X806.49	BRIDGES	CULVERTS	ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED	CONTRACT NUMBER	
	PS&E DATE: 06/21/2012				FIBER OPTIC SPLICING TABLE - 3 MINIHUB BQE1	D262095	
						DRAWING NO. FST-03	
						SHEET NO. 77	
	COUNTY: KINGS AND QUEEN						

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11
DOCUMENT NAME: x80649_cph_ltdfst_03.dgn

[illegible]

SH NO.	COMMUNICATIONS SHELTER
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AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	ITS TRAVEL TIME I-278 AND I-495	PIN X806.49	BRIDGES	CULVERTS	ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED	CONTRACT NUMBER	
	PS&E DATE: 06/21/2012				FIBER OPTIC SPLICING TABLE - 4	D262095	
						DRAWING NO. FST-04	
	COUNTY: KINGS AND QUEEN					SHEET NO. 78	

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11
DOCUMENT NAME: x80649_cph.ltd.fst.04.dgn



1. THIS DIAGRAM IS PROVIDED FOR ILLUSTRATION PURPOSES ONLY. IN CASE OF CONFLICT, SPECIFICATIONS TO GOVERN. ALL MULTIPLEXERS TO BE SIZED AND INSTALLED BASED ON MAXIMUM NUMBER OF CHANNELS REQUIRED.
2. TIME SLOTS IN TABLE REPRESENT BANDWIDTH ALLOCATIONS, NOT SPECIFIC TIME SLOTS.
3. FOR SITES REQUIRING MULTIPLE CONNECTIONS TO A FIBER OPTIC TRANSCEIVER, THE CONTRACTOR SHALL PROVIDE A PORT EXPANDER BETWEEN THE TRANSCEIVER AND THE TERMINAL EQUIPMENT.
4. CONTRACTOR SHALL ASSIGN LOCAL ADDRESSES FOR ITS EQUIPMENT, SUBJECT TO THE APPROVAL OF THE ENGINEER.
5. THIS DRAWING CONTAINS PART OF PS&E PACKAGE OF X22865, D259712, AND IS PROVIDED FOR INFORMATION ONLY. THE LOCATION OF THE LISTED EXISTING ITS EQUIPMENT WAS NOT VERIFIED IN FIELD.

* EQUIPMENT AFFECTED BY PROPOSED WORK
FIBER *S REMAIN SAME AS EXISTING.


CONNECTED DIRECTLY TO COMMUNICATION SHELTER BQE 2			
	CW QME 1545	CCTV	NIC


CONNECTED DIRECTLY TO MINIHUB LIE 2			
	CEUE 4474	CCTV	NIC

CABINET	TERMINATION	FUNCTION	FIBER
CONTROL CABINET NO. 2	RD-01 SLAVE DATA XCVR	PRI TX	1-E
		SEC RX	2-E
		SEC TX	3-E
	VM-01 MASTER DATA XCVR	PRI TX	4-E
		PRI RX	13-E
		SEC RX	14-E
		SEC TX	15-E
		PRI RX	16-E
	C-E-QME 8-980	CCTV	25-E

CONNECTED DIRECTLY TO MINIHUB LIE 3			
	CE LIE 7-475	CCTV	NIC

CABINET	TERMINATION	FUNCTION	FIBER
CONTROL CABINET NO. 43	RD-04 SLAVE DATA XCVR	PRI TX	5-W
		SEC RX	6-W
		SEC TX	7-W
	VM-02 SLAVE DATA XCVR	PRI TX	8-W
		SEC TX	9-W
		SEC RX	10-W
		SEC TX	11-W
	C-W-LIE 8-890	PRI RX	12-W
		CCTV	25-W

AFFIX SEAL: ON:	ALTERED BY: ON:
	

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	ITS TRAVEL TIME I-278 AND I-495	PIN X806.49	BRIDGES	CULVERTS	ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED	CONTRACT NUMBER	
	PS&E DATE: 06/21/2012				FIBER TERMINATION TABLE - 1	D262095	
						DRAWING NO. FTT-01	
	COUNTY: KINGS AND QUEEN					SHEET NO. 79	
<p>IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.</p>					<p>NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11</p> <p>DOCUMENT NAME: X80649_cph.ltd_ftt_01.dgn</p>		

AS-BUILT REVISIONS
DESCRIPTION OF ALTERATIONS:

ITS TRAVEL TIME I-278 AND I-495
PS&E DATE: 06/21/2012
COUNTY: KINGS AND QUEEN


PIN X806.49

BRIDGES

CULVERTS

ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED

FIBER TERMINATION TABLE - 2

CONTRACT NUMBER	
D262095	
DRAWING NO. FTT-02	
SHEET NO. 80	

CABINET	TERMINATION	FUNCTION	FIBER
MINI HUB BQE1	RDO1 MASTER DATA XCVR	PRI TX	1-W
		SEC RX	2-W
		PRI RX	4-W
		SEC TX	3-W
	RDO2 MASTER DATA XCVR	PRI TX	5-W
		SEC RX	6-W
		PRI RX	8-W
		SEC TX	7-W
	RDO3 MASTER DATA XCVR	PRI TX	1-E
		SEC RX	2-E
		PRI RX	4-E
		SEC TX	3-E
	RDO4 MASTER DATA XCVR	PRI TX	5-E
		SEC RX	6-E
		PRI RX	8-E
		SEC TX	7-E
	VMO1 MASTER DATA XCVR	PRI TX	9-W
		SEC RX	10-W
		PRI RX	12-W
		SEC TX	11-W
	VMO2 MASTER DATA XCVR	PRI TX	9-E
		SEC RX	10-E
		PRI RX	12-E
		SEC TX	11-E
	HAR01 E'NET SWITCH	OUT	13-E
	C-W-BQE11314	CCTV	25-W
	C-E-BQE11314	CCTV	26-W
	C-W-BQE11455	CCTV	27-W
	C-E-BQE11455	CCTV	28-W
	C-W-BQE12016	CCTV	29-W
	C-E-BQE12510	CCTV	30-W
	CE-W-BQE 12909	TRANSMIT	21-W
CE-W-BQE 12909	TRANSMIT	22-W	
CE-W-BQE 12909	TRANSMIT	23-W	
CE-W-BQE 12909	TRANSMIT	24-W	
C-E-BQE12986	CCTV	25-W	
C-W-BQE13616	CCTV	26-W	
C-E-BQE14433	CCTV	27-W	
C-W-BQE14858	CCTV	28-E	
C-W-BQE15062	CCTV	29-E	
C-W-BQE15294	CCTV	30-E	
C-W-BQE15494	CCTV	31-E	
C-E-BQE15776	CCTV	32-E	

TILLARY ST

CABINET	TERMINATION	FUNCTION	FIBER
COMMUNICATION SHELTER BQE 02	RD05 MASTER DATA XCVR	PRI TX	1-W
		SEC RX	2-W
		PRI RX	4-W
		SEC TX	3-W
		PRI TX	5-W
	RD06 MASTER DATA XCVR	SEC RX	6-W
		PRI RX	8-W
		SEC TX	7-W
		PRI TX	9-W
	RD07 MASTER DATA XCVR	SEC RX	10-W
		PRI RX	12-W
		SEC TX	11-W
		PRI TX	1-E
	RD08 MASTER DATA XCVR	SEC RX	2-E
		PRI RX	4-E
		SEC TX	3-E
		PRI TX	5-E
	RD09 MASTER DATA XCVR	SEC RX	6-E
		PRI RX	8-E
		SEC TX	7-E
	DT-01 MASTER DATA XCVR	PRI TX	15-W
		SEC RX	17-W
		PRI RX	18-W
		SEC TX	19-W
54 AVE	DT-02 MASTER DATA XCVR	PRI TX	21-W
		SEC RX	22-W
		SEC TX	23-W
		PRI RX	24-W
	VM03 MASTER DATA XCVR	PRI TX	13-W
		SEC RX	14-W
		PRI RX	16-W
		SEC TX	15-W
	VM04 MASTER DATA XCVR	PRI TX	13-E
		SEC RX	14-E
		PRI RX	16-E
		SEC TX	15-E
HAR2 E'NET SWITCH	OUT	17-E	
C-W-BQE15853	CCTV	25-E	
C-W-BQE15933	CCTV	26-E	
C-W-BQE16119	CCTV	27-E	
C-E-BQE16510	CCTV	28-E	
C-W-BQE17295	CCTV	29-E	
C-W-BQE18046	CCTV	30-E	
C-E-BQE18588	CCTV	31-E	
C-E-BQE19759	CCTV	32-E	
C-W-BQE20120	CCTV	33-E	
C-W-BQE20195	CCTV	25-W	
C-W-BQE21040	CCTV	26-W	
C-E-BQE0249	CCTV	27-W	
C-E-BQE0965	CCTV	28-W	
C-E-BQE1436	CCTV	29-W	
C-E-BQE1797	CCTV	30-W	

1. THIS DIAGRAM IS PROVIDED FOR ILLUSTRATION PURPOSES ONLY. IN CASE OF CONFLICT, SPECIFICATIONS TO GOVERN. ALL MULTIPLEXERS TO BE SIZED AND INSTALLED BASED ON MAXIMUM NUMBER OF CHANNELS REQUIRED.
2. TIME SLOTS IN TABLE REPRESENT BANDWIDTH ALLOCATIONS, NOT SPECIFIC TIME SLOTS.
3. FOR SITES REQUIRING MULTIPLE CONNECTIONS TO A FIBER OPTIC TRANSCEIVER, THE CONTRACTOR SHALL PROVIDE A PORT EXPANDER BETWEEN THE TRANSCEIVER AND THE TERMINAL EQUIPMENT.
4. CONTRACTOR SHALL ASSIGN LOCAL ADDRESSES FOR ITS EQUIPMENT, SUBJECT TO THE APPROVAL OF THE ENGINEER.
5. THE LOCATION OF THE LISTED EXISTING ITS EQUIPMENT WAS NOT VERIFIED IN FIELD.

FILE NAME = x80649_cph_utd_mod.01.dgn
DATE/TIME = 03-AUG-2012 16:12
USER = kdesai

DESIGN SUPERVISOR RANDOLPH HUNT

JOB MANAGER MAREK CWIOK

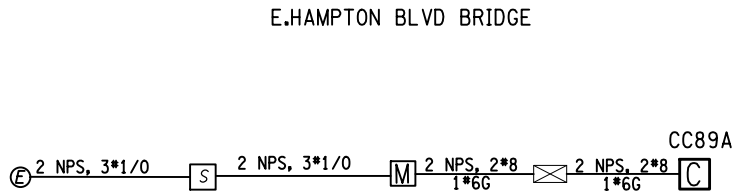
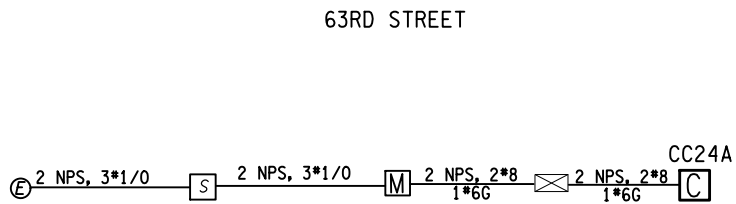
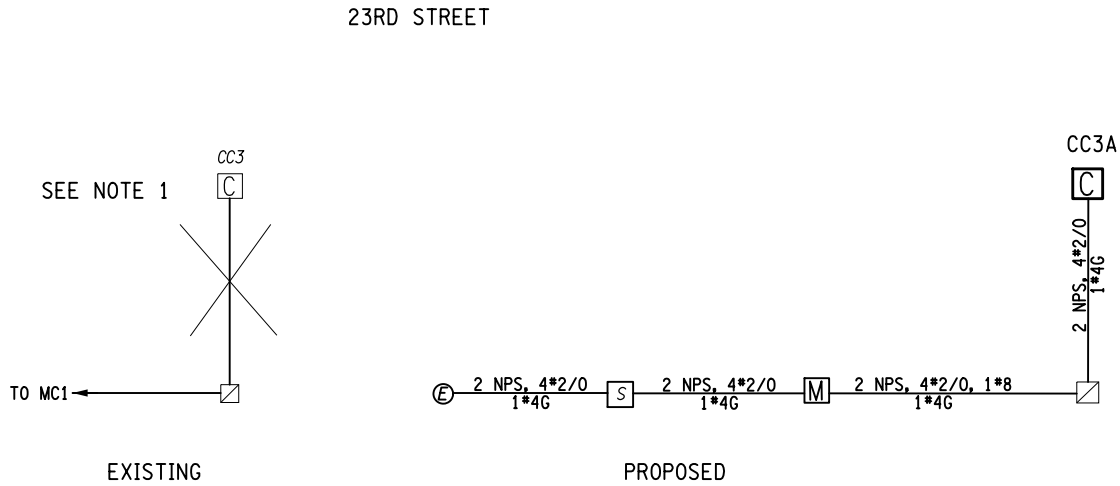
DESIGN MAREK CWIOK

CHECK MAREK CWIOK

DRAFTING RAISA STARTSEVA

CHECK MAREK CWIOK

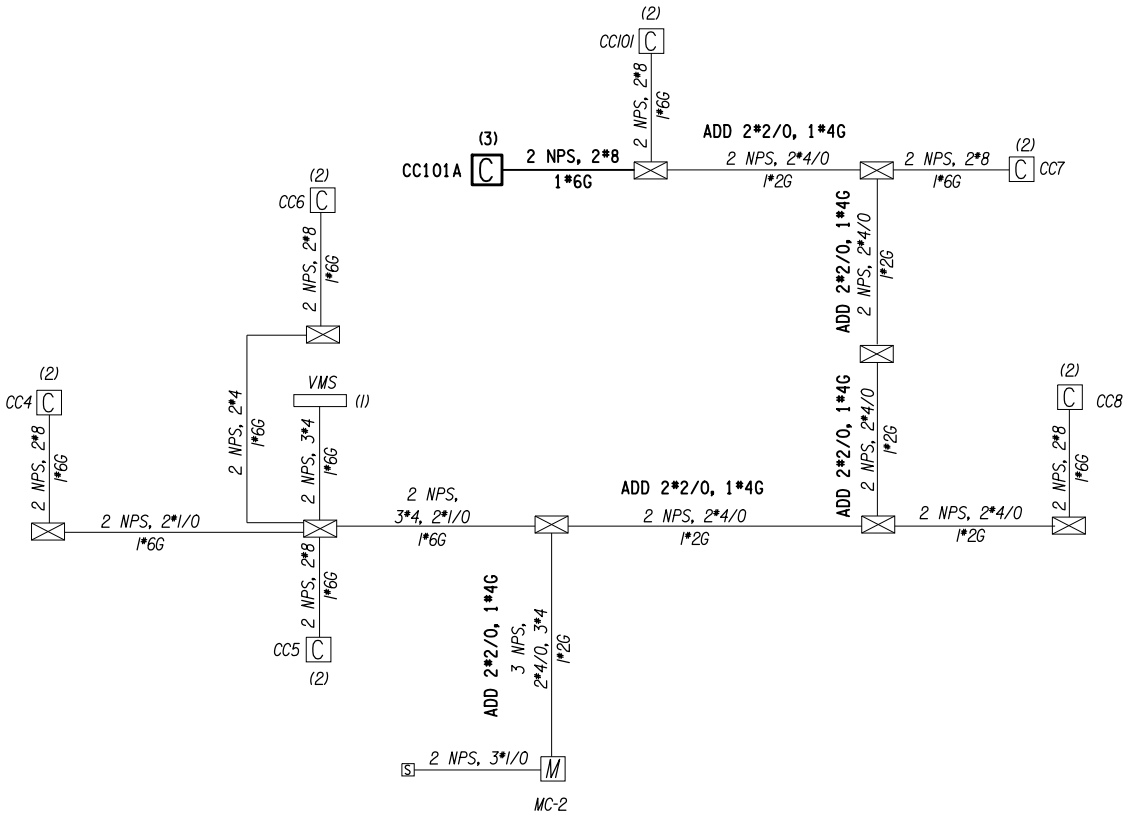
PROJECT MANAGER RAJ AMIN



- LEGEND:
- CONTROL CABINET
 - CONTROL CABINET EXISTING
 - METER CABINET PROPOSED
 - HIGHWAY ADVISORY RADIO
 - ELECTRICAL PULL BOX EXISTING
 - ELECTRICAL PULL BOX EXISTING OVERHEAD
 - COMMUNICATIONS SHELTER EXISTING
 - VARIABLE MESSAGING SYSTEM (VMS)
 - CIRCUIT DESIGNATION

NOTE:
REMOVE CONDUCTORS BETWEEN CONTROL CABINET CC3 AND NEAREST JUNCTION BOX SAFE OF THE REMAINING CIRCUIT.

AFFIX SEAL: ON:	ALTERED BY: ON:							
	AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	ITS TRAVEL TIME I-278 AND I-495	PIN X806.49	BRIDGES	CULVERTS	ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED	CONTRACT NUMBER	
		PS&E DATE: 06/21/2012				RISER DIAGRAM - 1	D262095	
							DRAWING NO. MCD-01	
		COUNTY: KINGS AND QUEEN					SHEET NO. 81	
	IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.		NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11 DOCUMENT NAME: x80649_cph_utd_mcd.01.dgn					



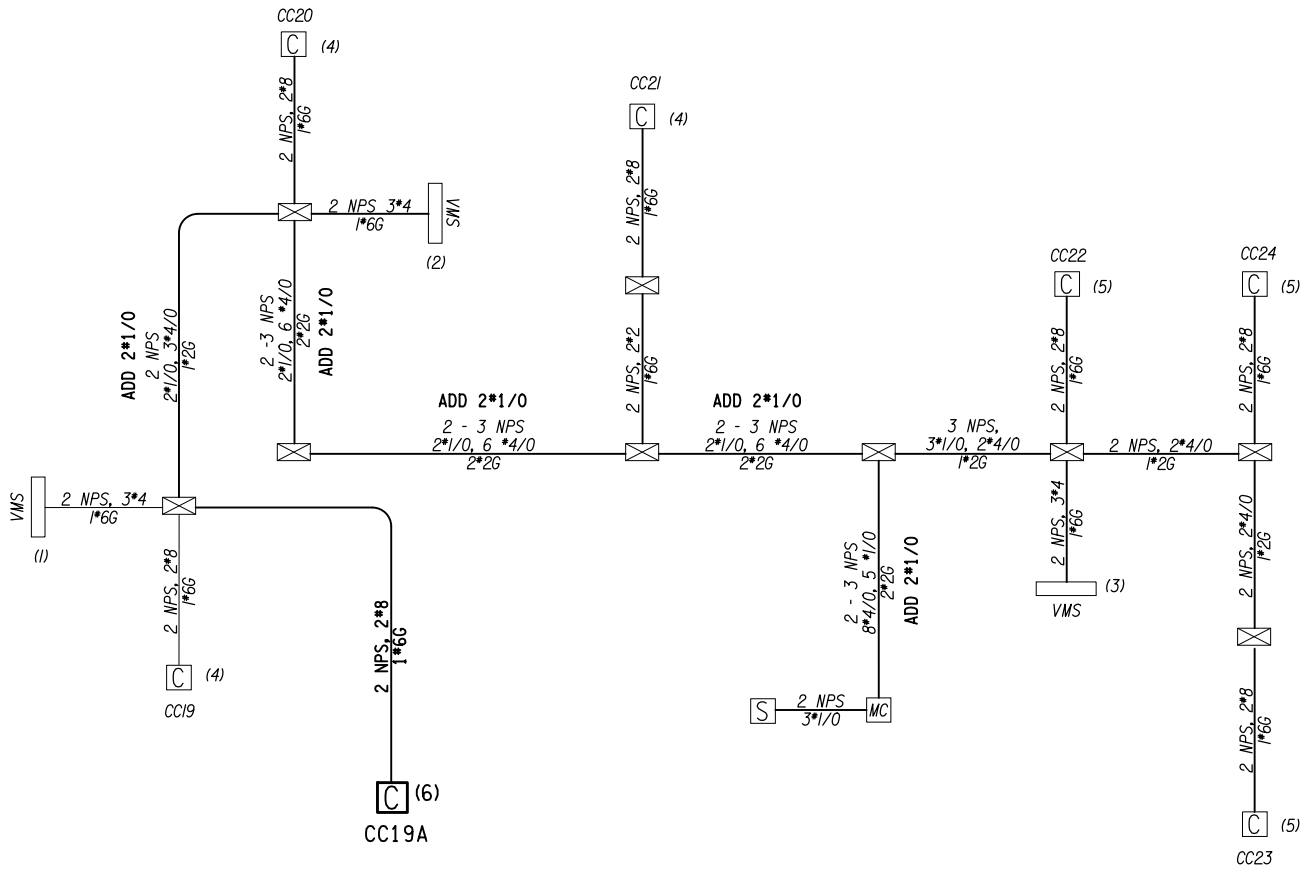
- LEGEND:
- CONTROL CABINET
 - CONTROL CABINET EXISTING
 - METER CABINET EXISTING
 - HIGHWAY ADVISORY RADIO
 - ELECTRICAL PULL BOX EXISTING
 - COMMUNICATIONS SHELTER EXISTING
 - VARIABLE MESSAGING SYSTEM (VMS)
 - CIRCUIT DESIGNATION

- NOTE:
- ADD 2*2/0 AND 1*4G IN THE EXISTING CONDUIT BETWEEN METER CABINET MC-2 AND EXISTING PULLBOX NEAR CABINET CC101 ALONG WITH EXISTING CABLES.
 - SEE NOTES 11& 12 DWG CON-02.

MC-2
RISER DIAGRAM

AFFIX SEAL: ON:	ALTERED BY: ON:

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	ITS TRAVEL TIME I-278 AND I-495	PIN X806.49	BRIDGES	CULVERTS	ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED	CONTRACT NUMBER	
	PS&E DATE: 06/21/2012					D262095	
						DRAWING NO. MCD-02	
	COUNTY: KINGS AND QUEEN					SHEET NO. 82	
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.						NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11 DOCUMENT NAME: X80649_cph_utd_mcd.02.dgn	





MC-4
RISER DIAGRAM

LEGEND:

- CONTROL CABINET
- CONTROL CABINET EXISTING
- METER CABINET EXISTING
- HIGHWAY ADVISORY RADIO
- ELECTRICAL PULL BOX EXISTING
- COMMUNICATIONS SHELTER EXISTING
- VARIABLE MESSAGING SYSTEM (VMS)
- CIRCUIT DESIGNATION

NOTE:

INSTALL 2*1/0 CABLES ALONG WITH EXISTING *1/0 CABLES OR AOE.

AFFIX SEAL: ON:		ALTERED BY: ON:										
	AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:		ITS TRAVEL TIME I-278 AND I-495		PIN X806.49	BRIDGES	CULVERTS	ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED	CONTRACT NUMBER			
			PS&E DATE: 06/21/2012						D262095			
								RISER DIAGRAM - 3			DRAWING NO. MCD-03	
											SHEET NO. 83	
					COUNTY: KINGS AND QUEEN							
			IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.									
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USER = kdesai

DESIGN SUPERVISOR RANDOLPH HUNT

JOB MANAGER MAREK CWIOK

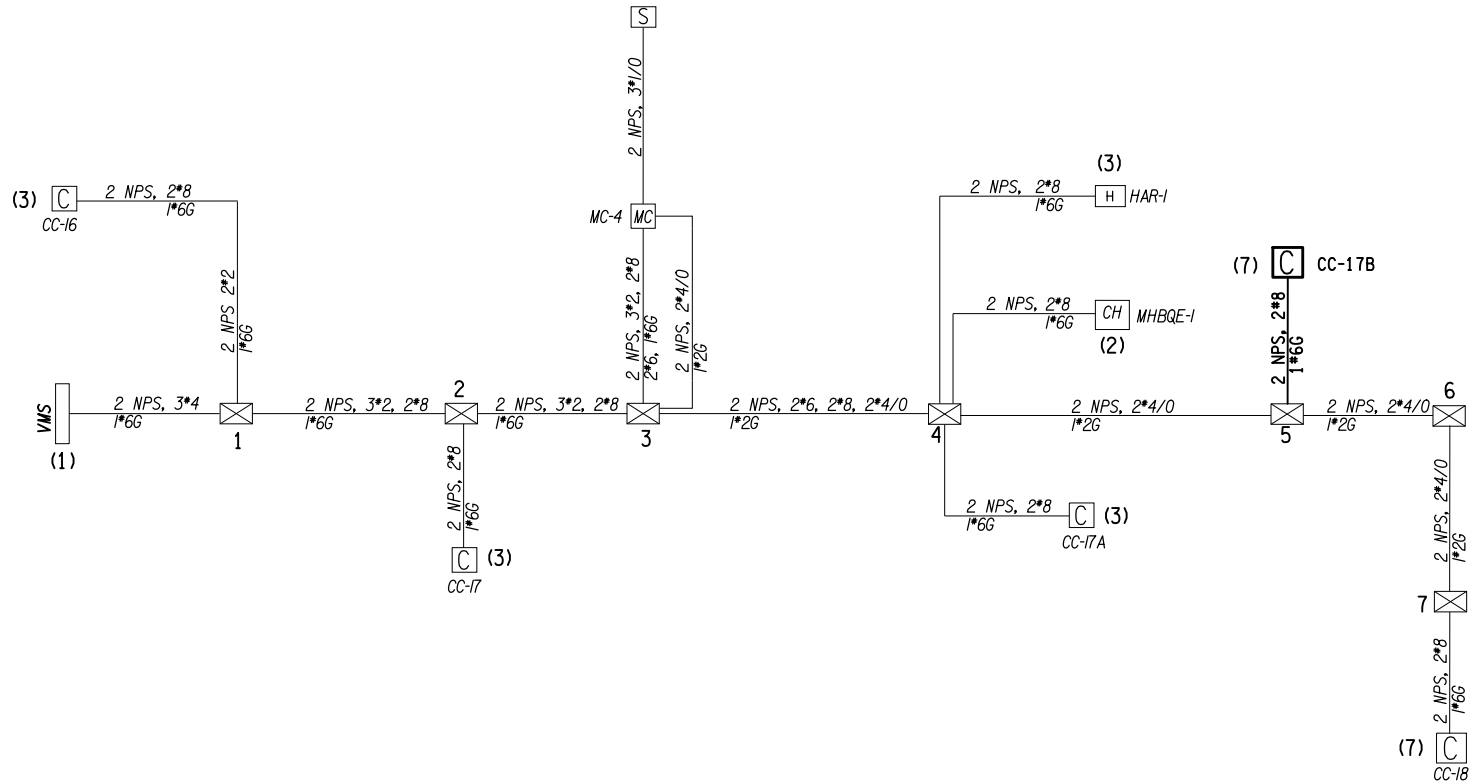
DESIGN MAREK CWIOK

CHECK MAREK CWIOK

DRAFTING RAISA STARTSEVA

CHECK MAREK CWIOK

PROJECT MANAGER RAJ AMIN



- LEGEND:**
- CONTROL CABINET
 - CONTROL CABINET EXISTING
 - METER CABINET EXISTING
 - HIGHWAY ADVISORY RADIO
 - ELECTRICAL PULL BOX EXISTING
 - COMMUNICATIONS SHELTER EXISTING
 - VARIABLE MESSAGING SYSTEM (VMS)
 - CIRCUIT DESIGNATION

NOTE:
POWER SUPPLY TO CONTROL CABINET CC-17B AT TILLARY STREET SHALL BE SPLICED TO EXISTING CIRCUIT #7 AT PULBOX #5

AFFIX SEAL: ON:		ALTERED BY: ON:													
		AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:		ITS TRAVEL TIME I-278 AND I-495		PIN X806.49	BRIDGES	CULVERTS	ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED		CONTRACT NUMBER				
				PS&E DATE: 06/21/2012					RISER DIAGRAM - 4		D262095				
											DRAWING NO. MCD-04				
											SHEET NO. 84				
						COUNTY: KINGS AND QUEEN									
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.														NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11	
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DESIGN SUPERVISOR RANDOLPH HUNT

JOB MANAGER MAREK CWIOK

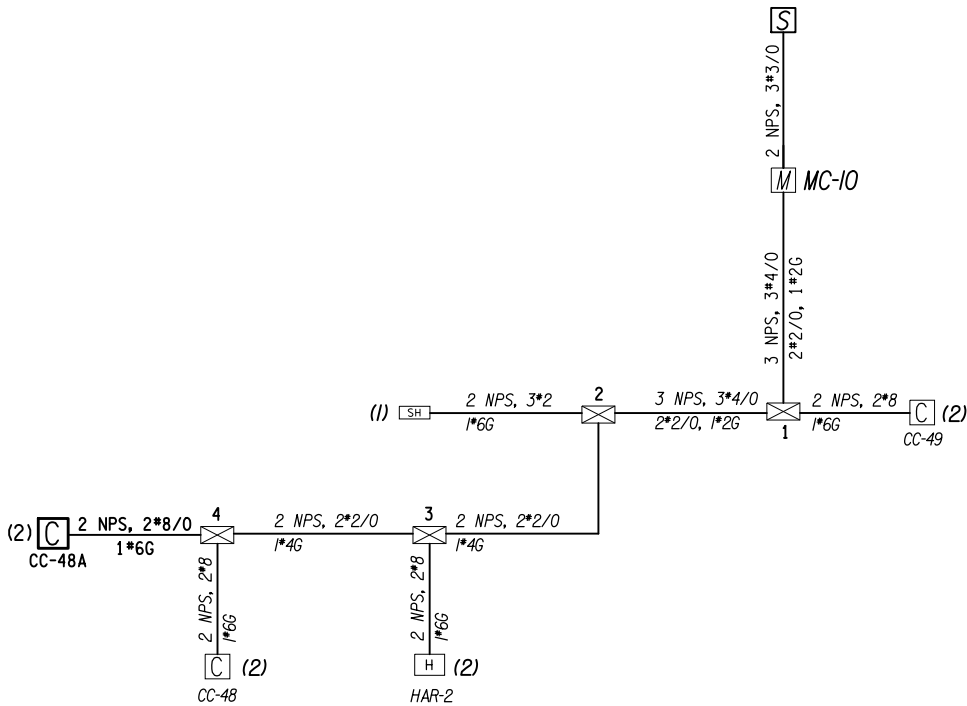
DESIGN MAREK CWIOK

CHECK MAREK CWIOK

DRAFTING RAISA STARTSEVA

CHECK MAREK CWIOK

PROJECT MANAGER RAJ AMIN



MC-10
RISER DIAGRAM

LEGEND:

- CONTROL CABINET
- CONTROL CABINET EXISTING
- METER CABINET EXISTING
- HIGHWAY ADVISORY RADIO
- ELECTRICAL PULL BOX EXISTING
- COMMUNICATIONS SHELTER EXISTING
- VARIABLE MESSAGING SYSTEM (VMS)
- CIRCUIT DESIGNATION

NOTE:

- CONTROL CABINET CC-48A AT 54TH AVE SHALL BE CONNECTED TO METER CABINET MC-10 LOCATED AT 45TH STREET THROUGH PULL BOX*4/CIRCUIT *2.

AFFIX SEAL: ON:		ALTERED BY: ON:														
		AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:		ITS TRAVEL TIME I-278 AND I-495		PIN X806.49		BRIDGES		CULVERTS		ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED		CONTRACT NUMBER		
				PS&E DATE: 06/21/2012								RISER DIAGRAM - 5		D262095		
														DRAWING NO. MCD-05		
						COUNTY: KINGS AND QUEEN								SHEET NO. 85		
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.																
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DESIGN SUPERVISOR RANDOLPH HUNT

JOB MANAGER MAREK CWTOK

DESIGN

RAISA STARTSEVA

CHECK

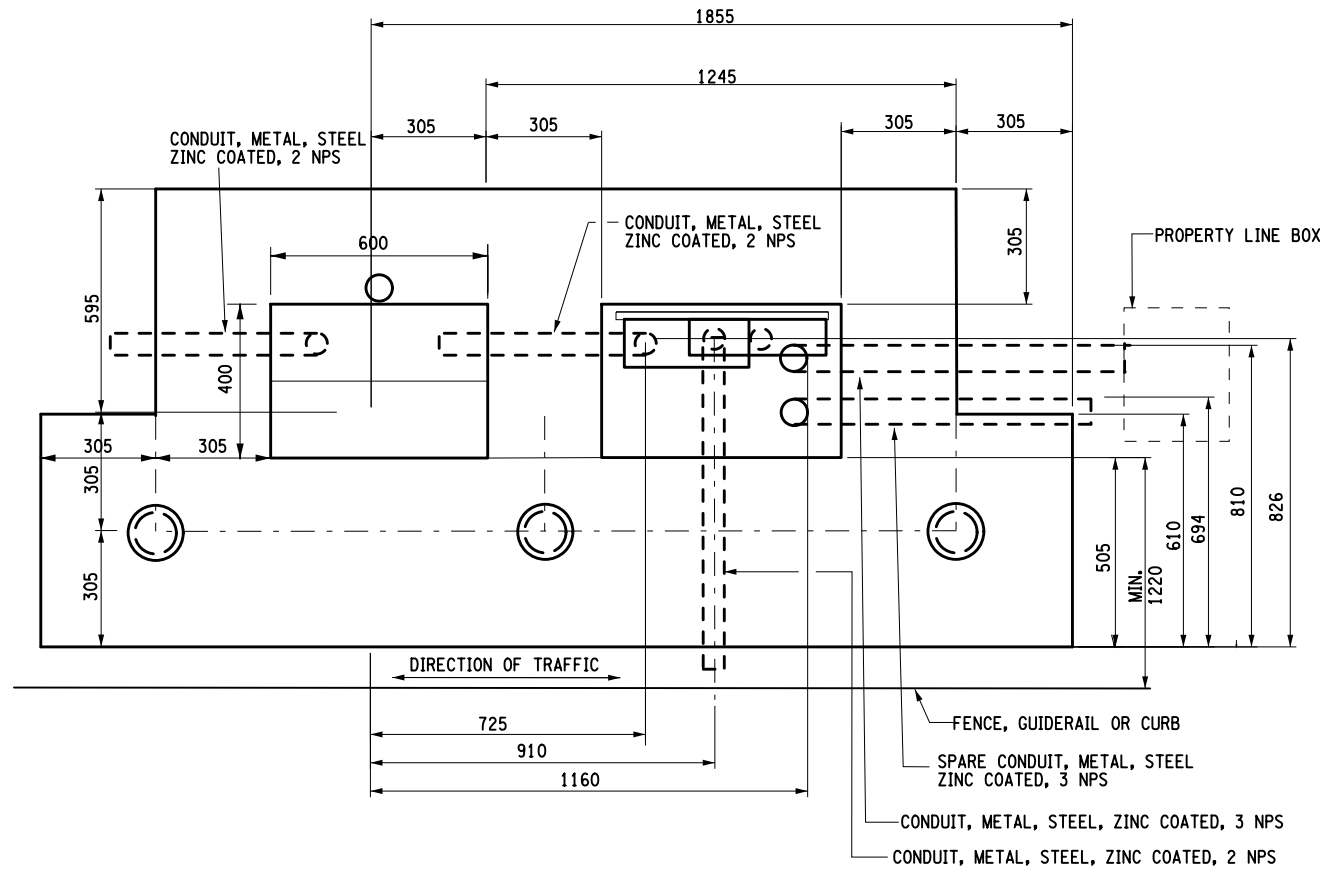
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DRAFTING RAISA STARTSEVA

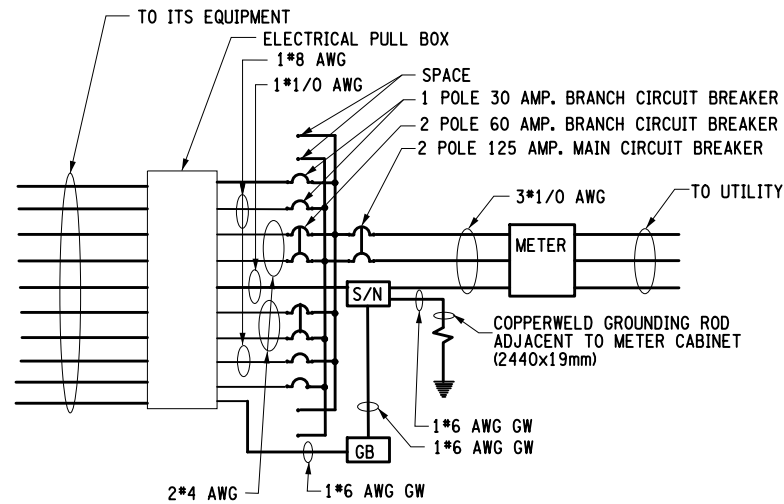
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MAREK CWTOK

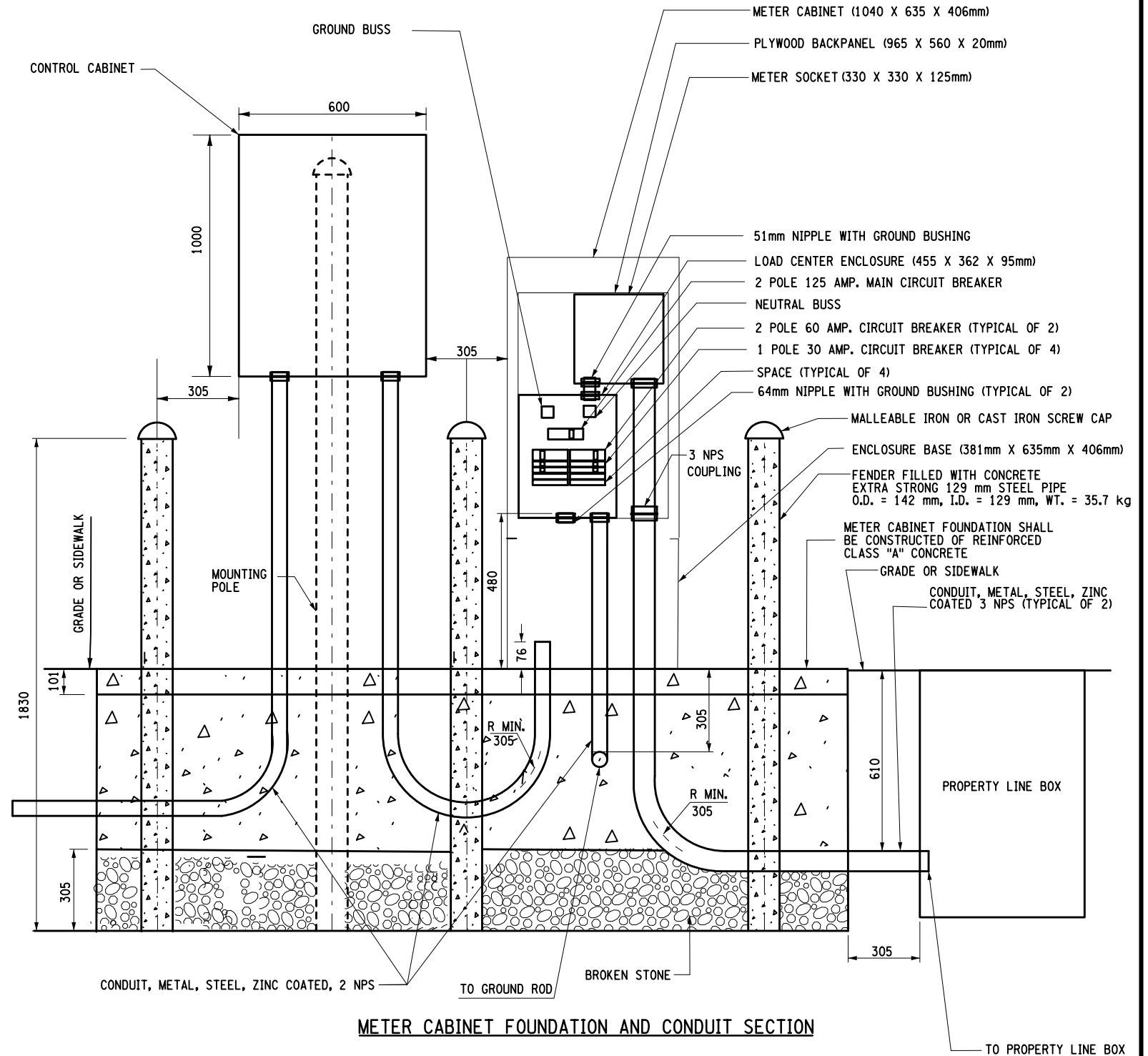
PROJECT MANAGER RAJ AMIN



METER CABINET FENDER AND CONDUIT LAYOUT - (3 FENDERS)
CABINETS AT 23RD STREET



METER CABINET WIRING DIAGRAM



METER CABINET FOUNDATION AND CONDUIT SECTION

NOTES:

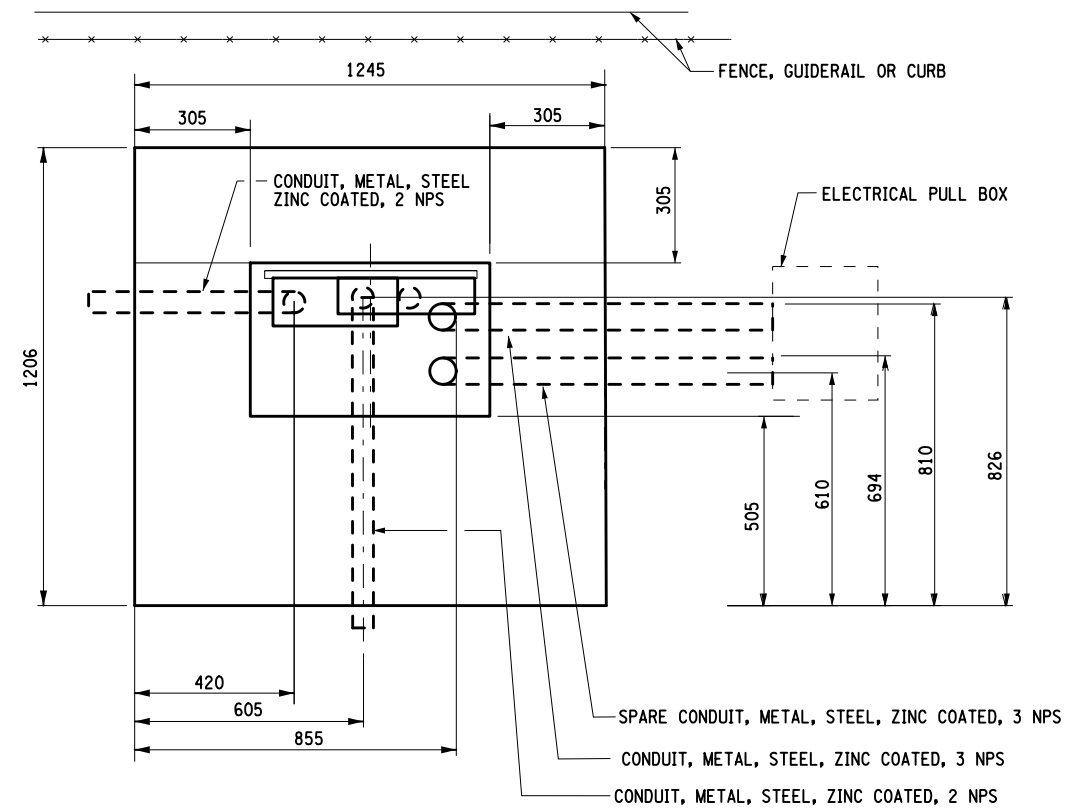
1. ALL PARTS OF FENDER SHALL CLEANED OF SCALE, RUST, DIRT AND SHALL BE COATED ON THE OUTSIDE WITH ONE COAT OF RED PRIMER IN ADDITION, PROVIDE ONE COAT OF BLACK ASPHALT PAINT BELOW THE GROUND AND ONE COAT OF ALUMINUM PAINT ABOVE THE GROUND.
2. ALL WORK SHOWN OR NOTED SHALL BE PAID FOR UNDER ITEM 680.95533211, UNLESS OTHERWISE NOTED.
3. ORIENT FENDERS, FOUNDATION AND METER CABINET, AS REQUIRED, TO PROVIDE MAXIMUM PROTECTION AND ACCESSIBILITY. COORDINATE EXACT REQUIREMENTS WITH CON-EDISON.
4. ALL DIMENSIONS ARE SHOWN IN MILLIMETERS UNLESS OTHERWISE NOTED.
5. METER CABINET SHALL BE PROVIDED WITH A PADLOCK HASP AND A 4-DIGIT RESETTABLE BRASS COMBINATION PADLOCK FOR USE BY BOTH NYSDOT MAINTENANCE AND CON-EDISON.

AFFIX SEAL: ON:	ALTERED BY: ON:

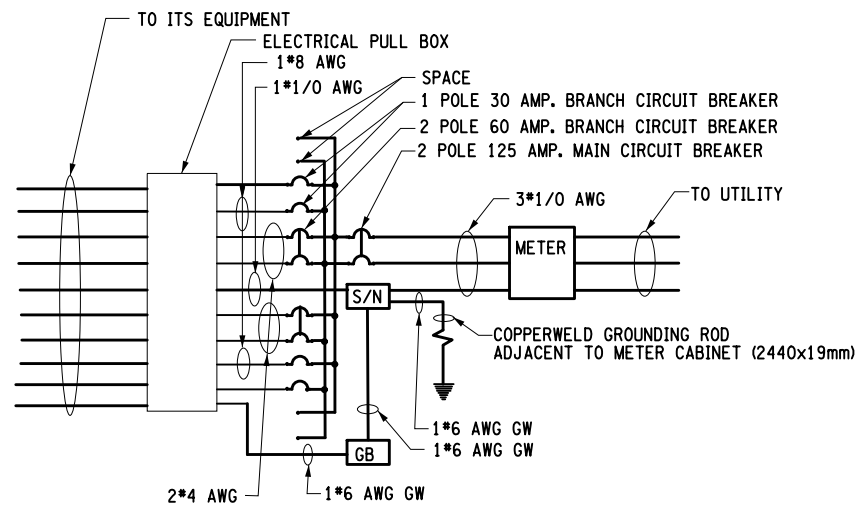
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	PS&E DATE: 06/21/2012					METER CABINET DETAILS - 1	
						23 STREET	DRAWING NO. MCD-06
		COUNTY: KINGS AND QUEEN					SHEET NO. 86

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

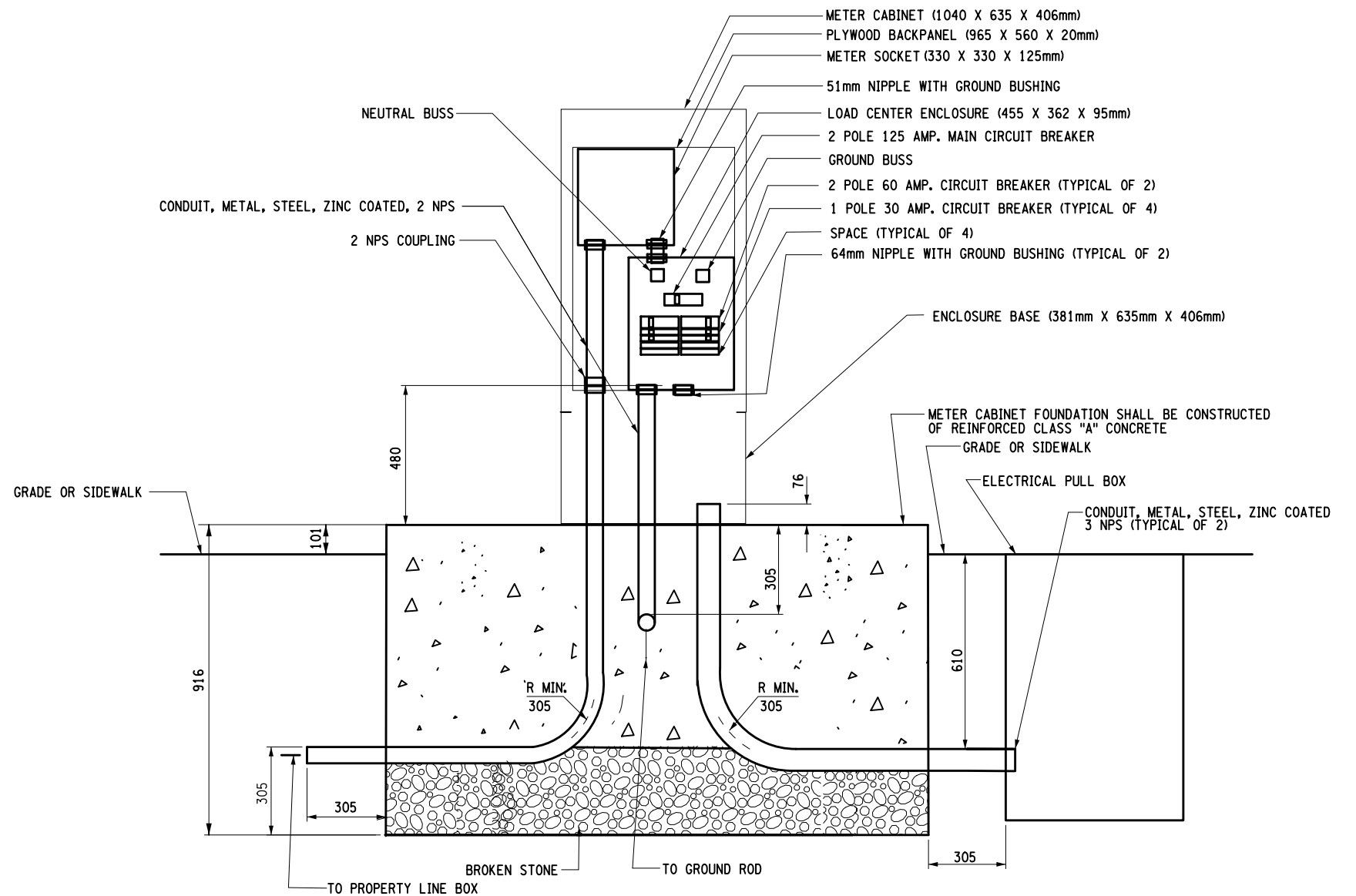
NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11
DOCUMENT NAME: x80649_cph_utfd_mcd_06.dgn



METER CABINET CONDUIT LAYOUT - (NO FENDERS)
63RD STREET





METER CABINET WIRING DIAGRAM

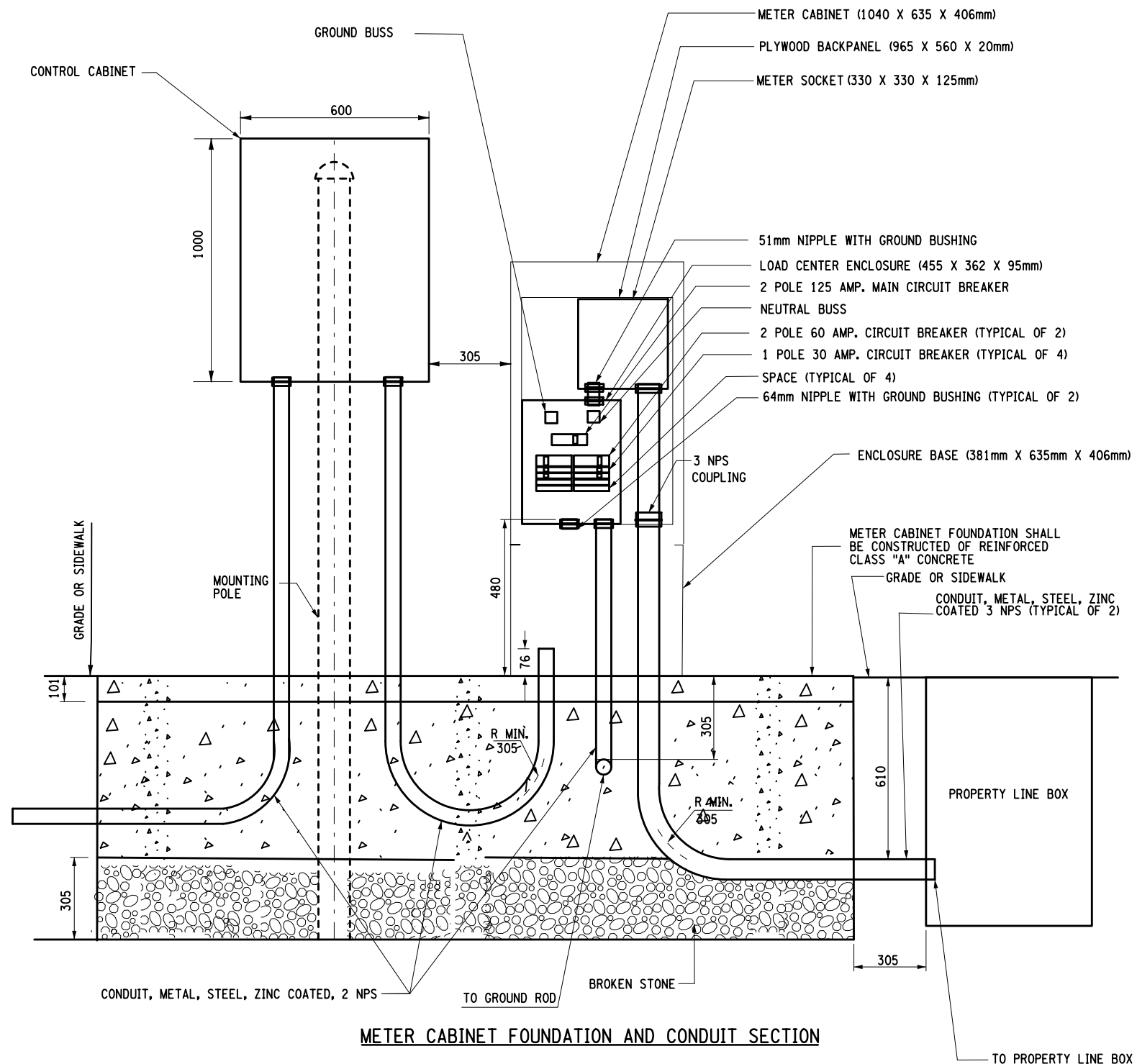


METER CABINET FOUNDATION AND CONDUIT SECTION

NOTES:


1. VACANT
2. ALL WORK SHOWN OR NOTED SHALL BE PAID FOR UNDER ITEM 680.95533211, METER CABINET, UNLESS OTHERWISE NOTED IN CONSTRUCTION PLANS.
3. ORIENT FOUNDATION AND METER CABINET, AS REQUIRED, TO PROVIDE MAXIMUM PROTECTION AND ACCESSIBILITY. COORDINATE EXACT REQUIREMENTS WITH CON-EDISON.
4. ALL DIMENSIONS ARE SHOWN IN MILLIMETERS UNLESS OTHERWISE NOTED.
5. METER CABINET SHALL BE PROVIDED WITH A PADLOCK HASP AND A 4-DIGIT RESETTABLE BRASS COMBINATION PADLOCK FOR USE BY BOTH NYSDOT MAINTENANCE AND CON-EDISON.

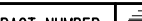
AFFIX SEAL: ON:		ALTERED BY: ON:						
		4. ALL DIMENSIONS ARE SHOWN IN MILLIMETERS UNLESS OTHERWISE NOTED. 5. METER CABINET SHALL BE PROVIDED WITH A PADLOCK HASP AND A 4-DIGIT RESETTABLE BRASS COMBINATION PADLOCK FOR USE BY BOTH NYSDOT MAINTENANCE AND CON-EDISON.						
AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:		ITS TRAVEL TIME I-278 AND I-495 PS&E DATE: 06/21/2012 COUNTY: KINGS AND QUEEN	PIN X806.49	BRIDGES	CULVERTS	ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED METER CABINET DETAILS - 2 63RD STREET	CONTRACT NUMBER D262095 DRAWING NO. MCD-07 SHEET NO. 87	
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.						NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11 DOCUMENT NAME: x80649_cph-utd_mcd.07.dgn		



1. ALL WORK SHOWN OR NOTED SHALL BE PAID FOR UNDER ITEM 680.95533211, UNLESS OTHERWISE NOTED.
2. ORIENT FOUNDATION AND METER CABINET, AS REQUIRED, TO PROVIDE MAXIMUM PROTECTION AND ACCESSIBILITY. COORDINATE EXACT REQUIREMENTS WITH CON-EDISON.

3. ALL DIMENSIONS ARE SHOWN IN MILLIMETERS UNLESS OTHERWISE NOTED.
4. METER CABINET SHALL BE PROVIDED WITH A PADLOCK HASP AND A 4-DIGIT RESETTABLE BRASS COMBINATION PADLOCK FOR USE BY BOTH NYS DOT MAINTENANCE AND CON-EDISON.

AFFIX SEAL: ON:	ALTERED BY: ON:
 <p>The seal is circular with a double border. The outer border contains the text "STATE OF NEW YORK" at the top and "LICENSED PROFESSIONAL ENGINEER" at the bottom, separated by two stars. The inner circle features a central coat of arms depicting a Native American figure holding a bow and arrow, with a rising sun in the background. Above the coat of arms, the words "EUREKA HUNT" are inscribed. The year "1784" is visible on the right side of the inner circle.</p>	

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	ITS TRAVEL TIME I-278 AND I-495	PIN X806.49	BRIDGES	CULVERTS	ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED	CONTRACT NUMBER	
	PS&E DATE: 06/21/2012				METER CABINET DETAILS - 3	D262095	
					E HAMPTON BLVD BRIDGE	DRAWING NO. MCD-08	
						SHEET NO. 88	
	COUNTY: KINGS AND QUEEN						
<p>IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.</p>							<p>NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11</p> <p>DOCUMENT NAME: x80649_cph.utd.mcd.08.dgn</p>

FOR INFORMATION ONLY

SUMMARY OF ITS COMMUNICATION EQUIPMENT BY
CONTROL CABINET FOR SHELTER BQE02 (SEE DWG NO ST-1)

CONTROL CABINET			CCTV CAMERA		RANGING RADAR DETECTOR		VMS ASSEMBLY TYPE A		CLASSIFICATION DETECTORS	
CONTROL CABINET NO.	CABINET TYPE	CABINET STATION NO.	CAMERA STATION NO.	CAMERA ID NO.	DETECTOR STATION NO.	DETECTOR ID NO.	VMS STATION NO.	VMS ID NO.	CLASS STATION NO.	CLASS ID NO.
CC1	A	EB QME 8-980								
CC2	A	EB QME 8-980	C-E-QME 8-980	CQES201	D-E-QME 8-980	DQES201				
CC3	D	EB-QME 9-350			D-E-QME 9-350	DQES203				
CC4	A	EB QME 9-758	C-E-QME 9-758	CQES202	D-E-QME 9-758	DQES204				
CC5	A	EB QME 0-289			D-E-QME 0-289A	DQES205	S-E-QME 0-289	SQES202		
					D-E-QME 0-289B	DQES206				
CC6	D	WB QME 0-289			D-W-QME 0-289	DQWS207				
CC101	A	WB QME 0-648							V-E-QME 0-648A	VQES201
									V-E-QME 0-648B	VQES202
									V-E-QME 0-648C	VQES203
									V-E-QME 0-648D	VQES204
									V-W-QME 0-648A	VQWS205
									V-W-QME 0-648B	VQWS206
									V-W-QME 0-648C	VQWS207
									V-W-QME 0-648D	VQWS208
									V-W-QME 0-648E	VQWS209
CC7	D	WB QME 0-735			D-W-QME 0-735	DQWS208				
CC8	A	EB QME 0-905	C-E-QME 0-905	CQES203	D-E-QME 0-905	DQES209				
CC10	A	WB QME 1-382			D-W-QME 1-382B	DQWS210	S-W-QME 1-382	SQWS203		
					D-W-QME 1-382A	DQES211				
CC11	A	EB QME 1-360	C-E-QME 1-360	CQES204	D-E-QME 1-360	DQES212				
					D-E-QME 1-365	DQES213				
CC12	A	WB QME 1-545	C-W-QME 1-545	CQES205	D-W-QME 1-556	DQWS214				
CC13	A	EB QME 1-645	C-E-QME 1-645	CQES206	D-E-QME 1-645	DQES215				
CC14	A	EB QME 1-824	C-E-QME 1-824	CQES207	D-E-QME 1-824	DQES216				
CC15	D	WB QME 1-824			D-W-QME 1-824	DQWS217				
CC16	A	EB QME 1-880	C-E-QME 1-880	CQES208	D-W-QME 1-866	DQWS218				
					D-E-QME 1-880	DQES219				
CC17	D	WB QME 2-060			D-W-QME 2-060	DQWS220				
CC18	A	EB QME 2-065	C-E-QME 2-065	CQES209	D-E-QME 2-065	DQES221				
CC19	A	EB QME 2-307			D-E-QME 2-307	DQES222	S-E-QME 2-307	SQES204		
CC20	A	WB QME 2-307			D-W-QME 2-307	DQWS223	S-W-QME 2-307	SQWS205		
CC21	A	WB QME 2-486	C-W-QME 2-486	CQES210	D-E-QME 2-486	DQES224				
					D-W-QME 2-486	DQWS225				
CC22	A	WB QME 2-704			D-W-QME 2-704	DQWS226	S-W-QME 2-704	SQWS206		

**SUMMARY OF ITS COMMUNICATION EQUIPMENT BY
CONTROL CABINET FOR MINIHUB LIE2 (SEE DWG NO ST-2)**

CONTROL CABINET			CCTV CAMERA		RANGING RADAR		DETECTOR		VMS ASSEMBLY TYPE A		CLASSIFICATION DETECTORS		HAR	
CONTROL CABINET NO.	CABINET TYPE	CABINET STATION NO.	CAMERA STATION NO.	CAMERA ID NO.	DETECTOR STATION NO.	DETECTOR ID NO.	VMS STATION NO.	VMS ID NO.	CLASS STATION NO.	CLASS ID NO.	HAR STATION NO.	HAR ID NO.		
0023	A	EB UE 2022	C-EUE 2008	CLEM21	D-EUE 2022	DLEM227								
									V-EUE 2022A	VLEM200				
									V-EUE 2022B	VLEM201				
									V-EUE 2022C	VLEM202				
									V-EUE 2022D	VLEM203				
0024	A	WB UE 2022			DW-UE 2022	DUM228			VW-UE 2022A	VUM204				
									VW-UE 2022B	VUM205				
									VW-UE 2022C	VUM206				
									VW-UE 2022D	VUM207				
									VW-UE 2022E	VUM208				
0025	A	EB UE 2130			D-EUE 2130	DLEM229	S-EUE 2130	SLEM207						
0026	A	WB UE-3-330	C-EUE 2127	CLEM202	DW-UE 3-330	DUM230	SW-UE-3-330	SUM208						
					D-EUE 3-330	DLEM231								
0027	D	EB UE 3-672			D-EUE 3-672	DLEM232								
0028	A	WB UE 3-740			DW-UE 3-740A	DUM233								
			CW-UE 3-735	CUM203	DW-UE 3-740B	DUM234								
0029	D	EB UE 4-040			D-EUE 4-040	DLEM235								
CH UE 2	-	CH-EUE 4-460			D-EUE 4-460	DLEM236								
0030	A	WB UE 4-467	C-EUE 4-474	CLEM204	DW-UE 4-467A	DUM2237	SW-UE 4-467	SUM209						
					DW-UE 4-467B	DUM238								
					DW-UE 4-467C	DUM239								
0031	A	EB UE 4-990					SW-UE 5027	SUM210						
0032	A	WB UE 5-863												
0033	D	WB UE 5-565	CW-UE 5-863	CUM205	DW-UE 5-565	DUM240								
0034	D	EB UE 5-644			D-EUE 5-644	DLEM241					HW-UE 5-490	HUM201		

NOTE:

1. THIS DRAWING IS EXTRACTED FROM SHEET*125F1
PROJECT NO. D259712 PIN X228.65 AND PROVIDED
FOR INFORMATION ONLY.

AFFIX SEAL:
ON:

ALTERED BY:
ON:



AS-BUILT REVISIONS
DESCRIPTION OF ALTERATIONS:

ITS TRAVEL TIME I-278 AND I-495

PS&E DATE: 06/21/2012

COUNTY: KINGS AND QUEEN

PIN	X806.49
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BRIDGES

CULVERTS

ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED

CONTRACT NUMBER	
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D262095

RAWING NO. ITT-01

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11



DOCUMENT NAME: x80649_cph_itt_01.dgn


FOR INFORMATION ONLY

RADAR DETECTOR TABLE								
PLAN SHEET	DETECTOR STATION	EQUIP. ID NO.	DETECTOR LOCATION	DETECTOR MOUNTED ON	DETECTOR COVERAGE	NO. OF LANES	SETBACK "X"	HEIGHT "Y"
LIE-16	D-E-LIE 2-022	DLEM227	EAST OF 60TH STREET	EXISTING GANTRY	MAINLINE	3		5.8
LIE-16	D-W-LIE 2-022	DLWM228	EAST OF 60TH STREET	EXISTING GANTRY	MAINLINE	3		5.8
LIE-19	D-E-LIE 2-730	DLEM229	GRAND AVENUE	PROPOSED GANTRY	SPEED DETECTOR			
LIE-21	D-W-LIE 3-330	DLWM230	MAZEAU STREET	PROPOSED GANTRY	MAINLINE	4		5.2
LIE-21	D-E-LIE 3-330	DLEM231	MAZEAU STREET	PROPOSED GANTRY	MAINLINE	4		5.2
LIE-22	D-E-LIE 3-672	DLEM232	75TH STREET	EXISTING CANTILEVER	MAINLINE	4		5.2
LIE-22	D-W-LIE 3-740A	DLWM233	75TH STREET	EXISTING GANTRY	MAINLINE	3		5.2
LIE-22	D-W-LIE 3-740B	DLWM234	75TH STREET	EXISTING GANTRY	RAMP	2		5.2
LIE-23	D-E-LIE 4-040	DLEM235	80TH STREET	PROPOSED CAMERA POLE	MAINLINE	3		5.2
LIE-25	D-W-LIE 4-467A	DLWM2237	84TH STREET	WALL MOUNTED	MAINLINE	3		5.8
LIE-25	D-W-LIE 4-467B	DLWM238	84TH STREET	PROPOSED GANTRY	SPEED DETECTOR			
LIE-25	D-W-LIE 4-467C	DLWM239	84TH STREET	PROPOSED GANTRY	SPEED DETECTOR			
LIE-29	D-W-LIE 5-565	DLWM240	94TH STREET	EXISTING GANTRY	MAINLINE	4		5.8
LIE-31	D-E-LIE 6-200A	DLEM342	99TH STREET	PROPOSED GANTRY	MAINLINE	5		5.8
LIE-31	D-E-LIE 6-200B	DLEM343	99TH STREET	PROPOSED GANTRY	SPEED DETECTOR			


NOTE:

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- THIS DRAWING IS EXTRACTED FROM SHEET 184F1.
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- CONTRACTOR NEED TO SCHEDULE THE WORK A SUCH WAY AS TO MINIMIZE DOWNTIME OF EXISTING DEVICES.
- ALL ITS DEVICES MUST BE TURNED ON AT THE END OFF EACH WORKDAY.
- ALL DEVICES DAMAGED OR NOT BEING ABLE TO COMMUNICATE WITH JTMC, AFTER WORK COMPLETION MUST BE REPAIRED BY CONTRACTOR AT NO COST TO STATE.

AFFIX SEAL: ON:		ALTERED BY: ON:												
		AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:		ITS TRAVEL TIME I-278 AND I-495		PIN X806.49	BRIDGES	CULVERTS	ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED		CONTRACT NUMBER			
				PS&E DATE: 06/21/2012					ITS EQUIPMENT TABLES - 2		D262095			
											DRAWING NO. ITT-02			
				COUNTY: KINGS AND QUEEN							SHEET NO. 90			
		IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.												
													DOCUMENT NAME: x80649_cph.itt_02.dgn	

AFFIX SEAL: ON:	ALTERED BY: ON:
	

HIGHWAY ADVISORY RADIO STATION				
PLAN SHEET	HAR STATION	HAR ID NO.	HAR LOCATION	HAR MOUNTED ON
LIE-28	HAR-W LIE 5+420	HLWH201	93RD STREET	NEW HAR

- | | | | | | | | |
|---|---------------------------------|-------------|---------|----------|--|--------------------|---|
| AS-BUILT REVISIONS
DESCRIPTION OF ALTERATIONS: | ITS TRAVEL TIME I-278 AND I-495 | PIN X806.49 | BRIDGES | CULVERTS | ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED | CONTRACT NUMBER |  |
| | PS&E DATE: 06/21/2012 | | | | ITS EQUIPMENT TABLES - 3 | D262095 | |
| | | | | | | DRAWING NO. ITT-03 | |
| | COUNTY: KINGS AND QUEEN | | | | | SHEET NO. 91 | |
- IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.
- NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11
DOCUMENT NAME: X80649_cph.itt-03.dgn

FOR INFORMATION ONLY

RADAR DETECTOR TABLE								
PLAN SHEET	DETECTOR STATION	EQUIP. ID NO.	DETECTOR LOCATION	DETECTOR MOUNTED ON	DETECTOR COVERAGE	NO. OF LANES	SETBACK "X"	HEIGHT "Y"
LIE-1	D-E-QME 8+980	DQES201	EAST OF PULASKI BRIDGE	PROPOSED CAMERA POLE	MAINLINE	6		5.8
LIE-2	D-E-QME 9+350	DQES203	WEST OF 25TH STREET	EXISTING GANTRY	MAINLINE	3		5.2
LIE-4	D-E-QME 9+758	DQES204	EAST OF 27TH STREET	PROPOSED CAMERA POLE	MAINLINE	6		5.8
LIE-6	D-E-QME 0+289A	DQES205	BRADLEY AVENUE	PROPOSED GANTRY	MAINLINE	3		5.2
LIE-6	D-E-QME 0+289B	DQES206	BRADLEY AVENUE	PROPOSED GANTRY	SPEED DETECTOR			
LIE-6	D-W-QME 0+289	DQWS207	BRADLEY AVENUE	PROPOSED GANTRY	MAINLINE	4	4	5.2
LIE-7	D-W-QME 0+735	DQWS208	GREENPOINT AVENUE	EXISTING GANTRY	MAINLINE	6		5.8
LIE-8	D-E-QME 0+905	DQES209	38TH STREET	PROPOSED CAMERA POLE	MAINLINE	3		5.2
LIE-10	D-W-QME 1+382B	DQWS210	43RD STREET	EXISTING GANTRY	SPEED DETECTOR			
LIE-10	D-W-QME 1+382A	DQES211	43RD STREET	EXISTING GANTRY	MAINLINE	3		5.2
LIE-10	D-E-QME 1+360	DQES212	43RD STREET	PROPOSED CAMERA POLE	MAINLINE	4		6
LIE-10	D-E-QME 1+365	DQES213	43RD STREET	PROPOSED POLE	RAMP	1		5.8
LIE-10	D-W-QME 1+556	DQWS214	BROOKLYN-QUEENS EXPWY	WALL MOUNTED	RAMP	1		5.2
LIE-11L	D-E-QME 1+645	DQES215	46TH STREET	WALL MOUNTED	RAMP	1		5.2
LIE-11U	D-E-QME 1+824	DQES216	WEST OF 48 STREET	PROPOSED CAMERA POLE	MAINLINE	3		5.2
LIE-11U	D-W-QME 1+824	DQWS217	WEST OF 48 STREET	PROPOSED POLE	MAINLINE	3		5.2
LIE-11L	D-W-QME 1-866	DQWS218	48TH STREET	WALL MOUNTED	MAINLINE	3		5.2
LIE-11L	D-E-QME 1+880	DQES219	48TH STREET	WALL MOUNTED	MAINLINE	3		5.2
LIE-12U	D-W-QME 2+060	DQWS220	WEST OF CEMETERY ROAD	PROPOSED POLE	MAINLINE	3		5.2
LIE-12U	D-E-QME 2+065	DQES221	WEST OF CEMETERY ROAD	PROPOSED CAMERA POLE	MAINLINE	3		5.2
LIE-13L	D-E-QME 2+307	DQES222	EAST OF CEMETERY ROAD	PROPOSED GANTRY	SPEED DETECTOR			
LIE-13L	D-W-QME 2+307	DQWS223	EAST OF CEMETERY ROAD	PROPOSED GANTRY	SPEED DETECTOR			
LIE-14L	D-E-QME 2+486	DQES224	58TH STREET	WALL MOUNTED	MAINLINE	3		5.2
LIE-14L	D-W-QME 2+486	DQWS225	58TH STREET	WALL MOUNTED	MAINLINE	3		5.2
LIE-14U	D-W-QME 2+704	DQWS226	WEST OF MAURICE AVENUE	PROPOSED GANTRY	SPEED DETECTOR			
LIE-16	D-E-LIE 2+022	DLEM227	EAST OF 60TH STREET	EXISTING GANTRY	MAINLINE	3		5.8
LIE-16	D-W-LIE 2+022	DLWM228	EAST OF 60TH STREET	EXISTING GANTRY	MAINLINE	3		5.8
LIE-19	D-E-LIE 2+730	DLEM229	GRAND AVENUE	PROPOSED GANTRY	SPEED DETECTOR			
LIE-21	D-W-LIE 3+330	DLWM230	MAZEAU STREET	PROPOSED GANTRY	MAINLINE	4		5.2
LIE-21	D-E-LIE 3+330	DLEM231	MAZEAU STREET	PROPOSED GANTRY	MAINLINE	4		5.2
LIE-22	D-E-LIE 3+672	DLEM232	75TH STREET	EXISTING CANTILEVER	MAINLINE	4		5.2
LIE-22	D-W-LIE 3+740A	DLWM233	75TH STREET	EXISTING GANTRY	MAINLINE	3		5.2
LIE-22	D-W-LIE 3+740B	DLWM234	75TH STREET	EXISTING GANTRY	RAMP	2		5.2
LIE-23	D-E-LIE 4+040	DLEM235	80TH STREET	PROPOSED CAMERA POLE	MAINLINE	3		5.2
LIE-25	D-E-LIE 4+460	DLEM236	84TH STREET	WALL MOUNTED	MAINLINE	3		5.8
LIE-25	D-W-LIE 4+467A	DLWM237	84TH STREET	WALL MOUNTED	MAINLINE	3		5.8
LIE-25	D-W-LIE 4+467B	DLWM238	84TH STREET	PROPOSED GANTRY	SPEED DETECTOR			
LIE-25	D-W-LIE 4+467C	DLWM239	84TH STREET	PROPOSED GANTRY	SPEED DETECTOR			
LIE-29	D-W-LIE 5+565	DLWM240	94TH STREET	EXISTING GANTRY	MAINLINE	4		5.8
LIE-29	D-E-LIE 5+644	DLEM241	JUNCTION BLVD.	EXISTING GANTRY	MAINLINE	4		5.8
LIE-31	D-E-LIE 6+200A	DLEM342	99TH STREET	PROPOSED GANTRY	MAINLINE	5		5.8
LIE-31	D-E-LIE 6+200B	DLEM343	99TH STREET	PROPOSED GANTRY	SPEED DETECTOR			
LIE-31	D-W-LIE 6+225	DLWM344	99TH STREET	PROPOSED CAMERA POLE	MAINLINE	4		5.8
LIE-33	D-E-LIE 6+687	DLEM345	YELLOWSTONE BLVD.	EXISTING GANTRY	MAINLINE	4		5.8
LIE-33	D-W-LIE 6+687	DLWM346	YELLOWSTONE BLVD.	EXISTING GANTRY	MAINLINE	4		5.8
LIE-34	D-W-LIE 7+253A	DLWM347	112TH STREET	EXISTING GANTRY	MAINLINE	6		5.8
LIE-34	D-W-LIE 7+253B	DLWM348	112TH STREET	EXISTING GANTRY	SERVICE ROAD			
LIE-34	D-W-LIE 7+253C	DLWM349	112TH STREET	EXISTING GANTRY	SPEED DETECTOR			
LIE-34	D-E-LIE 7+253A	DLEM350	112TH STREET	EXISTING GANTRY	SPEED DETECTOR			
LIE-34	D-E-LIE 7+253B	DLEM351	112TH STREET	EXISTING GANTRY	SERVICE ROAD			
LIE-36	D-E-LIE 7+740A	DLEM352	BTWN GCP AND VWE	EXISTING PEDESTRIAN BRIDGE	MAINLINE	3		5.2
LIE-36	D-E-LIE 7+740B	DLEM353	BTWN GCP AND VWE	EXISTING PEDESTRIAN BRIDGE	SERVICE ROAD	3		5.2
LIE-36	D-W-LIE 7+771A	DLWM354	BTWN GCP AND VWE	EXISTING GANTRY	MAINLINE	3		5.8
LIE-36	D-W-LIE 7+771B	DLWM355	BTWN GCP AND VWE	EXISTING GANTRY	SERVICE ROAD	3		5.2
LIE-39	D-W-LIE 8+444	DLWM356	LAWRENCE STREET	EXISTING GANTRY	MAINLINE	3		5.2
LIE-39	D-E-LIE 8+535	DLEM357	THOMPSON PLACE	EXISTING CANTILEVER	MAINLINE	3		5.2
LIE-40	D-W-LIE 8+895A	DLWM358	138TH STREET	PROPOSED GANTRY	MAINLINE	3		5.2
LIE-40	D-W-LIE 8+895B	DLWM359	138TH STREET	PROPOSED GANTRY	SPEED DETECTOR			

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AFFIX SEAL: ON: ALTERED BY: ON:

AS-BUILT REVISIONS
DESCRIPTION OF ALTERATIONS:

ITS TRAVEL TIME I-278 AND I-495

PS&E DATE: 06/21/2012

COUNTY: KINGS AND QUEEN

PIN X806.49

BRIDGES

CULVERTS

ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED

CONTRACT NUMBER

D262095

DRAWING NO. ITT-04

SHEET NO. 92

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FILE NAME = x80649_cph.itt_05.dgn
DATE/TIME = 03-AUG-2012 16:12
USER = kdesai

DESIGN SUPERVISOR RANDOLPH HUNT

JOB MANAGER MAREK CWIOK

DESIGN RAISA STARTSEVA

CHECK MAREK CWIOK

DRAFTING RAISA STARTSEVA

CHECK MAREK CWIOK

PROJECT MANAGER RAJ AMIN

FOR INFORMATION ONLY



		CCTV CAMERA		RANGING RADAR DETECTOR		VMS ASSEMBLY TYPE A		TRANSMIT DETECTOR	
CONTROL CABINET NO.	CABINET TYPE	STATION NO.	CAMERA ID	STATION NO.	DETECTOR ID	STATION NO.	VMS ID	TRANSMIT NO.	TRANSMIT ID
SH GOWO3									
CC1	A	C-W-BQE8900	CBWS301						
CC2	W	C-W-BQE9425	CBWS302						
CC3	W	C-W-BQE9545	CBWS303						
CC4	A			D-W-BQE9875	RBWS301	S-W-BQE9875	SBWS301		
				D-E-BQE9875	RBES302				
CC5	A	C-W-BQE10045	CBWS304			S-E-BQE10050	SBES302		
CC6	A	C-E-BQE10244	CBES305	D-W-BQE10244	RBWS303				
				D-E-BQE10244	RBES304				
CC7	A	C-W-BQE10440	CBWS306						
		C-E-BQE10440	CBES307						
CC8	D			D-E-BQE10665	RBEH105				
CC9	A	C-W-BQE11314	CBWH108						
		C-E-BQE11314	CBEH109						
CC10	A			D-E-BQE11358	RBEH106				
CC11	A	C-W-BQE11455	CBWH110						
		C-E-BQE11455	CBEH111						
CC12	A			D-E-BQE11755	RBEH107	S-E-BQE11755	SBEH103		
CC13	D			D-W-BQE11830	RBWH108				
CC14	D			D-E-BQE11959	RBEH109				
CC15	A	C-W-BQE12016	CBWH112						
CC16	A			D-W-BQE12400A	RBWH110	S-W-BQE12400	SBWH104		
				D-W-BQE12400B	RBWH111				
CC17	D			D-E-BQE12460	RBEH112				
MH BQE1		C-W-BQE12510	CBWH113					DT-E-BQE12910A	--
CC17B	A							DT-W-BQE12910B	--
								DT-W-BQE12910C	--
CC18	D			D-E-BQE12875	RBEH113				
CC19	D			D-W-BQE12900	RBWH114				
CC20	A	C-E-BQE12986	CBEH114						
CC21	D			D-W-BQE13330	RBWH115				
CC22	A	C-W-BQE13616	CBWH115	D-W-BQE13616A	RBWH116	S-W-BQE13616	SBWH105		
				D-W-BQE13616B	RBWH117				
CC23	A			D-E-BQE13616A	RBEH118	S-E-BQE13616	SBEH106		
				D-E-BQE13616B	RBEH119				
CC24	D			D-W-BQE14356	RBEH120				
				D-E-BQE14356	RBEH121				
CC25	A	C-E-BQE14433	CBEH116						
CC26	A	C-W-BQE14858	CBWH117	D-W-BQE14858	RBWH122				
CC27	A	C-W-BQE15062	CBWH118						
CC28	A	C-W-BQE15291	CBWH119	D-W-BQE15291	RBWH123				
CC29	A	C-W-BQE15494	CBWH120						
CC30	D			D-E-BQE15668	RBWH124				
CC31	D			D-W-BQE15745	RBWH125				
CC32	A	C-E-BQE15776	CBEH121						

		CCTV CAMERA		RANGING RADAR DETECTOR		VMS ASSEMBLY TYPE A		TRANSMIT DETECTOR	
CONTROL CABINET NO.	CABINET TYPE	STATION NO.	CAMERA ID	STATION NO.	DETECTOR ID	STATION NO.	VMS ID	TRANSMIT NO.	TRANSMIT ID
CC33	A	C-W-BQE15853	CBWS222						
CC34	A	C-W-BQE15933	CBWS223						
CC35	A	C-W-BQE16119	CBWS224						
CC36	D			D-E-BQE16154	RBES226				
CC37	D			D-W-BQE16189	RBWS227				
CC38	D			D-W-BQE16478	RBWS228				
CC39	A	C-E-BQE16510	CBES225	D-E-BQE16510	RBES229				
CC40	A			D-W-BQE17035A	RBWS230	S-W-BQE17035	SBWS207		
				D-W-BQE17035B	RBWS231				
CC41	A			D-E-BQE17035A	RBES232	S-E-BQE17035	SBEH208		
				D-E-BQE17035B	RBES233				
CC42	A	C-W-BQE17295	CBWS226	D-W-BQE17295	RBWS234				
				D-E-BQE17295	RBES235				
CC43	D			D-E-BQE17700	RBES236				
CC44	D			D-W-BQE17715	RBWS237				
CC45	A	C-W-BQE18046	CBWS227	D-W-BQE18046	RBWS238				
				D-E-BQE18046	RBES239				
CC46	A	C-E-BQE18588	CBES228	D-E-BQE18588	RBES240				
CC47	A					S-E-BQE19053	SBES209		
CC48	A	C-E-BQE19759	CBES229	D-W-BQE19759	RBWS241				
				D-E-BQE19759	RBES242				
SH BQE02		C-W-BQE20115	CBWS230						
CC49A	A							DT-E-BQE19759A	--
								DT-E-BQE19759B	--
								DT-W-BQE19759C	--
								DT-W-BQE19759D	--
CC49	A	C-W-BQE20195	CBWS231	D-W-BQE20195A	RBWS243				
				D-W-BQE20195B	RBWS244				
CC50	A	C-W-BQE21040	CBWS232	D-W-BQE21040A	RBWS245	S-W-BQE21040	SBWS210		
				D-W-BQE21040B	RBWS246				
				D-E-BQE21040	RBES247				
CC51	A			D-E-BQE21453A	RBES248	S-E-BQE21453	SBEH211		
				D-E-BQE21453B	RBES249				
				D-W-BQE21453A	RBWS250				
				D-W-BQE21453B	RBWS251				
CC52	A	C-E-BQE0249	CBES233						
CC54	D			D-W-BQE0535	RBWS252				
				D-E-BQE0535	RBES253				
CC55	A	C-E-BQE0965	CBES234	D-E-BQE0965	RBES254				
CC56	A	C-E-BQE1436	CBES235	D-E-BQE1449	RBES255				
CC57	A	C-E-BQE1797	CBES236			S-E-BQE1797	SBES212		

SUMMARY OF ITS COMMUNICATION EQUIPMENT BY CONTROL CABINET

NOTE:

1. THIS DRAWING IS EXTRACTED FROM SHEET*149
PROJECT NO. D259437 PIN X735.72

AFFIX SEAL: ON:		ALTERED BY: ON:	
		AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	
		ITS TRAVEL TIME I-278 AND I-495	
		PS&E DATE: 06/21/2012	
		COUNTY: KINGS AND QUEEN	
		PIN X806.49	BRIDGES
			CULVERTS
		ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED	
		ITS EQUIPMENT TABLES - 5	
		CONTRACT NUMBER	
		D262095	
		DRAWING NO. ITT-05	
		SHEET NO. 93	
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.			
NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11 DOCUMENT NAME: x80649_cph.itt_05.dgn			

FILE NAME = X80649_cph_lad_01.dgn
DATE/TIME = 03-AUG-2012 16:12
USER = kdesai

DESIGN SUPERVISOR RANDOLPH HUNT

JOB MANAGER MAREK CWIOK

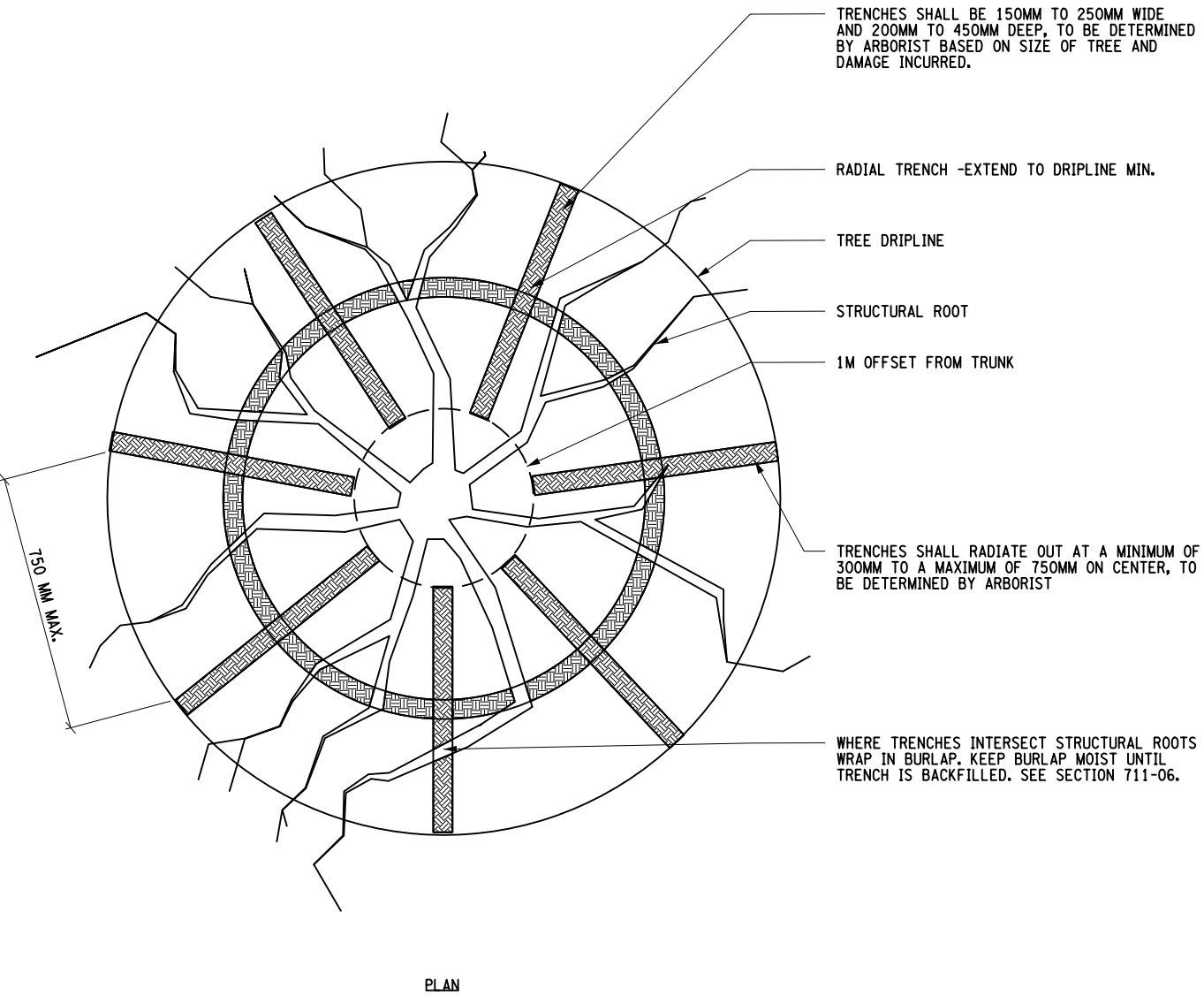
DESIGN YELENA GRIG

CHECK MAREK CWIOK

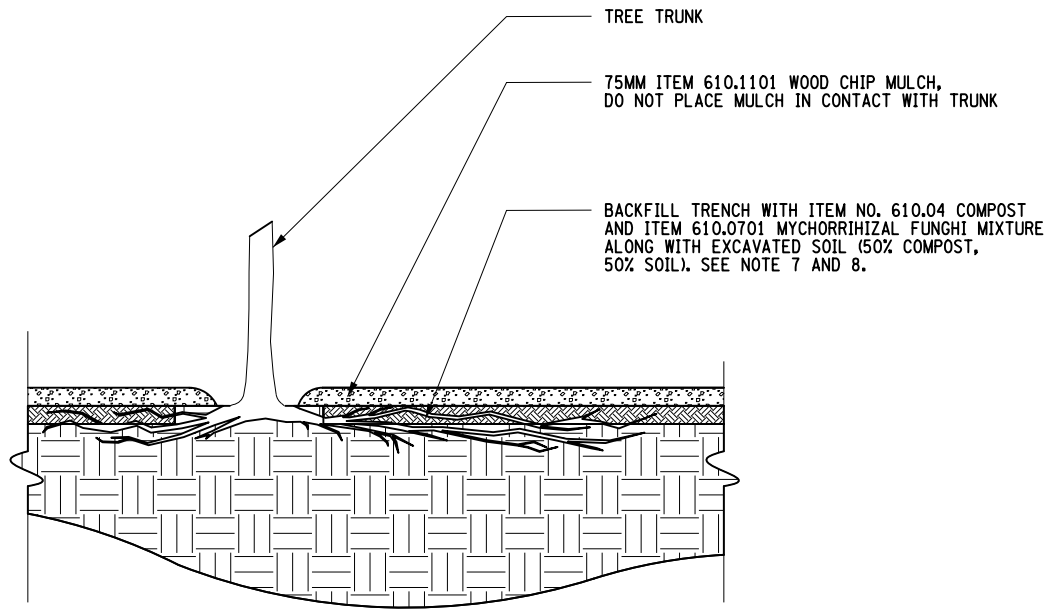
DRAFTING YELENA GRIG

CHECK MAREK CWIOK

PROJECT MANAGER RAJ AMIN



PLAN



SECTION

NOTES:

1. ALL WORK TO BE PERFORMED UNDER DIRECT SUPERVISION OF ARBORIST. ALL WORK LIMITS TO BE DETERMINED IN FIELD BY ARBORIST.
2. NO STAGING OR STORAGE OF EQUIPMENT OR MATERIALS SHALL OCCUR WITHIN THE TREE/LANDSCAPE PROTECTION ZONE OF THE PROJECT.
3. TREE/LANDSCAPE PROTECTION FENCING SHALL BE RETURNED TO ITS APPROVED, STAKED LOCATION UPON THE COMPLETION OF RADIAL TRENCHING PROCEDURES.
4. AIR-SPADE SHALL BE OPERATED BY ONLY PERSONNEL HAVING MORE THAN 1 YEAR OF EXPERIENCE OPERATING THE AIR-SPADE FOR ARBORICULTURAL PURPOSES SUCH AS ROOT COLLAR EXCAVATIONS AND ROOT INVIGORATION TECHNIQUES.
5. FOR DUST CONTROL; WORK AREA TO BE WATERED AT LEAST 24 HOURS IN ADVANCE OF BUT NO MORE THAT 48 HOURS PRIOR TO THE START OF ANY PNEUMATIC EXCAVATION. SEE ITEM 206.0401--11 FOR DUST CONTROL MEASURES.
6. WATER TRUCK TO BE PRESENT ON SITE DURING ALL RADIAL TRENCHING AND BACKFILL PROCEDURES. IRRIGATE ENTIRE ROOT ZONE TO FULL DEPTH OF TREATMENT. 6. CONTRACTOR TO PROVIDE CONTAINMENT STRUCTURE OR BARRIER TO CONTAIN SOIL PER ITEM 206.0401--11.
7. IF RECAPTURE OF EXCAVATED SOIL IS LESS THAN 50% FOR THE BACKFILL OF TRENCHES, THE DIFFERENCE SHALL BE MADE UP WITH ADDITIONAL TOPSOIL, ITEM 610.1402 TOPSOIL - ROADSIDE.
8. IF RECAPTURE OF EXCAVATED SOIL IS MORE THAT 50% FOR THE BACKFILL OF TRENCHES, THE EXCESS SOIL SHALL BE REMOVED FROM THE SITE OR REUSED AS DIRECTED BY THE REGIONAL LANDSCAPE ARCHITECT.

1 RADIAL TRENCHING
NTS

AFFIX SEAL: ON:	ALTERED BY: ON:

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	ITS TRAVEL TIME I-278 AND I-495	PIN X806.49	BRIDGES	CULVERTS	ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED	CONTRACT NUMBER	
	PS&E DATE: 06/21/2012					D262095	
						DRAWING NO. LAD-01	
	COUNTY: KINGS AND QUEEN					SHEET NO. 95	
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DESIGN SUPERVISOR RANDOLPH HUNT

JOB MANAGER MAREK CWIOK

DESIGN YELENA GRIG

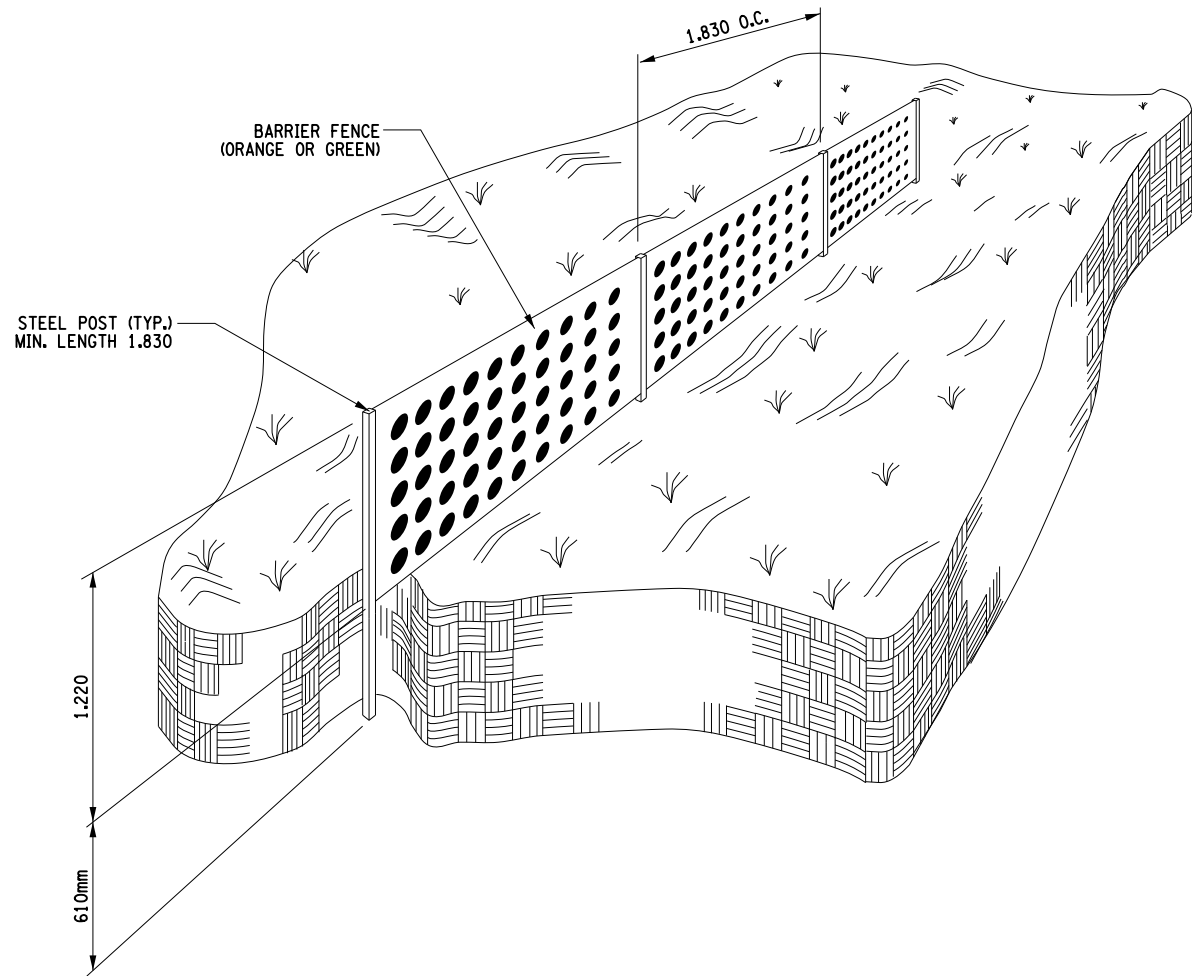
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DRAFTING YELENA GRIG

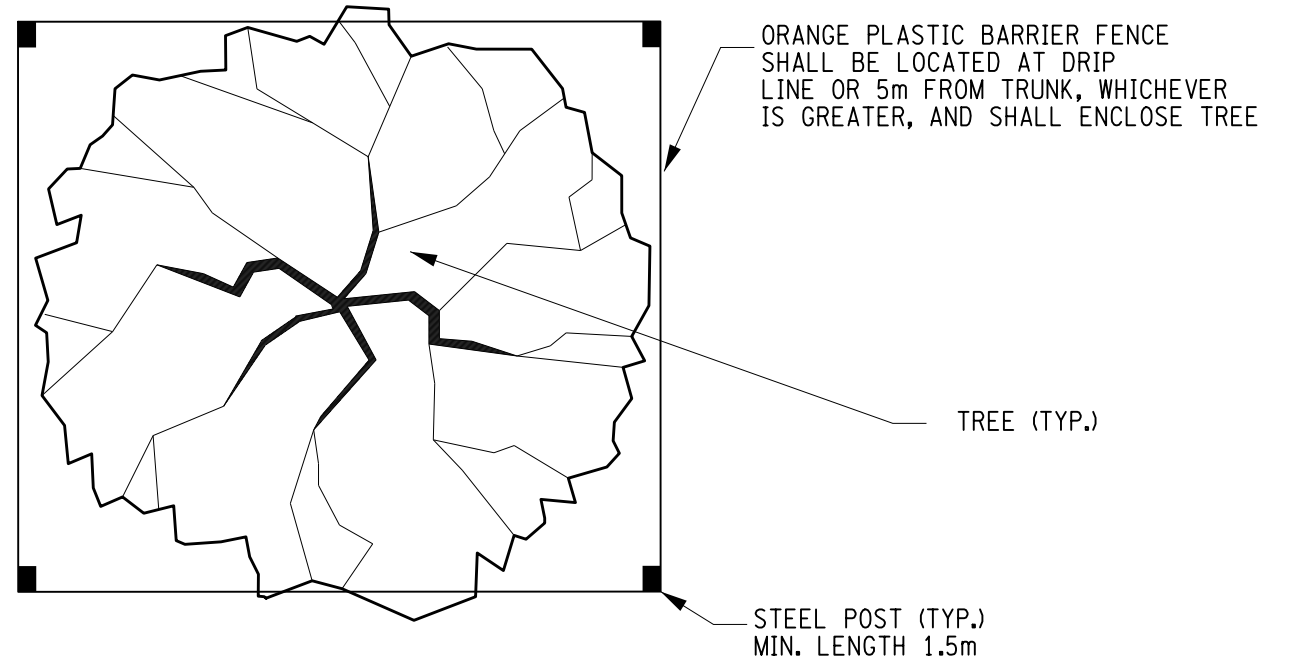
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PROJECT MANAGER RAJ AMIN

1 VEGETATION PROTECTION BARRIER - LINEAR MEASURE (TYPICAL ISOMETRIC VIEW)
SCALE N.T.S.



2 TEMPORARY TREE PROTECTION BARRIER FOR INDIVIDUAL TREE. ITEM 607.41010010
SCALE N.T.S.



PLAN VIEW

NOTES:

1. ALL WORK TO BE PERFORMED UNDER DIRECT SUPERVISION OF ARBORIST. ALL WORK LIMITS TO BE DETERMINED IN FIELD BY ARBORIST.
2. NO STAGING OR STORAGE OF EQUIPMENT OR MATERIALS SHALL OCCUR WITHIN THE TREE/LANDSCAPE PROTECTION ZONE OF THE PROJECT.
3. ALL WORK ON THIS SHEET TO BE PAID FOR UNDER ITEM 607.4101 10 TEMPORARY PLASTIC BARRIER FENCE UNLESS NOTED OTHERWISE.
4. SEE SPECIAL SPECIFICATIONS FOR WARNING SIGN. (SIZE, LETTERING, AND MOUNTING INTERVALS.)
5. SEE SPECIAL SPECIFICATIONS FOR PROPER ATTACHMENT TO POSTS AND PROPER OVERLAPPING.
6. LOCATIONS ARE AS SHOWN, IN TABLE, OR A.O.B.E..

AFFIX SEAL: ON:	ALTERED BY: ON:

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	ITS TRAVEL TIME I-278 AND I-495	PIN X806.49	BRIDGES	CULVERTS	ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED	CONTRACT NUMBER	
	PS&E DATE: 06/21/2012					D262095	
						DRAWING NO. LAD-02	
	COUNTY: KINGS AND QUEEN					SHEET NO. 96	
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.						NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11 DOCUMENT NAME: X80649_cph_lad_02.dgn	

FILE NAME = X80649_cph_lad.03.dgn
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USER = kdesai

DESIGN SUPERVISOR RANDOLPH HUNT

JOB MANAGER MAREK CWTOK

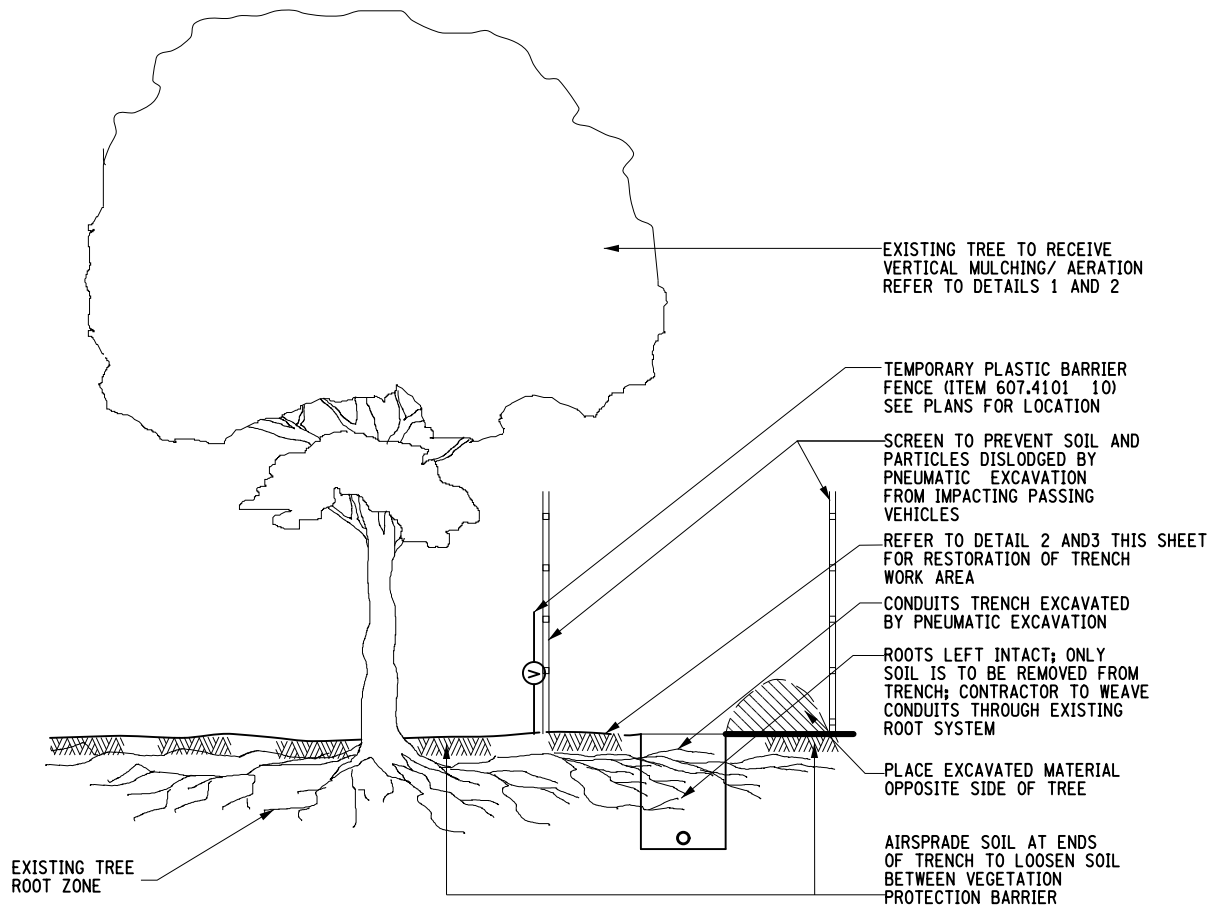
DESIGN VELENA GRIG

CHECK MAREK CWTOK

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CHECK MAREK CWTOK

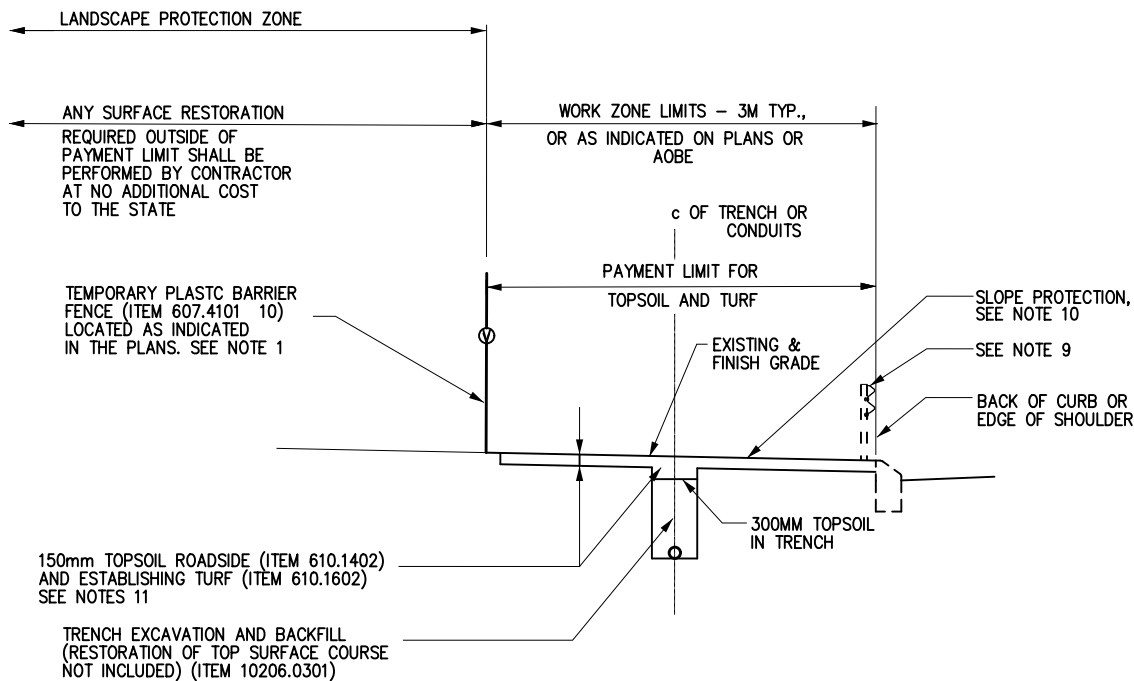
PROJECT MANAGER RAJ AMIN



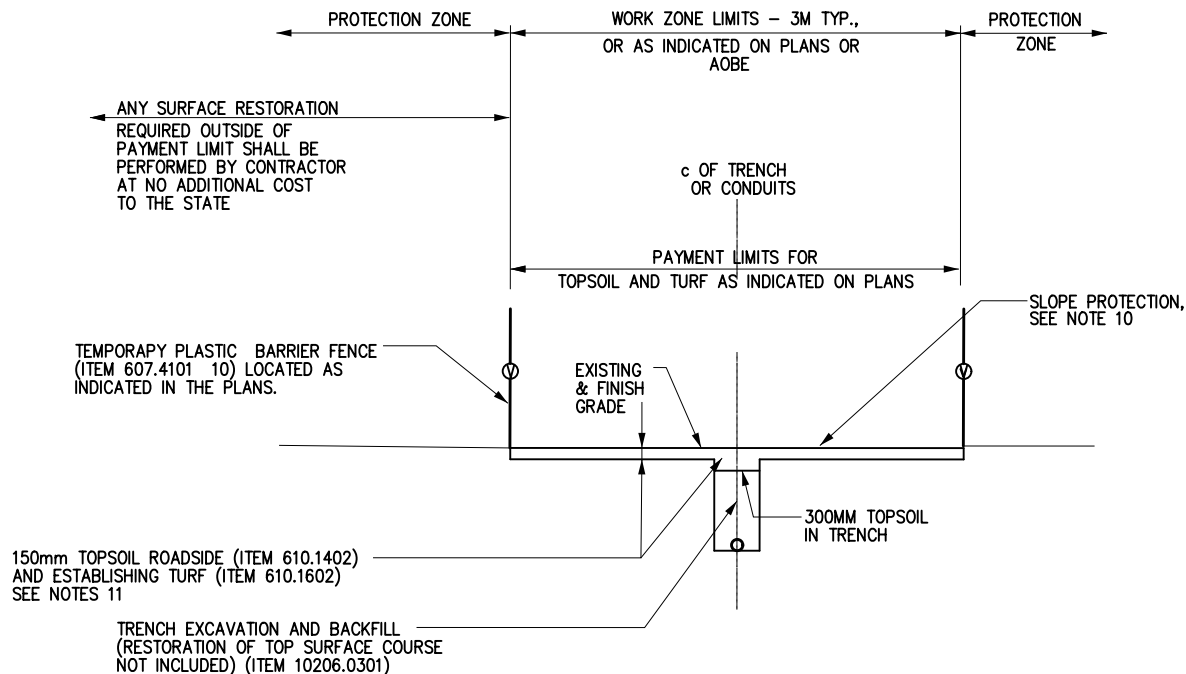
NOTES:

1. THE INTENT OF PNEUMATIC EXCAVATION IS TO LIMIT THE DAMAGE OF TREE ROOTS DURING THE EXCAVATION AND CONDUITS INSTALLATION PROCESS. CONTRACTOR IS TO REMOVE SOIL AND LEAVE THE EXISTING ROOT SYSTEM INTACT. PNEUMATIC EXCAVATION WILL EXPOSE ROOTS SO THAT CONTRACTOR MAY INSTALL CONDUITS THROUGH THE EXISTING ROOT SYSTEM.
2. HAND EXCAVATION IS TO BE USED ANY TIME EXCAVATION IS TO OCCUR UNDER THE DRIPLINE OF EXISTING TREES OR WHERE CALLED FOR IN THE CONTRACT DOCUMENTS.
3. ALL TREES WHICH ENCOUNTER PNEUMATIC EXCAVATION THROUGH THEIR ROOT ZONES ARE TO RECEIVE TREE ROOT ZONE TREATMENT - VERTICAL MULCHING/AERATION(614.01.11M).

① PNEUMATIC EXCAVATION
NTS



② TYPICAL WORK ZONE PROTECTION / RESTORATION (SHOULDER AREAS)
NTS



③ TYPICAL OPEN FIELD TRENCH PROTECTION / RESTORATION
NTS

NOTES:

1. THE EXACT LOCATIONS OF THE TREE PROTECTION FENCE AND TRENCH SHALL BE LOCATED IN THE FIELD BY THE CONTRACTOR AND APPROVED BY THE ENGINEER IN CHARGE AND NYSDOT ARBORIST AND LANDSCAPE ARCHITECT PRIOR TO THE INSTALLATION OF THE TREE PROTECTION FENCE AND PERFORMANCE OF TRENCHING OPERATIONS. REFER TO DWG. NO. LAN-01 AND LAN-02 FOR ADDITIONAL LANDSCAPE PROTECTION NOTES.
2. THE NEW YORK CITY DEPARTMENT OF TRANSPORTATION ARTERIAL MAINTENANCE DIVISION WILL ISSUE A \$4000 ECB SUMMONS FOR EACH OCCURRENCE OF WORK WHICH IS PERFORMED BY THE CONTRACTOR EITHER BEFORE THE TREE PROTECTION FENCING IS INSTALLED AS DESCRIBED ABOVE, OR IF WORK IS PERFORMED WITHIN ANY PROTECTED AREA.
3. THE NYSDOT ARBORIST MUST BE PRESENT TO PROVIDE DIRECTION AND APPROVAL FOR ALL TRENCHING, EXCAVATING, AND PRUNING WORK THAT IS TO BE PERFORMED WITHIN THE DRIPLINES OF ANY EXISTING TREES OR OTHER WOODY VEGETATION.
4. WHEN ROOT PRUNING, ROOTS ARE TO BE CLEANLY CUT USING A TRENCHING MACHINE, ROCK SAW OR VIBRATORY KNIFE. ANY REMAINING ROOTS SHALL BE HAND PRUNED. PAYMENT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR CARE OF TREE ITEM NOS. 614.0411, 614.0421 AND 614.0431. THE TRENCH SHALL BE IMMEDIATELY BACKFILLED TO PRECLUDE THE DRYING-OUT OF THE REMAINING VIABLE ROOTS AND CONCURRENTLY WATERED TO REMOVE VOIDS.
5. FOR LOCATIONS WHERE ROOT PRUNING IS ANTICIPATED, SEE DWG. NOS. LAN-04 TO LAN-07. THE COST FOR WATERING TREES THAT REQUIRE ROOT PRUNING SHALL BE PAID FOR UNDER ITEM 610.19-WATERING VEGETATION.
6. THE CONTRACTOR SHALL CONTROL HIS OPERATIONS TO MINIMIZE THE EXTENT OF REQUIRED SURFACE RESTORATION. PAYMENT FOR TOPSOIL AND ESTABLISHING TURF SHALL BE BASED UPON THE ACTUAL VOLUME OF TOPSOIL USED AND ACTUAL AREA OF TURF ESTABLISHED WITHIN THE PAYMENT LIMITS INDICATED.
7. PRIOR TO THE START OF ANY SURFACE RESTORATION THE CONTRACTOR SHALL MOW AND RAKE ALL GRASS AND REMOVE ALL TRASH, DEBRIS, ROCKS AND OTHER FOREIGN MATERIALS. PAYMENT TO BE INCLUDED IN THE PRICE BID FOR ITEM 610.1602.
8. AS SOON AS PRACTICAL FOLLOWING THE BACKFILLING OF TRENCHES AND INSTALLATION OF ITS EQUIPMENT, THE CONTRACTOR SHALL SPREAD TOPSOIL ROADSIDE (ITEM 610.1402) AND SEED FOR TURF. THE TOPSOIL SHALL HAVE A NOMINAL DEPTH OF 150mm, FEATHERED OUT AT EDGES, AND DEEPER WHERE REQUIRED TO FILL RUTS, HOLES AND OTHER SURFACE DEPRESSIONS. TOPSOIL SHALL BE RAKED AS REQUIRED IN ORDER TO CREATE A SMOOTH AND EVEN FINISH GRADE. CONTRACTOR SHALL PROVIDE RESTORATION OF THE AREAS BASED UPON CONDITIONS A-E AS SHOWN ON SHEET LAD-04. THIS SHALL BE FIELD VERIFIED PRIOR TO BEGINNING OF WORK.
9. AT SOME LOCATIONS, AS SHOWN ON THE PLANS, THE LANDSCAPE PROTECTION ZONE IS DEFINED BY EXISTING GUIDERAIL OR R.O.W. FENCING AND NO VEGETATION FENCING IS REQUIRED.
10. USE CLASS II, TYPE A EROSION CONTROL MATERIAL (ITEM 209.1901) ON SURFACES TO BE RESTORED THAT SLOPE IN EXCESS OF 1:12 OR WHERE ORDERED BY THE ENGINEER.
11. THE COST FOR CLEARING, EXCAVATION, DISPOSAL, GRADING AND SUBGRADE PREPARATION REQUIRED FOR TOPSOIL PLACEMENT AND ESTABLISHING TURF SHALL BE INCLUDED IN THE PRICE BID FOR THE VARIOUS LANDSCAPE ITEMS.

AFFIX SEAL: ON:	ALTERED BY: ON:

AS-BUILT REVISIONS
DESCRIPTION OF ALTERATIONS:

ITS TRAVEL TIME I-278 AND I-495

PS&E DATE: 06/21/2012

COUNTY: KINGS AND QUEEN

PIN X806.49

BRIDGES

CULVERTS

ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED

CONTRACT NUMBER

LANDSCAPE DETAILS - 3

D262095

DRAWING NO. LAD-03

SHEET NO. 97

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11

DOCUMENT NAME: X80649_cph_lad.03.dgn

FILE NAME = X80649_cph_lad_04.dgn
DATE/TIME = 03-AUG-2012 16:12
USER = kdesai

DESIGN SUPERVISOR RANDOLPH HUNT

JOB MANAGER MAREK CWIOK

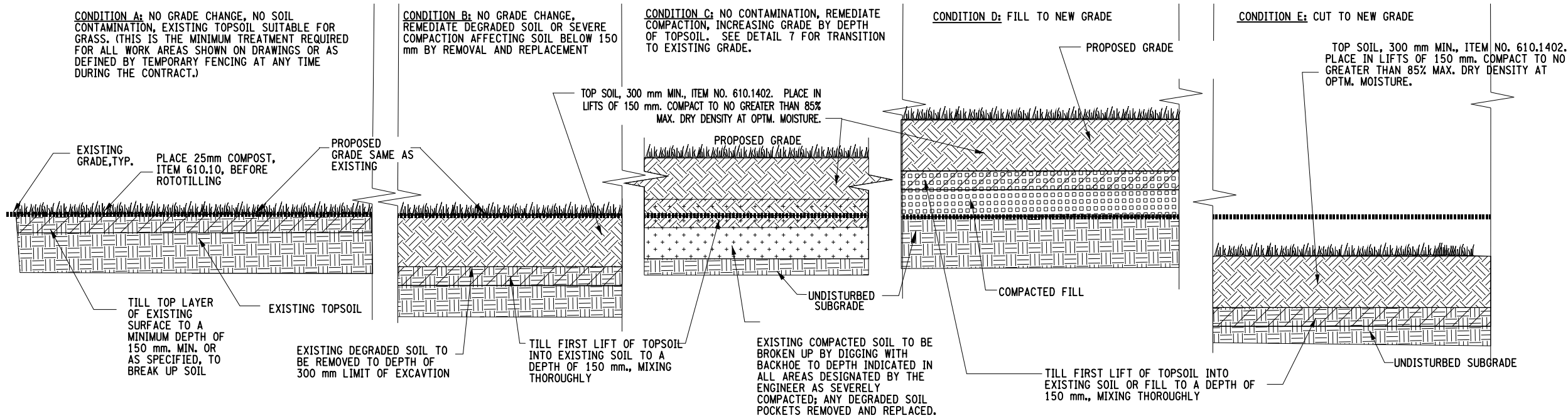
DESIGN YELENA GRIG

CHECK MAREK CWIOK

DRAFTING YELENA GRIG

CHECK MAREK CWIOK

PROJECT MANAGER RAJ AMIN



NOTES:


- SEE DETAILS 2 ON DWG LAD-05 FOR SOIL TREATMENTS TO BE USED WITHIN DRIPLINE OF EXISTING TREES.
- PROVIDE SUFFICIENT TOPSOIL TO MEET PROPOSED OR RE-ESTABLISH EXISTING GRADES AFTER COMPACTION AND OR SETTLING AND TO FILL DEPRESSIONS AND IRREGULARITIES IN EXISTING GRADES.
- ON SLOPES 1:3 OR GREATER, PROVIDE JUTE MESH, ITEM 209.1901, OVER SOIL FOR EROSION CONTROL.
- THE ENGINEER-IN-CHARGE, IN CONSULTATION WITH THE REGIONAL LANDSCAPE ARCHITECT, MAY ADJUST THE AREAS ASSIGNED TO THESE CONDITIONS BASED ON FIELD CONDITIONS.
- DEGRADED SOILS ARE: SOILS SHOWING ANY EVIDENCE OF RESIDUES OF PETROLEUM, CONCRETE WASH OR OTHER CHEMICAL CONTAMINANTS; DEBRIS (BRICK, CONCRETE, WOOD, MASONRY STONE, PLASTIC, GLASS, METAL, ETC.; GRAVEL (STONE >5mm MORE THAN 5% BY VOLUME); HARDPAN, FRAGIPAN OR CEMENTED PAN; OR ANY SOIL UNSUITABLE FOR OPTIMUM PLANT GROWTH FOR REASONS OTHER THAN LOW NUTRIENT/LOW ORGANIC LEVELS.
- THE REMOVAL AND DISPOSAL OF THE EXISTING SOIL SHALL BE PAID FOR UNDER ITEM 203.02.


- SUBMIT A REPRESENTATIVE SAMPLE OF TOPSOIL FOR ANALYSIS TO A CERTIFIED INDEPENDENT LABORATORY TO ENSURE CONFORMANCE TO THE SPECIFICATIONS. ANY ANALYSIS, WHERE BY THE DATE OF TESTING BY THE CERTIFIED INDEPENDENT LABORATORY IS IN EXCESS OF ONE MONTH PRIOR TO THE ACTUAL DATE OF DELIVERY TO THE CONSTRUCTION SITE, SHALL NOT BE ACCEPTED.
- PRIOR TO CONSTRUCTION SITE DELIVERY, ADVISE THE RESIDENT ENGINEER OF THE LOCATION OF THE SOURCE OF THE TOPSOIL AND SUBMIT A TWO POUND SAMPLE TO THE ENGINEER. DO NOT DELIVER TOPSOIL TO THE CONSTRUCTION SITE UNTIL THE ENGINEER HAS APPROVED THE SUBMITTAL IN WRITING.
- AFTER DELIVERY OF TOPSOIL TO THE CONSTRUCTION SITE, SUBMIT A REPRESENTATIVE SAMPLE FOR ANALYSIS TO A CERTIFIED, INDEPENDENT LABORATORY TO ENSURE CONFORMANCE WITH SPECIFICATIONS. SUBMIT TEST RESULTS TO THE RESIDENT ENGINEER FOR APPROVAL. IN THE EVENT THAT THE DELIVERED SAMPLE IS NOT CONSISTENT WITH THE SAMPLE APPROVED PRIOR TO DELIVERY, REMOVE THE DELIVERED TOPSOIL FROM THE CONSTRUCTION SITE AND REPLACE IT WITH MATERIAL THAT DOES CONFORM, ALL AT THE EXPENSE OF THE CONTRACTOR.

SOIL TREATMENT/REMEDIATION WITH GRASS OR GROUND COVER PLANTING OUTSIDE DRIPLINE OF EXISTING TREES

1

SCALE: NIS

AFFIX SEAL: ON:	ALTERED BY: ON:
	

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	ITS TRAVEL TIME I-278 AND I-495	PIN X806.49	BRIDGES	CULVERTS	ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED	CONTRACT NUMBER	
	PS&E DATE: 06/21/2012					D262095	
						DRAWING NO. LAD-04	
	COUNTY: KINGS AND QUEEN					SHEET NO. 98	
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JOB MANAGER MAREK CWTOK

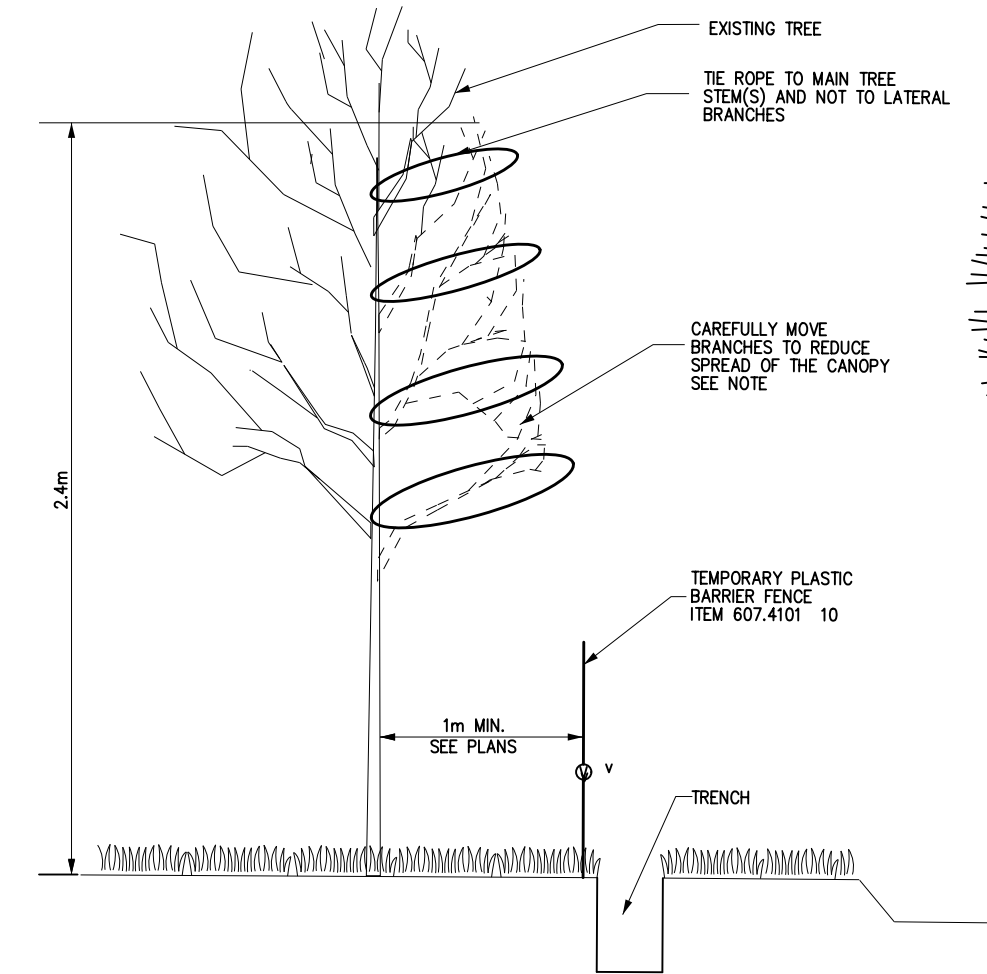
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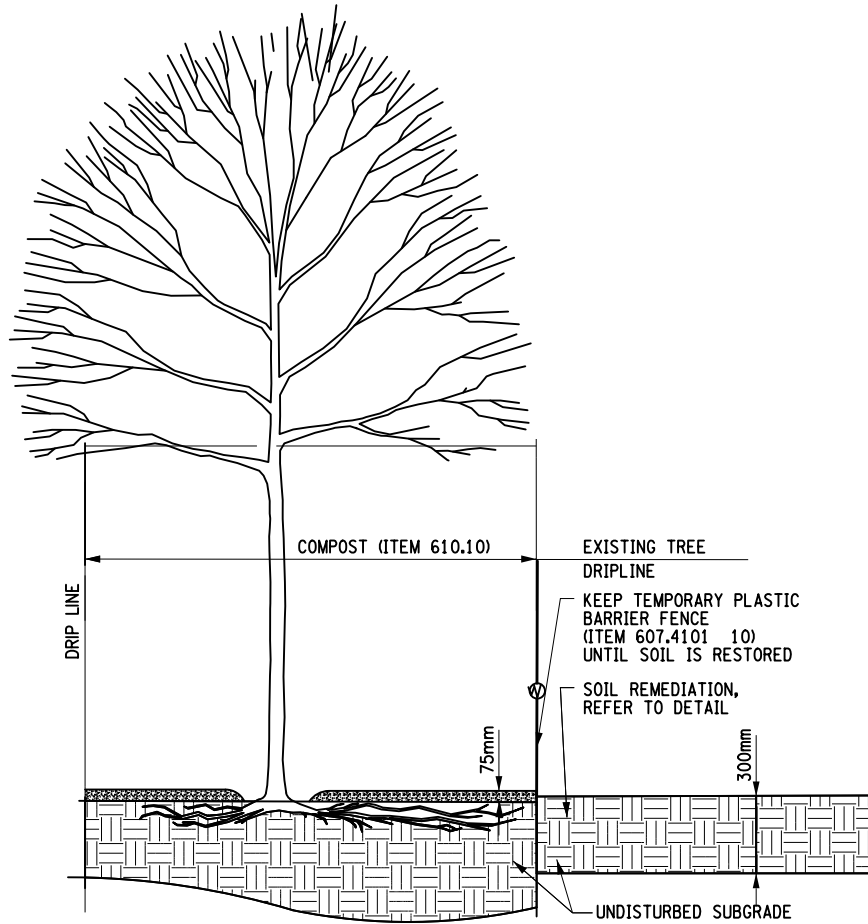
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PROJECT MANAGER RAJ AMIN



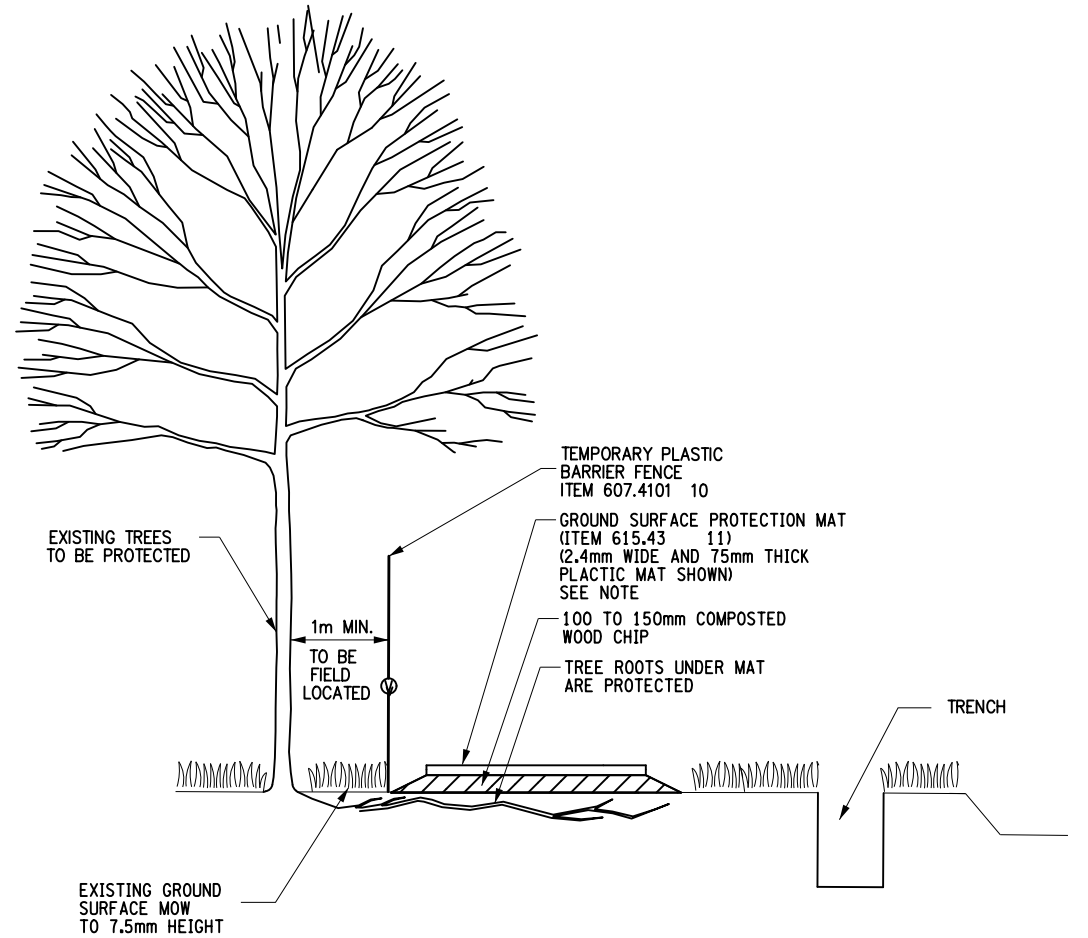
NOTE:
AT LOCATIONS WHERE THE BRANCHES OF EXISTING TREES EXTEND OVER OR ARE CLOSE TO AN AREA WHERE TRENCHING OR OTHER WORK WILL BE PERFORMED, THE CONTRACTOR, UNDER THE SUPERVISION AND GUIDANCE OF THE ARBORIST, SHALL CAREFULLY TIE-BACK THE BRANCHES USING A ROPE, MAKING SURE THAT NO BRANCHES ARE BROKEN OR FOLIAGE DAMAGED. THE BRANCHES SHALL NOT BE TIED-BACK ANY LONGER THAN THE TIME NEEDED TO PERFORM THE TRENCHING AND CONDUITS INSTALLATION WORK IMMEDIATELY ADJACENT TO THE TIED-BACK TREES. IN NO CASE SHALL THE BRANCHES BE TIED-BACK LONGER THAN 24 HOURS. COST TO BE INCLUDED IN THE PRICE BID FOR TRENCHING ITEMS.

1 TREE BRANCH TIE-BACK
SCALE: NTS



NOTE:
1. ALL WORK PERFORMED IN THIS DETAIL MUST BE DIRECTLY SUPERVISED BY THE PROJECT ARBORIST.
2. PRIOR TO APPLYING COMPOST, ROTO-TILLING AND ESTABLISHING TURF, CONTRACTOR SHALL REMOVE ALL TRASH AND DEBRIS WITHIN THE TREE DRIPLINE AND APPLY HERBICIDE TO ERADICATE ALL BROAD-LEAFED WEEDS. AFTER WEEDS HAVE DIED, CONTRACTOR SHALL CUT ALL TURF AREA WITHIN THE DRIPLINE TO A HEIGHT OF 50mm. PAYMENT AS ITEM 610.10 (COMPOST). THE CONTRACTOR SHALL SCARIFY THE SOIL SURFACE AND SPREAD THE COMPOST USING A HAND RAKE.
3. WITHIN EXISTING TREE DRIPLINE, RAKE IN 25mm OF COMPOST INTO TOP 50mm OF EXISTING SOIL. RAKE TO A UNIFORM SURFACE AND MAINTAIN APPROXIMATE EXISTING GRADE. PAYMENT AS ITEM 610.10 (COMPOST). NO SEPARATE PAYMENT FOR COMPOST.

2 REMEDIATION OF DISTURBED SOIL WITHIN TREE DRIPLINE
SCALE: NTS



NOTE:
CONTRACTOR TO PREPARE A PLAN FOR REVIEW AND APPROVAL BY THE ARBORIST PRIOR TO THE PLACEMENT OF GROUND SURFACE PROTECTION MATS.

3 GROUND SURFACE PROTECTION
SCALE: NTS

AFFIX SEAL: ON:	ALTERED BY: ON:

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	ITS TRAVEL TIME I-278 AND I-495	PIN X806.49	BRIDGES	CULVERTS	ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED LANDSCAPE DETAILS - 5	CONTRACT NUMBER	
	PS&E DATE: 06/21/2012					D262095	
						DRAWING NO. LAD-05	
	COUNTY: KINGS AND QUEEN					SHEET NO. 99	
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REGION 11 LANDSCAPE PROTECTION AND RESTORATION NOTES AND
SUPPLEMENTAL LANDSCAPE SPECIFICATIONS-NOVEMBER 8, 2011 VERSION

1. PRE-CONSTRUCTION LANDSCAPE INSPECTION: IF WORK ZONES ARE DETAILED IN THE CONTRACT DOCUMENTS, THE ENGINEER-IN-CHARGE SHALL CONTACT THE REGIONAL LANDSCAPE ARCHITECT, AND THE NYCDOT OFFICE OF CONSTRUCTION MITIGATION AND COORDINATION (WHO WILL IN TURN CONTACT THE DIRECTOR OF NYCDOT ARTERIAL MAINTENANCE AND, IF NYC DEPARTMENT OF PARKS AND RECREATION (NYCDPR) PERMITS ARE INVOLVED, THEIR INTERAGENCY COORDINATOR), WITHIN TEN (10) BUSINESS DAYS OF THE AWARD OF THE CONTRACT TO ARRANGE FOR A PRECONSTRUCTION SITE INSPECTION WITH THE CONTRACTOR.

ALL PRE-EXISTING LANDSCAPE FEATURES AND CONDITIONS, INCLUDING DAMAGE TO PLANTS AND STRUCTURES WITHIN THE WORK ZONE SHALL BE DOCUMENTED THROUGH PHOTOGRAPHS (STILL OR VIDEO) AND DESCRIPTIONS. THE LIMITS OF THE WORK ZONE, WHICH ARE THE PROJECT LIMITS AS SHOWN ON THE TITLE SHEET, WILL BE IDENTIFIED IN THE FIELD THROUGH MARKERS SUCH THAT THEY ARE CLEARLY IDENTIFIABLE. THIS PHOTO-DOCUMENTATION WILL BE KEPT IN THE FIELD OFFICE FOR LATER USE IN VERIFYING THE ADEQUACY OF LATER SITE RESTORATION.

2. WORK ZONE AND TREE/ LANDSCAPE PROTECTION STAKEOUT: ALSO PRIOR TO ANY OTHER WORK, THE CONTRACTOR SHALL STAKE OUT THE LIMITS OF "TREE/ LANDSCAPE PROTECTION AREAS" WITHIN THE WORK ZONE AS SHOWN ON THE PLANS FOR APPROVAL BY THE EIC IN CONSULTATION WITH THE REGIONAL LANDSCAPE ARCHITECT AND THE REGIONAL CONSTRUCTION ENVIRONMENTAL COORDINATOR, MAJID BUKHARI AT 718-482-4798 (AND NYCDOT AND NYCDPR AS APPROPRIATE). IF NO TREE/ LANDSCAPE PROTECTION AREA LIMITS ARE SPECIFICALLY SHOWN ON THE PLANS, AND WORK WILL OCCUR IN OR NEAR TREES OR VEGETATED AREAS, THE EIC, IN CONSULTATION AS ABOVE, WILL DIRECT THE CONTRACTOR, THE INTENT OF THE LIMITS IS TO PROTECT THE ROOT ZONE OF INDIVIDUAL TREES AND GROUPINGS OF TREES (USING THE "DRIPLINE"--THE VERTICAL PROJECTION TO THE GROUND OF THE TREES CANOPY--AS A GUIDE), LAWNS AND OTHER VALUABLE VEGETATION TO THE MAXIMUM EXTENT FEASIBLE WHILE ALLOWING THE CONTRACTOR SUFFICIENT ROOM TO OPERATE. THEREFORE, THE CONTRACTOR MUST ASSESS THE ADEQUACY OF THE ALLOWED SPACE FOR ALL CONCEIVABLE ACTIVITIES INCLUDING THE PARKING OF PERSONAL VEHICLES. IT IS UNDERSTOOD THAT WORK MAY NEED TO OCCUR IN THE ROOT ZONE OF TREES. IN SUCH CASES, THE CONTRACTOR MAY PROPOSE ADJUSTMENTS TO THE STAKEOUT OF PROTECTION LIMITS TO SUIT FIELD CONDITIONS AND SUCH OPERATIONS. ANY SUCH ADJUSTMENTS SHALL BE IN WRITING AND/OR SHOWN ON A PLAN AND/OR APPROVED IN THE FIELD BY THE EIC IN CONSULTATION AS ABOVE.

AFTER APPROVAL OF THE STAKEOUT AND ADJUSTMENTS, "VEGETATION PROTECTION BARRIER/FENCING" (PAID FOR UNDER THE APPROPRIATE ITEM) SHALL BE PLACED ALONG THESE LIMITS UNLESS OTHERWISE SPECIFIED. STREET TREES OR OTHER INDIVIDUAL TREES IN PITS IN PAVED AREAS WILL BE PROTECTED BY WOODEN BARRIERS (NYCDPR STANDARD) AS SHOWN IN THE PLANS, TO BE PAID FOR UNDER ITEM 594.01M.

ONCE VEGETATION PROTECTION BARRIER/FENCING IS IN PLACE, THE CONTRACTOR SHALL NOT ENTER, DAMAGE OR DIMINISH THE LANDSCAPE OR ANY PORTION THEREOF WITHIN THE DEFINED TREE/LANDSCAPE PROTECTION AREAS. ADDITIONALLY, WHERE WORK AREAS MUST ENCROACH ON TREE ROOT ZONES, THE CONTRACTOR, IF ORDERED BY THE EIC SHALL FURNISH AN 8 INCH LAYER OF WOOD CHIPS WITHIN THE DRIPLINE AREA TO REDUCE SOIL COMPACTION (PAID FOR UNDER THE APPROPRIATE ITEM). ALSO, WHEN OCCUPYING DRIPLINE AREAS, THE VEGETATION PROTECTION BARRIER FENCING MUST BE IN PLACE AND THE CONTRACTOR SHALL AVOID STOCKPILING, STORING EQUIPMENT, DRIVING OR PARKING ANY VEHICLES TO

THE MAXIMUM EXTENT POSSIBLE. ANY SUCH STOCKPILING SHALL BE REMOVED AS SOON AS POSSIBLE. BY THE END OF THE PROJECT, THE ENTIRE WORK ZONE, WITH THE EXCEPTION OF ITEMS DOCUMENTED DURING THE PRE-CONSTRUCTION INSPECTION, SHALL BE RESTORED PER THE CONTRACT DOCUMENTS AND/OR TO STANDARDS MUTUALLY AGREED UPON IN ADVANCE BY NYCDOT AND NYSDOT (AND NYCDPR, IF WITHIN THEIR JURISDICTION).

3. APPROVED TREE REMOVALS OR PRUNING: PRIOR TO THE PERFORMANCE OF ANY TREE PRUNING OR REMOVALS WITHIN NYCDPR JURISDICTION AS DETERMINED BY NYSDOT (OTHER THAN REMOVALS WHERE IT IS DETERMINED BY NYSDOT THAT "VOLUNTEER" TREES ARE CLEARLY CAUSING A SAFETY HAZARD OR THREATENING A STRUCTURE), THE CONTRACTOR SHALL CONTACT, THROUGH THE ENGINEER-IN-CHARGE, THE NEW YORK CITY PARKS DEPARTMENT (NYCDPR) BOROUGH FORESTER(S) FOR THE BOROUGH(S) IN WHICH THE WORK IS LOCATED AND SHALL OBTAIN ANY REQUIRED FORESTRY PERMIT(S). FOR ANY SUCH WORK SHOWN ON THE CONTRACT PLANS, NYCDPR PERMITS HAVE BEEN OBTAINED BY NYSDOT AND ARE PROVIDED TO THE CONTRACTOR IN THE PROPOSAL OF THE CONTRACT DOCUMENTS. THE NYCDPR BOROUGH FORESTERS ARE:

BOROUGH	NAME	TELEPHONE/ FAX:
BRONX:	MR. JAMES LEMIRE	718-430-1820/ 1818
BROOKLYN:	MR. JAMEY HEWITT	718-965-7737/ 7753
MANHATTAN:	MR. WILLIAM STEYER	212-860-1844/ 1359
QUEENS:	MR. JOSEPH BONKOWSKI	718-699-6724/ 7491
RICHMOND:	MR. JOHN KILCULLEN	718-390-2080/ 816-9194
	MR. BRIAN HALEY	

NO TREE PRUNING MAY BE PERFORMED EXCEPT BY (OR UNDER THE SUPERVISION OF) A QUALIFIED TREE-CARE PROFESSIONAL (CERTIFICATION BY THE INTERNATIONAL SOCIETY OF ARBORICULTURE, AMERICAN HORTICULTURAL SOCIETY OR APPROVED EQUAL) APPROVED BY THE REGIONAL LANDSCAPE ARCHITECT AND, WHERE WITHIN THE JURISDICTION OF THE NYCDPR, WITHOUT THE PERMISSION OF THE NYCDPR.

IN CASE OF A TREE REMOVAL, ALL REMNANTS INCLUDING, BUT NOT LIMITED TO STUMPS, TRUNKS, LIMBS, BRANCHES, AND FOLIAGE SHALL BE DISPOSED OF AS EXPEDITIOUSLY AS POSSIBLE. (IN ASIAN LONGHORN BEETLE QUARANTINE AREAS, RESTRICTIONS APPLY; SEE OTHER NOTES LATER IN THIS SECTION). THE COST OF DISPOSAL AS SPECIFIED ABOVE SHALL BE INCLUDED IN THE PRICE BID FOR THE RESPECTIVE TREE REMOVAL ITEMS.

4. "UNAUTHORIZED" TREE REMOVALS: IF THE CONTRACTOR REMOVES TREES NOT IDENTIFIED ON THE CONTRACT PLANS OR REMOVES TREES WITHIN THE JURISDICTION OF NYCDPR, AS DETERMINED BY NYSDOT, WHICH ARE NOT APPROVED BY THE BOROUGH FORESTER, OR SO SEVERELY DAMAGES TREES THAT IN THE JUDGMENT OF THE BOROUGH FORESTER THEY MUST BE REMOVED, THE CONTRACTOR SHALL PROVIDE REPLACEMENT TREES AT ITS OWN EXPENSE ACCORDING TO NYCDPR'S BASAL AREA FORMULA OR OTHER CRITERIA STATED IN THE NYCDPR PERMITS. THE FINAL LOCATION OF REPLACEMENT TREES (TO BE PLANTED IN ACCORDANCE WITH NYSDOT PLANTING SPECIFICATIONS FOR 3.15 INCH CALIPER TREES, UNLESS OTHERWISE SPECIFIED) SHOULD BE WITHIN THE PROJECT LIMITS AND WILL BE AS DIRECTED BY THE ENGINEER-IN-CHARGE AFTER CONSULTATION WITH THE REGIONAL LANDSCAPE ARCHITECT, (NYCDPR AND NYCDOT AS APPROPRIATE). IF PLANTING WITHIN THE PROJECT LIMITS IS NOT POSSIBLE, NYSDOT, NYCDOT AND NYCDPR WILL AGREE ON APPROPRIATE MITIGATION. ANY REPLACEMENT TREES PLANTED AS MITIGATION MUST BE WATERED, MAINTAINED AND GUARANTEED PER NYSDOT STANDARD SPECIFICATIONS AT NO COST TO THE STATE.
5. GENERAL PROTECTION OF EXISTING LANDSCAPE: AT ALL TIMES DURING THE COURSE OF THE PROJECT, THE CONTRACTOR SHALL AVOID OR MINIMIZE: SOIL COMPACTION, POLLUTION, EROSION AND IMPACTS TO EXISTING VEGETATION EVEN WHEN REMOVAL, SELECTIVE THINNING OR CLEARING AND GRUBBING ARE SPECIFIED IN THE CONTRACT PLANS. THE CONTRACTOR SHALL PLACE APPROVED WOOD CHIPS AND/OR GEOTEXTILE A.O.B.E. ON UNPAVED AREAS WHERE MATERIALS WILL BE STOCKPILED, TO MINIMIZE SOIL COMPACTION AND PREVENT CONTAMINATION OF EXISTING SOIL. UNDER NO CIRCUMSTANCES MAY PETROLEUM PRODUCTS, CONCRETE WASH WATER, PAINT, OR OTHER POLLUTANTS BE ALLOWED TO SEEP INTO THE LANDSCAPE OR CITY DRAINAGE SYSTEM.

6. STAGING/STORAGE AREAS: IF THE CONTRACT PLANS IDENTIFY STAGING/STORAGE AREAS ON PARKWAYS OR OTHER PROPERTIES UNDER THE JURISDICTION OF THE NYCDPR AND/OR NYCDOT, AS DETERMINED BY NYSDOT, SUCH AREAS HAVE BEEN APPROVED BY THE NYCDPR AND/OR NYCDOT, AND HAVE BEEN INCLUDED IN THEIR CONSTRUCTION PERMIT.

IF THE CONTRACTOR PROPOSES ANY OTHER CONTRACTOR YARDS, MAIN STAGING AREAS OR ACCESS ROUTES ON PARKWAYS OR OTHER PROPERTIES UNDER THE JURISDICTION OF THE NYCDPR AND/OR NYCDOT, AS DETERMINED BY NYSDOT, IT WILL BE THE CONTRACTORS RESPONSIBILITY, THROUGH THE EIC, TO OBTAIN NYCDPR'S AND/OR NYCDOT'S APPROVAL AND, IF REQUESTED, A SEPARATE CONSTRUCTION PERMIT AT ITS OWN EXPENSE PRIOR TO THE USE OF ANY SUCH SITE (FOR NYCDPR PROPERTY, THIS WILL BE THROUGH THE DIRECTOR OF CONSTRUCTION PERMITS, 718-760-6737, FAX 6730). THE CONTRACTOR IS CAUTIONED THAT STANDARD FEES MAY APPLY FOR USE OF NYCDPR PROPERTY. WHERE STAGING AND/OR STORAGE IS PROPOSED ON STATE OF NEW YORK PROPERTY, NYCDOT AND/OR NYCDPR APPROVAL IS NOT REQUIRED. IT IS UNDERSTOOD THAT LOCALIZED STAGING/STORAGE AREAS MAY BE NECESSARY IN ADDITION TO ANY MAIN AREAS SHOWN ON PLANS. OUTSIDE TREE/LANDSCAPE PROTECTION AREAS THE ENGINEER-IN-CHARGE (IN CONSULTATION WITH NYCDPR AND NYCDOT AS APPROPRIATE) WILL DESIGNATE SUCH OTHER LOCALIZED AREAS APPROPRIATE FOR STORAGE OF MATERIALS, EQUIPMENT AS WELL AS PARKING OF CONTRACTORS VEHICLES AND ACCESS ROUTES THROUGH THE ACTIVE WORK ZONE. THESE AREAS MUST BE DEFINED BY APPROPRIATE FENCING AND MUST MEET ALL WORK ZONE TRAFFIC CONTROL (WZTC) AND SAFETY CRITERIA AS WELL.

ON SLOPING AREAS, EROSION-CONTROL METHODS (PER NYSDOT STANDARD EROSION CONTROL ITEMS) WILL BE USED TO PREVENT MOVEMENT OF SOIL, ESPECIALLY INTO STORM DRAINS. WHERE IT WILL NOT IMPACT ON TREES, EXISTING TOPSOIL MAY BE SCRAPED OFF AND STORED IN PILES, TO BE REPLACED AT TIME OF SITE RESTORATION.

7. LANDSCAPE MAINTENANCE DURING CONSTRUCTION: DURING THE COURSE OF THE PROJECT, THE CONTRACTOR SHALL MAINTAIN THE APPEARANCE OF THE PROJECT SITE BY REMOVING LITTER, DEBRIS AND EXCESS MATERIALS AS A RESULT OF THE CONSTRUCTION OPERATION FROM THE WORK SITE ON A REGULAR BASIS AND STORING ALL CONSTRUCTION EQUIPMENT AND CONSTRUCTION MATERIAL IN AN ORGANIZED FASHION THROUGHOUT THE CONSTRUCTION PERIOD. THIS WILL DETER ILLEGAL DUMPING AND ENCOURAGE THE PUBLIC TO RESPECT THE PROJECT AND THE REST OF THE ROADWAY.

ALL AREAS INACCESSIBLE TO AGENCIES MAINTENANCE FORCES MUST BE KEPT AS CLEAN AS POSSIBLE BY THE CONTRACTOR. THE EIC WILL CONSULT WITH THE CONTRACTOR AND NYCDOT TO DETERMINE AND AGREE UPON THE LIMITS OF SUCH AREAS. ADDITIONALLY, ANY VEGETATION REQUIRING MAINTENANCE SUCH AS MOWING OF GRASS WILL BE MAINTAINED TO THE STANDARD OCCURRING ON THE REMAINDER

ADDITIONAL NOTES

1. AREAS TO BE SEEDED ON THIS PROJECT MAY BE SUBJECT TO VEHICLE INTRUSIONS WHICH MAY RESULT IN THE NECESSITY OF RESEEDING AT NO COST TO THE STATE. THE CONTRACTOR IS ADVISED TO TAKE ADEQUATE PROTECTIVE MEASURES TO MINIMIZE SUCH VEHICLE INTRUSIONS. THE FOLLOWING PROTECTIVE MEASURES ARE SUGGESTED:
- A ORANGE PAINTED 38MM x 38MM WOODEN STAKES EXTENDING 0.3M ABOVE THE GROUND SURFACE SHOULD BE PLACED IMMEDIATELY IN BACK OF THE CURB AT 15M INTERVALS.
 - B SIGNS APPROXIMATELY 450MM x 150MM IN SIZE, PROJECTING 0.3M ABOVE THE GROUND, WITH THE WORDS "SEEDED AREA" SHOULD BE PLACED AT MAXIMUM 60M INTERVALS AND 2.1M OFFSET FROM THE CURB THROUGHOUT THE SEEDED AREA SUBJECT TO INTRUSION.
 - C REFLECTORIZED DRUMS, AT A 2.1M OFFSET FROM THE CURB, SHALL BE PLACED AT 60M LONGITUDINAL INTERVALS. THESE DRUMS SHOULD ALTERNATE WITH THE "SEEDED AREA" SIGNS SUGGESTED UNDER 'B' ABOVE.

PROVISION OF THE ABOVE SUGGESTED PROTECTIVE MEASURES SHALL NOT IN ANY WAY RELIEVE THE CONTRACTOR OF HIS/HER RESPONSIBILITY FOR A SATISFACTORY TURF UNDER SUBSECTION 610-3.02(F) AND FOR CARE OF SEEDED TURF AREAS AND FOR PROTECTION OF THESE AREAS FROM TRAFFIC AND OTHER DAMAGE UNDER SUBSECTION 610-3.02(G) OF THE SPECIFICATIONS. THE CONTRACTOR SHALL COORDINATE THESE PROTECTIVE MEASURES WITH THE SOIL AND EROSION CONTROL MEASURES AND MAINTENANCE AND PROTECTION OF TRAFFIC STAGING.

2. NO TREES ARE TO BE PLANTED IN A CLEAR ZONE.

3. A) LIMITS OF SITE CLEARING OUTSIDE OF PAVED ROADWAYS SHALL BE LIMITED TO WITHIN THE WORK AREAS DESIGNATED ON THE PLANS. ADDITIONAL CLEARING MAY BE REQUIRED TO FACILITATE ACCESSING EXISTING EQUIPMENT, UTILITY POLES AND MANHOLES, OR DUE TO FIELD CONDITIONS AT TIME OF CONSTRUCTION. SITE CLEARING MAY ONLY BE PERFORMED OUTSIDE THE DESIGNATED WORK ZONES INDICATED ON THE PLANS AS APPROVED BY THE NEW YORK CITY DEPARTMENT OF TRANSPORTATION ARTERIAL MAINTENANCE DIVISION AND THE ENGINEER-IN-CHARGE.

B) CLEARING OR PRUNING MAY BE REQUIRED AT OTHER ROADSIDE LOCATIONS WITHIN THE CONTRACT LIMITS AOE TO PROVIDE ADEQUATE SIGHT DISTANCES TO TRAFFIC CONTROL DEVICES AND ITS EQUIPMENT. ADDITIONAL CLEARING OR PRUNING SHALL ONLY BE PERFORMED IN ACCORDANCE WITH THE RESTRICTIONS AS STIPULATED UNDER PART (A) ABOVE, THE CONTRACT DOCUMENTS AND AS ORDERED BY THE ENGINEER.

4. PAYMENT FOR ALL NECESSARY SITE CLEARING UNDER THIS CONTRACT SHALL BE INCLUDED IN THE PRICE BID FOR THE VARIOUS CONTRACT ITEMS UNLESS OTHERWISE INDICATED. TREE PRUNING, IF NECESSARY AND APPROVED BY THE ENGINEER IN CHARGE, SHALL BE PAID FOR UNDER THE APPLICABLE PAYMENT ITEMS FOR TREE CARE AND PRUNING.


5. RESTORATION OF UNPAVED WORK AREAS IS TO BE FOR THE ENTIRE WIDTH OF THE DISTURBED AREA AS SHOWN ON THE DRAWINGS AND AS PER SECTION 206-5.04 "CONDUIT EXCAVATION AND BACKFILL", SECTION 107-08 "PROTECTION AND RESTORATION OF PROPERTY AND LANDSCAPE" THE RESTORATION IN GRASSED AREAS SHALL BE PAID FOR UNDER ITEM 610.0203 ESTABLISHING TURF AND ITEM 613.02 TOPSOIL (150mm DEPTH) EXCEPT AS OTHERWISE NOTED ON THE PLANS.


6. THE NYSDOT ARBORIST/TREE CONSULTANT WILL BE PRESENT WHENEVER TRENCHING OCCURS WITHIN THE DRIPLINE FOR ANY TREE. PNEUMATIC EXCAVATION AND BACKFILL WILL BE REQUIRED FOR ANY TRENCHING WITHIN THE TREE DRIPLINE, EVEN THOUGH THE TRENCH MAY BE OUTSIDE OF THE TREE PROTECTION ZONE AS DELINEATED BY THE TREE/VEGETATION PROTECTION BARRIER. THERE WILL BE NO SEPARATE PAYMENT FOR ANY REQUIRED HAND EXCAVATION. PNEUMATIC EXCAVATION AND BACKFILL SHALL BE PAID FOR AS INDICATED UNDER THE NOTES FOR PNEUMATIC (AIR SPADE) EXCAVATION.

7. THE CONTRACTOR SHALL STAKE OUT THE LOCATIONS OF ALL WORK FOR APPROVAL BY THE EIC IN CONSULTATION WITH THE REGIONAL LANDSCAPE ARCHITECT (AND NYCDOT AND NYCDPR AS APPROPRIATE). NO WORK SHALL BE PERFORMED PRIOR TO THE APPROVAL OF EACH LOCATION. NO WORK SHALL BE INSTALLED WITHOUT PRIOR APPROVAL BY THE RLA, NYCDPR AND NYCDOT.

8. A FILLED WATER TRUCK SHALL BE ON SITE PRIOR TO ANY EXCAVATION OR BACKFILL OF TRENCHES FOR IMMEDIATE WATERING OF BACKFILLED TRENCHES TO REDUCE VOIDS.

9. THE PROJECT MAY REQUIRE REMOVAL OF THE TREES WHICH WILL BE IDENTIFIED IN THE FIELD DURING CONSTRUCTION.

AFFIX SEAL: ON:	ALTERED BY: ON:
	

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	COUNTY: KINGS AND QUEEN					SHEET NO. 100	

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DESIGN SUPERVISOR RANDOLPH HUNT

JOB MANAGER MAREK CWTOK

DESIGN YELENA GRIG

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PROJECT MANAGER RAJ AMIN

PNEUMATIC (AIR SPADE) EXCAVATION NOTES:

- AT LOCATIONS AS INDICATED ON THE DRAWINGS OR AS ORDERED BY THE ENGINEER WHERE CONDUIT EXCAVATION WILL OCCUR IN PROXIMITY TO THE TREE DRIPLINE, THE EXCAVATION SHALL BE PERFORMED USING A PNEUMATIC EXCAVATING TOOL.
- THE REQUIRED EQUIPMENT SHALL INCLUDE A PNEUMATIC EXCAVATING TOOL SPECIFICALLY DESIGNED TO REMOVE SOIL WITHOUT DAMAGE TO TREE ROOTS, AN AIR COMPRESSOR TO SUPPLY THE AIR FLOW TO THE PNEUMATIC EXCAVATING TOOL, AND A VACUUM TRUCK TO COLLECT THE EXCAVATED SOIL FROM THE TRENCH AS IT IS PNEUMATICALLY REMOVED.
- THE CONTRACTOR SHALL PLACE WET BURLAP OR COTTON TOWELS UPON BOTH THE FIBROUS AND STRUCTURAL ROOTS IMMEDIATELY AFTER THEY HAVE BEEN EXPOSED BY THE PNEUMATIC EXCAVATING TOOL. THE PLASTIC SHEETING SHALL BE PLACED OVER BURLAP OR COTTON TOWELS. TO MAINTAIN ROOT MOISTURE UNTIL CONDUIT INSTALLATION IS COMPLETE AND BACKFILLING OPERATIONS BEGIN.
- APPROVED SUBSOIL MATERIAL SHALL BE USED AS BACKFILL UP TO A DEPTH OF 300 mm BELOW FINISH GRADE. SUBSOIL MATERIAL MAY CONSIST OF SUITABLE EXCAVATED MATERIAL OR APPROVED TOPSOIL THAT IS FREE OF VEGETATIVE OR NON-DECOMPOSED ORGANIC MATERIAL (I.E. STICKS, BRANCHES).
- THE TOP 300 mm OF THE EXCAVATION SHALL BE BACKFILLED WITH APPROVED TOPSOIL AND WILL BE PAID FOR AS TOPSOIL - ITEM 613.02. THE BACKFILLED TRENCH WILL BE RAKED TO A UNIFORM FINISHED GRADE, LIMED, FERTILIZED, SEEDED AND MULCHED - PAYMENT AS ESTABLISHING TURF - ITEM 610.0203.

CONTINGENCY TREE PRUNING:

- BASED ON ACTUAL FIELD CONDITIONS DURING CONSTRUCTION, BRANCH PRUNING OF A LIMITED NUMBER OF TREES MAY BE REQUIRED TO FACILITATE ITS EQUIPMENT INSTALLATION, OBTAIN ACCESS TO UTILITY SERVICE POINTS, OR TO PROVIDE ADEQUATE SIGHT LINES FOR MOTORIST SAFETY OR FOR ITS EQUIPMENT OPERATION. THIS CONTINGENCY TREE BRANCH PRUNING WILL ONLY BE PERFORMED IF DIRECTED BY THE EIC. PRUNING OF TREE BRANCHES SHALL BE PAID FOR UNDER THE FOLLOWING ITEMS AS APPLICABLE:

ITEM 614.0411 - CARE OF TREES UP TO 300 MM DIAMETER AT BREAST HEIGHT - PRUNING
ITEM 614.0421 - CARE OF TREES OVER 300 MM-600 MM DIAMETER AT BREAST HEIGHT PRUNING

EI 08-001 NOTE:

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TREE ROOT PRUNING:

- TREES REQUIRING ROOT PRUNING WILL BE ACCORDING TO THE FOLLOWING ITEMS AS APPLICABLE:

ITEM 614.0411 - CARE OF TREES UP TO 300 MM DIAMETER AT BREAST HEIGHT - PRUNING
ITEM 614.0421 - CARE OF TREES OVER 300 MM-600 MM DIAMETER AT BREAST HEIGHT PRUNING
- THE NYSDOT ARBORIST /TREE CONSULTANT SHALL IDENTIFY LOCATIONS WHERE TREE ROOTS MAY BE ENCOUNTERED PRIOR TO THE START OF TRENCH EXCAVATIONS IN THE GENERAL VICINITY OF TREES. TREE ROOTS IN SUCH LOCATIONS SHALL BE UNCOVERED WITH CARE AND CLEANLY CUT WITH SHARP TOOLS PRIOR TO CONSTRUCTION EXCAVATION AS DIRECTED BY EIC IN CONSULTATION WITH NYSDOT ARBORIST/TREE CONSULTANT.



TREE ROOTS GREATER THAN 25mm IN DIAMETER, MEASURED AT THE EDGE OF EXCAVATION, SHALL BE PRUNED WITHIN 24 HOURS OF THE TIME THEY HAVE BEEN DAMAGED BY EXCAVATION OPERATIONS. THE SEVERED ROOT SHALL BE PRUNED AT THE EDGE OF EXCAVATION, OR 25mm BEYOND THE ENTIRE DAMAGED PORTION OF THE TREE ROOT, IF DAMAGED ROOT EXTENDS BEYOND THE EDGE OF EXCAVATION INTO UNDISTURBED SOIL. ALL CUTS SHALL BE CLEANLY MADE WITH SHARP TOOLS.
- THE EXCAVATED AREA AROUND THE EXPOSED TREE ROOTS SHALL BE BACKFILLED AS SOON AS CONSTRUCTION ACTIVITIES PERMIT. IF THE EXCAVATED AREA SURROUNDING THE EXISTING TREE ROOTS IS NOT BACKFILLED WITHIN 24 HOURS, THE EXPOSED ROOTS SHALL BE KEPT MOIST USING A METHOD APPROVED BY REGIONAL LANDSCAPE ARCHITECT (RLA) TO PREVENT DESICCATION. AMENDED EXISTING SOIL SHALL BE USED AS BACKFILL MATERIAL WITHIN THE DISTURBED ROOT ZONE. EXISTING SOIL SHALL BE AMENDED WITH HUMUS OR PEAT, PEAT MOSS OR SOURCE SEPARATED COMPOST IN THE RATIO OF ONE PART ORGANIC TO SEVEN PARTS EXCAVATED SOIL - PAYMENT TO BE INCLUDED IN THE PRICE BID FOR THE TREE CARE AND PRUNING ITEMS.
- UPON FINISH GRADING, THE AREA WITHIN THE DRIPLINE OF THE TREE SHALL BE MULCHED WITH 3" WOOD CHIP AND WATERED AT A RATE OF 25 LITERS PER SM UNLESS OTHERWISE DIRECTED BY EIC.

ARBORIST NOTES:

THE CONTRACTOR SHALL HIRE A CERTIFIED ARBORIST TO PERFORM DUTIES AS DESCRIBED HEREIN. THE ARBORIST SHALL ACT AT THE CONTRACTOR'S REPRESENTATIVE IN FULFILLING THE TASKS DESCRIBED BELOW AND TO ENSURE THE CONTRACTOR'S COMPLIANCE TO ALL APPLICABLE REQUIREMENTS OF THE LANDSCAPE PLANS AND SPECIFICATIONS, AND TO MINIMIZE THE IMPACT TO THE EXISTING LANDSCAPING DUE TO CONSTRUCTION ACTIVITIES. ALL WORK SHALL BE PERFORMED IN CONSULTATION WITH THE ENGINEER AND THE REGIONAL LANDSCAPE ARCHITECT. PAYMENT FOR THE SERVICES OF THE ARBORIST SHALL BE INCLUDED UNDER RELATED CONTRACT ITEMS. NO SEPARATE PAYMENT WILL BE MADE FOR SUPPLYING THE ARBORIST.

THE PROPOSED SCOPE OF DUTIES AND RESPONSIBILITIES FOR THE CERTIFIED ARBORIST DURING THE PROJECT'S CONSTRUCTION PHASE SHALL INCLUDE:

- SUPERVISE INSTALLATION OF VEGETATION PROTECTION BARRIERS AND OTHER SAFEGUARDS TO VEGETATION PER CONTRACT.
- CONSULT WITH THE ENGINEER AND CONTRACTOR TO INCLUDE ANY MODIFICATIONS TO PROTECTION LIMITS AND OTHER SAFEGUARDS, AND TREE CARE TASKS. DOCUMENT CHANGES TO CONTRACT LIMITS AND REQUIREMENTS.
- PROVIDE RECOMMENDATIONS FOR MITIGATIONS FROM IMPACTS TO TREES GENERATED BY ANY MODIFICATIONS TO CONTRACT VEGETATION PROTECTION LIMITS.
- PROVIDE MONTHLY INSPECTION AND REPORTS OF VEGETATION AND PROTECTION MEASURES, AND WHEN REQUESTED BY THE ENGINEER, BE AVAILABLE FOR MEETINGS/INSPECTIONS OF PROJECT, AS REQUIRED, WITHIN 72 HOURS NOTICE.
- DIRECTLY SUPERVISE ALL WORK PERFORMED ON TREES OR IN AND ON THE SOIL UNDER TREE CANOPIES.
- TASKS ARE TO BE CONTINUED DURING PLANTING AND PERIOD OF ESTABLISHMENT, AND SHALL INCLUDE REPORT AND SUPERVISION ON THOSE TASKS.

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DESIGN SUPERVISOR RANDOLPH HUNT

JOB MANAGER MAREK Cwik


DESIGN YELENA GRIG

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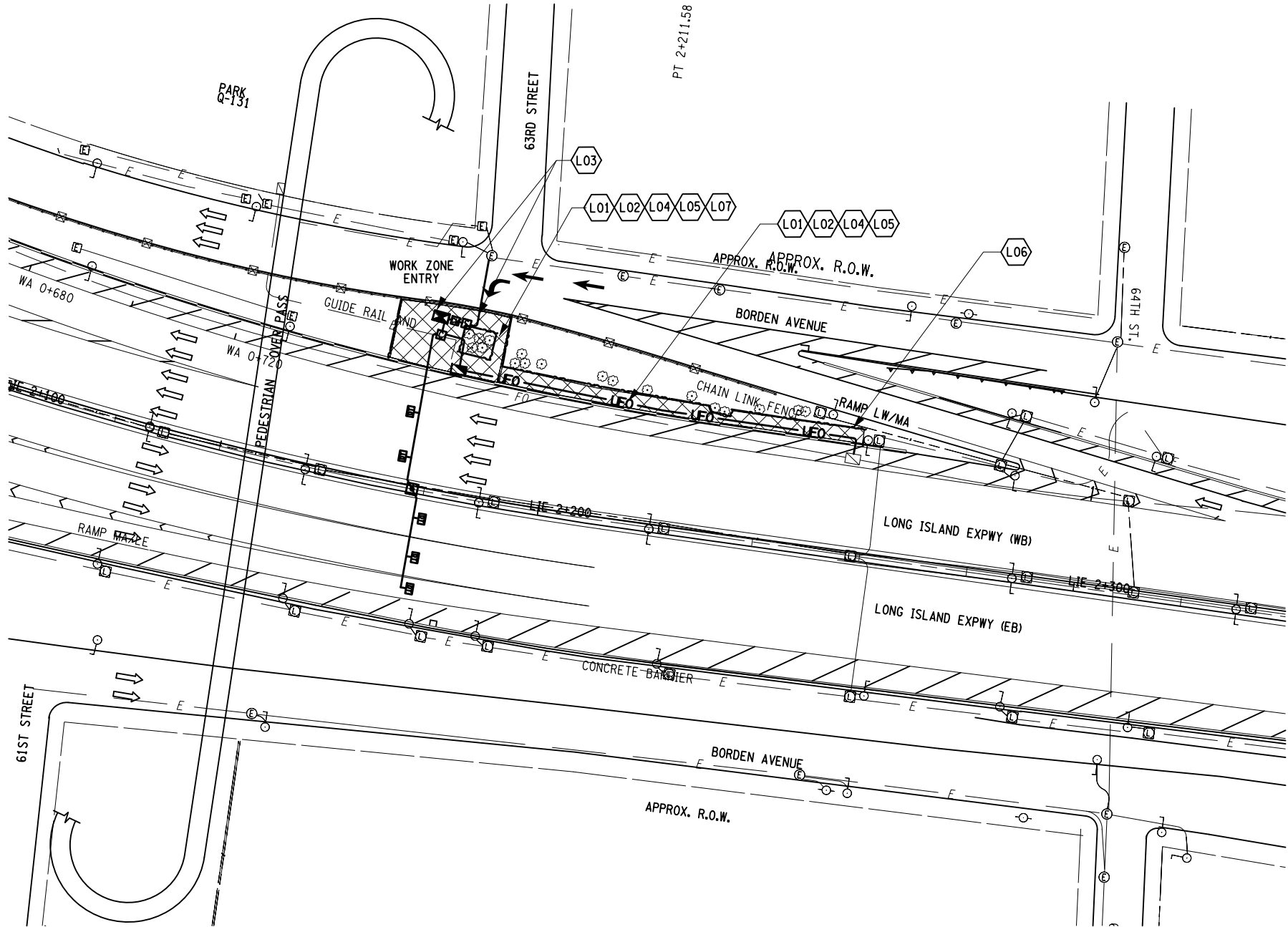
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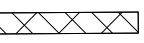
PROJECT MANAGER RAJ AMIN

AFFIX SEAL: ON:	ALTERED BY: ON:
	

ID	ITEM NUMBER	DESCRIPTION	UNIT	QTY
L01	201.07	CLEARING AND GRUBBING	HA	0.05
L02	610.1602	TURF ESTABLISHMENT - LAWNS	SQM	420
L03	610.1402	TOPSOIL - ROADSIDE	CM	20
L04	610.15	PREPARATION OF SUBSOIL FOR TURF ESTABLISHMENT	SQM	420
L05	610.19	WATERING VEGETATION	KL	410
L06	607.4101 10	TEMPORARY PLASTIC BARRIER FENCE	M	120
L07	615.43 11	GROUND SURFACE PROTECTION MATS	SQM	30

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	ITS TRAVEL TIME I-278 AND I-495	PIN X806.49	BRIDGES	CULVERTS	ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED	CONTRACT NUMBER	
	PS&E DATE: 06/21/2012					D262095	
						DRAWING NO. LAP-01	
	COUNTY: KINGS AND QUEEN					SHEET NO. 102	
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.						NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11 DOCUMENT NAME: x80649_cph_lap_01.dgn	






LEGEND	
FEATURE	SYMBOL
TREE DECIDUOUS	
VEGETATION PROTECTION BARRIER	
LANDSCAPE WORK ZONE- CLEARING AND GRUBBING/ REMEDIATION DISTURBED SOIL/ ESTABLISHING TURF	

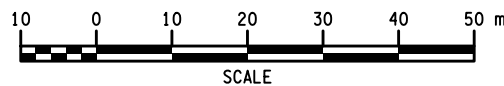
NOTES:

- EACH TREE WITHIN WORK AREA SHOULD BE PROTECTED BY PLASTIC BARRIER FENCE, SEE DRG LAD-05 AND PAID UNDER 607.4101 10.
- SITES ARE TO BE REVIEWED/VISITED PRIOR TO CONSTRUCTION IN ORDER TO IDENTIFY TREES IN CONFLICT WITH WORK.



LEGEND	
FEATURE	SYMBOL
TREE DECIDUOUS	
VEGETATION PROTECTION BARRIER	
LANDSCAPE WORK ZONE- CLEARING AND GRUBBING/ REMEDICATION DISTURBED SOIL/ ESTABLISHING TURF	

1. EACH TREE WITHIN WORK AREA SHOULD BE PROTECTED BY PLASTIC BARRIER FENCE, SEE DRG LAD-05 AND PAID UNDER 607.4101 10.
2. SITES ARE TO BE REVIEWED/VISITED PRIOR TO CONSTRUCTION IN ORDER TO IDENTIFY TREES IN CONFLICT WITH WORK.



AS-BUILT REVISIONS
DESCRIPTION OF ALTERATIONS:

PS&E DATE: 06/21/2012

COUNTY: KINGS AND QUEEN

BRIDGES

CULVERTS

ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED

LANDSCAPE - EAST HAMPTON - 1
LIE STA7+793 TO STA8+067

CONTRACT NUMBER

D262095

DRAWING NO. LAP-02

SHEET NO. 103

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY:" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11

DOCUMENT NAME: x80649_cph_lap_02.dgn

FILE NAME = x80649_cph_lap_03.dgn
DATE/TIME = 03-AUG-2012 16:13
USER = kdesai

DESIGN SUPERVISOR RANDOLPH HUNT

JOB MANAGER MAREK CMIK

DESIGN YELENA GRIG


CHECK MAREK CMIK

DRAFTING YELENA GRIG


CHECK MAREK CMIK

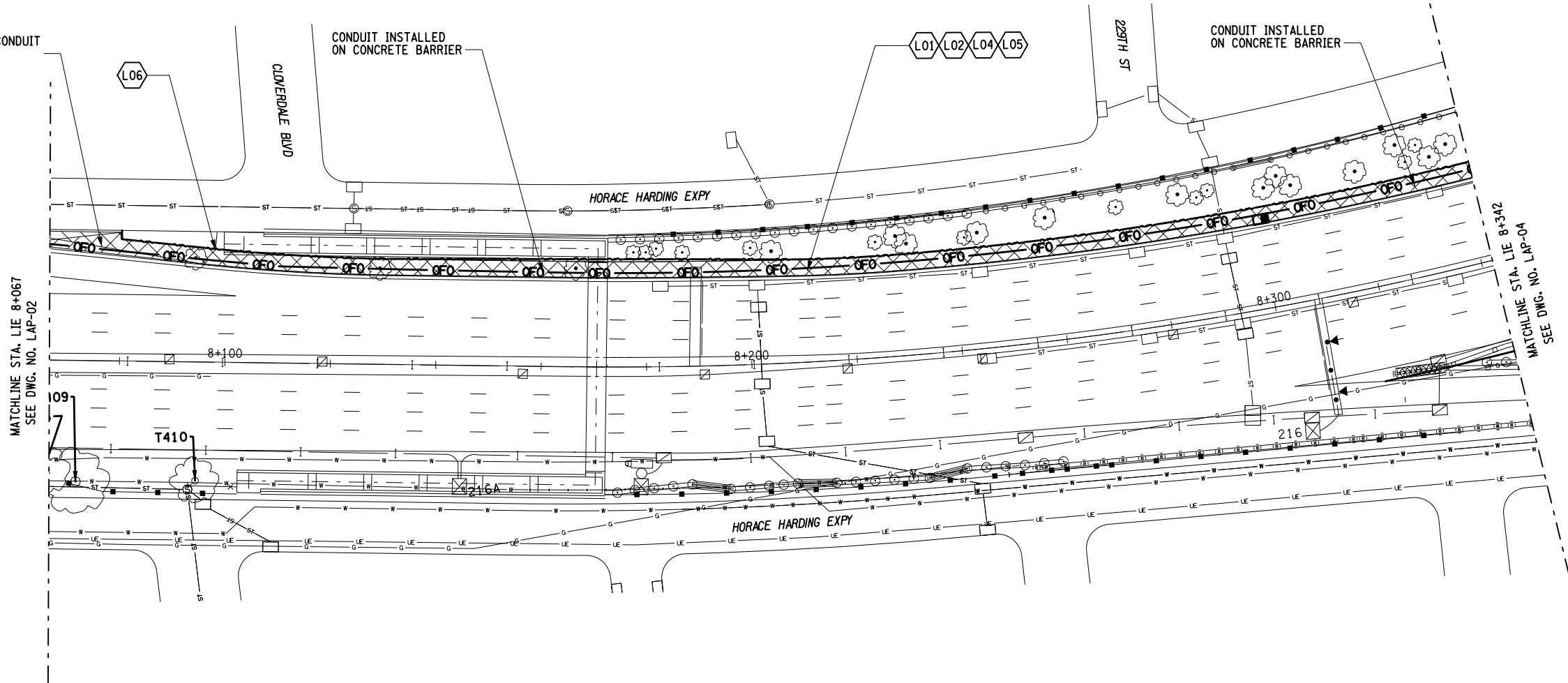
PROJECT MANAGER RAJ AMIN

AFFIX SEAL:
ON:



ALTERED BY:
ON:

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	ITS TRAVEL TIME I-278 AND I-495	PIN X806.49	BRIDGES	CULVERTS	ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED LANDSCAPE - EAST HAMPTON - 2 LIE STA8+067 TO STA8+342	CONTRACT NUMBER	
	PS&E DATE: 06/21/2012					D262095	
						DRAWING NO. LAP-03	
						SHEET NO. 104	
	COUNTY: KINGS AND QUEEN						
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.						NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11 DOCUMENT NAME: x80649_cph_lap_03.dgn	



ID	ITEM NUMBER	DESCRIPTION	UNIT	QTY
L01	201.07	CLEARING AND GRUBBING	HA	0.10
L02	610.1602	TURF ESTABLISHMENT - LAWNS	SQM	870
L04	610.15	PREPARATION OF SUBSOIL FOR TURF ESTABLISHMENT	SQM	870
L05	610.19	WATERING VEGETATION	KL	850
L06	607.4101 10	TEMPORARY PLASTIC BARRIER FENCE	M	260

- NOTES:
- EACH TREE WITHIN WORK AREA SHOULD BE PROTECTED BY PLASTIC BARRIER FENCE, SEE DRG LAD-05 AND PAID UNDER 607.4101 10.
 - SITES ARE TO BE REVIEWED/VISITED PRIOR TO CONSTRUCTION IN ORDER TO IDENTIFY TREES IN CONFLICT WITH WORK.

FILE NAME = x80649_cph_lap_04.dgn
DATE/TIME = 03-AUG-2012 16:13
USER = kdesai

DESIGN SUPERVISOR RANDOLPH HUNT

JOB MANAGER MAREK CWIOK

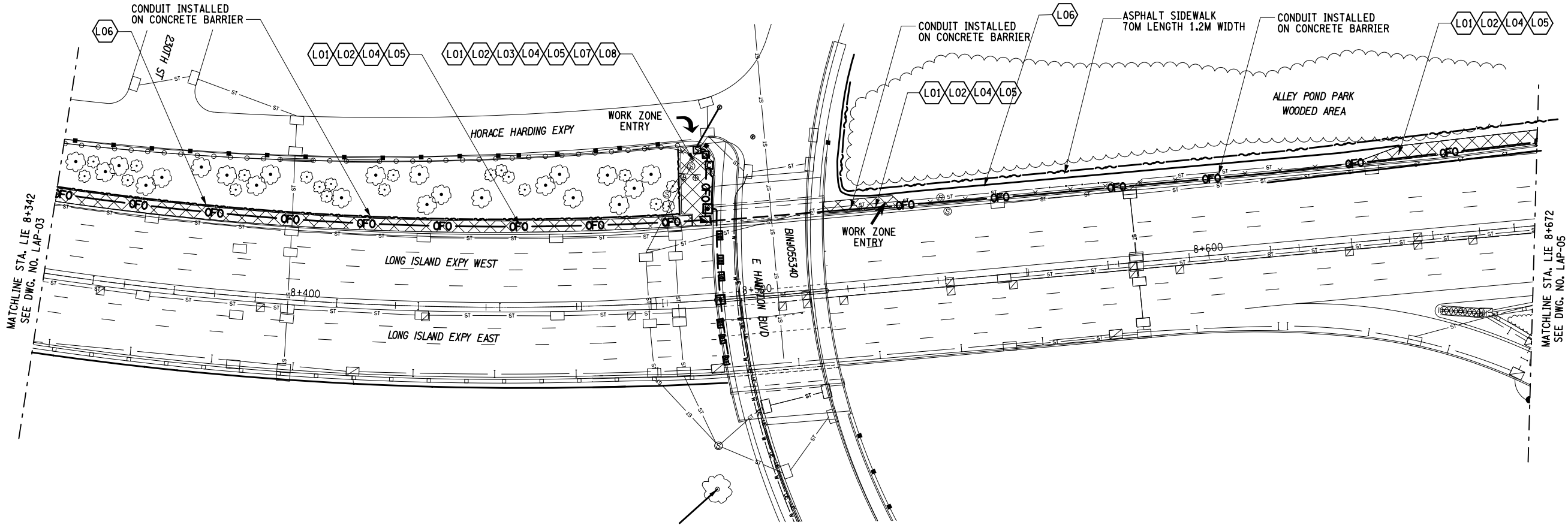
DESIGN YELENA GRIG

CHECK MAREK CWIOK

DRAFTING YELENA GRIG

CHECK MAREK CWIOK

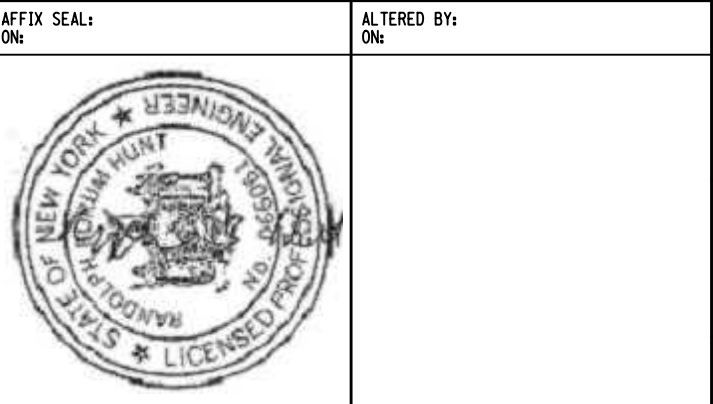
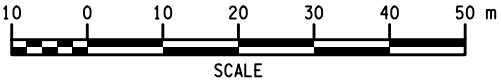
PROJECT MANAGER RAJ AMIN



ID	ITEM NUMBER	DESCRIPTION	UNIT	QTY
L01	201.07	CLEARING AND GRUBBING	HA	0.07
L02	610.1602	TURF ESTABLISHMENT - LAWNS	SQM	650
L03	610.1402	TOPSOIL - ROADSIDE	CM	30
L04	610.15	PREPARATION OF SUBSOIL FOR TURF ESTABLISHMENT	SQM	650
L05	610.19	WATERING VEGETATION	KL	640
L06	607.4101 10	TEMPORARY PLASTIC BARRIER FENCE	M	340
L07	615.43 11	GROUND SURFACE PROTECTION MATS	SQM	115
L08	209.1901	ROLLED EROSION CONTROL PRODUCT, CLASS II TYPE A, INTERMEDIATE	SQM	115

LEGEND	
FEATURE	SYMBOL
TREE DECIDUOUS	
VEGETATION PROTECTION BARRIER	
LANDSCAPE WORK ZONE- CLEARING AND GRUBBING/ REMEDIATION DISTURBED SOIL/ ESTABLISHING TURF	

- NOTES:
- EACH TREE WITHIN WORK AREA SHOULD BE PROTECTED BY PLASTIC BARRIER FENCE, SEE DRG LAD-05 AND PAID UNDER 607.4101 10.
 - SITES ARE TO BE REVIEWED/VISITED PRIOR TO CONSTRUCTION IN ORDER TO IDENTIFY TREES IN CONFLICT WITH WORK.



AS-BUILT REVISIONS
DESCRIPTION OF ALTERATIONS:

ITS TRAVEL TIME I-278 AND I-495

PS&E DATE: 06/21/2012

COUNTY: KINGS

PIN X806.49
PS&E DATE:

BRIDGES
1055340

CULVERTS

ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED

LANDSCAPE - EAST HAMPTON - 3

CONTRACT NUMBER

D262095

DRAWING NO. LAP-04

SHEET NO. 105

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11
DOCUMENT NAME: x80649_cph_lap_04.dgn

FILE NAME = x80649_cph_lap_06.dgn
DATE/TIME = 03-AUG-2012 16:13
USER = kdesai

DESIGN SUPERVISOR RANDOLPH HUNT

JOB MANAGER MAREK CWTOK

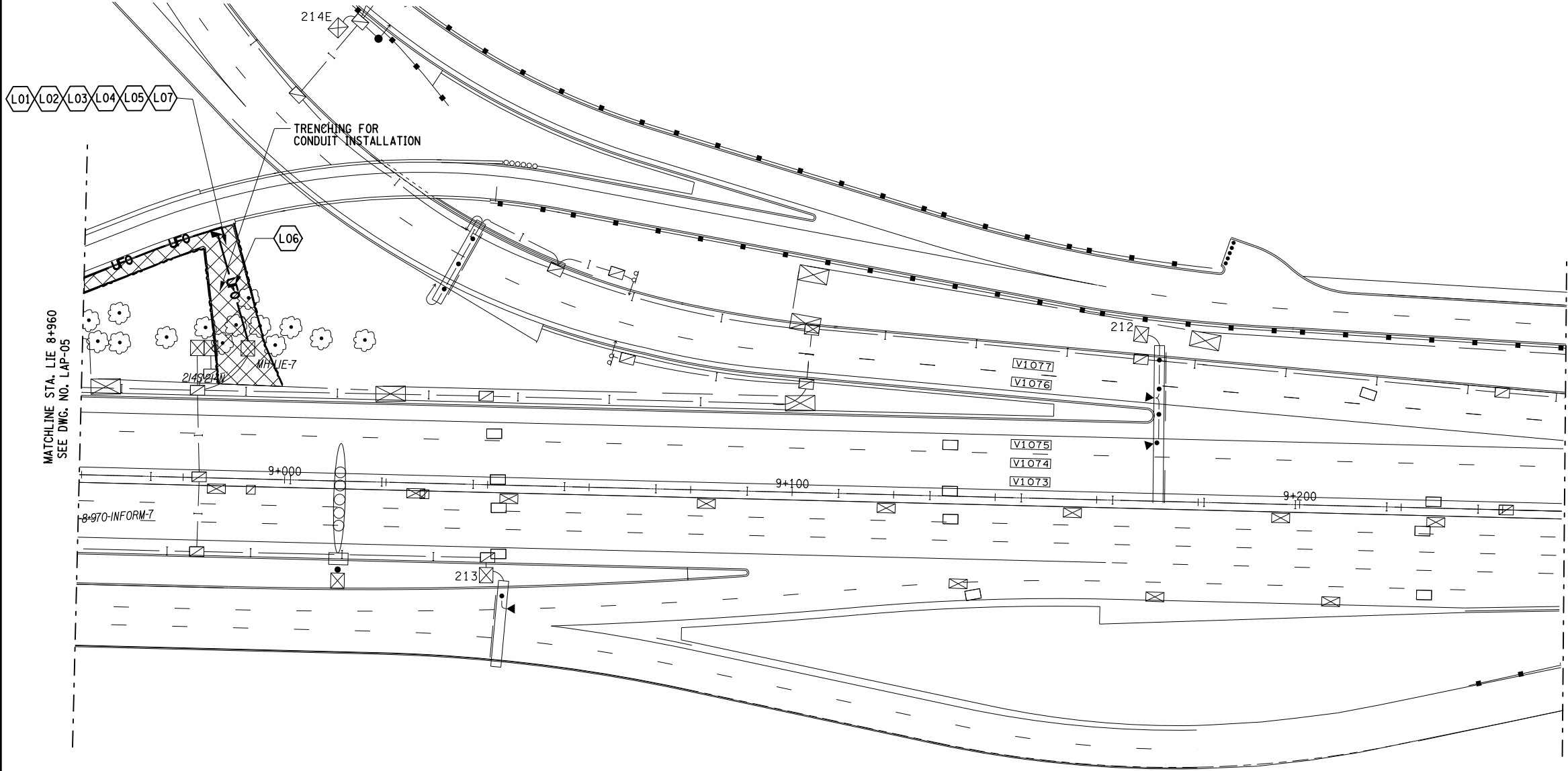
DESIGN YELENA GRIG

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DRAFTING YELENA GRIG

CHECK MAREK CWTOK

PROJECT MANAGER RAJ AMIN



ID	ITEM NUMBER	DESCRIPTION	UNIT	QTY
L01	201.07	CLEARING AND GRUBBING	HA	0.02
L02	610.1602	TURF ESTABLISHMENT - LAWNS	SQM	230
L03	610.1402	TOPSOIL - ROADSIDE	CM	12
L04	610.15	PREPARATION OF SUBSOIL FOR TURF ESTABLISHMENT	SQM	230
L05	610.19	WATERING VEGETATION	KL	220
L06	607.4101 10	TEMPORARY PLASTIC BARRIER FENCE	M	90
L07	615.43 11	GROUND SURFACE PROTECTION MATS	SQM	100

LEGEND	
FEATURE	SYMBOL
TREE DECIDUOUS	
VEGETATION PROTECTION BARRIER	
LANDSCAPE WORK ZONE- CLEARING AND GRUBBING/ REMEDIATION DISTURBED SOIL/ ESTABLISHING TURF	

- NOTES:
- EACH TREE WITHIN WORK AREA SHOULD BE PROTECTED BY PLASTIC BARRIER FENCE, SEE DRG LAD-05 AND PAID UNDER 607.4101 10.
 - SITES ARE TO BE REVIEWED/VISITED PRIOR TO CONSTRUCTION IN ORDER TO IDENTIFY TREES IN CONFLICT WITH WORK.

AFFIX SEAL: ON:	ALTERED BY: ON:

AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	ITS TRAVEL TIME I-278 AND I-495	PIN X806.49	BRIDGES	CULVERTS	ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED	CONTRACT NUMBER	
	PS&E DATE: 06/21/2012				LANDSCAPE - EAST HAMPTON - 5 LIE STA8+960	D262095	
						DRAWING NO. LAP-06	
	COUNTY: KINGS AND QUEEN					SHEET NO. 107	
IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.							
NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11 DOCUMENT NAME: x80649_cph_lap_06.dgn							

FILE NAME = X80649_cph_lap_07.dgn
DATE/TIME = 03-AUG-2012 16:13
USER = kdesai

DESIGN SUPERVISOR RANDOLPH HUNT

JOB MANAGER MAREK CWIOK




DESIGN YELENA GRIG

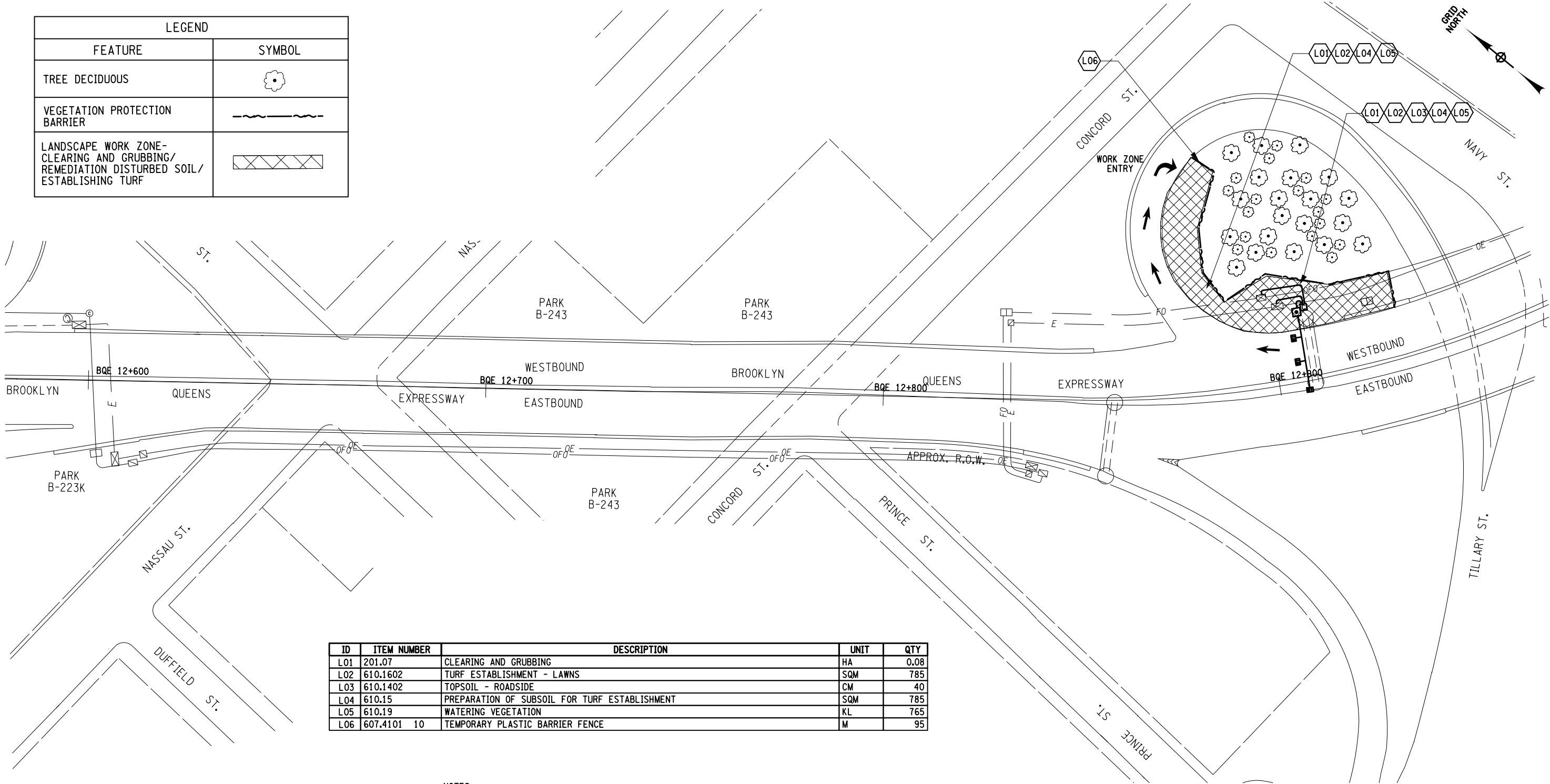
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DRAFTING YELENA GRIG

CHECK MAREK CWIOK

PROJECT MANAGER RAJ AMIN

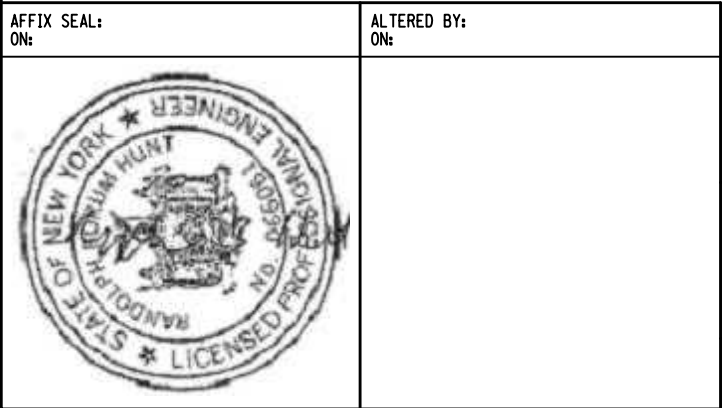
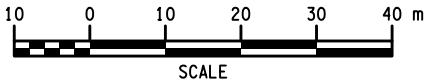
LEGEND	
FEATURE	SYMBOL
TREE DECIDUOUS	
VEGETATION PROTECTION BARRIER	
LANDSCAPE WORK ZONE- CLEARING AND GRUBBING/ REMEDIATION DISTURBED SOIL/ ESTABLISHING TURF	




ID	ITEM NUMBER	DESCRIPTION	UNIT	QTY
L01	201.07	CLEARING AND GRUBBING	HA	0.08
L02	610.1602	TURF ESTABLISHMENT - LAWNS	SQM	785
L03	610.1402	TOPSOIL - ROADSIDE	CM	40
L04	610.15	PREPARATION OF SUBSOIL FOR TURF ESTABLISHMENT	SQM	785
L05	610.19	WATERING VEGETATION	KL	765
L06	607.4101 10	TEMPORARY PLASTIC BARRIER FENCE	M	95

NOTES:

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AS-BUILT REVISIONS DESCRIPTION OF ALTERATIONS:	ITS TRAVEL TIME I-278 AND I-495	PIN X806.49	BRIDGES	CULVERTS	ALL DIMENSIONS IN m UNLESS OTHERWISE NOTED	CONTRACT NUMBER	
	PS&E DATE: 06/21/2012					D262095	
						DRAWING NO. LAP-07	
						SHEET NO. 108	
	COUNTY: KINGS AND QUEEN						

IT IS A VIOLATION OF LAW FOR ANY PERSON, UNLESS THEY ARE ACTING UNDER THE DIRECTION OF A LICENSED PROFESSIONAL ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR, TO ALTER AN ITEM IN ANY WAY. IF AN ITEM BEARING THE STAMP OF A LICENSED PROFESSIONAL IS ALTERED, THE ALTERING ENGINEER, ARCHITECT, LANDSCAPE ARCHITECT, OR LAND SURVEYOR SHALL STAMP THE DOCUMENT AND INCLUDE THE NOTATION "ALTERED BY" FOLLOWED BY THEIR SIGNATURE, THE DATE OF SUCH ALTERATION, AND A SPECIFIC DESCRIPTION OF THE ALTERATION.

NEW YORK STATE DEPARTMENT OF TRANSPORTATION REGION 11
DOCUMENT NAME: X80649_cph_lap_07.dgn

FILE NAME = X80649_cph_lap_08.dgn
DATE/TIME = 03-AUG-2012 16:13
USER = kdesai

DESIGN SUPERVISOR RANDOLPH HUNT

JOB MANAGER MAREK CWIOK

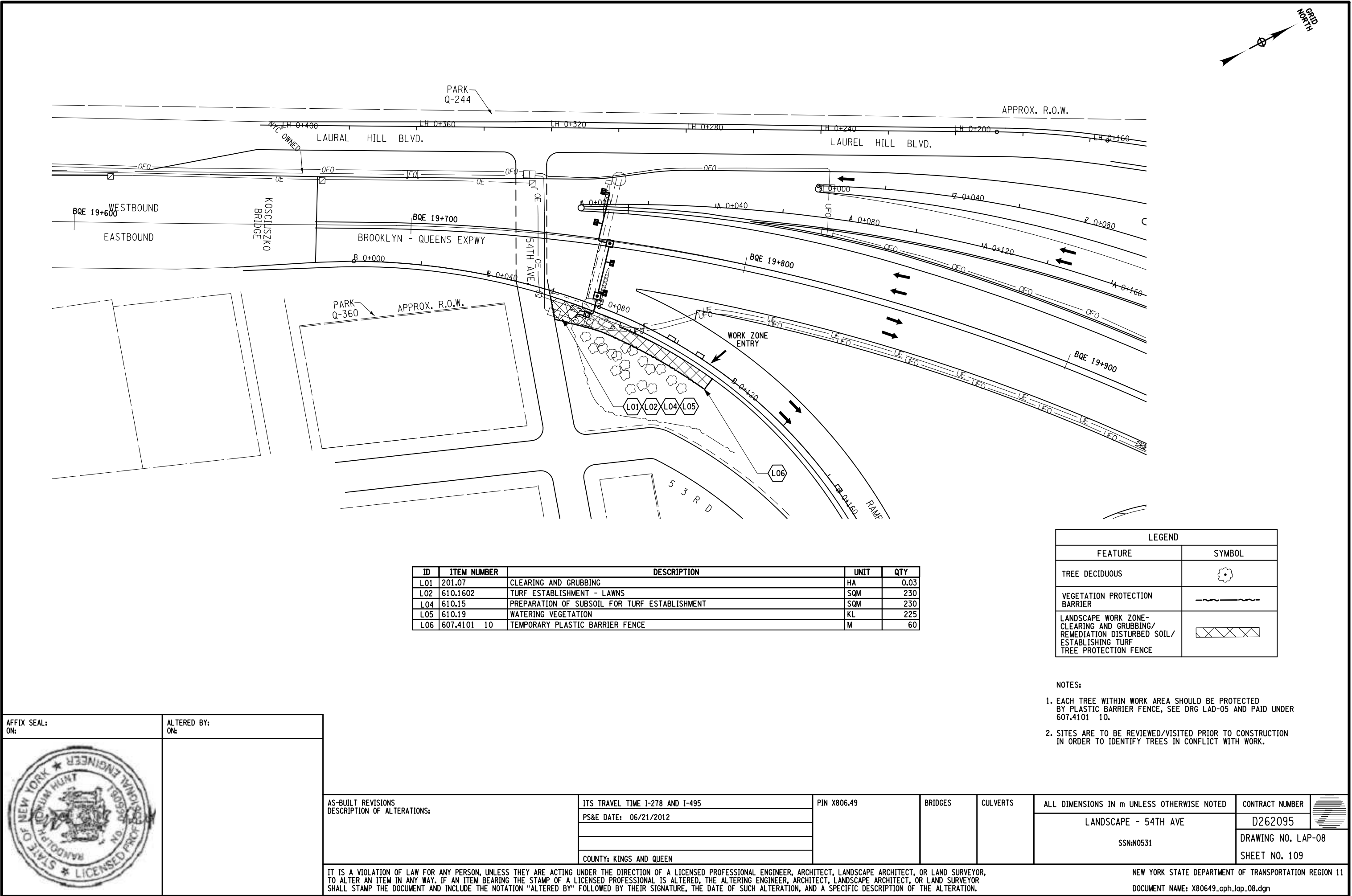
DESIGN YELENA GRIG

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DRAFTING YELENA GRIG

CHECK MAREK CWIOK

PROJECT MANAGER RAJ AMIN



ESTIMATE OF QUANTITIES				
ITEM NUMBER	DESCRIPTION	UNIT	QUANTITY	
201.07	CLEARING AND GRUBBING	HA	1.0	
206.03	CONDUIT EXCAVATION AND BACKFILL INCLUDING SURFACE RESTORATION	M	100.0	
206.0301 10	CONDUIT EXCAVATION AND BACKFILL - (RESTORING TOP SURFACES NOT INCLUDED)	M	200	
206.0312 11	CONDUIT INSTALLED ON ABOVE GRADE STRUCTURES	M	1400	
206.0401 11	PNEUMATIC EXCAVATION AND BACKFILL OF TRENCHES	M	350	
206.05	TEST PIT EXCAVATION	EA	10.0	
209.1901	ROLLED EROSION CONTROL PRODUCT, CLASS II TYPE A, INTERMEDIATE	SQM	200.0	
502.2001 18	FULL-DEPTH PORTLAND CEMENT CONCRETE SAW CUTS	M	100	
586.02	DRILLING AND GROUTING BOLTS OR REINFORCEMENT BARS	EA	10.0	
606.51	RESETTING CORRUGATED BEAM GUIDE RAILING	M	30.0	
606.5710	RESETTING PRECAST CONCRETE BARRIER - HALF SECTION	M	30.0	
607.4016	OPTIONAL FENCE GATE (SINGLE LEAF 1220 MILLIMETER OPENING 1830 MILLIMETER HIGH)	EA	1.0	
607.4101 10	TEMPORARY PLASTIC BARRIER FENCE	M	1500	
607.97 08	REMOVE AND RESET EXISTING FENCE	M	20	
608.1004 05	REMOVE, RESET OR REPLACE EXISTING STONE BLOCK PAVING	SQM	150	
610.0701	MYCORRHIZAL FUNGI	KG	500.0	
610.10	COMPOST	CM	50.0	
610.1101	MULCH FOR PLANTING TYPE A, B & D - WOOD CHIPS AND SHREDDED BARK	CM	50.0	
610.1402	TOPSOIL - ROADSIDE	CM	150.0	
610.15	PREPARATION OF SUBSOIL FOR TURF ESTABLISHMENT	SQM	4500.0	
610.1602	TURF ESTABLISHMENT - LAWNS	SQM	4300.0	
610.19	WATERING VEGETATION	KL	4500.0	
614.0411	CARE OF TREES UP TO 300 MM DIAMETER AT BREAST HEIGHT - PRUNING	EA	25.0	
614.0421	CARE OF TREES OVER 300 MM-600 MM DIAMETER AT BREAST HEIGHT PRUNING	EA	25.0	
615.43 11	GROUND SURFACE PROTECTION MATS	SQM	1000	
619.01	BASIC WORK ZONE TRAFFIC CONTROL	LS	1.0	
619.24	NIGHTTIME OPERATIONS	LS	1.0	
625.01	SURVEY OPERATIONS	LS	1.0	
637.13	ENGINEER'S FIELD OFFICE - TYPE 3	MNTH	24.0	

ESTIMATE OF QUANTITIES				
ITEM NUMBER	DESCRIPTION	UNIT	QUANTITY	
637.31	INSPECTION VEHICLE	MNTH	24.0	
637.34	OFFICE TECHNOLOGY AND SUPPLIES	DC	5000.0	
637.36	CONSTRUCTION TESTING SUPPLIES - CONSUMABLES	DC	5000.0	
639.1022 01	OCPM (CRITICAL PATH METHOD) PROGRESS SCHEDULE - TYPE 2	LS	1	
644.11	ANCHOR BOLTS	KG	100.0	
644.4401	NON-STANDARD SIGN STRUCTURE	EA	1.0	
647.21	REMOVAL OF SINGLE SPAN OVERHEAD SIGN PANEL(S), STRUCTURE, AND FOUNDATIONS	EA	1.0	
656.01	MISCELLANEOUS METALS	KG	3000.0	
670.40	CAST IRON JUNCTION BOXES	EA	15.0	
670.7002	SINGLE CONDUCTOR CABLE, NUMBER 2 GAGE	M	450.0	
670.7005	SINGLE CONDUCTOR CABLE, NUMBER 8 GAGE	M	100.0	
670.7010	SINGLE CONDUCTOR CABLE, NUMBER 1/0 GAGE	M	800.0	
670.7020	SINGLE CONDUCTOR CABLE, NUMBER 2/0 GAGE	M	1500.0	
670.7501	GROUND WIRE NO. 6 AWG	M	50.0	
670.7502 11	GROUND WIRE NUMBER 2 AWG	M	100	
670.7504 11	GROUND WIRE NUMBER 4 AWG	M	600	
670.99081225	SINGLEMODE FIBER OPTIC CABLE - FIBER 12	M	250	
680.0101 11	CLEAN EXISTING CONDUIT AND REINSTALL CABLES	M	1600	
680.51400211	CONCRETE FIBER OPTIC PULL BOX (1219MM X 914MM X 1219MM)	EAC	2	
680.51500111	ELECTRICAL PULL BOX (610MM X 457MM X 914MM)	EAC	2	
680.5196 11	FURNISH AND INSTALL CONCRETE FIBER OPTIC PULL BOX	EAC	6	
680.520106	CONDUIT, METAL STEEL, ZINC COATED, 2 NPS	M	550.0	
680.520108	CONDUIT, METAL STEEL, ZINC COATED, 3 NPS	M	150.0	
680.520203	TRAFFIC SIGNAL CONDUIT, FLEXIBLE LIQUID TIGHT STEEL, 1 NPS	M	30.0	
680.520206	TRAFFIC SIGNAL CONDUIT, FLEXIBLE LIQUID-TIGHT STEEL, 2 NPS	M	10.0	
680.6717	TRAFFIC SIGNAL POLE-POST TOP MOUNT 1.7 METER MOUNTING HEIGHT	EA	2.0	
680.84554811	FIBER OPTIC DATA TRANSCEIVER, SM, MASTER	EAC	6	
680.84554911	FIBER OPTIC DATA TRANSCEIVER, SM, SLAVE	EAC	7	
680.92020410	MULTI-CELL GALVANIZED STEEL CONDUIT, 4 DUCTS	M	1000	

AS-BUILT REVISIONS
DESCRIPTION OF ALTERATIONS:

COUNTY: QUEENS

PIN X806.49

BRIDGES

CULVERTS

ALL DIMENSIONS IN M+ UNLESS OTHERWISE NOTED

ESTIMATE OF QUANTITIES - 1

CONTRACT NUMBER

D262095

DRAWING NO. EQ-01

SHEET NO. 110

ESTIMATE OF QUANTITIES				
ITEM NUMBER	DESCRIPTION	UNIT	QUANTITY	

